
Memorandum

DATE: August 9, 2012

SUBJECT: Petition No. PLNPCM2010-00647 – 400 South Livable Communities Station Area Plans (University TRAX line) – Rezone properties along 400 South from 200 East to 1000 East to Transit Station Area (TSA) zoning classifications, adjust related zoning regulations and amend the Central Community Master Plan

Petitioner – Mayor Ralph Becker

AFFECTED COUNCIL DISTRICTS: If the ordinance is adopted the zoning regulation changes would affect Council Districts 4.

FROM: Janice Jardine, Land Use Policy Analyst

ADMINISTRATIVE DEPT. AND CONTACT PERSON: Community Development Department, Planning Division
Maryann Pickering, Principal Planner

COUNCIL PHILOSOPHY STATEMENTS: Council Philosophy Statements that relate to this petition include Economic Health, Neighborhood Quality of Life, and Historic Preservation

COUNCIL PROCESS: *The Council is scheduled to receive an initial briefing from the Administration on August 14, 2012. Tentative dates for follow-up discussions, a public hearing and Council action are:*

<i>August 14, 2012</i>	<i>Set public hearing date</i>
<i>September 4, 2012</i>	<i>Public Hearing</i>
<i>September 18, 2012</i>	<i>Potential Action</i>

- A. The 400 South Corridor temporary land use regulations expire on September 27, 2012. There is a limited number of Council meetings available prior to that date and a need to quickly move this item through the Council process.
- B. The Administration has provided detailed information in the City Council Transmittal cover letter. This memo will provide a brief summary of key elements related to the 400 South Livable Communities Station Area Plans (University TRAX line), rezoning and related zoning regulation changes. Please refer to the Administration’s paperwork for details.
- C. Items brought forward for ease of reference:
- Proposed zoning map
 - Proposed ordinance

KEY ELEMENTS:

An ordinance has been prepared for Council consideration that would rezone properties along 400 South from 200 East to 1000 East to Transit Station Area (TSA) classifications, adjust related Transit Station Area zoning regulations and amend the Central Community Master Plan.

Proposed Central Community Master Plan Changes

- A. The proposed 400 South Livable Communities Station Area Plans provide a framework for land use and urban design decisions to assist in transitioning 400 South from an auto-oriented street to a street that accommodates transportation options for people of all ages and abilities. The new information is provided for use in future land use decisions, consideration of public investments, allocation of resources and other processes that may impact the 400 South corridor.
- B. The Transit Oriented Development section of the Central Community Master Plan will be updated with new information and revised policies. The “400 South and University Boulevard Station Area Plans” will be included in the Addendum section.
- C. The Plan divides the study area into 3 smaller Station Area Plans – Library Station, Trolley Station and 900 East Station. Each station area plan provides general discussion of the area, specific land use policies and identifies specific station area goals that should be used to guide future land uses, capital improvements and budget decisions.

Proposed Rezoning

- A. Based on the public process and Planning staff analysis, properties along 400 South from 200 East to 400 East will be zoned Transit Station Area - Urban Center (TSA-UC) and properties from 400 East to 1000 East will be zoned Transit Station Area - Urban Neighborhood (TSA-UN).
- B. Currently, the area is a mix of zoning districts including Transit Corridor District (TC-75), Moderate Density Multi-Family Residential (RMF-35) Community Shopping (CS) and Commercial Corridor (CC).

Proposed Zoning Regulation Changes

- A. Design and development standards are identified for each 400 South station area. Development projects will be required to comply with zoning regulations, and design and development standards that generally include:
 - a. Building height and setbacks
 - b. Minimum lot size and coverage
 - c. Open space
 - d. Circulation and connectivity
 - e. Parking location and structures
 - f. Minimum design requirements including building façade architectural variety and scale, ground floor building materials, first floor glass, the location of doors and entrances, residential uses and densities, mechanical equipment and service areas.
- B. New language will require an additional setback (or building height to be stepped back) when a project is adjacent to a low-density residential or open space zoning district.
- C. An expanded table of prohibited uses is proposed and varies depending on the station area type. All other uses are permitted.

D. Other changes include some general clean-up corrections to the current Transit Station Area zoning regulations.

cc: David Everitt, Bianca Shreeve, Karen Hale, Art Raymond, Holly Hilton, Ed Rutan, Lynn Pace, Paul Nielson, Rick Graham, Jeff Niermeyer, Tom Ward, Frank Gray, Mary De La Mare-Schafer, Orion Goff, Les Koch, Larry Butcher, Robin Hutcheson, Kevin Young, DJ Baxter, Justin Belliveau, Bob Farrington, LuAnn Clark, Craig Spangenberg, Randy Isbell, Wilf Sommerkorn, Cheri Coffey, Joel Paterson, Nick Norris, Maryann Pickering, City Council Liaisons, Mayors Liaisons

File Location: Community and Economic Development Dept., Planning Division, 400 South Livable Communities Station Area Plans, (University TRAX line) – Rezone properties along 400 South from 200 East to 1000 East to Transit Station Area (TSA) zoning classifications, adjust related zoning regulations and amend the Central Community Master Plan

Transit Station Area (TSA) Zoning - Summary

(The following information is a general explanation of the current Transit Station Area zoning regulations. It is provided for background purposes.)

- A. The Transit Station Area (TSA) zoning regulations are a new concept combining commonly used zoning regulations with design and development standards and guidelines. The focus is on the form of development and how development relates to adjacent public spaces and buildings and the surrounding neighborhoods. Station area plans are created to provide a substantial amount of area specific information that is in turn is incorporated into the zoning regulations. Station area plans are intended to work hand in hand with the Transit Station Area Zoning District and Transit Station Area Development Guidelines.

- B. The overall goals of the Transit Station Area (TSA) zoning regulations are to:
 - 1. Put public participation up front in the planning process, instead of as a reaction to individual developments.
 - 2. Make it easier for desirable development to be processed.
 - 3. Create options for applicants in terms of how they design their project and how they want to be processed.
 - 4. Provide the option of predefined outcomes.

- C. The Transit Station Area (TSA) zoning regulations are intended to regulate the physical form of buildings and how they relate to transit, the street, public spaces, adjacent buildings, and adjacent neighborhoods. Less emphasis is placed on use. A table of prohibited uses, which varies depending on the type of station area, lists those uses that are not allowed. All other uses are permitted.

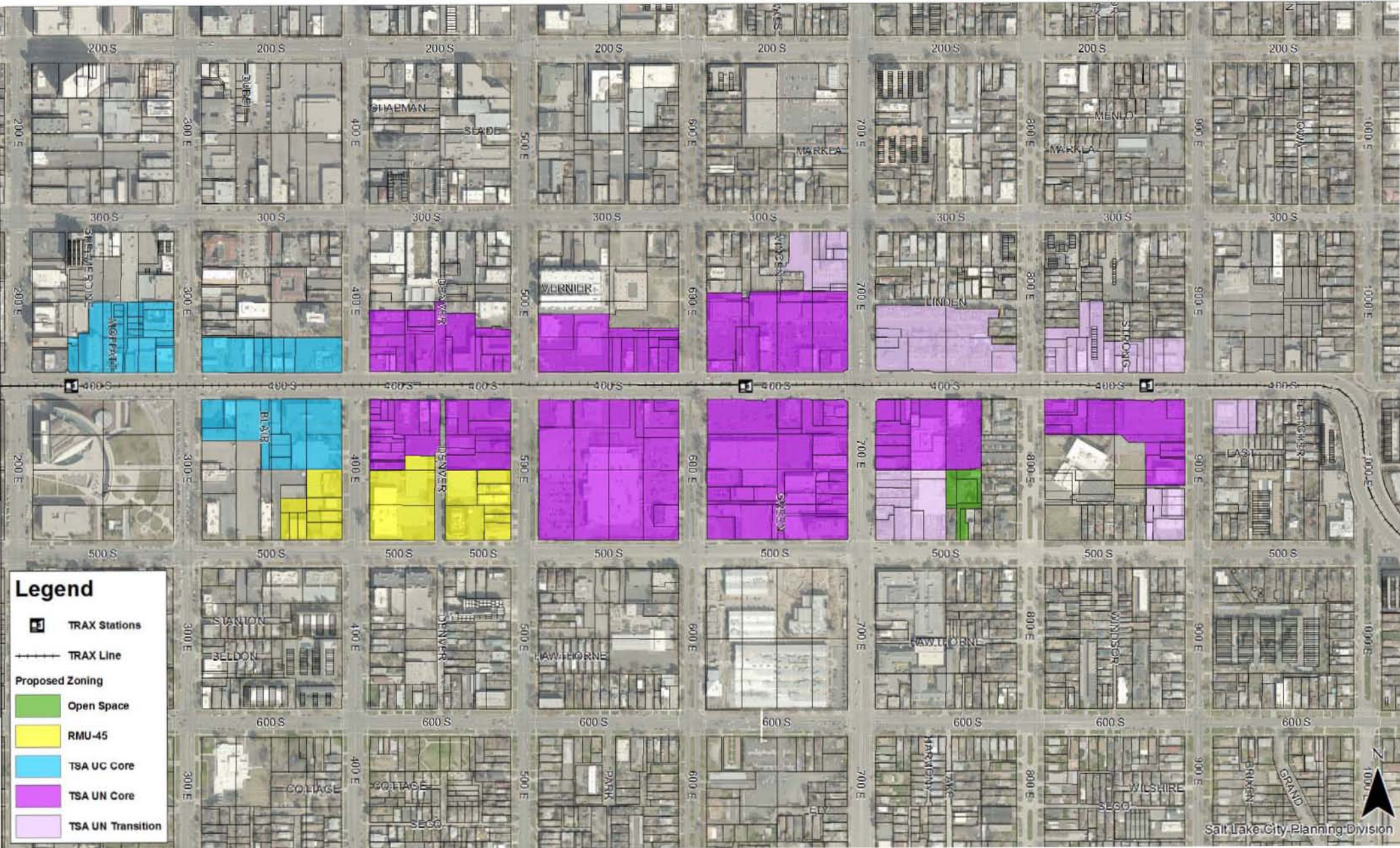
- D. The purpose statement includes a detailed overall description of the Transit Station Area District and specific descriptions of the Core and Transition Areas.

- E. A station area typology is based on the use of characteristics, such as building types, mix of land use, transit service and street network to create generalizations about an area that can be used to define a common vision for development of a transit station area. Prior to classifying a transit station into a specific type, a station area plan must be adopted by the City Council prior to applying this zoning district to a geographic area. Only those stations that have an adopted station area plan will be classified. Station area types include Urban Center, Urban Neighborhood, Mixed-Use Employment Center, and Special Purpose.

- F. Design and development standards are identified in the zoning regulations. Development projects will be required to comply with regulations that include:
 - 1. Building height and setbacks
 - 2. Special setback provisions depending on the station area location
 - 3. Minimum lot area and coverage
 - 4. Open space
 - 5. Circulation and connectivity
 - 6. Parking locations and structures

7. Minimum design requirements include building façade elements, architectural variety and scale, ground floor building materials, first floor glass, the location of doors and entrances, residential uses and densities, mechanical equipment and service areas.
- G. Additional development standards are identified in the “Transit Station Area Development Guidelines” book has been adopted by reference and is not included in the Zoning regulations. The review process is outlined in the zoning regulations. The development guidelines are to be used by the City in the review process for each project located in the Transit Station Area zoning district. The intent is to reward high quality, desired development through the use of incentives (such as increased building height) and by providing a quicker review process.
 - H. Following the guidelines is voluntary and is not required for any project. Compliance with the guidelines entitles applicants to faster review and can guide new development and redevelopment in implementing the vision of the station area plans.
 - I. Each guideline provides an assigned value based on several factors including the importance of the guideline in implementing the vision of a station area plan or other applicable land use related policies of the City and the cost of addressing the guideline. The resulting development score is established by adding the values of each guideline that has been incorporated into a project.
 - J. The approval process for all new development and redevelopment is based on the development score. The higher the development score, the more streamlined the approval process. A series of threshold tiers has been established, indicating which approval process is required.
 1. Tier 1 – projects with a development score less than 50 points will require Planning Commission approval through the Conditional Building and Site Design Review process and standards. This process requires public input prior to scheduling a public hearing with the Planning Commission. Estimated process timeframe – 3-6 months.
 2. Tier 2 – projects with a development score between 50 and 99 points require an Administrative Hearing. This process allows for a relatively quick review and approval process, but still requires a public hearing. Estimated process timeframe – 1-3 months.
 3. Tier 3 – projects with a development score more than 100 points allow Administrative approval without a public hearing providing a streamlined approval process. Estimated process timeframe – similar to a permitted use “over-the-counter” building permit review process.
 - K. Additional regulations are required for developments that exceed 5 acres. The primary purpose is to ensure that circulation and connectivity issues are addressed. The design of an internal circulation system and connectivity to public rights of way is subject to approval from the Transportation Division. This would not be a departure from the current review process, but would provide guidance to developers in terms of what is expected.
 - L. Special minimum and maximum parking ratios and specific sign regulations are also provided for station area types including core or transition areas and mixed-use developments.

Exhibit A - 400 South Proposed Zoning



FRANK B. GRAY
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DEPUTY DIRECTOR

ROBERT FARRINGTON, JR.
DEPUTY DIRECTOR

SALT LAKE CITY CORPORATION

DEPARTMENT OF COMMUNITY & ECONOMIC DEVELOPMENT
OFFICE OF THE DIRECTOR

SCANNED TO: *Mayor*
SCANNED BY: *Becker*
RALPH BECKER
DATE: 7/5/2012

CITY COUNCIL TRANSMITTAL



David Everitt, Chief of Staff

Date Received: _____

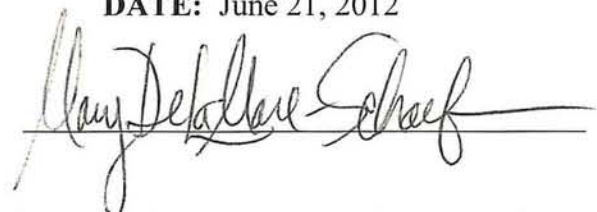


Date Sent to City Council: 07/06/2012

TO: Salt Lake City Council
Søren Simonsen

DATE: June 21, 2012

FROM: Mary DeLaMare-Schaefer
Deputy Director, CED



Mary DeLaMare-Schaefer

RE: Petition PLNPCM2010-00647 – 400 South Livable Communities Station Area Plans

STAFF CONTACTS: Maryann Pickering, Principal Planner, at 801-535-7660 or maryann.pickering@slcgov.com

RECOMMENDATION: That the City Council hold a briefing and schedule a Public Hearing

DOCUMENT TYPE: Ordinance

DISCUSSION:

Issue Origin: In the fall of 2010, the Community & Economic Development Department was awarded a matching grant from the Department of Housing and Urban Development (HUD) to undertake a planning process for a portion of the University TRAX line. The amount of the grant from HUD was \$22,620, with a commitment from the city to provide an additional \$10,000 in the form of staff resources for the project. The primary purpose of the grant was to reach out to individuals and groups who do not normally participate in the planning process and to determine if the existing policies and regulations were capable of fulfilling the goals listed below or if changes were needed:

- a. Provide more transportation choices.
- b. Promote equitable, affordable housing.
- c. Enhance economic competitiveness.
- d. Support existing communities.

- e. Coordinate policies and leverage investments.
- f. Value communities and neighborhoods.

Starting in March of 2011, Planning Division staff began working on a master plan amendment and rezoning project for the University TRAX line. Three stations were identified to be included as part of the rezoning process. Those stations are: Library, Trolley and 900 East.

In addition to the above, this project assists in the implementation of the Wasatch Choices 2040 Plan, which is a regional plan created by residents, businesses, and other stakeholders to manage future growth in the region by focusing it on certain nodes, particularly those where transit facilities are already in place. Further, Planning Division staff felt that this area needed to be studied for a potential change to the current zoning, because despite the current zoning of Transit Corridor District (TC-75), very little private investment has been made in the area since the TC-75 zoning was created in 2005 and existing zoning east of 600 East was not consistent with the Central Community Master Plan.

Analysis: The proposed 400 South Livable Communities Station Area Plans project is a reflection of the community's vision for 400 South. The creation of the plan was done with the intent of incorporating major themes identified through the public process. Once these items were identified, a series of best practices that were applicable to the community's vision were incorporated into the plan to guide future development in a manner that can help turn the community vision into reality.

After analyzing the comments from the community, the desire for a different type of development along 400 South eliminated the option to make no changes. If the proposed station area plans are not adopted, the existing policies and regulations would remain in effect. Community input and existing conditions indicate that there are unique situations and characteristics for each station, and that a one-size-fits-all approach would not capitalize on the unique assets at each station. Making limited changes near the station areas would not provide enough land area to accommodate future projected growth.

Zoning Analysis: Prior to 2005, the majority of the 400 South Corridor was zoned in a manner that promoted auto oriented development. The CC Commercial Corridor zoning district was mapped from about 250 East to 900 East. Some CS (Community Shopping) zoning was mapped on the 600 East blocks of 400 South. This zoning district was reflective of the development patterns that were created as cities, including Salt Lake City, planned around and for the private automobile. With the construction of the University Light Rail Line, the City acknowledged that the zoning in the corridor should be changed. In 2005, the City adopted Transit Corridor Zoning (TC-75) between 250 East and 600 East. For various reasons, no changes were made east of 600 East despite the Central Community Master Plan identifying the area as Transit Oriented Development on the Future Land Use Map. One of the goals of this project was to revisit the zoning along the corridor, particularly east of 600 East and work with stakeholders to identify the issues and formulate a plan and zoning modifications to implement the vision identified. Based on the public process and staff analysis, the proposal is to utilize the Transit Station Area Urban Neighborhood east of 400 East and Transit Station Area Urban Center west of 400 East.

A majority of the properties proposed to be changed are Transit Corridor District (TC-75), Moderate Density Multi-Family Residential (RMF-35) CS and CC (Commercial Corridor). There are a few other zoning designations throughout the corridor. All of these existing zoning districts are considered Euclidian-based zoning districts, which are based off of the theory of separating incompatible uses. However, due to new technology, building techniques and societal norms, certain segments of the population desire to live in mixed-use neighborhoods, where only the most impactful types of uses are not allowed. From an environmental and public expenditure standpoint, the City encourages mixed use development. In order to implement the vision identified in the Station Area Plans by the Planning Division staff, the current zoning designations were reviewed and found that those existing zoning districts were not capable of implementing the station area plans. Even the more recently approved TC-75 for the corridor has not seen any reinvestment in the neighborhood. Therefore, Planning staff determined that the Transit Stations Area (TSA) zoning designations that were already part of the zoning ordinance were the best designations to implement the vision of transit oriented development and mixed use projects.

The TSA zoning regulations are intended to regulate the physical form of buildings and how they relate to transit, the street, public spaces, adjacent buildings, and adjacent neighborhoods. Less emphasis is placed on use. A table of prohibited uses lists those uses that are not allowed. All other uses would be permitted.

Design and Development Standards can be found within the existing TSA zoning district. These are regulations that are required of all projects. These include items such as building setbacks, height requirements, parking locations, and minimum design requirements such as first floor glass, the location of doors and entrances, etc.

Instead of using the traditional permitted and conditional uses to determine review and approval processes, the proposed ordinance intends to incentivize new development and redevelopment through a series of Development Guidelines. These Guidelines include a value which is based on several factors, including the importance of that guideline towards implementing the policies of the station area plans or other adopted policies and goals of the City, and the cost to include specific guidelines in a project. Each project is reviewed and assigned a development score. The development score is the sum of the values assigned to each Development Guideline.

Following the Guidelines is voluntary and is not required for any project. However, proposed projects will be reviewed based on the value of the Guidelines the applicant chooses to incorporate into the project. A series of thresholds have been identified to guide the process. Those projects that choose not to include any of the Guidelines or that choose to only include a minimum number (Tier 1) will be processed following the existing Conditional Building and Site Design Review process. Those projects that obtain a Development Score within Tier 2 will be processed using the Administrative Hearing process already authorized in the Zoning Ordinance. Projects that are assigned a Development Score in Tier 3 will be processed as over the counter projects after the Development Score is assigned. Tier 3 projects are deemed to be the most compliant with the vision for a specific station area. Because the vision, urban design framework, policies and strategies were developed in an extensive public process, public participation process is put at the beginning of the development process, instead of as a reaction

to individual developments. The premise behind the tiered process is that desirable development, as indicated in the station area plans, should be easier to realize than less desirable development.

Additional regulations are required for any development that exceeds five acres. The primary purpose of this is to ensure that circulation and connectivity issues are addressed. The design of an internal circulation system and connectivity points with public rights-of-way are subject to approval from the Transportation Division. This would not be a departure from our current review process, but would provide guidance to developers in terms of what is expected.

The Development Guidelines were developed as part of the TSA rezoning process for the North Temple or Airport TRAX line corridor. They are a collection of design and development concepts that were either identified through the planning process or were already identified as a policy or goal of the City in a policy document that applies to the area or the City as a whole. Each guideline includes a description of what is desirable, images that are to be used as examples, and a way to measure how the guideline has been implemented. For example, the Design Guideline for Density and Intensity of Use includes specific density qualifications (number of dwelling units per acre), building height measures (reaching a certain percentage of the permitted building height) and floor to lot area ratios to determine if a project complies with the Design Guideline and whether it can count towards the Development Score.

The overall goals of the proposed zoning ordinance are:

- Put public participation up front in the planning process, instead of as a reaction to individual developments.
- Make it easier for desirable development to be processed.
- Create options for applicants in terms of how they design their project and how they want to be processed.
- To have the option of predefined outcomes.

Zoning Text Amendment

The proposed changes, as part of this petition, to the existing zoning text found in Chapter 21A.26.078 are minimal. The TSA zoning district has been part of the zoning ordinance for a few years now. Several projects along the North Temple/Airport TRAX line have been reviewed with these standards and staff has found that few changes are needed.

One of the changes proposed to be made are some general clean up of some typographical errors that have been found in the chapter. Other changes include adding specific standards for the 400 South Livable Communities corridor, as there are some different types of development adjacent to the street where the light rail is located. The biggest change is new language that ensures an additional setback is in place whenever a project in a TSA zoning district is adjacent to a low density residential or open space zoning district. Staff felt that this additional setback is necessary to protect the existing neighborhoods from new development and to allow adequate light and solar access.

The table of prohibited uses has been rearranged so that it is more consistent with other terms in the zoning ordinance. Some uses have been added to the table, as it has been evaluated by staff that additional uses were necessary in order to maintain more transit and not automobile-oriented uses within all TSA zoning districts.

Zoning Map Amendment

The purpose of the zoning map amendment is to put the regulations in place that will guide new development and redevelopment towards implementing the 400 South Livable Communities Station Area Plans. The proposed zoning map amendment would follow the station area boundaries identified in the station area plans.

The primary concerns with any large scale zoning map amendment are the potential impacts it has on existing businesses and property values.

This ordinance does not impact existing businesses. All existing businesses that are listed as prohibited uses would be considered legal, nonconforming land uses. These uses are allowed to continue operating. These uses will become subject to zoning ordinance Section 21A.38 Nonconforming Uses and Non-complying Structures. Under this section, nonconforming uses are authorized to continue. There are specific regulations that govern the moving, enlarging or altering of nonconforming uses of land and structures. If destroyed by fire, earthquake or other natural disasters, a nonconforming use would be allowed to occupy a new building on the site. The most impacted land uses are those that include drive through windows, gas stations, and auto service types of uses. Those uses are all either permitted or conditional under the current zoning. They will be prohibited in the proposed ordinance.

In most cases, the development potential of land east of 600 East will increase. The development potential is based on what can be done with the property under existing zoning regulations. This will vary depending on the existing zoning and the proposed zoning. Typically, a property with more development potential will have a higher property value. The zoning of a property does impact the real estate value of the property. When other market influences are considered, it is difficult to determine the long term impact on property values. However, it is not anticipated that changing the zoning of these properties will, by themselves, decrease property values. Properties west of 600 East are mostly zoned TC-75, which permits buildings up to 75 feet in height and allows building up to 125 feet as a conditional use. The proposal reduces the conditional height in the urban neighborhood areas to 75 feet and 90 feet in the urban center neighborhood. Projects in the urban center areas that meet certain thresholds may add up to two additional stories of building height.

The impact of taller and more intense development has been raised as a concern, although it has not been as big of a concern as anticipated. Regardless, the boundaries of the core and transition areas were drawn after considering many factors. The primary factor was the proximity to a transit platform. The core areas are all relatively close to a platform, while transition areas are further away. The transition area was created in order to step down development height and intensity as it gets closer to existing low density residential neighborhoods. Streets, which are typically 132 feet wide in most areas along 400 South, were used as boundaries where possible. As stated in the zoning text amendment sections, regulations requiring an increased setback

when adjacent to residential zoning districts are included as an additional protection to the impacts of height and intensity. In addition, increased setbacks have been incorporated when a TSA designated property is located adjacent to a street, which is less than 50 feet in width and includes certain residentially zoned property.

Master Plan Considerations: The City does not have specific standards for Master Plan Amendments. The Central Community Master Plan already includes a section on transit oriented development (TOD) and most of the corridor within the study area has a TOD future land use designation. The purpose of reviewing the Master Plan was to insure that the vision for the corridor was still valid, identify what TOD means for the corridor and make necessary modifications to strengthen the vision identified during the public process.

In considering an amendment to the Central Community Master Plan as part of the 400 South Livable Communities project, Planning staff also analyzed the following documents related to land use:

- Salt Lake City Futures Commission Report (1998)
- Salt Lake City Urban Design Element (1990)
- Salt Lake City Community Housing Plan (2010 – currently under review by City Council)
- Salt Lake City Transportation Plan (1996)
- Central Community Master Plan (2005)
- Wasatch Choices 2040 (2011)

Salt Lake City Futures Commission Report

The Salt Lake City Futures Commission report is a citywide document that is general in nature. It includes a number of recommendations grouped by category. Those relevant to the project include:

- Arts and Culture
- Built Environment
- Economics
- Natural Environment
- Neighborhoods
- Social Environment

This document recommends providing adequate public spaces that are equipped to handle gatherings of various sizes at different locations throughout the City. Providing live/work space for artists is also recommended. The 400 South Livable Community Station Area Plans recommends a broad range of housing types, including live/work units near the light rail stations.

The Built Environment section identifies a number of key recommendations that are relevant to the 400 South Livable Communities Station Area Plan. Assertion A states:

“An integrated transportation system, including alternative modes of transportation such as pedestrian ways, bicycles, mass transit, freight vehicles and personal automobiles ensure the enjoyable movement of people and products within the City.”

The Station Area Plans identify all these modes of travel and recognize the importance of effectively managing each mode. The success of each area depends on the efficient movement of people and goods.

Assertion B creates a hierarchy upon which urban design should be based:

1. Focus on the needs of the pedestrians and bicycles first;
2. Focus on mass transit second; and
3. Focus on the automobile third.

This section continues by saying: “public transit systems, such as light rail, are user friendly and designed with the pedestrian in mind; and all citizens have access to public transit within 1,200 feet of their homes.” By strategically focusing future growth and development around transit stations, current and future residences and workers will have better access to transit.

This section introduces the importance of design and mentions that high aesthetic standards, integrating urban design and building design, having streets with character and unique neighborhoods contribute to a more livable City and nurture a strong community. The Station Area Plans include a number of policies and strategies that attempt each of the assertions in the Built Environment section.

An important aspect of the Futures Commission Report is identifying that all people have a stake in the planning and building of the City. From the beginning of the planning process for 400 South, Planning Division staff has intended for this plan to be based on community input, especially from those who are traditionally under-represented in the planning process. The broad participation that was received in this process is an important aspect of the plan and important to the long term success of the plan. It should be noted that the Planning Division set a goal of engaging at least 200 people as part of the outreach project, with at least 60 participants being from groups who are traditionally under-represented in the planning process. The Planning Team exceeded this goal. While the numbers are modest, they establish a baseline for future planning activities in the City and hopefully a turning point in how the City engages its residents, property owners, business owners, and service providers.

The Economics section of the Futures Commission Report identifies that planning and zoning are important economic development tools. Many of the policies, strategies and key projects are aimed at promoting economic development along the corridor to support the business community, enhance the neighborhoods, project the City’s tax base, and improve the economic condition of the neighborhoods along the corridor and the City as a whole. Other parts of the Economic Development section discuss:

- Light rail being critical to the transportation system as well as improving air quality; and
- Promoting housing and mixed use development throughout the City.

The Natural Environment section focuses on air and water quality, solid waste management, open space, and gateways. The 400 South Livable Communities Station Area Plans address these issues by promoting compact development that uses less land and provides people with options on how they move, where they live, and where they shop, dine, work and play. The plans also allow for the creation of new open spaces.

Neighborhoods are the backbone of any city and the neighborhoods along 400 South are no exception. Preserving the neighborhoods in the area provides a foundation for future development in the area. With the anticipated growth in Salt Lake City, future development and residential density should be strategically located so that the existing neighborhoods are preserved. By concentrating new development near the existing transit stations, the City can adequately provide services to new development and preserve the neighborhoods at the same time. Providing a range of housing options helps to create diverse neighborhoods and provides people with different needs options as to where they live.

The Social Environment section defines itself as “everything in our society that improves our lives, expands our minds, and helps us to be healthy, caring, educated and productive citizens.” This section has recommendations related to promoting community involvement, expanding recreational opportunities, and addressing issues that impact our community. The station area plans project on some of these principles and have had an extensive public involvement process. Providing adequate housing for people with different needs, providing transportation options and enhancing our open spaces and access to our trail system improves our community.

Salt Lake City Urban Design Element

The purpose of the Urban Design Element is to define urban design objectives for the City and illustrate a process for making decisions regarding the City’s future character. To that end, the Urban Design Element recommends a number of policies and strategies. A key strategy is to recognize that land use intensities and building heights should reflect the relationship between the district that they are located within and adjacent neighborhoods and their respective role in the City. The document also states “indiscriminate high rise construction outside of the downtown core adversely affects the strong downtown development concentration characteristic of the City.” The Station Area Plans recognize this concept by limiting building height to a level that transitions from the core of a stations down towards existing neighborhoods. In addition, the building height complements downtown by having lower building heights and continues the downward progression of building height between Downtown and the 900 East Station, while allowing adequate development potential to accommodate future growth within the corridor. The Urban Design Element lists many other policies and strategies that are relative to 400 South and addressed in the Station Area Plans, including:

- Allowing individual districts to develop in response to their unique characteristics within the overall urban design scheme for the City;
- Treat building height, scale and character of significant features of a districts image;
- Ensure that features of building design such as color, detail, materials and scale are responsive to district character, neighborhood buildings and the pedestrian;
- Maintain a pedestrian-oriented environment at the ground floor of buildings;

- Introduce pedestrian-oriented elements such as landscaping, sidewalk lighting, pedestrian oriented building and site design into neighborhood commercial centers;
- Use street spaces, patterns and rhythms to unify the image of a district;
- Preserve prominent view corridors and city vistas. Prominent land forms, buildings and monuments should remain clearly visible as city landmarks. Special attention should be given to the design of buildings adjacent to prominent streets and vista corridors.
- Encourage pedestrian walkway networks that connect individual buildings, blocks, groups of blocks and entire districts; and
- Require new buildings to respect the pedestrian elements of the street.

The 400 South Livable Communities Station Area Plans provide additional guidance for land use decisions and includes policies which complement the Urban Design Element. The Station Area Plans provide more focus of the urban design concepts because there are specific urban design policies related to each station area.

Salt Lake City Community Housing Plan

The goal of the Community Housing Plan is to enhance, maintain and sustain a livable community that includes a vibrant downtown integrated with surrounding neighborhoods that offer a wide range of housing choices, mixed uses and transit oriented design. The key concepts addressed in the Housing Plan include:

- Foster and celebrate the urban residential tradition;
- Respect the character and charm of predominantly residential districts, including those with historic character and qualities, while also providing opportunities for the provision of local goods and services easily accessed by neighborhoods;
- Promote a diverse and balanced community by ensuring that a wide range of housing types and choices exists for all income levels, age groups, and types of households;
- Develop new housing opportunities throughout the City;
- Ensure that affordable housing is available in all neighborhoods and not concentrated in a few areas of the City;
- Emphasize the value of transit-oriented development, transit accessibility and proximity to services;
- Recognize that residents, business owners, and local government all have a role to play in creating and sustaining healthy neighborhoods;
- Create an appropriate balance of rental and ownership opportunities in neighborhoods without jeopardizing an adequate supply of affordable housing; and
- Strongly incentivize or require the use of green building techniques and sustainability practices in public and private housing developments.

The Station Area Plans include a number of policies that support the above concepts. The development concepts identified in the Station Area Plans include a major focus on creating a range of housing options for people with different housing needs. The plan also discusses preserving existing housing in existing neighborhoods.

Salt Lake City Transportation Plan

The Salt Lake City Transportation Plan includes policies related to all forms of transportation, including automobile, mass transit, pedestrians, and bicycles. The plan correctly identifies the important link between transportation and land use and provides the following relevant direction for future land use:

- Salt Lake City will preserve and enhance residential communities within the City which allow residents to live, work and play in the same area;
- Salt Lake City will explore opportunities to increase residential and destination densities at major bus and rail transit nodes along transit corridors;
- Salt Lake City will promote development that is transit, pedestrian and bicycle friendly.

The Station Area Plans are consistent with this direction and aim at providing opportunities for land use to support mass transit and vice versa. The Transportation Plan provides direction for increasing the number of bicycle lanes within the City and maintaining those lanes to a high standard. The Station Area Plans indicate that a finer grain network of bicycle paths and trails will be warranted as the station areas develop over time and bicycle use increases. While 400 South may not be able to provide all modes of transportation in a safe and convenient manner, it should be viewed as a portion of a network, with nearby parallel streets providing other opportunities, particularly for bicyclists.

Central Community Master Plan

The Central Community Master Plan was adopted in 2005. It identifies and discusses TOD corridors along 400 South and notes TOD districts create a walkable environment that encourages residents and employees to use modes of transit rather than the automobile. The Station Area Plans will implement that vision for the community that has been in effect for some time.

Wasatch Choices 2040 Plan

The Wasatch Choices 2040 Plan is a four county vision for land use and transportation in the future. Although not an officially-adopted plan of the City, it includes many of the same goals discussed in the plans listed above and helps identify Salt Lake City's role in the region and the State. The plan states, "Over the coming years, the Wasatch Front is expected to annually add a population comparable to the City of Murray, or about 34,000. Growth in our region is largely inevitable; over two-thirds of this population will come from our children and grandchildren. Our challenge is to preserve or even enhance quality of life in the face of growth." With this statement in mind, the plan contains specific principles and objectives for transportation planning, some of which are noted below.

- Optimize use and maintenance of existing infrastructure.
- Promote compact development consistent with market demand.
- Encourage contiguous growth to reduce infrastructure expenses.
- Develop a balanced, multi-modal transportation system.
- Coordinate transportation with regional employment, housing, educational and activity centers.

- Encourage future commercial and residential areas within close proximity of each other to reduce travel distances.
- Encourage a balance of jobs and housing in each part of the region to reduce travel distances.
- Support actions that reduce growth in per capita vehicle miles of travel.
- Make land-use and transportation decisions based on comprehensive understanding of their impact on each other.
- Encourage land use and housing policies to accommodate the need for a variety of housing types throughout the region.
- Encourage housing and other development near transit to maximize the efficiency of the public transportation system.

The 400 South Livable Communities Station Area Plans provide additional guidance for land use and transportation policies noted above. The Station Area Plans recognize the growth will be occurring over the next several years and that compact development that utilizes existing investments in infrastructure is the best way to approach the increase.

Master Plan Summary

The proposed 400 South Livable Communities Station Area Plans are generally consistent with the policies and guidelines of the listed city and regional wide plan along with the adopted Community Master Plan. The Station Area Plans provide finer detail, are more specific to geographic areas and provide adequate guidance on future land use decisions. It is critical that future zoning be compatible with the Station Area Plans, reflect the communities' vision for each station area and can provide the necessary flexibility, processes and regulations to produce desired development. The plans provide for appropriate height, densities, and land use intensities in various geographic sections along the corridor. These policies are important to achieve the City's goals of environmental, economical and social sustainability, as well as ensure the large public investment in infrastructure along 400 South is effective in revitalizing this area of the city and providing for the needs of the residents, business community and other stakeholders in the area.

PUBLIC PROCESS:

The following is a list of public meetings that have been held related to the proposed project. A summary report that contains comments from most of the public meetings can be found as an attachment.

- Community Council meetings held on the following dates:
 - Central Community – April 6, 2011 and March 7, 2012.
 - East Central Community – May 12, 2011 and March 8, 2012.
 - Comments and notes can be found in attachment E.
- Community Workshops held on the following dates:
 - Salt Lake City Main Library – May 19, 2011
Approximately 5,400 owners and tenants within a ½ radius of the three transit stops by a postcard mailed to their address. Various flyers were posted throughout the area also.

- Salt Lake City Main Library – October 3, 2011
An email notice was sent through the listserve and also to anyone who had attended the first community workshop.
- Trolley Square – February 29, 2012
Approximately 1,350 postcard notices were mailed to the owners and occupants whose property was proposed to be changed or was located within 300 feet of a property proposed to be changed for the February and March open houses.
- Planning Division Open House – March 15, 2012
- Comments and notes can be found in attachment E.
- Stakeholder and Community Organization Meetings on the following dates:
 - University of Utah Transportation Meeting – June 27, 2011
 - YWCA – July 7, 2011
 - Refugee Services – July 21, 2011
 - Property Owner/Developer – August 10, 2011
 - Odyssey House – August 26, 2011
 - East High School AP US Government Class – April 11, 2012
 - Several requests were made to Bennion Elementary school about having a presentation. The school was not interested in having a presentation.
- Public comments have been left on the project's webpage during the course of May 2011 up until April, 2012.
- Various one-on-one meetings with local community members who have requested a meeting.
- Planning Commission briefing on March 28, 2012.
- Historic Landmark Commission briefing on April 5, 2012.
- Planning Commission public hearing on April 25, 2012.
- Historic Landmark Commission public hearing on May 17, 2012.
- Planning Commission public hearing on May 23, 2012.

In addition to the above public meetings or workshops, the item was placed on the City's webpage in the 'Open City Hall' section and various comments were made about the project. The summary report from Open City Hall is included, as well as the emails and letters we have received since the project was first introduced last May.

Notice in the newspaper and on the listserve for the April 25, 2012 Planning Commission meeting are noted below. A second Planning Commission public hearing was held on May 23, 2012 with postcards mailed to all affected property owners.

- Public hearing notice posted in newspaper on April 13, 2012.
- Public hearing notice posted on City and State websites on April 12, 2012.
- Public hearing notice emailed to the Planning Division listserve on April 12, 2012.

At the Planning Commission public hearing on May 23, 2012, there were three people who spoke regarding the project. The comments were generally supportive in nature, but there were concerns raised about the effect of taller buildings to the west of Gilgal Gardens. The concerns are that the buildings will be too tall and that the shadows will impact the plants and flowers that grow in the garden. Several emails were also received regarding this concern. As stated in the past, staff indicated that the proposed TSA zoning regulations would require buildings to be set

back and step back when adjacent to the Garden. Based on models that Planning staff has developed, the Garden will still maintain sunlight for at least six hours a day.

At the Planning Commission public hearing regarding the Transit Station Area Development Guidelines, a comment was made that too many points are awarded to projects which have parking structures. A project could obtain up to 50 points for building a parking structure and the feeling was that this was too many points. In their motion to approve, the Planning Commission did suggest that the City Council consider modifying the number of points awarded for parking structures. A number was not recommended by the Planning Commission.

RELEVANT ORDINANCES:

Master plans and amendments to master plans are not regulated by the Zoning Ordinance. However, sections 10-9a-204 and 205 of the Utah State Code Title 10, chapter 9a Municipal Land Use, Development and Management Act regulate the requirements for noticing a general plan amendment and land use ordinance amendment. The public hearings for the 400 South Livable Communities were notified in accordance with this statute.

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8. ORIGINAL PETITION

1. PROJECT CHRONOLOGY

PROJECT CHRONOLOGY
Petition PLNPCM2010-00647

September 13, 2010	Petition initiated by Mayor and assigned to Maryann Pickering, Principal Planner, for staff analysis and processing.
April 6, 2011	Central Community Council meeting.
May 2011 – April 2012	Comments left on the projects webpage.
May 12, 2011	East Central Community Council meeting.
May 19, 2011	Community workshop held at Salt Lake City Main Library.
June 27, 2011	Attended University of Utah Transportation Committee meeting.
July 7, 2011	YWCA presentation.
July 21, 2011	Refugee Services presentation.
August 10, 2011	Property owner/developer meeting and presentation.
August 26, 2011	Odyssey House presentation.
October 3, 2011	Community workshop held at Salt Lake City Main Library.
January 2012 – April 2012	One on one meeting with various community members who requested to meet. Approximately five meetings were held.
February 29, 2012	Community workshop held at Trolley Square.
March 7, 2012	Central Community Council meeting.
March 8, 2012	East Central Community Council meeting.
March 15, 2012	Community Open House held.
March 27, 2012	City Council adopts an interim ordinance allowing temporary zoning in place for most of the project area.
March 28, 2012	Planning Commission briefing held.
April 5, 2012	Historic Landmark Commission briefing held.
April 11, 2012	East High School AP US Government Class presentation.
April 13, 2012	Newspaper notification for Planning Commission public hearing.
April 25, 2012	First Planning Commission public hearing held.

May 3, 2012 Postcards mailed for Historic Landmark Commission to all property owners and adjacent property owners who would be affected by the proposed zoning changes within the boundaries of the Central City Historic District.

May 11, 2012 Postcards mailed for Planning Commission to all property owners and adjacent property owners who would be affected by the proposed zoning changes for the project as a whole.

May 17, 2012 Historic Landmark Commission public hearing held.

May 23, 2012 Second Planning Commission public hearing held.

May 25, 2012 Ordinance requested from City Attorney's office.

June 13, 2012 Planning Commission ratifies minutes of the May 23, 2012 Planning Commission public hearing.

June 27, 2012 Planning staff receives ordinance from the City Attorney's Office.

2. ORDINANCE

SALT LAKE CITY ORDINANCE
No. _____ of 2012

(Amending the Central Community Master Plan, zoning map and zoning ordinance pertaining to transit station area plans along the 400 South transit corridor)

An ordinance amending the Central Community Master Plan, zoning map and zoning ordinance to implement land use policies related to mixed use development and transportation options on the blocks adjacent to the 400 South transit corridor pursuant to Petition No. PLNPCM2010-00647.

WHEREAS, the Salt Lake City Planning Commission (“planning commission”) held public hearings on April 25, 2012 and May 23, 2012 on an application submitted by Mayor Ralph Becker (“Applicant”) to amend the Central Community Master Plan, zoning map and zoning ordinance to implement land use policies related to mixed use development and transportation options on the blocks adjacent to the 400 South transit corridor (between 1000 East and 200 East Streets) pursuant to Petition No. PLNPCM2010-00647; and

WHEREAS, at its May 23, 2012 meeting, the Planning Commission voted in favor of forwarding a positive recommendation to the Salt Lake City Council (“City Council”) on said application; and

WHEREAS, after a public hearing on this matter the City Council has determined that adopting this ordinance is in the City’s best interests.

NOW, THEREFORE, be it ordained by the City Council of Salt Lake City, Utah:

SECTION 1. Amending the Central Community Master Plan. That the Central Community Master Plan shall be and hereby is amended as follows:

- a) That page 15 of the Central Community Master Plan is amended to read as

follows:

Transit-Oriented Development (TOD)

INTRODUCTION

Transit-Oriented-Development (TOD) is a land use and urban design concept that emphasizes a mix of mixes land uses with pedestrian access located use near transit to maximize transportation options and provides people with choices about living near transit. A TOD neighborhood typically has a center with a light rail stations. Mixed land uses include residential, retail, office, cultural, institutional, and open space. Transit-oriented station or bus stop surrounded by relatively high-density development districts create a walkable environment that encourages residents and employees to use modes of transit other than the automobile. The goal of this approach is to mitigate the environmental impacts of the private automobile by reducing the cumulative vehicle miles traveled. TOD districts improve the quality of life in urban areas and may influence suburban commuters to move back into the Central Community. In the long term, this type of with progressively lower-density development can help reduce negative impacts of future regional growth on the environment, the quality of water and air, the availability of open space, and the cost of landspreading outward from the transit center. TOD's generally are located within a radius of one-quarter to one-half mile from a transit stop, as this is considered an acceptable walking distance.

TOD areas offer choices in housing, commerce and transportation. They are designed to be flexible so they can respond to changing economic and social conditions and endeavor to make development. TOD can assist in revitalizing economically viable from a number of perspectives (city, transit agency, developer, resident, employer, etc.). TOD districts encourage the stabilization and revitalization of existing neighborhoods in the Central Community, especially when retail, residential, and office, as new uses are combined designed to support existing neighborhood characteristics. Urban design requirements need to focus on pedestrian orientation and scale.

TOD districts improve the quality of life in urban areas by:

- Including mixed-use development that will use transit at all times of day.
- Creation of excellent pedestrian facilities such as high quality pedestrian crossings, narrow streets, and tapering of buildings as they become more distant from the public transport node.
- Ensuring compatibility and connectivity with surrounding neighborhoods.
- Creating compact development within an easy walk of public transit and with sufficient density to support transit ridership.
- To establish a hierarchy of transportation which places the pedestrian first, bicycle

second and auto third.

- Creating active places and livable communities that service daily needs and where people feel a sense of belonging and ownership.
- Including engaging, high quality public spaces (e.g. small parks or plazas) as organizing features and gathering places for the neighborhood.
- Encouraging a variety of housing types near transit facilities.
- Providing housing choices for a wide range of ages and incomes.
- Incorporating retail into a development if it is a viable use at the location. Ideally drawing customers both from both the TOD and a major street.
- Introducing creative parking strategies that integrate, rather than divide a site and reduce the presence of the auto.
- Creating TOD plans that are flexible so they can respond to changing conditions.
- Recognizing that all TOD's are not the same; each development is located within its own unique context and serves a specific purpose in the larger context.

TOD strengthens urban development but also helps manage future regional growth by encouraging growth to occur where the existing infrastructure is best suited to address it. The benefits of this type of development include:

- Opportunities for affordable and accessible living by incentivizing affordable and accessible housing goals, providing a range of housing options, and allows people to spend less of their income on the combined expenses related to housing and transportation.
- Reduction of the long term expenditures of tax dollars to build and maintain public utilities, roads and other auto-oriented infrastructure.
- More residents living close to commercial areas to support a strong, local oriented economy.

This reduces the overall cost of development and reduces negative environmental impacts on air and water quality while creating community oriented public places.

Transit-oriented development designations

Transit-oriented development districts within the Central Community have three designations: low-density, medium-density, and high-density. The Future Land Use map; on page 27, shows locations where these districts are supported by this master plan. In all designations, where Where conflicts between the transit-oriented district TOD and historic district overlay regulations occur, the historic overlay requirements govern.

Low-density transit-oriented development: (light sage green on map): Low-density TOD supports residential uses with a density ranging from 1-20 dwellings per acre. The emphasis of low-density TOD design and land use (light sage green on map) relates to existing lower density residential density and neighborhood commercial land uses. This low Low intensive development should be designed to assure compatibility in neighborhoods with established low-density characteristics; by focusing on the massing and scale of the existing structures. Medium intensive land uses, such as a coffee shop,

town house or daycare centers, may be appropriate near the light rail station- or busier intersections. Implementing low-density TOD areas may include development of accessory units in the rear yards of low-density residential land uses as well as small businesses that can be operated out of a residential structure. New structures should be in scale with the low-density neighborhood with modest increases in building height being appropriate if it provides opportunity for increasing the residential density. Zoning designations should require compatibility in these areas. ~~Structures should remain in scale with the low density neighborhood. Zoning designations should include regulations to ensure compatibility in these areas.~~ Low density transit oriented development supports residential uses with a density ranging from 1-20 dwellings per acre.

Medium-density transit-oriented development: (medium sage green on map): Medium-density TOD supports residential land uses with a density range of 20-50 dwelling units per acre. The design emphasis for medium-density TOD ~~(medium sage green on map)~~ is compatibility with existing medium- and low-density residential and commercial development. Higher intensive uses may be located near light rail stations where applicable. Medium-density TOD areas include four to five story buildings with a mix of ground level retail or office space components with multi-story residential development above the ground floor levels. These areas must also should have limits on the amount of space allocated for non-residential land uses: by locating them closer to transit and at busier intersections. Individual ~~solely~~ residential land uses could remain within the TOD area. Zoning districts that allow four to five story building, including pedestrian oriented design standards and allow 20-50 dwelling units per acre are appropriate in areas with this designation. Building height maximums would be regulated by the zoning designations. ~~Medium density transit oriented development supports residential land uses with a density range of 10-50 dwelling units per acre.~~

High-density transit-oriented development: ~~High density TOD~~ (dark sage green on map): High-density TOD is the same concept assimilar to medium-density TOD except at a greater scale. These areas are in should be centers of high population where with a concentration of pedestrians in close proximity to transit stations. These areas will be more successful in places that are more concentrated, well served by rail transit. Building heights are established for high density residential and higher intensity office or commercial uses. ~~They have a maximum of three floors-~~ The ideal building type in these areas is vertical mixed use structures tall than five stories. High-density TOD supports the development of office or retail space with multiple floors, compact urban centers with 50 or more dwelling units per acre. These centers can provide accessibility and a multitude of residential uses above. The intent is benefits (efficiencies that result when many activities are physically close together). Design standards are critical in these areas in order to create a revived downtown and strengthen the livability of the Central Community. High density transit people oriented spaces. Auto oriented development supports residential land uses with a density range of 50 or more dwellings per acres. should be prohibited. Standards and processes should be put in place to allow for the easy and financially viable transition of auto oriented developments to people oriented development.

The transit-oriented development land use designations are shown on the Central Community: [TOD map and on the Future Land Use map.](#)

Community input on Transit-Oriented Development

TOD impacts on residential neighborhood character

The TOD districts could increase residential densities without consideration of the existing neighborhood characteristics. Assembling property and removing residential structures can change the character of the neighborhood. In addition higher densities will change the demand for various types of services in residential neighborhoods.

TOD areas may attract too much commercial development

Commercial land uses could easily monopolize the land use in TOD districts to the detriment of residential uses.

Existing mixed use regulations do not require property owners to build the residential component of mixed use

Although the City should be able to enforce true mixed use in mixed use areas, City codes allow property owners to build for a single land use. In the existing Residential Mixed Use (R-MU) and Residential Office (R-O) zoning districts, most property owners have built either all residential or all commercial buildings, rather than mixing them within the same project. All too frequently, the result is an overabundance of commercial buildings.

Type and quality of housing permitted in the TOD districts

The Central Community Master Plan should address the concentration and variety of housing types and housing stock that will be allowed in TOD districts. This issue is addressed in the Residential Land Use Chapter.

Future Transit-Oriented Development Land Use Changes

The transit oriented development land use classifications will be implemented through the development of transit corridor zoning districts. These districts will be developed to implement the three levels of TOD land use designations. The purpose of each district will be to provide an environment for efficient and attractive transit and pedestrian oriented development to a scale that is appropriate to the land use designation and existing development character. Preserving the historic fabric of existing neighborhoods is a high priority within Transit-Oriented Development areas. The TOD approach must generate multifamily housing units on commercial properties without negatively

impacting historic neighborhood characteristics. As growth in the non-residential areas evolves into more mixed use, the historic residences adjacent to the TOD areas will be protected. Rehabilitating residential structures to create new multiple dwelling units in the historic districts can attract residents who want to be near transit services. Future light rail lines are planned to connect to the intermodal hub at 600 West and 200 South and extend along North Temple to the Airport. Potential future light rail stops planned at Main Street and 700 South and at 200 West and 900 South should be evaluated for TOD suitability. The existing and future light rail lines and stations and the commuter and intermodal hub locations are depicted on the Central Community TOD map on page 66. TOD's are implemented by the public and private sectors. The public sector sets the stage by providing the transit, investing in public infrastructure and applying appropriate zoning regulations near the transit stations. The private sector generally finances, constructs and markets TOD's. When based on a sound and agreed upon vision, TOD implementation should be a simple and straightforward process. To ensure this is the case, the TOD station area plans intend to create the vision and set the stage for the private sector. The City has developed certain zoning districts to implement the various intensity levels of TOD that are based on input from a broad range of stakeholders. The areas listed below have gone through a community visioning process that had identified the desired development characteristics and therefore, support zoning changes based on the specific goals for each area.

400 South and University Boulevard

In June of 2011, the City received a grant from the Housing and Urban Development Department (HUD) to initiate the 400 South and University Boulevard Livable Communities project. The focus of the project was to enhance the Community's vision of the corridor and make it a place where people could live, work and shop. Inherent in this process was the inclusion of people who rely on the corridor for their daily needs, but for various reasons do not normally participate. The group included those who are traditionally under-represented in the planning process. The project determined that future development at all 400 South stations should be compatible with the existing neighborhood scale and that development should not supplant or compete with the central business district regional services (beyond the existing Trolley Square development). Development should enhance existing urban neighborhoods within walking distance of the transit line to create areas where pedestrian activity is the focus of daily transportation needs, without excluding the automobile. These concepts are critical to groups who rely on public transportation to access housing, employment, education, health care, and other daily needs.

The 400 South and University Boulevard Trax lines run from downtown to the University of Utah. The corridor incorporates a mix of transportation options, including light rail, bus and automobile. There are several bicycle lanes that parallel and intersect the street to create a complete transportation network through the area. When the light rail line opened in 2001, the pedestrian environment improved by adding street trees and widening the sidewalk. Unfortunately, the parking lane was removed, placing a travel

lane immediately adjacent to the sidewalk. In order to make the corridor more pedestrian oriented, sidewalk improvements are necessary.

The Liveable Communities project identified specific land use goals and policies that can be found in the “400 South and University Boulevard Station Area Plans” in the addendum of this Central Community Master Plan. The station area plans include specific land use policies and a future land use map that should be used to help guide future land use, capital improvements and budget decisions.

West Temple Gateway

The West Temple Gateway area extends from 700 South to the 900 South Interstate off ramp and from 300 West to West Temple and includes the 200 West / 900 South future light rail stop. The West Temple Gateway area is part of a redevelopment project area created in 1987, which included two revitalization concept plans. These are not adopted policy plans but resource documents. The 1994 plan identified alternative concepts ranging from low-density residential infill to Big Box retail uses. A second analysis in 2001, after the light rail line was constructed, provided an Illustrative Plan that proposes a mixed use transit-oriented neighborhood containing residential, retail, office, and industrial land uses. Development of a West Temple Gateway small area master plan will provide detailed development guidelines for this area.

Transit Oriented Development ~~goal~~Goal

~~Establish the benefits~~To create TOD development with a balanced mix of Transit-Oriented Development uses that generates 24-hour transit ridership. This development will have places to work, to live, to learn, to relax and to shop for daily needs. The goal will be achieved through land use designations, ~~design~~development guidelines, zoning, and both public and private funding.

Transit Oriented Development policies

Transit-Oriented Development policies fall into these general categories: location and variety of land use.

Location

Policy TOD-1.0 Based on the Future Land Use map and specific station area plans, establish Transit-Oriented Districts that will provide residents with a range of land use housing, transportation and employment options at various densities near transit stations.

TOD-1.1 Develop station area plans for each transit station within the Central Community Planning Community.

TOD-1.2 Utilize a broad community involvement approach to identify appropriately located and scaled transit oriented development that put people first.

Variety of ~~land-use~~Land Use

Policy TOD-2.0 Encourage the development of mixed-use projects near light rail stations to create a livable, walkable urban environment.

TOD-2.1 Support a variety of low-, medium- and high-density residential uses around light rail stations in TOD districts, based on the Future Land Use map designations.

TOD-2.2 At light rail stations in TOD districts, establish a centralized core of land uses that support transit ridership. Anchor transit centers with land uses that act as destination points.

TOD-2.3 Encourage a variety of commercial uses that share the same clientele and patrons. For example, movie theaters provide a clientele to patronize restaurants, arcades, and retail businesses.

b) That the following text shall be added to the Central Community Master Plan in the section titled “Central Community Master Plan Committee Goals and Recommendations” immediately following subparagraph 15 of the subsection titled “Environment”, which presently appears on page 23 of that master plan;

400 South and University Boulevard Station Area Plans

Each of the three station area plans include subareas as identified below:

Core Area: The purpose of the core area is to provide areas for comparatively intense land development with a mix of land uses incorporating the principles of sustainable, transit oriented development and to enhance the area closest to a transit station as a lively, people oriented place. The core area is generally within a one-fourth (1/4) mile walk of a transit station platform. The core area may mix ground floor retail, office, commercial and residential space in order to activate the public realm. Buildings in this area should have minimal setbacks to encourage active outdoor use adjacent to the sidewalk, such as outdoor dining and patios that reflect the desired character of the area. Building facades should be varied and articulated, include storefronts adjacent to the street, windows on the street level, and have clearly defined entrances to provide visual interest to pedestrians. Buildings should be a minimum of two (2) or three (3) stories in height, depending on location, in order to define the street edge. Arcades, bays, and balconies

are encouraged. The configuration of buildings must balance the needs of all modes of circulation with the safety and comfort of pedestrians and bicyclists. A vertical mix of uses, with office and residential above ground floor commercial uses is encouraged. A minimum of thirty (30) dwelling units per acre is encouraged within the core.

Transition Area: The purpose of the transition area is to provide a moderate level of land development intensity that incorporates the principles of sustainable transit oriented development. The transition area is intended to provide an important support base to the core area and transit ridership as well as buffer surrounding neighborhoods from the intensity of the core area. These areas reinforce the viability of the core area and provide opportunities for a range of housing types at different densities. Transition areas are generally located within a one-half (½) mile from the station platform, but may vary based on the character of the area. Transition areas typically serve the surrounding neighborhood; include a broad range of building forms that house a mix of compatible land uses. The minimum desired density is ten (10) dwelling units per acre. Commercial uses may include office, retail, restaurant and other commercial land uses that are necessary to create mixed use neighborhoods. Commercial uses can be clustered around intersections and along block faces to create neighborhood nodes.

Library Station Area

The Library Urban Center Station has the highest intensity level and mix of uses along 400 South. It supports Salt Lake City's central business district in terms of building scale and use. The area has a twenty-four-hour population, active streetscapes, defined street walls and multiple types of public transit. Development generally occurs on surface parking lots or through redevelopment of underutilized parcels.

The street frontages in the Library Station area have a compact, dense, interconnected and walkable development pattern, while the interior of blocks are open generally with parking lots. Large scale development is closer to the station; and is scaled back as it moves to the areas away from 400 South. Building forms vary, but are typically oriented to the pedestrian, are multiple stories in height, and contain a horizontal and vertical mix of land uses. Buildings up to thirty stories in height are allowed within the D-1 zoned sections, while buildings in the transition zone can be approximately three to four stories in height.

The area features a variety of dense, mixed use commercial and housing developments, providing residents with a number of housing options. The station area contains important regional attractions, such as the Library, City & County Building, Leonardo Museum, and the Public Safety Building. These buildings are a regional draw, bringing thousands of people to the area each day. The area also features office, dining and entertainment options with a high level of pedestrian activity. The Library and Washington Square feature a number of civic and cultural festivals during the summer.

The station area comprises of core and transition areas. The purpose of creating the different areas is to recognize the scale and nature of existing development patterns and identify the appropriate locations for growth. The general concept is that bigger

buildings with the most dwelling units and a higher intensity level of commercial space should be located closest to the station in the core. The transition area reduces the scale, mass and intensity of new development as it moves away from the core.

In addition to the civic uses, and high density development surrounding the Library Station core, there is a significant amount of underutilized land. The Library Station Area Plan encourages the development of these areas to create medium to high density housing, mixed use development and appropriate support commercial along 400 South. By doing this, the area would further its role as a vibrant and contributory feature of Salt Lake City, while maintaining its function as a support to the downtown area.

The “Transit Station Area (TSA) Development Guidelines” will be used along 400 South to encourage a walkable urban neighborhood compatible with the adjacent historic district.

Library Station Area Goals:

1. Rezone properties fronting 400 South so that the zoning reflects the vision for the station area. Rezone the properties that are in the vicinity to a mix of zoning districts that promote high density residential development while allowing limited commercial types of uses.
2. Coordinate with Utah Department of Transportation (UDOT) to improve the functionality of 400 South for pedestrians. This could be accomplished by adding additional midblock crosswalks to the transit stations, allowing on-street parking during off peak travel hours, studying the impact of reducing the road width, whether in key spots or for longer stretches and other changes to the street that are aimed at creating a safe and functional transportation corridor for all users.
3. Work with the City Transportation Division to find appropriate bicycle routes that run on or are parallel to 400 South and that connect to appropriately located north/south bike routes.
4. Require midblock connections from 300 South and 500 South that connect to 400 South.
5. Incorporate way-finding features at the station area so people can easily locate station area assets such as Library, Library Plaza, City & County Building, Leonardo Museum and the Public Safety Building.
6. Maintain the original 15-foot landscaped setback along the 400 South street frontage.
7. Require properties to share driveway access as properties are redeveloped to reduce the number of drive approaches in the station area.
8. Minimize the number of driveways, garage entrances and dedicated turning lanes on

all major pedestrian routes.

9. Encourage community services, including schools, childcare and museums with pedestrian connections to transit and other land uses.
10. Extend 450 South midblock walkway through to 650 East.
11. Develop and enhance existing public gathering spaces, including parks, plazas and courtyards to attract people and transform 300 South and 500 South into active pedestrian places.
12. Increase building intensity and residential densities closest to the transit station and gradually step down further away. Parking requirements should be lower closer to the station.
13. Discourage additional or expanded stand alone automobile oriented uses such as: large surface parking lots, fuel stations, auto repair shops, auto dealerships, large scale retail development, car washes and drive-thru facilities.
14. Mid-block crossings across 400 South should be considered adjacent to the Trax stop to help pedestrians cross safely and to discourage jaywalking.

Trolley Station Area

The Trolley Station is defined as an Urban Neighborhood Station Area. Urban Neighborhoods are places that have an established development pattern that contain a mix of uses and can support an increase in residential density and supporting commercial activities. New development generally occurs as infill, occurring on undeveloped or underutilized properties. Redevelopment of surface parking lots that front on 400 South is a priority. A compact development pattern is desired in order to focus new growth at the station and respect the existing scale and intensity of the surrounding neighborhood. The highest residential density and intensity of commercial land use occur closest to the transit station and are scaled down the further one moves from the station.

The station area comprises of core and transition areas. The purpose of creating the different areas is to recognize the scale and nature of existing development patterns and identify the appropriate locations for growth. The general concept is that bigger buildings with the most dwelling units and a higher intensity level of commercial space should be located closest to the station in the core. The transition area reduces the scale, mass and intensity of new development as it moves away from the core area.

Trolley Station is a unique Transit Station Area because it is located within the Central City Historic District. The Central City Historic District is centered on the 600 East landscaped medians, which are a character defining feature of the historic district. Over time, the 600 East medians have been degraded by the continual pressure from commercial development to allow vehicle access through the medians. The policy of the Trolley Station Area is to prohibit further dissection of the 600 East medians for

vehicular traffic and to maintain the historical 15 foot landscaped setback of building along 600 East.

Despite being located within the Central City Historic District, the blocks that front on 400 South have lost most, if not all, of their historic character. Without the historic character being present, the boundaries of the Central City Historic District should be reviewed and amendments to the district boundaries should be proposed.

The “TSA Development Guidelines” will be used along 400 South to encourage a walkable urban neighborhood compatible with the adjacent historic district.

Trolley Station Area Goals:

1. The primary purpose of the Trolley Station Area is to provide housing and access to higher intensity employment, commercial centers, downtown, and to the University of Utah.
2. Rezone properties fronting 400 South so that the zoning reflects the vision for the station area. Rezone the properties that are in the vicinity to a mix of zoning districts that promote high density residential development, while allowing limited commercial types of uses in appropriate places. The properties fronting 400 South should be the focus of the station area and development should focus on creating an urban neighborhood in scale and purpose, and is not intended to supplant or compete with the much higher density central business district. Regional scale development beyond the existing Trolley Square commercial development is not encouraged.
3. Coordinate with the Utah Department of Transportation (UDOT) to improve the functionality of 400 South for pedestrians. This could be accomplished by adding additional midblock crosswalks to the transit stations, allowing on street parking during off peak travel hours, studying the impact of reducing the road width, whether in key spots or for longer stretches, and other changes to the street that are aimed at creating a safe and functional transportation corridor for all users.
4. Work with the Transportation Division to find appropriate bicycle routes that run on or parallel 400 South and that connect to appropriately located north/south bike routes.
5. Require midblock connections from 300 South and 500 South that connect to 400 South.
6. Incorporate way-finding features at the station area so people can easily locate station area assets such as Trolley Square, Gilgal Gardens and Liberty Park.
7. Maintain the original 15-foot landscaped setback along the 400 South street frontage.
8. Review the appropriateness of and consider amendments to the boundaries of the Central City Historic District to remove those blocks and portions of blocks that front

on 400 South from the historic district due to the lack of remaining historic character. See the “.....” map to determine the areas that should be considered for removal from the Central City Historic District.

9. Preserve the 600 East medians and prohibit further bisections of the medians for the purpose of allowing vehicular access and left turns to private property or streets.
10. Extend 450 South midblock walkway through to 650 East.
11. Further multi-modal solutions to change the way 600 South is utilized between 500 East and 700 East. Possible solutions include adding middle of the street parking, midblock crosswalks, medians, or removing a lane of traffic in each direction to add bike lanes, larger park strips or angled parking.
12. Identify zoning solutions for the block faces across from Trolley Square on 600 East and 600 South. The focus should be to encourage development on vacant parcels, increase residential density and promote the preservation and adaptive reuse of contributing structures. The surface parking lot south of Trolley Square should be rezoned to allow Trolley Square to building a parking structure, retain the historic structures fronting on 600 South and build housing.
13. Adopt zoning that increases housing potential, but does not introduce extensive commercial development on the south side of 500 South between 500 and 600 East and both sides of the street between 400 and 500 East.
14. Encourage redevelopment of strip centers into higher density with structured parking with a potential park and ride. The block bounded by 400 and 500 South and 600 and 700 East should have the highest commercial densities of the area.
15. Encourage development that is compatible with the historic development pattern in the Central City Historic District where appropriate.
16. Reevaluate the densities on the northwest quadrant of the block bounded by 300 South and 400 South and 600 East and 700 East, to allow higher densities and design that is compatible with the adjacent historic character.
17. Mid-block crossings across 400 South should be considered adjacent to the Trax stop to help pedestrians to cross safely and to discourage jaywalking.

900 East Station Area

The 900 East Station is an Urban Neighborhood Station Area due to the established and predominating residential character and the potential for infill development along 400 South which adds variations of density and intensity of building forms that blend in and complement the existing residential character of the area. The development strategy of an urban neighborhood station may include the mixing of building types and uses, including the allocation of commercial or office uses to the ground floor, and residential

uses above these floors. The highest residential densities and most intense land uses are generally located closest to the station platform along 400 South between 700 East and 900 East, particularly on the south side of 400 South.

The station area comprises of core and transition areas. The purpose of creating the different areas is to recognize the scale and nature of existing development patterns and identify the appropriate locations for growth. The general concept is that bigger buildings with the most dwelling units and a higher intensity level of commercial space should be located closest to the station in the core. The transition area reduces the scale, mass and intensity of new development as it moves away from the core area. The north side of 400 South is part of the transition area due to the close proximity of the relatively low scale nature of the residential area to the north and the desire to maintain that character and the impacts that taller buildings on 400 South would have on privacy and solar access.

Contributing landmarks and existing neighborhood characteristics will influence future development in and around the 900 East Station. These existing traits include institutional uses such as the Intermountain Health Care facility, Bennion Elementary School, and religious structures. There are also various types of commercial and a strong mixture of single and multi-family residential building types.

900 East Station Area Goals:

1. Protect historic landmark sites that currently exist in the Station Area.
2. Coordinate with UDOT to improve the functionality of 400 South for pedestrians. This could be accomplished by adding additional midblock crosswalks to the transit stations, allowing on street parking during off peak travel hours, studying the impact of reducing the road width, whether in key spots or for longer stretches, and other changes to the street that are aimed at creating a safe and functional transportation corridor for all users.
3. Maintain original 15-foot landscaped setback along the 400 South street frontage.
4. Incorporate way-finding features at the station area so people can easily locate station area assets such as Gilgal Gardens and health care facilities.
5. Protect the 800 East medians by not allowing vehicular access cuts through the existing landscaped area.
6. Encourage infill uses that will eliminate surface parking lots with an emphasis on those that front 400 South or areas that can easily access the station platform.
7. Encourage shared parking facilities between uses where applicable.

8. Focus on mixed-use development with commercial and residential uses along 400 South and the east side of 700 East. In remaining areas the primary redevelopment focus should be on residential uses.
9. If the Bennion Elementary School discontinues to be utilized as a school, the property should be redeveloped with an emphasis on residential uses along the frontages but much of the open space behind should remain, and be enhanced and available to the public.
10. Create mid-block connections that draw pedestrians to 400 South and specifically to the 900 East TRAX station. This should be emphasized between 300 South and 400 South where connections could more easily be created by linking existing rights-of-way such as, Laker Court and Strongs Court.
11. Encourage nonconforming manufacturing or warehousing uses to relocate to appropriate locations in the city.
12. Adopt zoning that promotes high density housing with appropriate design standards adjacent to Gilgal Gardens in order to activate and provide security for the park.
13. Mid-block crossings across 400 South should be considered adjacent to the Trax stop to help pedestrians to cross safely and to discourage jaywalking.

SECTION 2. Amending the Zoning Map. The Salt Lake City zoning map, as adopted by the Salt Lake City Code, relating to the fixing of boundaries and zoning districts, shall be and hereby is amended to reflect the changes to that map affecting the 400 South transit corridor (between 1000 East and 200 East Streets) as shown on Exhibit “A” attached hereto.

SECTION 3. Amending text of Salt Lake City Code section 21A.26.078 That section 21A.26.078 of the *Salt Lake City Code* (Zoning: Commercial Districts: TSA Transit Station Area District), shall be, and hereby is, amended to read as follows:

21A.26.078: TSA TRANSIT STATION AREA DISTRICT:

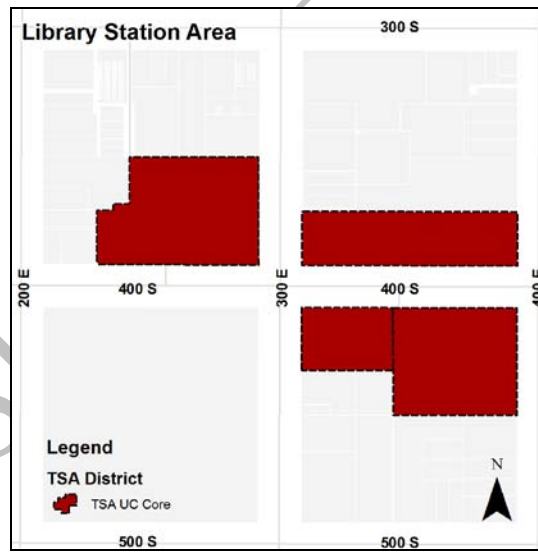
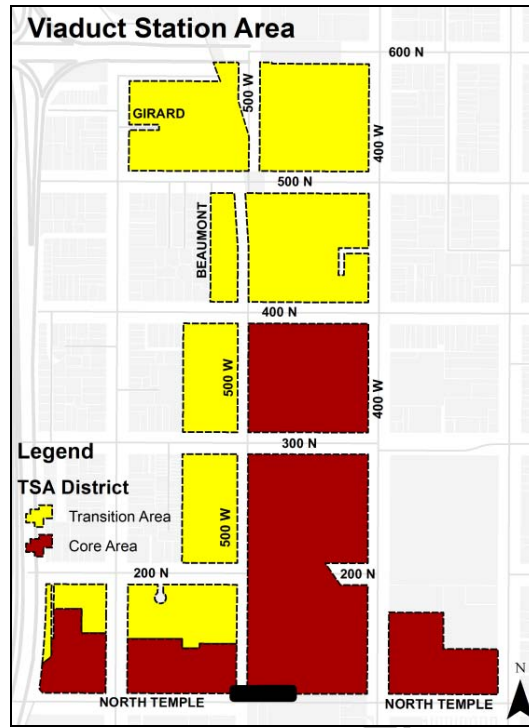
- A. Purpose Statement: The purpose of the TSA transit station area district is to provide an environment for efficient and attractive transit and pedestrian oriented commercial, residential and mixed use development around transit stations. Redevelopment, infill development and increased development on underutilized parcels should include uses that allow them to function as part of a walkable, mixed use district. Existing uses that are complementary to the district, and economically and physically viable, should

be integrated into the form and function of a compact, mixed use pedestrian oriented neighborhood. Each transit station is categorized into a station type. These typologies are used to establish appropriate zoning regulations for similar station areas. Each station area will typically have two (2) subsections: the core area and the transition area. Due to the nature of the area around specific stations, the restrictions of overlay zoning districts, and the neighborhood vision, not all station areas are required to have a core area and a transition area.

1. Core Area: The purpose of the core area is to provide areas for comparatively intense land development with a mix of land uses incorporating the principles of sustainable, transit oriented development and to enhance the area closest to a transit station as a lively, people oriented place. The core area is generally within a one-fourth ($\frac{1}{4}$) mile walk of a transit station platform. The core area may mix ground floor retail, office, commercial and residential space in order to activate the public realm. Buildings in this area should have minimal setbacks to encourage active outdoor use adjacent to the sidewalk, such as outdoor dining and patios that reflect the desired character of the area. Building facades should be varied and articulated, include storefronts adjacent to the street, windows on the street level, and have clearly defined entrances to provide visual interest to pedestrians. Buildings should be a minimum of two (2) or three (3) stories in height, depending on location, in order to define the street edge. Arcades, bays, and balconies are encouraged. The configuration of buildings must balance the needs of all modes of circulation with the safety and comfort of pedestrians and bicyclists. A vertical mix of uses, with office and residential above ground floor commercial uses is encouraged. A minimum of thirty (30) dwelling units per acre is encouraged within the core.
 2. Transition Area: The purpose of the transition area is to provide areas for a moderate level of land development intensity that incorporates the principles of sustainable transit oriented development. The transition area is intended to provide an important support base to the core area and transit ridership as well as buffer surrounding neighborhoods from the intensity of the core area. These areas reinforce the viability of the core area and provide opportunities for a range of housing types at different densities. Transition areas are generally located within a one-half ($\frac{1}{2}$) mile from the station platform, but may vary based on the character of the area. Transition areas typically serve the surrounding neighborhood; include a broad range of building forms that house a mix of compatible land uses. The minimum desired density is ten (10) dwelling units per acre. Commercial uses may include office, retail, restaurant and other commercial land uses that are necessary to create mixed use neighborhoods. Commercial uses can be clustered around intersections and along block faces to create neighborhood nodes.
- B. Station Area Types: A station area typology is the use of characteristics, such as building types, mix of land use, transit service and street network to create generalizations about an area that can be used to define a common vision for

development of a transit station area. Each typology recognizes the important difference among places and destinations and takes into account the local context of a station and its surroundings. Each station area typically will include a core area, where the most intense development will occur, and a transition area, which is intended to create a buffer area between the core and those areas with generally lower intensities and densities. Prior to classifying a transit station into a specific type, a ~~station area~~ specific area plan must be adopted by the city council prior to applying this zoning district to a geographic area. Only those stations that have an adopted ~~station area~~ plan that is supported by the regulations in this section will be classified. Refer to the official Salt Lake City Zoning Map to determine the zoning of the land within each station area.

1. Urban Center Station (TSA-UC): An urban center station contains the highest relative intensity level and mix of uses. The type of station area is meant to support Downtown Salt Lake and not compete with it in terms of building scale and use. The intensity level of the area is characterized by a twenty four (24) hour population, active streetscapes, defined street walls and the presence of multiple types of public transit or as a node where several rail transit lines converge. Development generally occurs on vacant parcels or through redevelopment of underutilized parcels. The station area has a compact, dense, interconnected and walkable development pattern. Large scale development occurs closer to the station platforms; and is scaled back as it gets closer to less intense areas. Building forms vary, but are typically oriented to the pedestrian, are multiple stories in height, and contain a horizontal and vertical mix of land uses. Buildings up to ten (10) stories in height are allowed in the core, while buildings in the transition zone are approximately half that size. The station area contains a number of regional attractions, such as destination retail, employment, dining and entertainment and a high level of pedestrian activity. A variety of dense housing options exist. Development includes civic amenities, such as public gathering places. Uses that help implement the vision for the station and that area commonly found in an intense urban area are appropriate. The following stations are considered an urban center type of station: North Temple Viaduct Transfer Station and the Library Station.

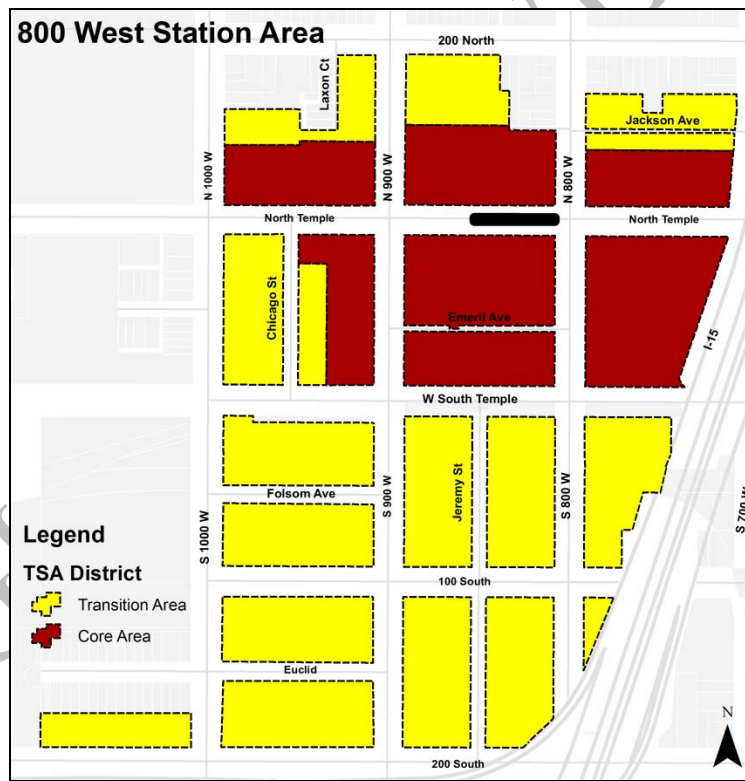


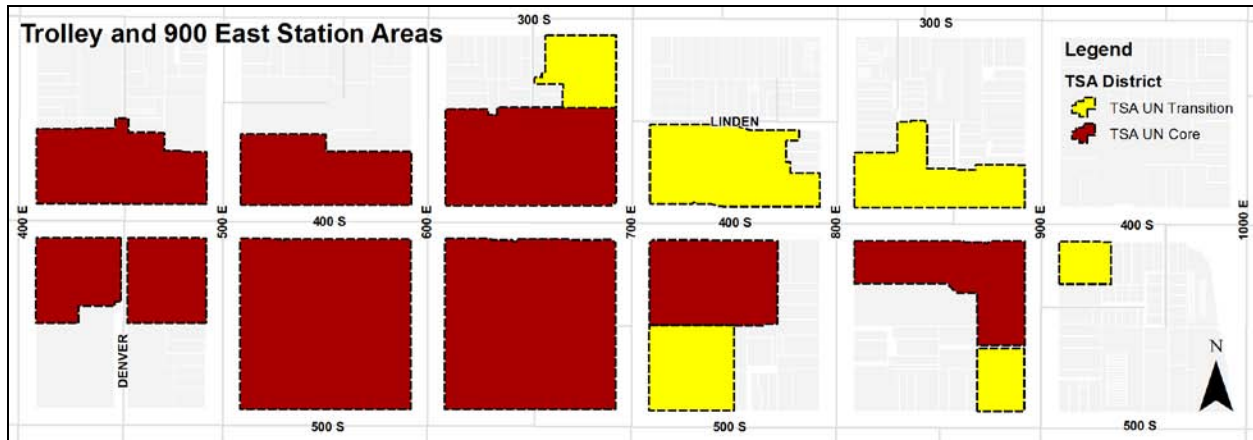
2. **Urban Neighborhood Station (TSA-UN):** An evolving and flexible development pattern defines an urban neighborhood station area. Development generally happens as infill on vacant parcels or redevelopment of underutilized parcels. These stations evolve in established residential areas where initial changes may add density and intensity in compact building forms that blend in with the residential character of the area. Urban neighborhoods consist of multilevel buildings that are generally lower scale than what is found in the urban center station area. The desired mix of uses would include ground floor commercial or office uses with the intent of creating a lively, active, and safe streetscape. A mix of building types are possible, ranging from single use structures to mixed use

buildings. Residential uses are generally located above the first floor, although they can be located on the ground floor in certain situations. The highest residential densities and most intense land uses are generally located closest to the station platform. Urban neighborhoods are served by at least two (2) forms of transit, including light rail and bus service. The uses serve the surrounding neighborhood with nearby destinations and have the potential to attract people from other neighborhoods.

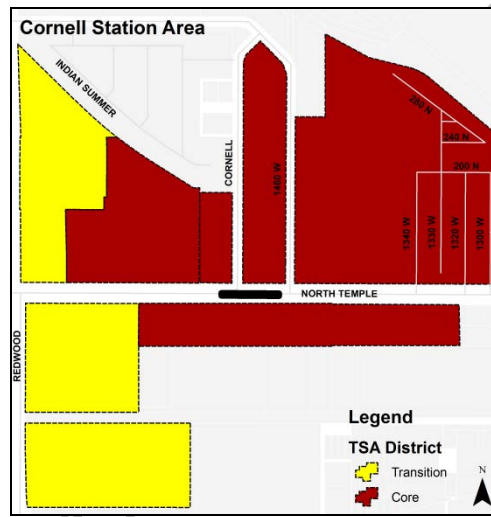
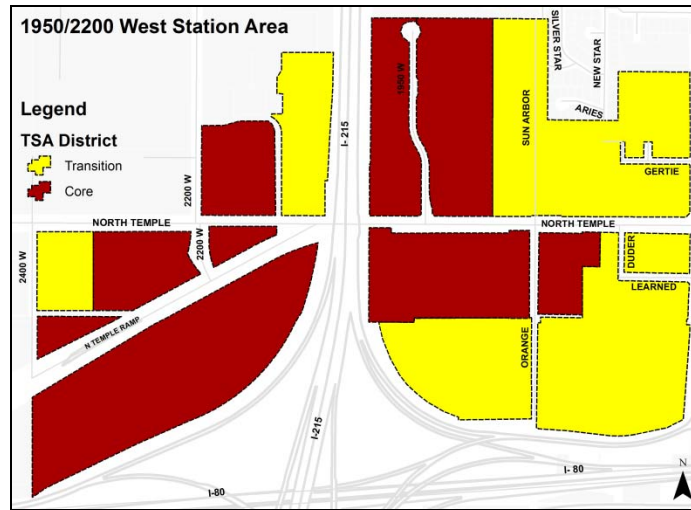
In some urban neighborhood station areas, a linear development pattern along commercial streets that intersect the transit corridor defines a neighborhood main street. Neighborhood main streets are approximately two (2) blocks long, with two (2) 4-story buildings located close to the sidewalk. The ground floors of buildings are typically occupied by active uses, such as retail or restaurants.

The following stations are considered to be urban neighborhood stations: 800 West, Trolley (600 East) and 900 East light rail stations.

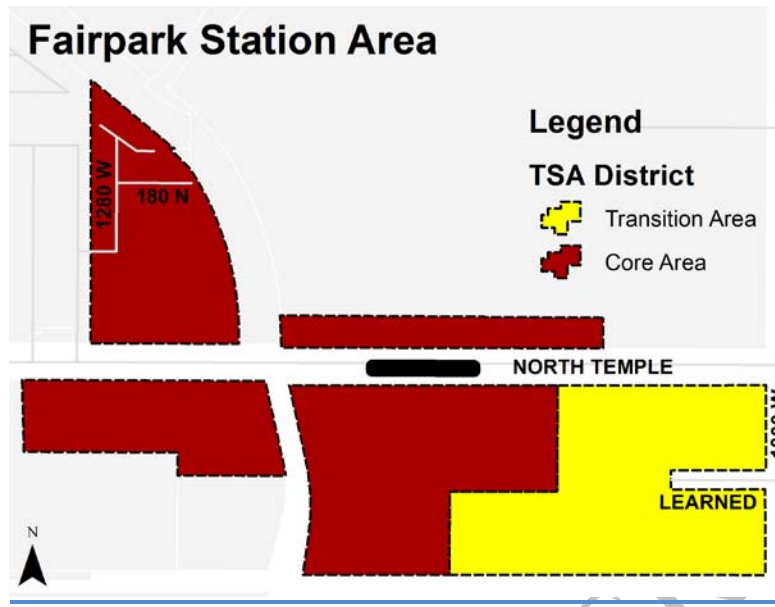




3. **Mixed Use Employment Center Station (TSA-MUEC):** A mixed use employment station is an area with a high concentration of jobs that attract people from the entire region. These areas generally start with a campus style development pattern and are dominated by a single type of use that generally employs a high number of people. Buildings are often large scale in nature and may have large footprints. New development occurs on vacant parcels. Redevelopment occurs on surface parking lots, underutilized land, or as additions to existing buildings as businesses expand. The primary mode of circulation is by automobile, but the area is served by at least two (2) types of mass transit which provides alternative modes of transportation for employees. Land uses that support the employment centers such as retail sales and service and restaurants are located throughout the station area and should occupy ground floor space in multi-story buildings oriented to the pedestrian and transit user. A mix of housing types and sizes are appropriate to provide employees with the choice to live close to where they work. Building types should trend toward more flexible building types over time. The area is likely to have large blocks and lacks a consistent street network. Connectivity for all modes of travel is important due to the limited street network. The following stations are considered to be mixed use employment center stations: 1950 West, 2200 West, and the Cornell light rail stations.



4. **Special Purpose Station (TSA-SP):** The special purpose station is typically centered on a specific land use or large scale regional activity. These areas are generally served by a mix of transit options, usually light rail or bus. New development is limited due to the nature of the primary function of the area, but redevelopment of underutilized parcels is likely to occur. Land uses such as restaurants and retail support the dominant land use and attract people to the area. A mix of housing types and sizes are appropriate in certain situations. Future development should be aimed at increasing the overall intensity and frequency of use in the station area by adding a mix of uses that can be arranged and designed to be compatible with the primary use. The following stations are considered to be special purpose stations: Fairpark light rail station.



C. Review Process: The intent of the review process is to make the process for desirable development easy to realize in a relatively quick time frame. The review process focuses on building forms and their relationship to adjacent buildings, the public street, transit and other public spaces. The review process for all new development and redevelopment within the transit station area zoning district is based on the development score which is generated by the “Transit Station Area Development Guidelines” hereby adopted by reference. The construction of new buildings require a development score. The following types of development are required to go through this review process:

- Any addition of 1,000 square feet or more that extend a street facing building façade or are located to the side of a building and are visible from a public space or
- Additions that increase the height of an existing building or change the existing roof line;
- Additions to the rear of buildings that are not adjacent to a public street, trail or other public space are not required to obtain a development score but must comply with all other applicable regulations. Signs, fences, accessory structures and any other structure or addition not listed in this section are not required to obtain a development score.

1. Presubmittal Conference: All applicants for development within the transit station area zoning district are required to attend a presubmittal conference with the planning division. The purpose of the presubmittal conference is to notify the applicant of the goals of the station area plans, the standards in this section, and the review and approval process.

2. Development Review Application: After a presubmittal conference, the developer can submit a development review application. This application and all submittal requirements will be used to determine the development score. The application shall include a score sheet on which the development guidelines and their assigned values are indicated and two (2) checklists: one for the applicant's use and one for the planning division's use.
3. Application Review: Table 21A.26.078C of this section summarizes the application review process. All applications shall be processed as follows:
 - a. Tier 1 Planning Commission Review: If a project is assigned a score less than 50 points, the project can only be approved by the planning commission through the conditional building and site design review process in chapter 21A.59 of this title. Once the applicant receives written notice of their score, they will be given thirty (30) days to notify the planning division of their intention to proceed with the project through the conditional building and site design review process or make necessary plan adjustments to increase their development score to the minimum level in order to go through an administrative hearing process.
 - b. Tier 2 Administrative Hearing: A project that has a development score between 50 and 99 points is eligible for an administrative hearing. Any project being reviewed at an administrative hearing shall be reviewed using the standards found in section 21A.59.060, "Standards For Design Review", of this title.
 - (1) Notice and Posting Requirements: Notice of the administrative hearing shall be done in accordance with subsection 21A.10.020C, "Conditional Building and Site Design Review", of this title.
 - (2) Administrative Hearing: After consideration of the information received from the applicant and any other interested party, the planning director, or designee may approve, approve with conditions, deny or refer the matter to the planning commission.
 - (3) Appeals of Administrative Hearing Decision: Any person aggrieved by the decision made by the planning director or designee at an administrative hearing may appeal the decision to the Salt Lake City planning commission by filing notice of appeal within ten (10) days after the record of decision is published. The notice shall state the reason(s) for the appeal. Reason(s) for the appeal shall be based upon procedural error, the development score of the project or the criteria set forth in section 21A.59.060, "Standards For Design Review", of this title.
 - c. Tier 3 Administrative Review: The planning director has the authority to approve a project scoring 100 points or more without holding a public

hearing. The project shall be allowed to go through the standard building permit process. A public hearing is not required because the project incorporates adequate design guidelines or development incentives to be deemed compliant with the vision for the station area.

TABLE 21A.26.078C APPLICATION REVIEW

Development Score	Review Process
0 – 49 points	Planning commission conditional building and site design review process
50 – 99 points	Administrative hearing process
100 or more points	Administrative review

D. **Development Score:** The purpose of the development score is to allow flexibility for designers while implementing the city’s vision of the applicable station area plans and the purpose of this zoning district. The development score measures the level of compatibility between a proposed project and the station area plan. A “station area plan” is a development, land use, urban design and placemaking policy document for the area around a specific transit station. The development score is based on the design guidelines and development incentives in the “Transit Station Area Development Guidelines” book, hereby adopted by reference. The “Transit Station Area Development Guidelines” shall be amended following the adopted procedures for zoning text amendments in chapter 21A.50, “Amendments”, of this title. Each design guideline is assigned a value.

1. **Formulating the Score:** The development score is formulated by calculating all of the development guideline values for a particular project. Each design guideline and incentive is given a value based on its importance. Some guidelines are considered more important and carry a higher value than others. The planning director shall evaluate each project in the transit station area zone and assign a development score. The development standards in subsection G of this section and the design standards in subsection J of this section shall be complied with by all projects and are not calculated in the development score.
2. **Using the Score:** Every development is required to meet a minimum development score. The minimum score represents a percentage of the total points possible.
3. **Project Review:** A development score shall be assigned to all projects within the transit station area zoning district after a complete development review application is submitted. The planning director shall provide, in writing, a copy of the review checklist and explanation of the outcome of the score to the applicant within thirty (30) days of submitting a complete application.

that sells gas								
Cemeteries, any type	X	X	X	X	X	X	X	X
<u>Check cashing/payday loan business</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>
Chemical manufacturing and storage	X	X	X	X	X	X	X	X
Commercial parking lots not located in a parking structure	X	X	X	X	X		X	
Community correction facilities, any size, large	X	X	X	X	X	X	X	X
<u>Community correction facility, small</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>		<u>X</u>
Concrete manufacturing	X	X	X	X	X	X	X	X
Contractor's yard/office	X	X	X	X	X	X	X	X
Detached single-family dwelling	X		X		X		X	
<u>Drop forge industry</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>
Drive-through windows and service	X	X	X	X	X	X	X	X
<u>Dwelling, single family (detached)</u>	<u>X</u>		<u>X</u>		<u>X</u>		<u>X</u>	

<u>service (indoor)</u>								
<u>Vehicle, automobile salvage and recycling (indoor)</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>
<u>Vehicle, automobile salvage and recycling (outdoor)</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>
<u>Vehicle, boat/recreational vehicle sales and service</u> <u>Vehicle, recreation</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>
<u>Vehicle, truck repair (large)</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>
<u>Vehicle, truck sales and rental</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>
<u>Warehouse as a primary use</u>	X	X	X	X	X		X	
<u>Welding shops</u>	X	X	X	X	X	X	X	X
<u>Wholesale distributors distribution</u>	X	X	X	X	X		X	X
<u>Woodworking mill</u>	X	X	X		X		X	
<u>Zoological park</u>	X	X	X	X	X	X		

1. Existing Uses ~~A~~ and Buildings: A use located within a station area legally existing at the time that this zoning district was adopted, but listed as a prohibited use in this subsection F, shall be considered a legal nonconforming use. A structure legally existing at the time this section was adopted, but not conforming to the standards in this chapter, shall be considered a legal noncomplying structure. Any legal

nonconforming use or legal noncomplying structure is subject to chapter 21A.38 of this title.

G. Development Standards:

1. Intent: The purpose of the following development standards is to promote an intense and efficient use of land at increased densities in the station areas. The development standards are intended to create a safe and pleasant environment near transit stations by encouraging an intensive area of mixed use development and activities, pedestrian amenities and by limiting conflicts between vehicles and pedestrians. Development standards are intended to create a reasonably continuous building edge that defines the exterior spatial enclosure of the street or open space and protect adjacent low density residential zoning districts. With some exceptions, buildings line a street at or near the public right of way to the greatest extent possible.
2. Application: The dimensional requirements in ~~table 21A.26.078GD~~ of this section apply to all new buildings and developments as well as additions to existing buildings. The following development standards apply to the core and transition areas of all station types:
 - a. Building Height: The minimum and maximum building heights are found in table 21A.26.070.G.a Building Height Regulations. Height limits are intended to control the overall scale of buildings, the compatibility with adjacent development, and the composition of the urban form of the block. Minimum building heights in the core area relate to the width of the street, with a minimum ratio of one foot (1') of building height for every three feet (3') of street width. Building height is measured from the finished grade to the highest point of the building. The following exceptions apply:
 - (1) The minimum building height applies to all structures that are adjacent to a public or private street. The building shall meet the minimum building height for at least 10% of the width of the street facing building wall.
 - (2) Elevator shafts, parapet walls, and other projections are permitted subject to subsection 21A.36.020C, "Height Exceptions", of this title.
 - (3) Projects that achieve a development score that qualifies for administrative review are eligible for an increase in height. The increase shall be limited to one story of ~~in~~habitable space. The height of the additional story shall be equal to or less than the average height of the other stories in the building.

TABLE 21A.28.078.G.2.a BUILDING HEIGHT REGULATIONS

	<u>Minimum Height¹</u>	<u>Maximum Height</u>
<u>Urban center</u>		
<u>Core</u>	<u>30'</u>	<u>90'²</u>
<u>Transition</u>	<u>25'</u>	<u>60'</u>
<u>Urban neighborhood</u>		
<u>Core</u>	<u>25'</u>	<u>75'</u>
<u>Transition</u>	<u>0'</u>	<u>50'</u>
<u>Mixed use employment center</u>		
<u>Core</u>	<u>25'</u>	<u>75'</u>
<u>Transition</u>	<u>0'</u>	<u>60'</u>
<u>Special Purpose</u>		
<u>Core</u>	<u>25'</u>	<u>75'</u>
<u>Transition</u>	<u>0'</u>	<u>60'</u>

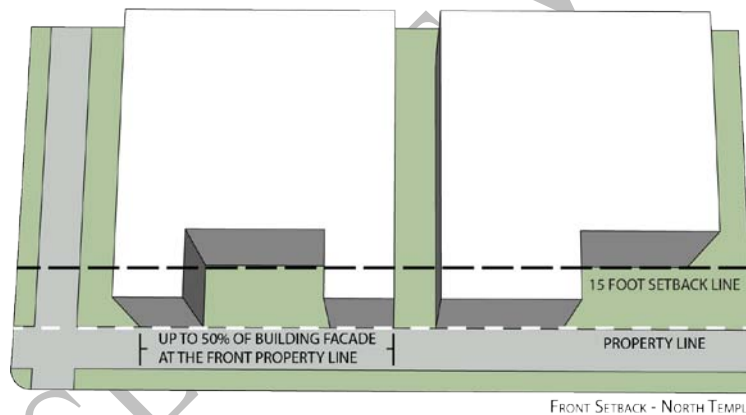
1. Minimum building heights apply to those properties with frontage on the street where fixed rail transit is located.
2. Buildings with a roof that has at least 2 sloping planes may be allowed up to 105 feet. The additional height may include habitable space.

b. Setbacks: Required building setbacks promote streetscapes that are consistent with the desired character of the street and various station typologies and its core and transition areas. Building setbacks create a safe environment that is inviting to pedestrians and transit users and maintain light, air and potential privacy for adjacent residential uses. In some instances, the setbacks limit the building envelope where the existing development pattern would be negatively impacted by taller buildings.

- (1) Front and Corner Yard Setback: Except as indicated below, there is no minimum setback. If a setback is provided, at least 50% of the street facing building façade shall be located within 5 feet of the front property line unless a larger setback is required below. All portions of a front yard not occupied by building, driveways, walkways or other similar feature must be completely

landscaped or include an active outdoor use, such as outdoor dining, plazas or other similar outdoor use with the space not dedicated to active outdoor use completely landscaped. Parking, drive aisles or other paved areas for motor vehicles are prohibited. Walls up to three feet (3') in height, patios and other similar elements intended to activate the sidewalk can be located to the property line.

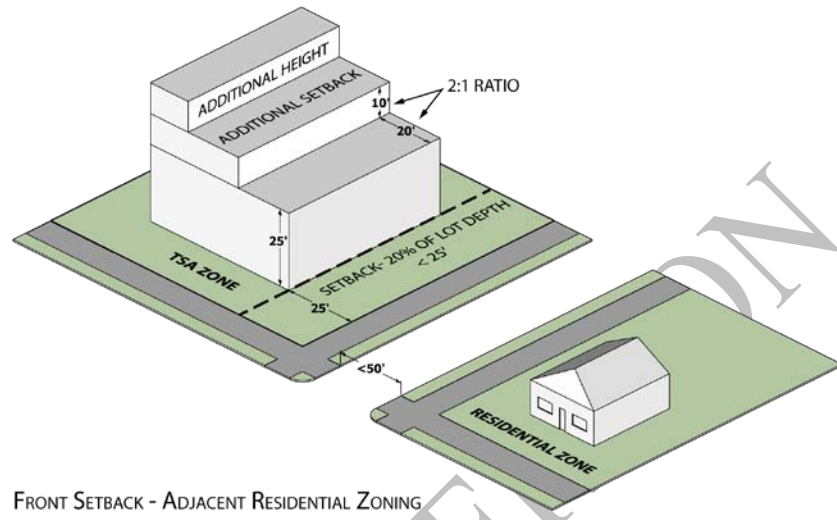
(2A)North Temple Boulevard: The front yard setback along North Temple Boulevard shall be fifteen feet (15') for a minimum of fifty percent (50%) of the width of the street facing facade. Up to fifty percent (50%) of a street facing facade may encroach up to the front property line. In this case, the area not occupied by the building footprint must be landscaped or include active outdoor use, such as outdoor dining, plazas, courtyards or other similar usable public space or use. Setbacks over fifteen feet (15') are not allowed. In locations where there is not a minimum sidewalk width of 10 (ten) feet, additional sidewalk width shall be installed by the developer so there is a minimum width of 10 feet when a new building is constructed or with additions that increase the gross building square footage by more than 50%.



(3B)400 South/University Boulevard: The front yard setback along 400 South/University Boulevard shall be a minimum of fifteen feet (15'). In locations where there is not a minimum sidewalk width of ten (10) feet, additional sidewalk width shall be installed by the developer so there is a minimum width of ten feet (10") when a new building is constructed or with additions that increase the gross building square footage by more than 50%.

(C) Streets with a Right of Way of 50 feet or less: When located on a street with a right of way 50 feet or less with an R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district on either side of the street, a minimum setback of 25% of the lot depth but no more than twenty five feet (25') is required. For buildings taller than twenty five feet (25'), the setback shall increase two feet (2') for every foot of height above twenty five feet (25')

in height. Buildings may be stepped so taller portions of the building are farther away from the front property line.



(23) Side Yard Setback:

(A) Drive aisles are allowed in the side yard setback. In the transition subarea, parking is allowed in the side yard subject to subsection L of this section.

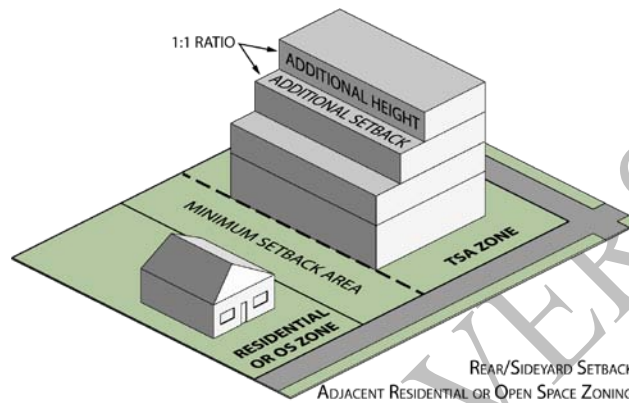
(B) Side yard setback when adjacent to a single family or two family certain zoning districts:

(i) Core Area: A minimum of twenty five feet (25') in the core area. When adjacent to an OS, R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district, the minimum side yard setback shall be increased one foot for every one foot increase in height above twenty five feet (25'). When a property in an R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district is separated from a TSA zoned property by an alley, the additional setback for height above twenty five feet (25') applies and shall be measured from the property line of the TSA zoned property. Buildings may be stepped so taller portions of a building are farther away from the side property line. The horizontal measurement of the step shall be equal to the vertical measurement of the taller portion of the building.

(ii) Transition Area: A minimum of fifteen feet (15') in the transition area. When adjacent to an OS, R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district, the minimum side yard setback shall be increased one foot for every one foot increase in height above fifteen feet (15'). When a property in an OS, R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district is separated from a TSA zoned property by an alley, the additional setback for height above fifteen

five feet (15') applies and shall be measured from the property line of the TSA zoned property. Buildings may be stepped so taller portions of a building are farther away from the rear property line. The horizontal measurement of the step shall be equal to the vertical measurement of the taller portion of the building.

(iii) Side yard setback when adjacent to other uses or districts: No minimum side yard required.



(34)Rear Yard Setback:

(A) Core area: A minimum of fifteen feet (25'). When adjacent to an OS, R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district, the minimum rear yard setback shall be increased one foot for every one foot increase in height above twenty five feet (25'). When a property in an OS, R-1, R-2, SR, RMF-30 RMF-35 or RMF-45 zoning district is separated from a TSA zoned property by an alley, the additional setback for height above twenty five feet (25') applies and shall be measured from the property line of the TSA zoned property. If a structure is located adjacent to a single family or two family residential zoning district, the rear setback shall be equal to the height of the building on the subject property in the transit station area district. Buildings may be stepped so taller portions of a building are farther away from the rear property line. The horizontal measurement of the step shall be equal to the vertical measurement of the taller portion of the building. When a public alley is between the two (2) properties, the width of the alley may be counted towards the setback.

(B) Transition area: A minimum of twenty five feet (25'). When adjacent to an OS, R-1, R-2, SR, RMF-30 or RMF-35 zoning district, the minimum rear yard setback shall be increased one foot for every one foot increase in height above twenty five feet (25'). When a property in an OS, R-1, R-2, SR, RMF-30 or RMF-35 zoning district is separated from a TSA zoned property by an alley, the additional setback for height above twenty five feet (25') applies and shall be measured from the property line of the TSA zoned property. Buildings may be stepped so taller portions of a building

-	Core	25'	75'	None -	10	None -	None -	70%	5,000
-	Transition	0'	50'	None -	10	None -	None -	50%	1,000
	Mixed use employment station:	-	-	-	-	-	-	-	-
-	Core	25'	75'	None -	15	None -	None -	60%	5,000
-	Transition	0'	60'	None -	15	None -	None -	50%	1,500
	Special purpose:	-	-	-	-	-	-	-	-
-	Core	25'	75'	None -	25	None -	None -	60%	10,000
-	Transition	0'	60'	None -	25	None -	None -	50%	1,500

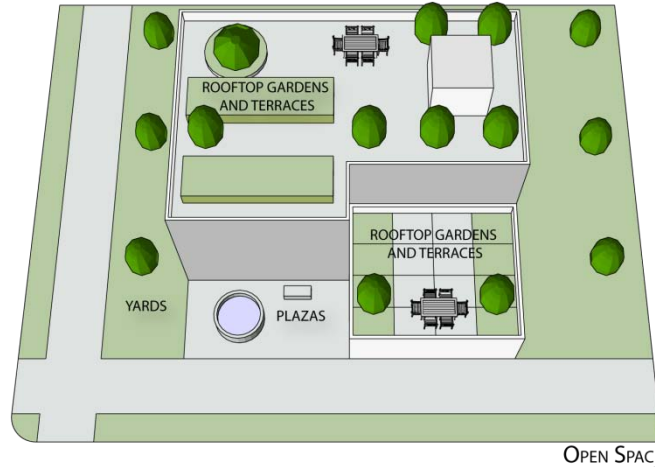
Notes:

1. Reference subsection G2 of this section for specific standards.
 2. Minimum building heights apply to those properties with frontage on the street where fixed rail transit is located.
 3. Buildings with a roof that has at least 2 sloping planes may be allowed up to 105 feet. The additional height may include inhabitable space.
- d. Minimum Lot Coverage: The intent of the minimum lot coverage is to ensure compact development.
- de. Open Space: In order to provide space for passive and active recreation, public and private use, offset storm drainage due to nonpermeable surfaces and as an amenity to individual developments and their residents, employees and customers, open space is required for all new developments.

(1) Core Area:

(A) Within the core area, open space may include landscaped yards, patios, public plazas, pocket parks, courtyards, rooftop and terrace gardens and other similar types of open space amenity.

(B) A minimum of ten percent (10%) of the land area up to five thousand (5,000) square feet.



(2) Transition Area:

- (A) Within the transition area, open space may include landscaped yards, patios, public plazas, pocket parks, courtyards, rooftop gardens and terraces, community gardens and other similar types of amenities.
- (B) The minimum open space requirement is ten percent (10%) of the land area up to two thousand five hundred (2,500) square feet.

(3) Access To Open Space: All required open space shall be accessible to the users of the building(s).

ef. Circulation And Connectivity: Development within the station area shall be easily accessible from public spaces and provide safe and efficient options for all modes of travel. Circulation networks, whether public or private, require adequate street, pedestrian and bicycle connections to provide access to development. The internal circulation network shall be easily recognizable, formalized and interconnected.

(1) All parking lots shall comply with the standards in section 21A.44.020, "General Off Street Parking Requirements", of this title.

(2) Parking is prohibited between the street facing building line and any front or corner side property line. This shall include any drive aisle that is not perpendicular to the front or corner side property line.

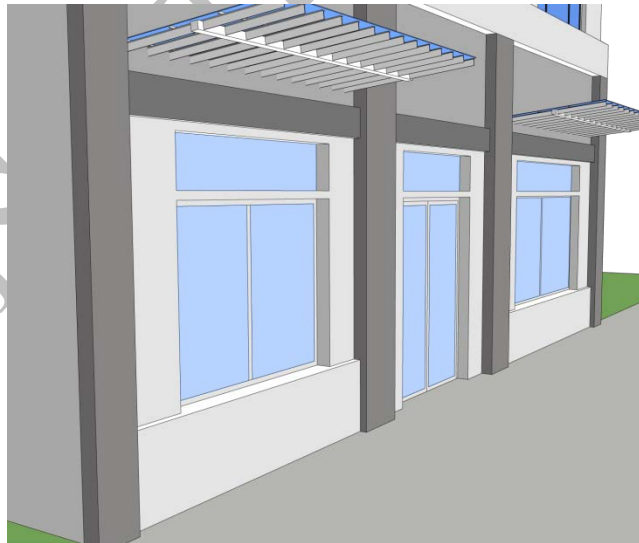
H. Residential Densities:

- 1. Core area: No maximum.
- 2. Transition area: No maximum.

I. Accessory Structures: No accessory structure shall be located in a required front yard or between the primary building and a property line adjacent to a public street.

J. Design Standards:

1. **Purpose:** Design standards create the fundamental characteristics of a transit oriented district and the basic design elements required for a successful transit station area. Design standards are intended to provide a safe and interesting walkable environment by connecting ground floor uses adjacent to the sidewalk areas, by encouraging the continuity of retail and service uses, providing surveillance opportunities on the street and public open spaces and framing the street by bringing portions of buildings up to the sidewalk. All buildings shall be designed for the context and character of the project and how they interact visually, functionally, and socially with the context of the public environment.
2. **Application:** The following design standards apply to all projects within the core and transition areas of all station area types:
 - a. **Building Walls Adjacent To Aa Street:** Street-facing building facades shall provide architectural variety and scale. Changes in plane, color, texture, materials, scale of materials, patterns, art, or other architectural detailing are acceptable methods to create variety and scale. Building walls parallel to a public street and greater than thirty feet (30') in length shall be broken up by architectural features such as bay windows, recessed entrances or windows, balconies, cornices, columns, or other similar architectural features. The architectural feature may be either recessed or project a minimum of twelve inches (12").



ARCHITECTURAL VARIETY AND SCALE

- b. **Ground Floor Building Materials:** Other than ground windows and doors, eighty percent (80%) of the remaining ground floor wall area shall be clad in durable materials. Durable materials include brick, masonry, textured or patterned concrete and/or cut stone. Other materials may be used as accent or trim provided

they cover twenty percent (20%) or less of the ground floor adjacent to a street.

- c. **Ground Floor Glass And Transparency:** All street-facing elevations of a development shall be designed so that the first floor street-facing facade has at least sixty percent (60%) clear glass between three (3) and eight feet (8') above grade to allow pedestrians to view activities inside the building or lighted display windows. There must be visual clearance behind the glass for a minimum of two feet (2'). Three-dimensional display windows at least two feet (2') deep are permitted and may be counted toward the sixty percent (60%) glass requirement. Ground floor windows of commercial uses shall be kept clear at night, free from any window covering, with internal illumination. When ground floor glass conflicts with the internal function of the building, other means shall be used to activate the sidewalk, such as display windows, public art, architectural ornamentation or detailing or other similar treatment. The first floor elevation facing a street of all new buildings, or buildings in which the property owner is modifying the size of windows on the front facade, shall not have less than sixty percent (60%) glass surfaces. The reflectivity in glass shall be limited to eighteen percent (18%) as defined by ASTA standards. The planning director may approve a modification to this requirement if the planning director finds:

- (1) The requirement would negatively affect the historic character of the building, or
- (2) The requirement would negatively affect the structural stability of the building.
- (3) The ground level of the building is occupied by residential uses, in which case the sixty percent (60%) glass requirement may be reduced to forty percent (40%).

Any appeal of an administrative decision made pursuant to this subsection may be made to the planning commission.

- d. **Building Entrances:** The intent of regulating building entrances is to promote security on the street and public spaces by providing frequent points of access and sources of activity. Functional entrances to nonresidential uses should be located at an average of seventy five feet (75') or less from one another. At least one operable building entrance per elevation facing a public street shall be provided. Each ground floor leasable space is required to have an operable entrance facing the street and a walkway to the sidewalk. If a plaza or open space is provided as part of the development, a minimum of one entrance opening onto the plaza or open space shall be provided. This entrance shall be counted towards the spacing of functional entrances identified in this section and may count as the primary entrance to the building. All street facing building entrances shall be functional entrances and shall not be limited to emergency or employee entrances.

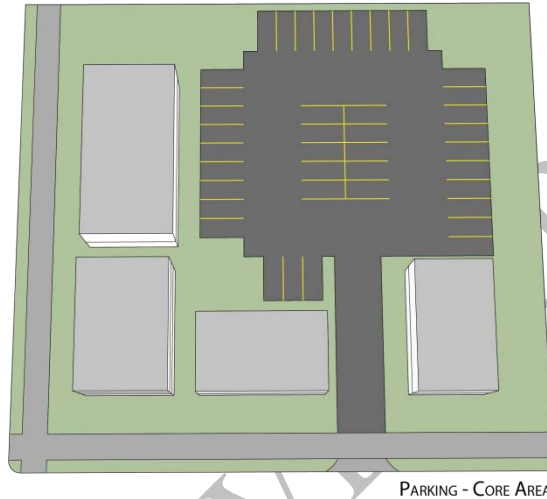
- e. **Ground Floor Residential Uses:** The interior floor elevation of ground floor residential units in the core area shall be a minimum of two feet (2') and a

maximum of five feet (5') above grade. Dwelling units located on the ground floor and facing a public or private street shall have a minimum of one primary entrance facing the street in the core area. The facades of all buildings in the core and transition areas with ground floor residential uses shall feature elements that signal habitation such as windows, entrances, stairs, porches, bay windows, and balconies that are visible from the public street. Attached single-family dwellings, townhomes, row houses, ~~entrances that open onto a courtyard facing a public street,~~ and other similar housing types shall have a primary entrance facing the street for each unit adjacent to a street. Units may have their primary entrance located on a courtyard, midblock walkway, or other similar area if the street facing facades have a primary entrance facing the street.

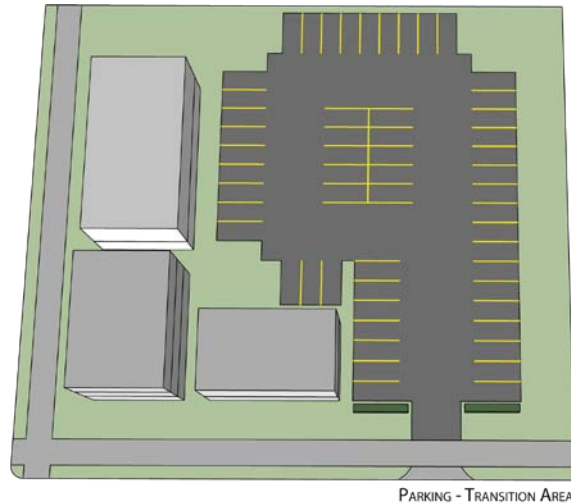
- f. **Parking Structures:** The intent of regulating parking structures is to minimize the visual impact of the structure and the cars parking within it, and to reduce their impact on the ground floors adjacent to public sidewalks and streets. Parking structures are permitted within the core and transition areas provided:
- (1) The ground floor of parking structures adjacent to a public street shall include an active use other than parking such as office, retail, residential leasing office, restaurant, etc. Parking is permitted behind the ground floor uses. If the ground floor does not include active use, then the structure must be set back behind a building or be a minimum of sixty feet (60') from a property line adjacent to a public street or sidewalk.
 - (2) The levels of parking above the first level facing the front or corner side lot line shall have horizontal floors and/or facades and not sloped.
 - (3) The levels of parking above the second level shall be designed to effectively screen the vehicles so they are not readily visible from an adjacent street.
 - (4) Below grade parking structures for structures with ground floor residential uses may extend a maximum of five feet (5') above the existing grade provided the above grade portion is screened with vegetation or architectural feature(s).
- g. **Mechanical Equipment:** All roof mounted mechanical and electrical equipment, communication antennas or dishes shall be enclosed, screened, organized, designed and located to be out of view from streets and public spaces. The parapet or enclosure shall be equal to or greater than the height of the equipment to be screened to reduce equipment noise and odors, and other impacts onto adjacent uses and maintain the integrity of overall architectural character and scale of the building. Mechanical equipment may be located on the ground provided it is behind the building, screened and not located in a required rear yard or side yard setback. Utility boxes are subject to section 21A.40.160, "Ground Mounted Utility Boxes", of this title.

- h. Service Areas: Service areas, loading docks, refuse containers and similar areas shall be fully screened from public view. All screening enclosures viewable from the street shall be either incorporated into the building architecture or shall incorporate building materials and detailing compatible with the building being served. All screening devices shall be a minimum of one foot (1') higher than the object being screened. Dumpsters must be located a minimum of twenty five feet (25') from any building on an adjacent lot that contains a residential dwelling or be located inside of an enclosed building ~~single or two family residential zoning district.~~
- K. Multiple Buildings ~~On A~~ Single Parcel: Multiple principal buildings on a single parcel are permitted provided each principal building meets the requirements of this chapter and each principal building obtained a separate development score. New principal buildings can be located towards the rear of a parcel provided there is an existing or additional new principal building that complies with the front yard building setbacks. If one principal building receives a development score lower than other principal buildings on the site, the project shall be processed based on the lowest development score obtained.
- L. Parking: The purpose of this subsection is to provide locations for off street parking. All off street surface parking lots should be located so that they are compatible with pedestrian oriented streets. New uses and development or redevelopment within this district shall comply with the requirements of this subsection.
1. Surface Parking Lots ~~A~~ and Structures ~~On~~ Corner Properties: On corner properties, surface parking lots and structures shall be located behind principal buildings or at least sixty feet (60') from a front and corner side lot lines. Only one driveway and drive aisle is permitted per street frontage and the access point shall be located a minimum of 100 feet from the intersection of the front and corner side property lines. If the front or corner side property line is less than 100 feet in length, than the drive approach shall be located within 20 feet of the side or rear property line.
 2. Surface Parking ~~In~~ ~~The~~ Core Area: Surface parking lots in the core area are required to be located behind the principal building or to the side of a principal building. When located to the side of a building, the parking lot shall be:
 - a. Set back a minimum of thirty feet (30') from a property line adjacent to a public street. The space between the parking lot and the property line adjacent to a public street shall be landscaped or activated with outdoor dining, plazas or similar feature.
 - b. Screened with a landscaped hedge or wall that is at least thirty six inches (36") above grade and no taller than forty two inches (42") above grade. Landscaping berms are not permitted.
 - c. The parking lot shall be no wider than what is required for ~~one~~ two rows of parking and one drive aisle as indicated in table 21A.44.020 of this title.

- d. Only one driveway and drive aisle is permitted per street frontage and the access point shall be located a minimum of 100 feet from the intersection of the front and corner side property lines. If the front or corner side property line is less than 100 feet in length, than the drive approach shall be located within 20 feet of the side or rear property line.
- e. Surface parking lots as a principal use: Surface parking lots as a principal use located on a lot that has frontage on a public street are prohibited.



- 3. Surface Parking in the Transition Area: Surface parking lots in the transition area are required to be located behind the principal building or to the side of a principal building. When located to the side of a principal building, the parking lot shall be:
 - a. Set back so that no portion of the parking area other than the driveway is closer to the street than the front wall setback of the building. In cases where the front wall of the building is located within five feet (5') of a property line adjacent to a street, the parking lot shall be set back a minimum of eight feet (8'). The space between the parking lot and the property line adjacent to a street shall be landscaped or activated with outdoor dining, plazas or similar feature.
 - b. Screened with a landscaped hedge or wall that is at least thirty six inches (36") above grade and no taller than forty two inches (42") above grade. Landscaping berms are not permitted.
 - c. Surface parking lots as a principal use: Surface parking lots as a principal use located on a lot that has frontage on a public street are prohibited.



4. Walkways Through Parking Lots: Parking lots with more than fifteen (15) stalls shall provide a pedestrian walkway through the parking lot to the primary building entrance or a sidewalk providing access to a primary building entrance. One walkway must be provided for every three (3) drive aisles. Walkways shall be curb separated from the parking areas and a minimum of five feet (5') wide. Vehicles shall not overhang the walkway. Parking lot landscaping requirements in chapter 21A.48 of this title shall be included on the side of the walkway. Where the walkway crosses a drive aisle, a crosswalk that is clearly identified by a change in color, material, or similar technique shall be used.

5. Surface Parking Lots as the Principal Use: Surface parking lots that are the principal use are permitted in the TSA zoning district provided the following standards are complied with:

a. The surface parking lot does not have street frontage on the street where the fixed rail transit is located.

b. The surface parking lot is setback a minimum of 15 feet from any property line adjacent to a public street.

c. The parking area is screened by a wall or landscaping that is a minimum of 36 inches and a maximum of 48 inches tall.

65. Other Applicable Standards: All other standards in chapter 21A.44, "Off Street Parking And Loading", of this title shall apply.

M. Conflicting Regulations: In cases where the regulations of this section conflict with another section of this zoning ordinance, this section shall take precedence except in situations where the conflict is related to the use of the property, in which case the more restrictive regulation takes precedence. In station areas within an overlay district, the overlay district shall take precedence.

N. Developments Over Five Acres:

1. Intent: Large scale developments have the potential to function as a self-contained mixed use neighborhood and could have both positive and negative impacts on nearby properties. All developments over five (5) acres in size shall be designed and planned to include a series of blocks and a network of public or private streets that connects to the existing public streets in the area and to adjacent development and neighborhoods. Buildings should be oriented to this street network. Regulating block size is necessary to provide development sites that are oriented to the pedestrian while accommodating other modes of transportation. A street network is required to ensure adequate circulation for pedestrians, bicycles, automobiles and service vehicles through the site, to adjacent sites and the public streets.
2. Application: These standards are in addition to all other applicable standards. In situations where the standards in this section conflict with a standard in another section, the standard in this section shall take precedence. A separate development score is required for each new principal building in a development over five (5) acres. A development over five (5) acres shall be subject to the applicable review process based on the lowest development score assigned to an individual building in the development.
 - a. Block Layout: The intent of regulating block size and dimension is to create a development pattern where all principal buildings have their primary facades facing a street, whether public or private. All developments over five (5) acres in size shall be designed to include a series of blocks based on the standards below:
 - (1) The maximum perimeter dimension of any block shall be one thousand six hundred feet (1,600'). The maximum length of any individual block face shall be four hundred forty feet (440').
 - (2) The maximum perimeter dimension of a block may be increased to two thousand four hundred (2,400) linear feet, and the maximum length of any block face increased to six hundred feet (600') provided a mid block pedestrian network is included. The mid block pedestrian network must be a minimum of twenty feet (20') wide and include pedestrian amenities such as lighting, benches, and other similar features. The mid block walkway shall connect to at least two (2) block faces or be extended to the property line to allow for future extension. The standards in subsection J2 of this section apply to building walls adjacent to a mid block walkway.
 - b. Housing Proximity To Transit: Developments that include housing should cluster the housing so a minimum of fifty percent (50%) of the housing is located within one-fourth (1/4) mile walking distance of a transit platform.
 - c. Connectivity To Public Streets, Sidewalks, And Bicycle Lanes: In order to ensure that the development will be fully integrated into the transit station area,

that safe and efficient travelways are provided, and to limit the impact on the primary transit street and other adjacent streets, the internal circulation system, including private streets, drive aisles, sidewalks and bicycle lanes shall connect to the public street, sidewalks and bicycle lanes. All new streets shall be designed as a “complete street” defined as a street that provides dedicated space for pedestrians, bicyclists and automobiles.

- d. **Vehicle Access: Regulating access to private property from public streets is necessary for integrating private development and public spaces. Limiting the number of access points and spacing between access points reduces areas of conflict between vehicles, pedestrians and bicycles. Maximum access widths promote a development pattern that is oriented to pedestrians and bicyclists while accommodating vehicles.**
 - (1) Access points located on public streets intended for vehicles shall be spaced a minimum of one hundred feet (100’) apart.
 - (2) No property shall have more than one vehicle access point for every two hundred (200) linear feet of frontage on a public street.
 - (3) No access drive shall be greater than twenty four feet (24’) wide.
 - (4) The location of all vehicle access points is subject to approval from the transportation division of the city. The standards of this section may be modified by the transportation division when, in the opinion of the director of the transportation division, a different design would improve the overall safety for all modes of transportation or improve the efficiency of the transportation network.
- e. **Internal Circulation: Internal circulation systems allow for vehicles, pedestrians and bicyclists to move safely and efficiently throughout a development site. A logical, simple and well designed internal circulation system that connects with adjacent circulation networks provides room for vehicles, safe walking paths for pedestrians through the parking lot and the site to the public way, and well marked routes for bicycles traveling from public spaces to bicycle parking areas within a site. All new developments over five (5) acres are required to submit an internal circulation network plan.**
 - (1) **Travel Lanes That Connect Parking Areas ~~W~~with ~~A~~a Public Street:** All internal vehicle travel lanes that connect internal parking areas with a public street shall be designed to meet the minimum requirements in section 21A.44.020 of this title.
 - (2) **Design Speed:** The internal circulation system shall be designed to move vehicles at speeds of twenty (20) miles per hour or less.
 - (3) **Future Access ~~T~~o Adjacent Properties ~~A~~and Rights ~~O~~f Way:** All internal drive aisles, sidewalks, and paths shall be extended to property lines to allow

for future cross access to adjacent properties when the adjacent property is undeveloped and to rights of way.

- (4) Centerlines: The centerline of all internal streets shall be in line with the centerline of a street on the opposite side of an intersecting street unless the intersecting street is divided by a median. Offset streets shall be a minimum of two hundred feet (200') apart, measured from centerline to centerline.
 - (5) Publicly Dedicated Streets: Any street that is to be publicly dedicated shall meet the city's minimum construction and design standards (including street lighting, park strip, street trees, etc.).
 - (6) Pedestrian Routes: Pedestrian routes that provide safe, comfortable, clear and direct access throughout the development shall be provided. Pedestrian paths shall be bordered by residential fronts, green space, active open space, or commercial store fronts.
 - (7) Bicycle Paths: A coordinated system of bicycle paths should be provided.
 - (8) Approval; Modification Of Standards: The internal circulation network is subject to approval from the transportation division of the city. The standards of this section may be modified by the transportation division when, in the opinion of the director of the transportation division, a different design would improve the overall safety for all modes of transportation or improve the efficiency of the transportation network.
- f. Parking: Parking may be provided along any private street within a development over five (5) acres. The parking shall be counted towards the applicable off street parking standard when provided on private streets. All parking areas and stalls must comply with the parking lane widths identified in table 21A.44.020 of this title.
- g. Open Space: In order to provide space for passive and active recreation, public and private gatherings, offset storm drainage due to nonpermeable surfaces and as an amenity to individual developments and their residents, employees and customers, usable open space is required for all new developments.
- (1) Required: In the core and transition areas of all station areas, a minimum of ten percent (10%) of the site, up to fifteen thousand (15,000) square feet, shall be devoted to open space. "Usable open space" is defined as landscaped areas, plazas, outdoor dining areas, terraces, rooftop gardens, stormwater retention areas, and any other similar type of area.
 - (2) Connectivity To Adjacent Open Space: When adjacent to public open space, parks, trails and pathways, open space on developments over five (5) acres in size are encouraged to provide access to the public open space.

- h. Landscaping: All areas not occupied by buildings, plazas, terraces, patios, parking areas, or other similar feature shall be landscaped. If a project is developed in phases, only those areas in a phase that is under construction shall be landscaped. Landscaping in future phases shall be installed as those phases develop. Areas in future phases may be used as community gardens or other active open space until such time as development of that phase begins.

SECTION 4. Effective Date. This ordinance shall become effective on the date of its first publication.

Passed by the City Council of Salt Lake City, Utah, this _____ day of _____, 2012.

CHAIRPERSON

ATTEST AND COUNTERSIGN:

CITY RECORDER

Transmitted to Mayor on _____.

Mayor's Action: _____ Approved. _____ Vetoed.

MAYOR

CITY RECORDER
(SEAL)

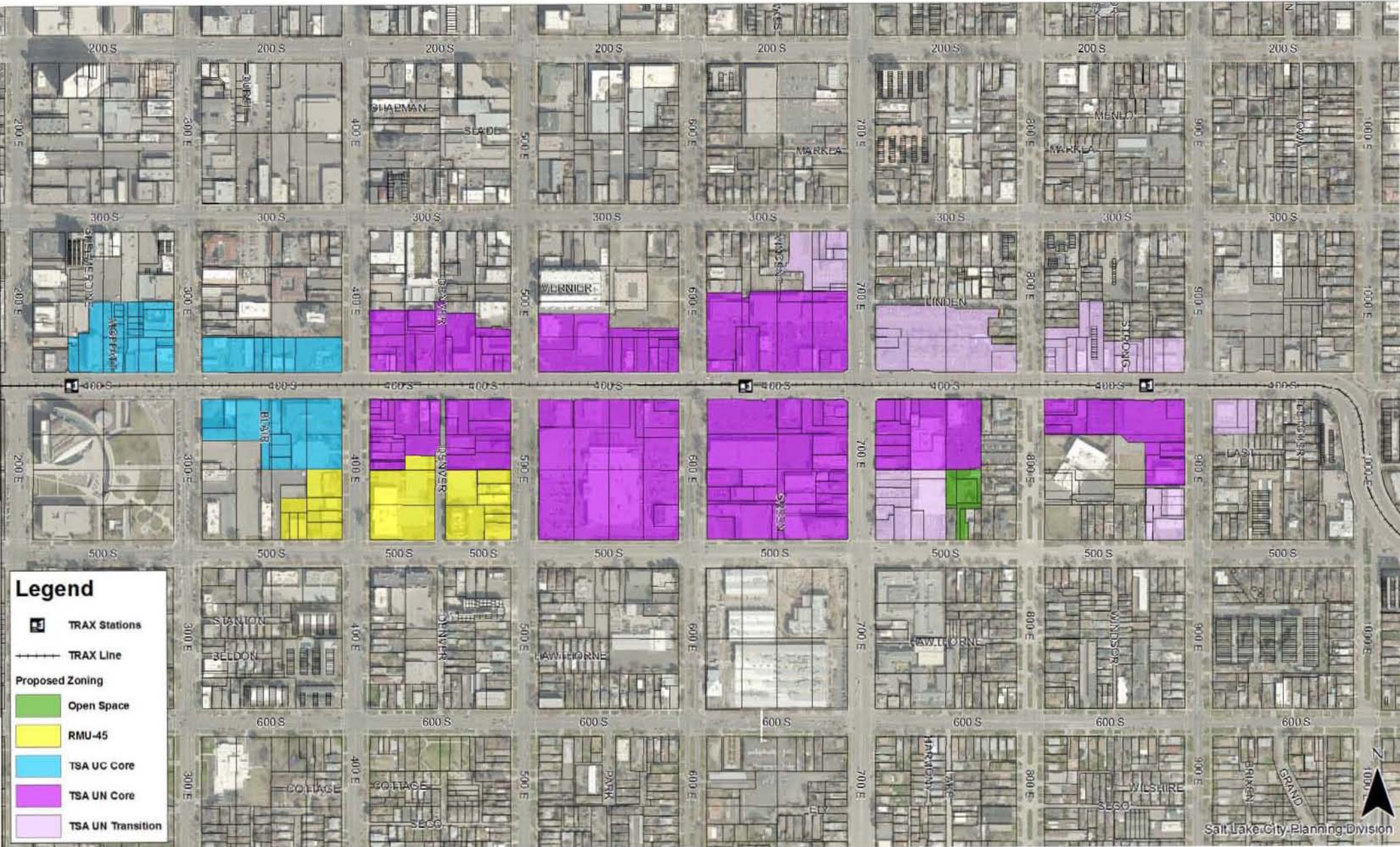
Bill No. _____ of 2012.

Published: _____.

Exhibit A - 400 South Current Zoning



Exhibit A - 400 South Proposed Zoning



SALT LAKE CITY ORDINANCE

No. _____ of 2012

(Amending the Central Community Master Plan, zoning map and zoning ordinance pertaining to transit station area plans along the 400 South transit corridor)

An ordinance amending the Central Community Master Plan, zoning map and zoning ordinance to implement land use policies related to mixed use development and transportation options on the blocks adjacent to the 400 South transit corridor pursuant to Petition No. PLNPCM2010-00647.

WHEREAS, the Salt Lake City Planning Commission (“planning commission”) held public hearings on April 25, 2012 and May 23, 2012 on an application submitted by Mayor Ralph Becker (“Applicant”) to amend the Central Community Master Plan, zoning map and zoning ordinance to implement land use policies related to mixed use development and transportation options on the blocks adjacent to the 400 South transit corridor (between 1000 East and 200 East Streets) pursuant to Petition No. PLNPCM2010-00647; and

WHEREAS, at its May 23, 2012 meeting, the Planning Commission voted in favor of forwarding a positive recommendation to the Salt Lake City Council (“City Council”) on said application; and

WHEREAS, after a public hearing on this matter the City Council has determined that adopting this ordinance is in the City’s best interests.

NOW, THEREFORE, be it ordained by the City Council of Salt Lake City, Utah:

SECTION 1. Amending the Central Community Master Plan. That the Central Community Master Plan shall be and hereby is amended as follows:

a) That the section of the Central Community Master Plan concerning transit-oriented development, which section is presently found on page 15 thereof, is amended to read as follows:

Transit-Oriented Development (TOD)

INTRODUCTION

Transit-Oriented-Development (TOD) is a land use and urban design concept that mixes land use near transit to maximize transportation options and provides people with choices about living near transit. A TOD neighborhood typically has a center with a light rail station or bus stop surrounded by relatively high-density development with progressively lower-density development spreading outward from the transit center. TOD's generally are located within a radius of one-quarter to one-half mile from a transit stop, as this is considered an acceptable walking distance.

TOD areas offer choices in housing, commerce and transportation. They are designed to be flexible so they can respond to changing economic and social conditions and endeavor to make development economically viable from a number of perspectives (city, transit agency, developer, resident, employer, etc.). TOD districts encourage the stabilization and revitalization of existing neighborhoods, as new uses are designed to support existing neighborhood characteristics.

TOD districts improve the quality of life in urban areas by:

- Including mixed-use development that will use transit at all times of day.
- Creation of excellent pedestrian facilities such as high quality pedestrian crossings, narrow streets, and tapering of buildings as they become more distant from the public transport node.
- Ensuring compatibility and connectivity with surrounding neighborhoods.
- Creating compact development within an easy walk of public transit and with sufficient density to support transit ridership.
- To establish a hierarchy of transportation which places the pedestrian first, bicycle second and auto third.
- Creating active places and livable communities that service daily needs and where people feel a sense of belonging and ownership.
- Including engaging, high quality public spaces (e.g. small parks or plazas) as organizing features and gathering places for the neighborhood.
- Encouraging a variety of housing types near transit facilities.
- Providing housing choices for a wide range of ages and incomes.
- Incorporating retail into a development if it is a viable use at the location. Ideally drawing customers both from both the TOD and a major street.

- Introducing creative parking strategies that integrate, rather than divide a site and reduce the presence of the auto.
- Creating TOD plans that are flexible so they can respond to changing conditions.
- Recognizing that all TOD's are not the same; each development is located within its own unique context and serves a specific purpose in the larger context.

TOD strengthens urban development but also helps manage future regional growth by encouraging growth to occur where the existing infrastructure is best suited to address it. The benefits of this type of development include:

- Opportunities for affordable and accessible living by incentivizing affordable and accessible housing goals, providing a range of housing options, and allows people to spend less of their income on the combined expenses related to housing and transportation.
- Reduction of the long term expenditures of tax dollars to build and maintain public utilities, roads and other auto-oriented infrastructure.
- More residents living close to commercial areas to support a strong, local oriented economy.

This reduces the overall cost of development and reduces negative environmental impacts on air and water quality while creating community oriented public places.

Transit-oriented development designations

Transit-oriented development districts within the Central Community have three designations: low-density, medium-density, and high-density. The Future Land Use map shows locations where these districts are supported by this master plan. Where conflicts between TOD and historic district overlay regulations occur, the historic overlay requirements govern.

Low-density transit-oriented development (light sage green on map): Low-density TOD supports residential uses with a density ranging from 1-20 dwellings per acre. The emphasis of low-density TOD design and land use relates to existing lower density residential and neighborhood commercial land uses. Low intensive development should be designed to assure compatibility in neighborhoods with established low-density characteristics by focusing on the massing and scale of the existing structures. Medium intensive land uses, such as a coffee shop, town house or daycare center, may be appropriate near the light rail station or busier intersections. Implementing low-density TOD areas may include development of accessory units in the rear yards of low-density residential land uses as well as small businesses that can be operated out of a residential structure. New structures should be in scale with the low-density neighborhood with modest increases in building height being appropriate if it provides opportunity for increasing the residential density. Zoning designations should require compatibility in these areas.

Medium-density transit-oriented development (medium sage green on map): Medium-density TOD supports residential land uses with a density range of 20-50 dwelling units per acre. The design emphasis for medium-density TOD is compatibility with existing medium- and low-density residential and commercial development. Higher intensive uses may be located near light rail stations where applicable. Medium-density TOD areas include four to five story buildings with a mix of ground level retail or office space components with multi-story residential development above. These areas should have limits on the amount of space allocated for non-residential land uses by locating them closer to transit and at busier intersections. Individual residential land uses could remain within the TOD area. Zoning districts that allow four to five story building, including pedestrian oriented design standards and allow 20-50 dwelling units per acre are appropriate in areas with this designation.

High-density transit-oriented development (dark sage green on map): High-density TOD is similar to medium-density TOD except at a greater scale. These areas should be centers of high population with a concentration of pedestrians in close proximity to transit stations. These areas will be more successful in places that are well served by rail transit. Building heights are established for high density residential and higher intensity office or commercial uses. The ideal building type in these areas is vertical mixed use structures tall than five stories. High-density TOD supports the development of compact urban centers with 50 or more dwelling units per acre. These centers can provide accessibility and a multitude of benefits (efficiencies that result when many activities are physically close together). Design standards are critical in these areas in order to create people oriented spaces. Auto oriented development should be prohibited. Standards and processes should be put in place to allow for the easy and financially viable transition of auto oriented developments to people oriented development.

The transit-oriented development land use designations are shown on the Central Community TOD map and on the Future Land Use map.

Future Transit-Oriented Development Land Use changes

TOD's are implemented by the public and private sectors. The public sector sets the stage by providing the transit, investing in public infrastructure and applying appropriate zoning regulations near the transit stations. The private sector generally finances, constructs and markets TOD's. When based on a sound and agreed upon vision, TOD implementation should be a simple and straightforward process. To ensure this is the case, the TOD station area plans intend to create the vision and set the stage for the private sector. The City has developed certain zoning districts to implement the various intensity levels of TOD that are based on input from a broad range of stakeholders. The areas listed below have gone through a community visioning process that had identified the desired development characteristics and therefore, support zoning changes based on the specific goals for each area.

400 South and University Boulevard

In June of 2011, the City received a grant from the Housing and Urban Development Department (HUD) to initiate the 400 South and University Boulevard Livable Communities project. The focus of the project was to enhance the Community's vision of the corridor and make it a place where people could live, work and shop. Inherent in this process was the inclusion of people who rely on the corridor for their daily needs, but for various reasons do not normally participate. The group included those who are traditionally under-represented in the planning process. The project determined that future development at all 400 South stations should be compatible with the existing neighborhood scale and that development should not supplant or compete with the central business district regional services (beyond the existing Trolley Square development). Development should enhance existing urban neighborhoods within walking distance of the transit line to create areas where pedestrian activity is the focus of daily transportation needs, without excluding the automobile. These concepts are critical to groups who rely on public transportation to access housing, employment, education, health care, and other daily needs.

The 400 South and University Boulevard Trax lines run from downtown to the University of Utah. The corridor incorporates a mix of transportation options, including light rail, bus and automobile. There are several bicycle lanes that parallel and intersect the street to create a complete transportation network through the area. When the light rail line opened in 2001, the pedestrian environment improved by adding street trees and widening the sidewalk. Unfortunately, the parking lane was removed, placing a travel lane immediately adjacent to the sidewalk. In order to make the corridor more pedestrian oriented, sidewalk improvements are necessary.

The Liveable Communities project identified specific land use goals and policies that can be found in the "400 South and University Boulevard Station Area Plans" in the addendum of this Central Community Master Plan. The station area plans include specific land use policies and a future land use map that should be used to help guide future land use, capital improvements and budget decisions.

West Temple Gateway

The West Temple Gateway area extends from 700 South to the 900 South Interstate off ramp and from 300 West to West Temple and includes the 200 West / 900 South future light rail stop. The West Temple Gateway area is part of a redevelopment project area created in 1987, which included two revitalization concept plans. These are not adopted policy plans but resource documents. The 1994 plan identified alternative concepts ranging from low-density residential infill to Big Box retail uses. A second analysis in 2001, after the light rail line was constructed, provided an Illustrative Plan that proposes a mixed use transit-oriented neighborhood containing residential, retail, office, and industrial land uses. Development of a West Temple Gateway small area master plan will provide detailed development guidelines for this area.

Transit Oriented Development Goal

To create TOD development with a balanced mix of uses that generates 24-hour transit ridership. This development will have places to work, to live, to learn, to relax and to shop for daily needs. The goal will be achieved through land use designations, development guidelines, zoning, and both public and private funding.

Transit Oriented Development policies

Transit-Oriented Development policies fall into these general categories: location and variety of land use.

Location

Policy TOD-1.0 Based on the Future Land Use map and specific station area plans, establish Transit-Oriented Districts that will provide residents with housing, transportation and employment options at various densities near transit stations.

TOD-1.1 Develop station area plans for each transit station within the Central Community Planning Community.

TOD-1.2 Utilize a broad community involvement approach to identify appropriately located and scaled transit oriented development that put people first.

Variety of Land Use

Policy TOD-2.0 Encourage the development of mixed-use projects near light rail stations to create a livable, walkable urban environment.

TOD-2.1 Support a variety of low-, medium- and high-density residential uses around light rail stations in TOD districts, based on the Future Land Use map designations.

TOD-2.2 At light rail stations in TOD districts, establish a centralized core of land uses that support transit ridership. Anchor transit centers with land uses that act as destination points.

TOD-2.3 Encourage a variety of commercial uses that share the same clientele and patrons. For example, movie theaters provide a clientele to patronize restaurants, arcades, and retail businesses.

b) That the following text shall be added to the Central Community Master Plan in the section titled “Central Community Master Plan Committee Goals and Recommendations”

immediately following subparagraph 15 of the subsection titled “Environment”, which presently appears on page 23 of that master plan:

400 South and University Boulevard Station Area Plans

Each of the three station area plans include subareas as identified below:

Core Area: The purpose of the core area is to provide areas for comparatively intense land development with a mix of land uses incorporating the principles of sustainable, transit oriented development and to enhance the area closest to a transit station as a lively, people oriented place. The core area is generally within a one-fourth ($\frac{1}{4}$) mile walk of a transit station platform. The core area may mix ground floor retail, office, commercial and residential space in order to activate the public realm. Buildings in this area should have minimal setbacks to encourage active outdoor use adjacent to the sidewalk, such as outdoor dining and patios that reflect the desired character of the area. Building facades should be varied and articulated, include storefronts adjacent to the street, windows on the street level, and have clearly defined entrances to provide visual interest to pedestrians. Buildings should be a minimum of two (2) or three (3) stories in height, depending on location, in order to define the street edge. Arcades, bays, and balconies are encouraged. The configuration of buildings must balance the needs of all modes of circulation with the safety and comfort of pedestrians and bicyclists. A vertical mix of uses, with office and residential above ground floor commercial uses is encouraged. A minimum of thirty (30) dwelling units per acre is encouraged within the core.

Transition Area: The purpose of the transition area is to provide a moderate level of land development intensity that incorporates the principles of sustainable transit oriented development. The transition area is intended to provide an important support base to the core area and transit ridership as well as buffer surrounding neighborhoods from the intensity of the core area. These areas reinforce the viability of the core area and provide opportunities for a range of housing types at different densities. Transition areas are generally located within a one-half ($\frac{1}{2}$) mile from the station platform, but may vary based on the character of the area. Transition areas typically serve the surrounding neighborhood; include a broad range of building forms that house a mix of compatible land uses. The minimum desired density is ten (10) dwelling units per acre. Commercial uses may include office, retail, restaurant and other commercial land uses that are necessary to create mixed use neighborhoods. Commercial uses can be clustered around intersections and along block faces to create neighborhood nodes.

Library Station Area

The Library Urban Center Station has the highest intensity level and mix of uses along 400 South. It supports Salt Lake City’s central business district in terms of building scale and use. The area has a twenty-four-hour population, active streetscapes, defined street walls and multiple types of public transit. Development generally occurs on surface parking lots or through redevelopment of underutilized parcels.

The street frontages in the Library Station area have a compact, dense, interconnected and walkable development pattern, while the interior of blocks are open generally with parking lots. Large scale development is closer to the station; and is scaled back as it moves to the areas away from 400 South. Building forms vary, but are typically oriented to the pedestrian, are multiple stories in height, and contain a horizontal and vertical mix of land uses. Buildings up to thirty stories in height are allowed within the D-1 zoned sections, while buildings in the transition zone can be approximately three to four stories in height.

The area features a variety of dense, mixed use commercial and housing developments, providing residents with a number of housing options. The station area contains important regional attractions, such as the Library, City & County Building, Leonardo Museum, and the Public Safety Building. These buildings are a regional draw, bringing thousands of people to the area each day. The area also features office, dining and entertainment options with a high level of pedestrian activity. The Library and Washington Square feature a number of civic and cultural festivals during the summer.

The station area comprises of core and transition areas. The purpose of creating the different areas is to recognize the scale and nature of existing development patterns and identify the appropriate locations for growth. The general concept is that bigger buildings with the most dwelling units and a higher intensity level of commercial space should be located closest to the station in the core. The transition area reduces the scale, mass and intensity of new development as it moves away from the core.

In addition to the civic uses, and high density development surrounding the Library Station core, there is a significant amount of underutilized land. The Library Station Area Plan encourages the development of these areas to create medium to high density housing, mixed use development and appropriate support commercial along 400 South. By doing this, the area would further its role as a vibrant and contributory feature of Salt Lake City, while maintaining its function as a support to the downtown area.

The “Transit Station Area (TSA) Development Guidelines” will be used along 400 South to encourage a walkable urban neighborhood compatible with the adjacent historic district.

Library Station Area Goals:

1. Rezone properties fronting 400 South so that the zoning reflects the vision for the station area. Rezone the properties that are in the vicinity to a mix of zoning districts that promote high density residential development while allowing limited commercial types of uses.
2. Coordinate with Utah Department of Transportation (UDOT) to improve the functionality of 400 South for pedestrians. This could be accomplished by adding additional midblock crosswalks to the transit stations, allowing on-street parking during off peak travel hours, studying the impact of reducing the road width, whether

in key spots or for longer stretches and other changes to the street that are aimed at creating a safe and functional transportation corridor for all users.

3. Work with the City Transportation Division to find appropriate bicycle routes that run on or are parallel to 400 South and that connect to appropriately located north/south bike routes.
4. Require midblock connections from 300 South and 500 South that connect to 400 South.
5. Incorporate way-finding features at the station area so people can easily locate station area assets such as Library, Library Plaza, City & County Building, Leonardo Museum and the Public Safety Building.
6. Maintain the original 15-foot landscaped setback along the 400 South street frontage.
7. Require properties to share driveway access as properties are redeveloped to reduce the number of drive approaches in the station area.
8. Minimize the number of driveways, garage entrances and dedicated turning lanes on all major pedestrian routes.
9. Encourage community services, including schools, childcare and museums with pedestrian connections to transit and other land uses.
10. Extend 450 South midblock walkway through to 650 East.
11. Develop and enhance existing public gathering spaces, including parks, plazas and courtyards to attract people and transform 300 South and 500 South into active pedestrian places.
12. Increase building intensity and residential densities closest to the transit station and gradually step down further away. Parking requirements should be lower closer to the station.
13. Discourage additional or expanded stand alone automobile oriented uses such as: large surface parking lots, fuel stations, auto repair shops, auto dealerships, large scale retail development, car washes and drive-thru facilities.
14. Mid-block crossings across 400 South should be considered adjacent to the Trax stop to help pedestrians cross safely and to discourage jaywalking.

Trolley Station Area

The Trolley Station is defined as an Urban Neighborhood Station Area. Urban Neighborhoods are places that have an established development pattern that contain a mix of uses and can support an increase in residential density and supporting commercial

activities. New development generally occurs as infill, occurring on undeveloped or underutilized properties. Redevelopment of surface parking lots that front on 400 South is a priority. A compact development pattern is desired in order to focus new growth at the station and respect the existing scale and intensity of the surrounding neighborhood. The highest residential density and intensity of commercial land use occur closest to the transit station and are scaled down the further one moves from the station.

The station area comprises of core and transition areas. The purpose of creating the different areas is to recognize the scale and nature of existing development patterns and identify the appropriate locations for growth. The general concept is that bigger buildings with the most dwelling units and a higher intensity level of commercial space should be located closest to the station in the core. The transition area reduces the scale, mass and intensity of new development as it moves away from the core area.

Trolley Station is a unique Transit Station Area because it is located within the Central City Historic District. The Central City Historic District is centered on the 600 East landscaped medians, which are a character defining feature of the historic district. Over time, the 600 East medians have been degraded by the continual pressure from commercial development to allow vehicle access through the medians. The policy of the Trolley Station Area is to prohibit further dissection of the 600 East medians for vehicular traffic and to maintain the historical 15 foot landscaped setback of building along 600 East.

Despite being located within the Central City Historic District, the blocks that front on 400 South have lost most, if not all, of their historic character. Without the historic character being present, the boundaries of the Central City Historic District should be reviewed and amendments to the district boundaries should be proposed.

The “TSA Development Guidelines” will be used along 400 South to encourage a walkable urban neighborhood compatible with the adjacent historic district.

Trolley Station Area Goals:

1. The primary purpose of the Trolley Station Area is to provide housing and access to higher intensity employment, commercial centers, downtown, and to the University of Utah.
2. Rezone properties fronting 400 South so that the zoning reflects the vision for the station area. Rezone the properties that are in the vicinity to a mix of zoning districts that promote high density residential development, while allowing limited commercial types of uses in appropriate places. The properties fronting 400 South should be the focus of the station area and development should focus on creating an urban neighborhood in scale and purpose, and is not intended to supplant or compete with the much higher density central business district. Regional scale development beyond the existing Trolley Square commercial development is not encouraged.

3. Coordinate with the Utah Department of Transportation (UDOT) to improve the functionality of 400 South for pedestrians. This could be accomplished by adding additional midblock crosswalks to the transit stations, allowing on street parking during off peak travel hours, studying the impact of reducing the road width, whether in key spots or for longer stretches, and other changes to the street that are aimed at creating a safe and functional transportation corridor for all users.
4. Work with the Transportation Division to find appropriate bicycle routes that run on or parallel 400 South and that connect to appropriately located north/south bike routes.
5. Require midblock connections from 300 South and 500 South that connect to 400 South.
6. Incorporate way-finding features at the station area so people can easily locate station area assets such as Trolley Square, Gilgal Gardens and Liberty Park.
7. Maintain the original 15-foot landscaped setback along the 400 South street frontage.
8. Review the appropriateness of and consider amendments to the boundaries of the Central City Historic District to remove those blocks and portions of blocks that front on 400 South from the historic district due to the lack of remaining historic character. See the “.....” map to determine the areas that should be considered for removal from the Central City Historic District.
9. Preserve the 600 East medians and prohibit further bisections of the medians for the purpose of allowing vehicular access and left turns to private property or streets.
10. Extend 450 South midblock walkway through to 650 East.
11. Further multi-modal solutions to change the way 600 South is utilized between 500 East and 700 East. Possible solutions include adding middle of the street parking, midblock crosswalks, medians, or removing a lane of traffic in each direction to add bike lanes, larger park strips or angled parking.
12. Identify zoning solutions for the block faces across from Trolley Square on 600 East and 600 South. The focus should be to encourage development on vacant parcels, increase residential density and promote the preservation and adaptive reuse of contributing structures. The surface parking lot south of Trolley Square should be rezoned to allow Trolley Square to building a parking structure, retain the historic structures fronting on 600 South and build housing.
13. Adopt zoning that increases housing potential, but does not introduce extensive commercial development on the south side of 500 South between 500 and 600 East and both sides of the street between 400 and 500 East.
14. Encourage redevelopment of strip centers into higher density with structured parking

with a potential park and ride. The block bounded by 400 and 500 South and 600 and 700 East should have the highest commercial densities of the area.

15. Encourage development that is compatible with the historic development pattern in the Central City Historic District where appropriate.
16. Reevaluate the densities on the northwest quadrant of the block bounded by 300 South and 400 South and 600 East and 700 East, to allow higher densities and design that is compatible with the adjacent historic character.
17. Mid-block crossings across 400 South should be considered adjacent to the Trax stop to help pedestrians to cross safely and to discourage jaywalking.

900 East Station Area

The 900 East Station is an Urban Neighborhood Station Area due to the established and predominating residential character and the potential for infill development along 400 South which adds variations of density and intensity of building forms that blend in and complement the existing residential character of the area. The development strategy of an urban neighborhood station may include the mixing of building types and uses, including the allocation of commercial or office uses to the ground floor, and residential uses above these floors. The highest residential densities and most intense land uses are generally located closest to the station platform along 400 South between 700 East and 900 East, particularly on the south side of 400 South.

The station area comprises of core and transition areas. The purpose of creating the different areas is to recognize the scale and nature of existing development patterns and identify the appropriate locations for growth. The general concept is that bigger buildings with the most dwelling units and a higher intensity level of commercial space should be located closest to the station in the core. The transition area reduces the scale, mass and intensity of new development as it moves away from the core area. The north side of 400 South is part of the transition area due to the close proximity of the relatively low scale nature of the residential area to the north and the desire to maintain that character and the impacts that taller buildings on 400 South would have on privacy and solar access.

Contributing landmarks and existing neighborhood characteristics will influence future development in and around the 900 East Station. These existing traits include institutional uses such as the Intermountain Health Care facility, Bennion Elementary School, and religious structures. There are also various types of commercial and a strong mixture of single and multi-family residential building types.

900 East Station Area Goals:

1. Protect historic landmark sites that currently exist in the Station Area.
2. Coordinate with UDOT to improve the functionality of 400 South for pedestrians.

This could be accomplished by adding additional midblock crosswalks to the transit stations, allowing on street parking during off peak travel hours, studying the impact of reducing the road width, whether in key spots or for longer stretches, and other changes to the street that are aimed at creating a safe and functional transportation corridor for all users.

3. Maintain original 15-foot landscaped setback along the 400 South street frontage.
4. Incorporate way-finding features at the station area so people can easily locate station area assets such as Gilgal Gardens and health care facilities.
5. Protect the 800 East medians by not allowing vehicular access cuts through the existing landscaped area.
6. Encourage infill uses that will eliminate surface parking lots with an emphasis on those that front 400 South or areas that can easily access the station platform.
7. Encourage shared parking facilities between uses where applicable.
8. Focus on mixed-use development with commercial and residential uses along 400 South and the east side of 700 East. In remaining areas the primary redevelopment focus should be on residential uses.
9. If the Bennion Elementary School discontinues to be utilized as a school, the property should be redeveloped with an emphasis on residential uses along the frontages but much of the open space behind should remain, and be enhanced and available to the public.
10. Create mid-block connections that draw pedestrians to 400 South and specifically to the 900 East TRAX station. This should be emphasized between 300 South and 400 South where connections could more easily be created by linking existing rights-of-way such as, Laker Court and Strongs Court.
11. Encourage nonconforming manufacturing or warehousing uses to relocate to appropriate locations in the city.
12. Adopt zoning that promotes high density housing with appropriate design standards adjacent to Gilgal Gardens in order to activate and provide security for the park.
13. Mid-block crossings across 400 South should be considered adjacent to the Trax stop to help pedestrians to cross safely and to discourage jaywalking.

SECTION 2. Amending the Zoning Map. The Salt Lake City zoning map, as adopted by the Salt Lake City Code, relating to the fixing of boundaries and zoning districts, shall be and hereby

is amended to reflect the changes to that map affecting the 400 South transit corridor (between 1000 East and 200 East Streets) as shown on Exhibit “A” attached hereto.

SECTION 3. Amending text of *Salt Lake City Code* section 21A.26.078 That section 21A.26.078 of the *Salt Lake City Code* (Zoning: Commercial Districts: TSA Transit Station Area District), shall be, and hereby is, amended to read as follows:

21A.26.078: TSA TRANSIT STATION AREA DISTRICT:

A. Purpose Statement: The purpose of the TSA transit station area district is to provide an environment for efficient and attractive transit and pedestrian oriented commercial, residential and mixed use development around transit stations. Redevelopment, infill development and increased development on underutilized parcels should include uses that allow them to function as part of a walkable, mixed use district. Existing uses that are complementary to the district, and economically and physically viable, should be integrated into the form and function of a compact, mixed use pedestrian oriented neighborhood. Each transit station is categorized into a station type. These typologies are used to establish appropriate zoning regulations for similar station areas. Each station area will typically have two (2) subsections: the core area and the transition area. Due to the nature of the area around specific stations, the restrictions of overlay zoning districts, and the neighborhood vision, not all station areas are required to have a core area and a transition area.

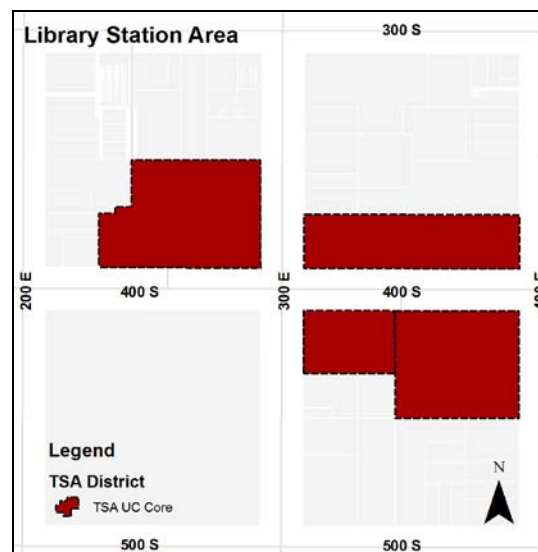
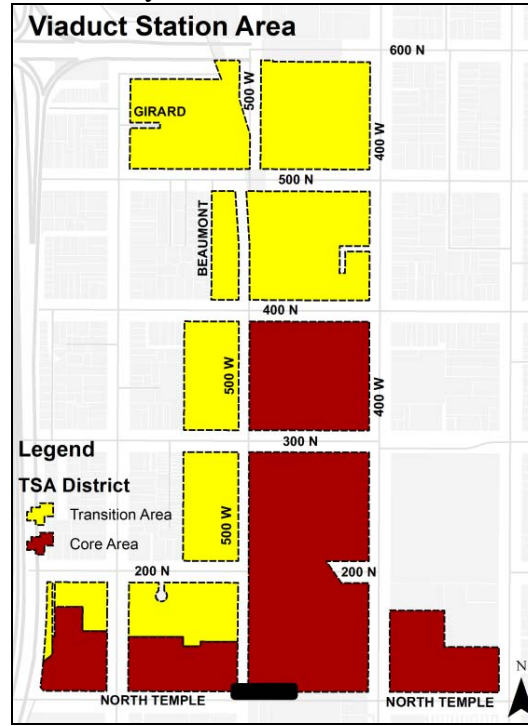
1. Core Area: The purpose of the core area is to provide areas for comparatively intense land development with a mix of land uses incorporating the principles of sustainable, transit oriented development and to enhance the area closest to a transit station as a lively, people oriented place. The core area is generally within a one-fourth (¼) mile walk of a transit station platform. The core area may mix ground floor retail, office, commercial and residential space in order to activate the public realm. Buildings in this area should have minimal setbacks to encourage active outdoor use adjacent to the sidewalk, such as outdoor dining and patios that reflect the desired character of the area. Building facades should be varied and articulated, include storefronts adjacent to the street, windows on the street level, and have clearly defined entrances to provide visual interest to pedestrians. Buildings should be a minimum of two (2) or three (3) stories in height, depending on location, in order to define the street edge. Arcades, bays, and balconies are encouraged. The configuration of buildings must balance the needs of all modes of circulation with the safety and comfort of pedestrians and bicyclists. A vertical mix of uses, with office and residential above ground floor commercial uses is encouraged. A minimum of thirty (30) dwelling units per acre is encouraged within the core.

2. Transition Area: The purpose of the transition area is to provide areas for a

moderate level of land development intensity that incorporates the principles of sustainable transit oriented development. The transition area is intended to provide an important support base to the core area and transit ridership as well as buffer surrounding neighborhoods from the intensity of the core area. These areas reinforce the viability of the core area and provide opportunities for a range of housing types at different densities. Transition areas are generally located within a one-half ($1/2$) mile from the station platform, but may vary based on the character of the area. Transition areas typically serve the surrounding neighborhood; include a broad range of building forms that house a mix of compatible land uses. The minimum desired density is ten (10) dwelling units per acre. Commercial uses may include office, retail, restaurant and other commercial land uses that are necessary to create mixed use neighborhoods. Commercial uses can be clustered around intersections and along block faces to create neighborhood nodes.

- B. Station Area Types: A station area typology is the use of characteristics, such as building types, mix of land use, transit service and street network to create generalizations about an area that can be used to define a common vision for development of a transit station area. Each typology recognizes the important difference among places and destinations and takes into account the local context of a station and its surroundings. Each station area typically will include a core area, where the most intense development will occur, and a transition area, which is intended to create a buffer area between the core and those areas with generally lower intensities and densities. Prior to classifying a transit station into a specific type, a specific area plan must be adopted by the city council prior to applying this zoning district to a geographic area. Only those stations that have an adopted plan that is supported by the regulations in this section will be classified. Refer to the official Salt Lake City Zoning Map to determine the zoning of the land within each station area.
1. Urban Center Station (TSA-UC): An urban center station contains the highest relative intensity level and mix of uses. The type of station area is meant to support Downtown Salt Lake and not compete with it in terms of building scale and use. The intensity level of the area is characterized by a twenty four (24) hour population, active streetscapes, defined street walls and the presence of multiple types of public transit or as a node where several rail transit lines converge. Development generally occurs on vacant parcels or through redevelopment of underutilized parcels. The station area has a compact, dense, interconnected and walkable development pattern. Large scale development occurs closer to the station platforms; and is scaled back as it gets closer to less intense areas. Building forms vary, but are typically oriented to the pedestrian, are multiple stories in height, and contain a horizontal and vertical mix of land uses. Buildings up to ten (10) stories in height are allowed in the core, while buildings in the transition zone are approximately half that size. The station area contains a number of regional attractions, such as destination retail, employment, dining and entertainment and a high level of pedestrian activity. A variety of

dense housing options exist. Development includes civic amenities, such as public gathering places. Uses that help implement the vision for the station and that area commonly found in an intense urban area are appropriate. The following stations are considered an urban center type of station: North Temple Viaduct Transfer Station and the Library Station.

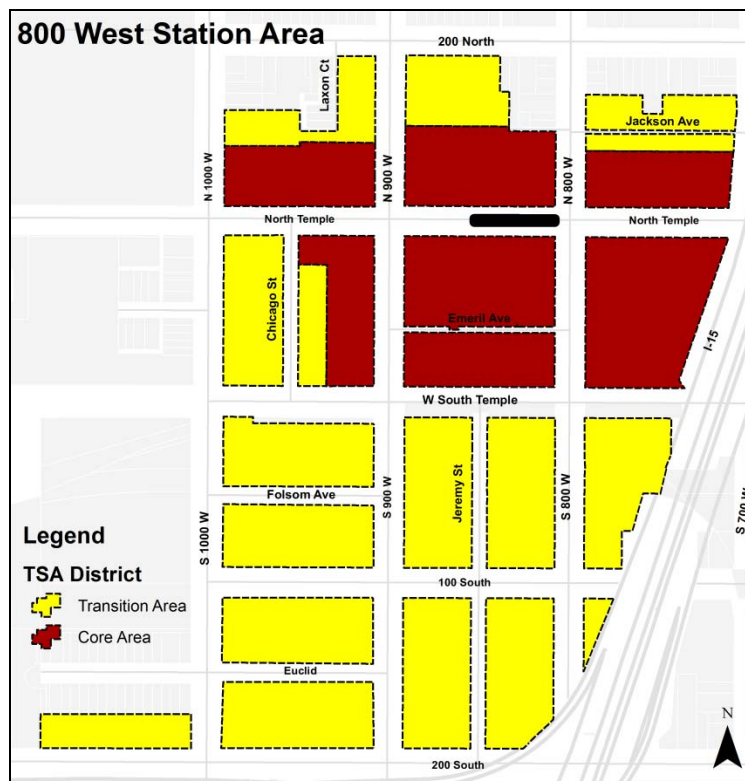


2. Urban Neighborhood Station (TSA-UN): An evolving and flexible development pattern defines an urban neighborhood station area. Development generally happens as infill on vacant parcels or redevelopment of underutilized parcels. These stations evolve in established residential areas where initial changes may add density and intensity in compact building forms that blend in with the

residential character of the area. Urban neighborhoods consist of multilevel buildings that are generally lower scale than what is found in the urban center station area. The desired mix of uses would include ground floor commercial or office uses with the intent of creating a lively, active, and safe streetscape. A mix of building types are possible, ranging from single use structures to mixed use buildings. Residential uses are generally located above the first floor, although they can be located on the ground floor in certain situations. The highest residential densities and most intense land uses are generally located closest to the station platform. Urban neighborhoods are served by at least two (2) forms of transit, including light rail and bus service. The uses serve the surrounding neighborhood with nearby destinations and have the potential to attract people from other neighborhoods.

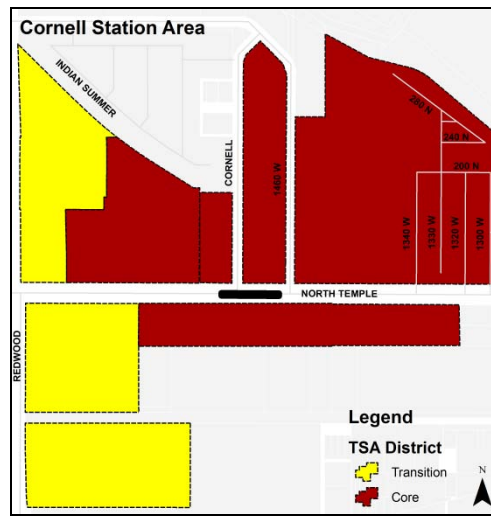
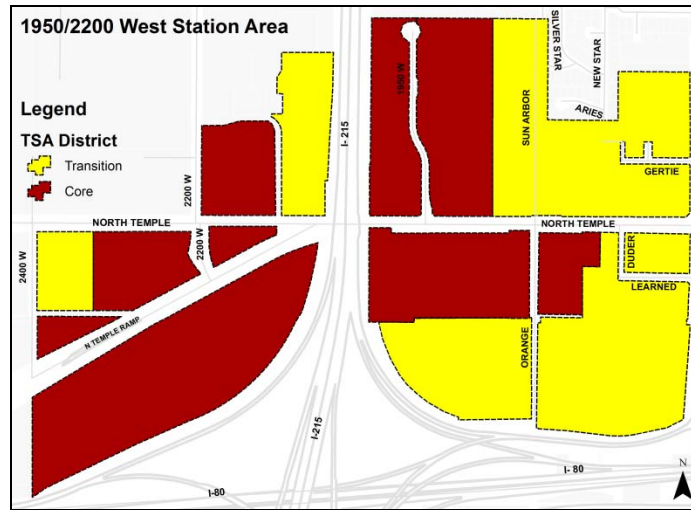
In some urban neighborhood station areas, a linear development pattern along commercial streets that intersect the transit corridor defines a neighborhood main street. Neighborhood main streets are approximately two (2) blocks long, with two (2) 4-story buildings located close to the sidewalk. The ground floors of buildings are typically occupied by active uses, such as retail or restaurants.

The following stations are considered to be urban neighborhood stations: 800 West, Trolley (600 East) and 900 East light rail stations.

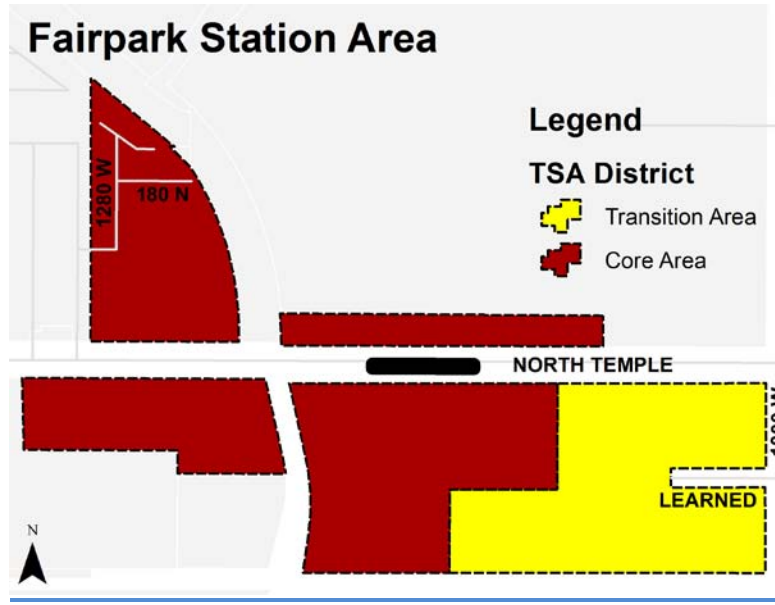




3. Mixed Use Employment Center Station (TSA-MUEC): A mixed use employment station is an area with a high concentration of jobs that attract people from the entire region. These areas generally start with a campus style development pattern and are dominated by a single type of use that generally employs a high number of people. Buildings are often large scale in nature and may have large footprints. New development occurs on vacant parcels. Redevelopment occurs on surface parking lots, underutilized land, or as additions to existing buildings as businesses expand. The primary mode of circulation is by automobile, but the area is served by at least two (2) types of mass transit which provides alternative modes of transportation for employees. Land uses that support the employment centers such as retail sales and service and restaurants are located throughout the station area and should occupy ground floor space in multi-story buildings oriented to the pedestrian and transit user. A mix of housing types and sizes are appropriate to provide employees with the choice to live close to where they work. Building types should trend toward more flexible building types over time. The area is likely to have large blocks and lacks a consistent street network. Connectivity for all modes of travel is important due to the limited street network. The following stations are considered to be mixed use employment center stations: 1950 West, 2200 West, and the Cornell light rail stations.



4. **Special Purpose Station (TSA-SP):** The special purpose station is typically centered on a specific land use or large scale regional activity. These areas are generally served by a mix of transit options, usually light rail or bus. New development is limited due to the nature of the primary function of the area, but redevelopment of underutilized parcels is likely to occur. Land uses such as restaurants and retail support the dominant land use and attract people to the area. A mix of housing types and sizes are appropriate in certain situations. Future development should be aimed at increasing the overall intensity and frequency of use in the station area by adding a mix of uses that can be arranged and designed to be compatible with the primary use. The following stations are considered to be special purpose stations: Fairpark light rail station.



C. Review Process: The intent of the review process is to make the process for desirable development easy to realize in a relatively quick time frame. The review process focuses on building forms and their relationship to adjacent buildings, the public street, transit and other public spaces. The review process for all new development and redevelopment within the transit station area zoning district is based on the development score which is generated by the “Transit Station Area Development Guidelines” hereby adopted by reference. The construction of new buildings require a development score. The following types of development are required to go through this review process:

- Any addition of 1,000 square feet or more that extend a street facing building façade or are located to the side of a building and are visible from a public space or
- Additions that increase the height of an existing building or change the existing roof line;
- Additions to the rear of buildings that are not adjacent to a public street, trail or other public space are not required to obtain a development score but must comply with all other applicable regulations. Signs, fences, accessory structures and any other structure or addition not listed in this section are not required to obtain a development score.

1. Presubmittal Conference: All applicants for development within the transit station area zoning district are required to attend a presubmittal conference with the planning division. The purpose of the presubmittal conference is to notify the applicant of the goals of the station area plans, the standards in this section, and the review and approval process.

2. Development Review Application: After a presubmittal conference, the developer can submit a development review application. This application and all submittal requirements will be used to determine the development score. The application shall include a score sheet on which the development guidelines and their assigned values are indicated and two (2) checklists: one for the applicant's use and one for the planning division's use.
3. Application Review: Table 21A.26.078C of this section summarizes the application review process. All applications shall be processed as follows:
 - a. Tier 1 Planning Commission Review: If a project is assigned a score less than 50 points, the project can only be approved by the planning commission through the conditional building and site design review process in chapter 21A.59 of this title. Once the applicant receives written notice of their score, they will be given thirty (30) days to notify the planning division of their intention to proceed with the project through the conditional building and site design review process or make necessary plan adjustments to increase their development score to the minimum level in order to go through an administrative hearing process.
 - b. Tier 2 Administrative Hearing: A project that has a development score between 50 and 99 points is eligible for an administrative hearing. Any project being reviewed at an administrative hearing shall be reviewed using the standards found in section 21A.59.060, "Standards For Design Review", of this title.
 - (1) Notice and Posting Requirements: Notice of the administrative hearing shall be done in accordance with subsection 21A.10.020C, "Conditional Building and Site Design Review", of this title.
 - (2) Administrative Hearing: After consideration of the information received from the applicant and any other interested party, the planning director, or designee may approve, approve with conditions, deny or refer the matter to the planning commission.
 - (3) Appeals of Administrative Hearing Decision: Any person aggrieved by the decision made by the planning director or designee at an administrative hearing may appeal the decision to the Salt Lake City planning commission by filing notice of appeal within ten (10) days after the record of decision is published. The notice shall state the reason(s) for the appeal. Reason(s) for the appeal shall be based upon procedural error, the development score of the project or the criteria set forth in section 21A.59.060, "Standards For Design Review", of this title.
 - c. Tier 3 Administrative Review: The planning director has the authority to approve a project scoring 100 points or more without holding a public

hearing. The project shall be allowed to go through the standard building permit process. A public hearing is not required because the project incorporates adequate design guidelines or development incentives to be deemed compliant with the vision for the station area.

TABLE 21A.26.078C APPLICATION REVIEW

Development Score	Review Process
0 – 49 points	Planning commission conditional building and site design review process
50 – 99 points	Administrative hearing process
100 or more points	Administrative review

D. Development Score: The purpose of the development score is to allow flexibility for designers while implementing the city’s vision of the applicable station area plans and the purpose of this zoning district. The development score measures the level of compatibility between a proposed project and the station area plan. A “station area plan” is a development, land use, urban design and placemaking policy document for the area around a specific transit station. The development score is based on the design guidelines and development incentives in the “Transit Station Area Development Guidelines” book, hereby adopted by reference. The “Transit Station Area Development Guidelines” shall be amended following the adopted procedures for zoning text amendments in chapter 21A.50, “Amendments”, of this title. Each design guideline is assigned a value.

1. Formulating the Score: The development score is formulated by calculating all of the development guideline values for a particular project. Each design guideline and incentive is given a value based on its importance. Some guidelines are considered more important and carry a higher value than others. The planning director shall evaluate each project in the transit station area zone and assign a development score. The development standards in subsection G of this section and the design standards in subsection J of this section shall be complied with by all projects and are not calculated in the development score.
2. Using the Score: Every development is required to meet a minimum development score. The minimum score represents a percentage of the total points possible.
3. Project Review: A development score shall be assigned to all projects within the transit station area zoning district after a complete development review application is submitted. The planning director shall provide, in writing, a copy of the review checklist and explanation of the outcome of the score to the applicant within thirty (30) days of submitting a complete application.

Extractive industries	X	X	X	X	X	X	X	X
Financial institution, with drive-through facility	X	X	X	X	X	X	X	X
Flammable liquids or gases, heating fuel distribution and storage	X	X	X	X	X	X	X	X
Food processing	X	X	X	X	X		X	
Gas stations	X	X	X	X	X	X	X	
Heavy manufacturing	X	X	X	X	X	X	X	X
Homeless shelters	X	X	X	X	X	X	X	X
Impound lot	X	X	X	X	X	X	X	X
Incinerator medical waste/hazardous waste	X	X	X	X	X	X	X	X
Industrial assembly	X	X	X	X				
Jails	X	X	X	X	X	X	X	X
Landfill	X	X	X	X	X	X	X	X
Limousine service (large)	X	X	X	X	X	X	X	X
Limousine service (small)	X	X	X	X	X	X	X	X
Manufactured/mobile home sales and service	X	X	X	X	X	X	X	X

products								
Restaurant with drive-through facility	X	X	X	X	X	X	X	X
Retail goods establishment with drive-through facility	X	X	X	X	X	X	X	X
Retail services establishment with drive-through facility	X	X	X	X	X	X	X	X
Reverse vending machines	X	X	X	X	X	X	X	X
Use	Urban Center		Urban Neighborhood		Mixed Use Employment Center		Special Purpose	
	Core	Transition	Core	Transition	Core	Transition	Core	Transition
Rock, sand and gravel storage and distribution	X	X	X	X	X	X	X	X
Sexually oriented business	X	X	X	X	X	X	X	X
Sign painting/fabrication	X	X	X	X	X	X	X	X
Slaughterhouse	X	X	X	X	X	X	X	X
Stadium		X		X		X		
Storage (outdoor)	X	X	X	X	X	X	X	X
Storage, mini-warehouse	X	X	X	X	X		X	

Storage, public (outdoor)	X	X	X	X	X	X	X	X
Store, pawnshop	X	X	X	X	X	X	X	X
Store, superstore and hypermarket	X	X	X	X	X		X	
Store, warehouse club	X	X	X	X	X		X	
Taxicab facility	X	X	X	X	X	X	X	X
Theater, live performance		X		X		X		
Theater, movie		X		X		X		X
Tire distribution retail/wholesale		X	X	X	X	X	X	X
Transportation terminal, including bus, rail and trucking	X	X	X	X	X	X	X	X
Truck freight terminal	X	X	X	X	X	X	X	X
Truck stop	X	X	X	X	X	X	X	X
Trucking, repair, storage, etc. associated with extractive industries	X	X	X	X	X	X	X	X
Utility, electric generation facility	X	X	X	X	X	X	X	X
Utility, sewage treatment plant	X	X	X	X	X	X	X	X
Utility, solid waste transfer station	X	X	X	X	X	X	X	X

vehicle sales and service								
Vehicle, recreation								
Vehicle, truck repair (large)	X	X	X	X	X	X	X	X
Vehicle, truck sales and rental	X	X	X	X	X	X	X	X
Warehouse	X	X	X	X	X		X	
Welding shops	X	X	X	X	X	X	X	X
Wholesale distribution	X	X	X	X	X		X	X
Woodworking mill	X	X	X		X		X	
Zoological park	X	X	X	X	X	X		

- Existing Uses and Buildings: A use located within a station area legally existing at the time that this zoning district was adopted, but listed as a prohibited use in this subsection F, shall be considered a legal nonconforming use. A structure legally existing at the time this section was adopted, but not conforming to the standards in this chapter, shall be considered a legal noncomplying structure. Any legal nonconforming use or legal noncomplying structure is subject to chapter 21A.38 of this title.

G. Development Standards:

- Intent: The purpose of the following development standards is to promote an intense and efficient use of land at increased densities in the station areas. The development standards are intended to create a safe and pleasant environment near transit stations by encouraging an intensive area of mixed use development and activities, pedestrian amenities and by limiting conflicts between vehicles and pedestrians. Development standards are intended to create a reasonably continuous building edge that defines the exterior spatial enclosure of the street or open space and protect adjacent low density residential zoning districts. With some exceptions, buildings line a street at or near the public right of way to the greatest extent possible.
- Application: The dimensional requirements of this section apply to all new buildings and developments as well as additions to existing buildings. The following development standards apply to the core and transition areas of all station types:

- a. **Building Height:** The minimum and maximum building heights are found in table 21A.26.070.G.a Building Height Regulations. Height limits are intended to control the overall scale of buildings, the compatibility with adjacent development, and the composition of the urban form of the block. Minimum building heights in the core area relate to the width of the street, with a minimum ratio of one foot (1') of building height for every three feet (3') of street width. Building height is measured from the finished grade to the highest point of the building. The following exceptions apply:
- (1) The minimum building height applies to all structures that are adjacent to a public or private street. The building shall meet the minimum building height for at least 10% of the width of the street facing building wall.
 - (2) Elevator shafts, parapet walls, and other projections are permitted subject to subsection 21A.36.020C, "Height Exceptions", of this title.
 - (3) Projects that achieve a development score that qualifies for administrative review are eligible for an increase in height. The increase shall be limited to one story of inhabitable space. The height of the additional story shall be equal to or less than the average height of the other stories in the building.

TABLE 21A.28.078.G.2.a BUILDING HEIGHT REGULATIONS

	Minimum Height ¹	Maximum Height
Urban center		
Core	30'	90' ²
Transition	25'	60'
Urban neighborhood		
Core	25'	75'
Transition	0'	50'
Mixed use employment center		
Core	25'	75'
Transition	0'	60'
Special Purpose		

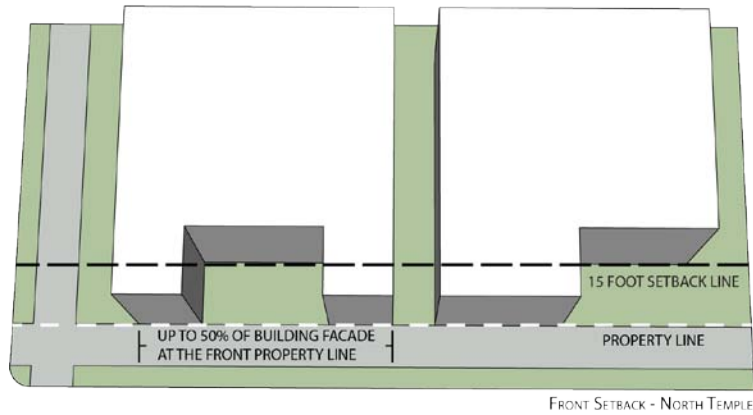
	Core	25'	75'
	Transition	0'	60'

1. *Minimum building heights apply to those properties with frontage on the street where fixed rail transit is located.*
2. *Buildings with a roof that has at least 2 sloping planes may be allowed up to 105 feet. The additional height may include habitable space.*

b. **Setbacks:** Required building setbacks promote streetscapes that are consistent with the desired character of the street and various station typologies and its core and transition areas. Building setbacks create a safe environment that is inviting to pedestrians and transit users and maintain light, air and potential privacy for adjacent residential uses. In some instances, the setbacks limit the building envelope where the existing development pattern would be negatively impacted by taller buildings.

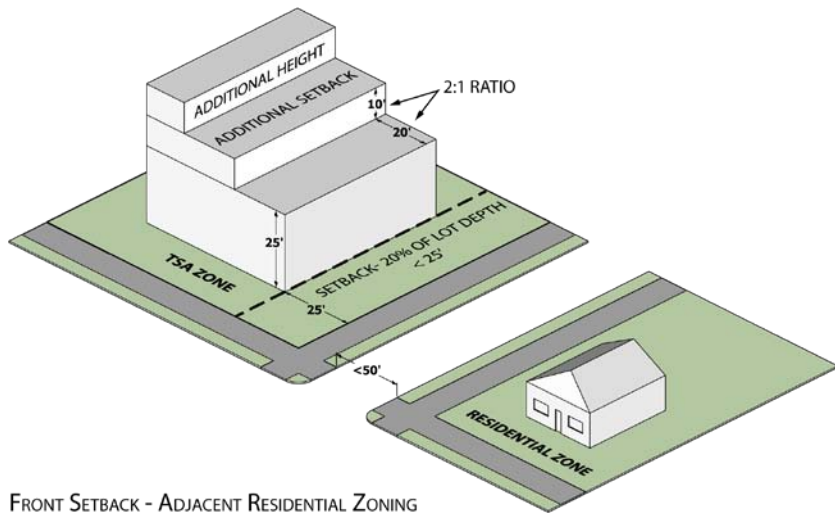
(1) **Front and Corner Yard Setback:** Except as indicated below, there is no minimum setback. If a setback is provided, at least 50% of the street facing building façade shall be located within 5 feet of the front property line unless a larger setback is required below. All portions of a front yard not occupied by building, driveways, walkways or other similar feature must be completely landscaped or include an active outdoor use, such as outdoor dining, plazas or other similar outdoor use with the space not dedicated to active outdoor use completely landscaped. Parking, drive aisles or other paved areas for motor vehicles are prohibited. Walls up to three feet (3') in height, patios and other similar elements intended to activate the sidewalk can be located to the property line.

(A) **North Temple Boulevard:** The front yard setback along North Temple Boulevard shall be fifteen feet (15') for a minimum of fifty percent (50%) of the width of the street facing facade. Up to fifty percent (50%) of a street facing facade may encroach up to the front property line. In this case, the area not occupied by the building footprint must be landscaped or include active outdoor use, such as outdoor dining, plazas, courtyards or other similar usable public space or use. Setbacks over fifteen feet (15') are not allowed. In locations where there is not a minimum sidewalk width of 10 (ten) feet, additional sidewalk width shall be installed by the developer so there is a minimum width of 10 feet when a new building is constructed or with additions that increase the gross building square footage by more than 50%.



(B) 400 South/University Boulevard: The front yard setback along 400 South/University Boulevard shall be a minimum of fifteen feet (15'). In locations where there is not a minimum sidewalk width of ten (10) feet, additional sidewalk width shall be installed by the developer so there is a minimum width of ten feet (10') when a new building is constructed or with additions that increase the gross building square footage by more than 50%.

(C) Streets with a Right of Way of 50 feet or less: When located on a street with a right of way 50 feet or less with an R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district on either side of the street, a minimum setback of 25% of the lot depth but no more than twenty five feet (25') is required. For buildings taller than twenty five feet (25'), the setback shall increase two feet (2') for every foot of height above twenty five feet (25') in height. Buildings may be stepped so taller portions of the building are farther away from the front property line.



(2) Side Yard Setback:

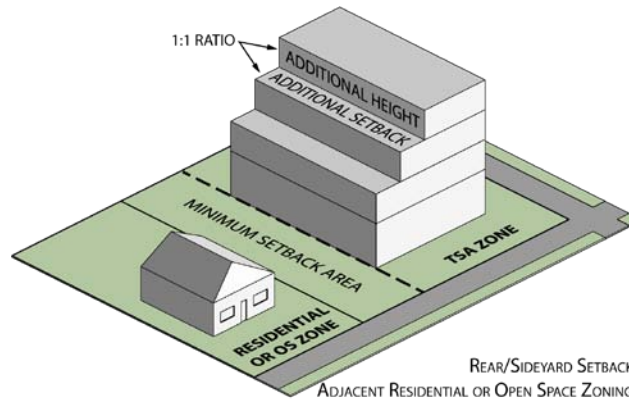
(A) Drive aisles are allowed in the side yard setback. In the transition subarea, parking is allowed in the side yard subject to subsection L of this section.

(B) Side yard setback when adjacent to a certain zoning districts:

(i) Core Area: A minimum of twenty five feet (25') in the core area. When adjacent to an OS,R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district, the minimum side yard setback shall be increased one foot for every one foot increase in height above twenty five feet (25') . When a property in an R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district is separated from a TSA zoned property by an alley, the additional setback for height above twenty five feet (25') applies and shall be measured from the property line of the TSA zoned property. Buildings may be stepped so taller portions of a building are farther away from the side property line. The horizontal measurement of the step shall be equal to the vertical measurement of the taller portion of the building.

(ii) Transition Area: A minimum of fifteen feet (15') in the transition area. When adjacent to an OS, R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district, the minimum side yard setback shall be increased one foot for every one foot increase in height above fifteen five feet (15') . When a property in an OS,R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district is separated from a TSA zoned property by an alley, the additional setback for height above fifteen five feet (15') applies and shall be measured from the property line of the TSA zoned property. Buildings may be stepped so taller portions of a building are farther away from the rear property line. The horizontal measurement of the step shall be equal to the vertical measurement of the taller portion of the building.

(iii) Side yard setback when adjacent to other uses or districts: No minimum side yard required.



(3)Rear Yard Setback:

(A) Core area: A minimum of fifteen feet (25'). When adjacent to an OS, R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district, the minimum rear yard setback shall be increased one foot for every one foot increase in height above twenty five feet (25'). When a property in an OS, R-1, R-2, SR, RMF-30 RMF-35 or RMF-45 zoning district is separated from a TSA zoned property by an alley, the additional setback for height above twenty five feet (25') applies and shall be measured from the property line of the TSA zoned property. Buildings may be stepped so taller portions of a building are farther away from the rear property line. The horizontal measurement of the step shall be equal to the vertical measurement of the taller portion of the building.

(B) Transition area: A minimum of twenty five feet (25'). When adjacent to an OS, R-1, R-2, SR, RMF-30 or RMF-35 zoning district, the minimum rear yard setback shall be increased one foot for every one foot increase in height above twenty five feet (25'). When a property in an OS, R-1, R-2, SR, RMF-30 or RMF-35 zoning district is separated from a TSA zoned property by an alley, the additional setback for height above twenty five feet (25') applies and shall be measured from the property line of the TSA zoned property. Buildings may be stepped so taller portions of a building are farther away from the rear property line. The horizontal measurement of the step shall be equal to the vertical measurement of the taller portion of the building.

(4) Special Setback Provisions for Properties Adjacent to Jordan River: For properties that are adjacent to the Jordan River, the building setback from the Jordan River shall be fifty feet (50'), measured from the annual high water line as defined in section 21A.34.130 of this title. For buildings over fifty feet (50') in height, the setback shall increase one foot (1') for every foot in height over fifty feet (50') up to a maximum of seventy five feet (75'). Portions of buildings over fifty feet (50') in height may be stepped back to comply with this standard.

c. Minimum Lot Area and street frontage requirements:

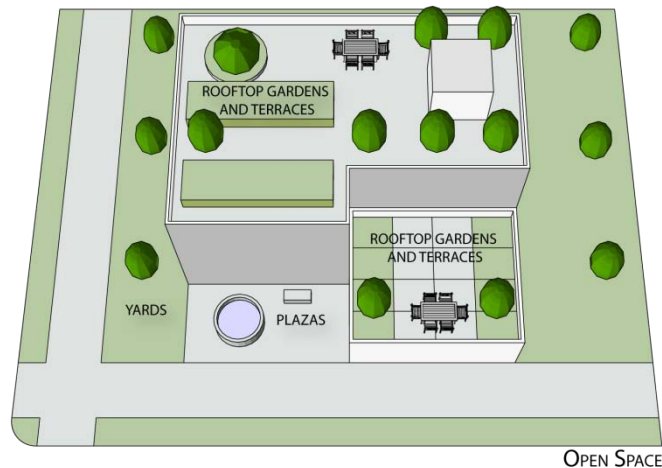
- (1) The minimum lot area applies to all new subdivisions of land and shall not be used to calculate residential density.
- (2) Any legally existing lot may be developed without having to comply with the minimum lot size requirements.
- (3) The minimum lot area for all areas of the TSA zoning district is 2,500 square feet.

- (4) All subdivisions of land or combination of parcels must have a minimum of 40 feet of street frontage.
- d. Open Space: In order to provide space for passive and active recreation, public and private use, offset storm drainage due to nonpermeable surfaces and as an amenity to individual developments and their residents, employees and customers, open space is required for all new developments.

(1) Core Area:

(A) Within the core area, open space may include landscaped yards, patios, public plazas, pocket parks, courtyards, rooftop and terrace gardens and other similar types of open space amenity.

(B) A minimum of ten percent (10%) of the land area up to five thousand (5,000) square feet.



(2) Transition Area:

(A) Within the transition area, open space may include landscaped yards, patios, public plazas, pocket parks, courtyards, rooftop gardens and terraces, community gardens and other similar types of amenities.

(B) The minimum open space requirement is ten percent (10%) of the land area up to two thousand five hundred (2,500) square feet.

(3) Access to Open Space: All required open space shall be accessible to the users of the building(s).

- e. Circulation and Connectivity: Development within the station area shall be easily accessible from public spaces and provide safe and efficient options for all modes of travel. Circulation networks, whether public or private, require adequate street, pedestrian and bicycle connections to provide access to development. The

internal circulation network shall be easily recognizable, formalized and interconnected.

(1) All parking lots shall comply with the standards in section 21A.44.020, “General Off Street Parking Requirements”, of this title.

(2) Parking is prohibited between the street facing building line and any front or corner side property line. This shall include any drive aisle that is not perpendicular to the front or corner side property line.

H. Residential Densities:

1. Core area: No maximum.

2. Transition area: No maximum.

I. Accessory Structures: No accessory structure shall be located in a required front yard or between the primary building and a property line adjacent to a public street.

J. Design Standards:

1. Purpose: Design standards create the fundamental characteristics of a transit oriented district and the basic design elements required for a successful transit station area. Design standards are intended to provide a safe and interesting walkable environment by connecting ground floor uses adjacent to the sidewalk areas, by encouraging the continuity of retail and service uses, providing surveillance opportunities on the street and public open spaces and framing the street by bringing portions of buildings up to the sidewalk. All buildings shall be designed for the context and character of the project and how they interact visually, functionally, and socially with the context of the public environment.

2. Application: The following design standards apply to all projects within the core and transition areas of all station area types:

a. Building Walls Adjacent to a Street: Street-facing building facades shall provide architectural variety and scale. Changes in plane, color, texture, materials, scale of materials, patterns, art, or other architectural detailing are acceptable methods to create variety and scale. Building walls parallel to a public street and greater than thirty feet (30’) in length shall be broken up by architectural features such as bay windows, recessed entrances or windows, balconies, cornices, columns, or other similar architectural features. The architectural feature may be either recessed or project a minimum of twelve inches (12”).



ARCHITECTURAL VARIETY AND SCALE

- b. **Ground Floor Building Materials:** Other than ground windows and doors, eighty percent (80%) of the remaining ground floor wall area shall be clad in durable materials. Durable materials include brick, masonry, textured or patterned concrete and/or cut stone. Other materials may be used as accent or trim provided they cover twenty percent (20%) or less of the ground floor adjacent to a street.
- c. **Ground Floor Glass and Transparency:** All street-facing elevations of a development shall be designed so that the first floor street-facing facade has at least sixty percent (60%) clear glass between three (3) and eight feet (8') above grade to allow pedestrians to view activities inside the building or lighted display windows. There must be visual clearance behind the glass for a minimum of two feet (2'). Three-dimensional display windows at least two feet (2') deep are permitted and may be counted toward the sixty percent (60%) glass requirement. Ground floor windows of commercial uses shall be kept clear at night, free from any window covering, with internal illumination. When ground floor glass conflicts with the internal function of the building, other means shall be used to activate the sidewalk, such as display windows, public art, architectural ornamentation or detailing or other similar treatment. The first floor elevation facing a street of all new buildings, or buildings in which the property owner is modifying the size of windows on the front facade, shall not have less than sixty percent (60%) glass surfaces. The reflectivity in glass shall be limited to eighteen percent (18%) as defined by ASTA standards. The planning director may approve a modification to this requirement if the planning director finds:
 - (1) The requirement would negatively affect the historic character of the building,
or
 - (2) The requirement would negatively affect the structural stability of the building.
 - (3) The ground level of the building is occupied by residential uses, in which case the sixty percent (60%) glass requirement may be reduced to forty percent

(40%).

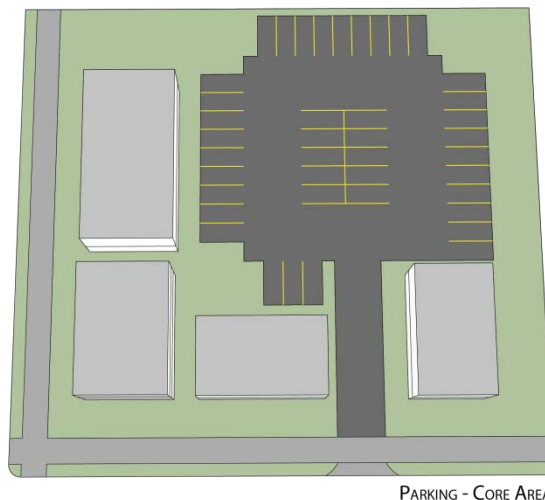
Any appeal of an administrative decision made pursuant to this subsection may be made to the planning commission.

- d. **Building Entrances:** The intent of regulating building entrances is to promote security on the street and public spaces by providing frequent points of access and sources of activity. Functional entrances to nonresidential uses should be located at an average of seventy five feet (75') or less from one another. At least one operable building entrance per elevation facing a public street shall be provided. Each ground floor leasable space is required to have an operable entrance facing the street and a walkway to the sidewalk. If a plaza or open space is provided as part of the development, a minimum of one entrance opening onto the plaza or open space shall be provided. This entrance shall be counted towards the spacing of functional entrances identified in this section and may count as the primary entrance to the building. All street facing building entrances shall be functional entrances and shall not be limited to emergency or employee entrances.
- e. **Ground Floor Residential Uses:** The interior floor elevation of ground floor residential units in the core area shall be a minimum of two feet (2') and a maximum of five feet (5') above grade. Dwelling units located on the ground floor and facing a public or private street shall have a minimum of one primary entrance facing the street in the core area. The facades of all buildings in the core and transition areas with ground floor residential uses shall feature elements that signal habitation such as windows, entrances, stairs, porches, bay windows, and balconies that are visible from the public street. Attached single-family dwellings, townhomes, row houses, and other similar housing types shall have a primary entrance facing the street for each unit adjacent to a street. Units may have their primary entrance located on a courtyard, midblock walkway, or other similar area if the street facing facades have a primary entrance facing the street.
- f. **Parking Structures:** The intent of regulating parking structures is to minimize the visual impact of the structure and the cars parking within it, and to reduce their impact on the ground floors adjacent to public sidewalks and streets. Parking structures are permitted within the core and transition areas provided:
 - (1) The ground floor of parking structures adjacent to a public street shall include an active use other than parking such as office, retail, residential leasing office, restaurant, etc. Parking is permitted behind the ground floor uses. If the ground floor does not include active use, then the structure must be set back behind a building or be a minimum of sixty feet (60') from a property line adjacent to a public street or sidewalk.
 - (2) The levels of parking above the first level facing the front or corner side lot line shall have horizontal floors and/or facades and not sloped.

- (3) The levels of parking above the second level shall be designed to effectively screen the vehicles so they are not readily visible from an adjacent street.
 - (4) Below grade parking structures for structures with ground floor residential uses may extend a maximum of five feet (5') above the existing grade provided the above grade portion is screened with vegetation or architectural feature(s).
- g. **Mechanical Equipment:** All roof mounted mechanical and electrical equipment, communication antennas or dishes shall be enclosed, screened, organized, designed and located to be out of view from streets and public spaces. The parapet or enclosure shall be equal to or greater than the height of the equipment to be screened to reduce equipment noise and odors, and other impacts onto adjacent uses and maintain the integrity of overall architectural character and scale of the building. Mechanical equipment may be located on the ground provided it is behind the building, screened and not located in a required rear yard or side yard setback. Utility boxes are subject to section 21A.40.160, "Ground Mounted Utility Boxes", of this title.
- h. **Service Areas:** Service areas, loading docks, refuse containers and similar areas shall be fully screened from public view. All screening enclosures viewable from the street shall be either incorporated into the building architecture or shall incorporate building materials and detailing compatible with the building being served. All screening devices shall be a minimum of one foot (1') higher than the object being screened. Dumpsters must be located a minimum of twenty five feet (25') from any building on an adjacent lot that contains a residential dwelling or be located inside of an enclosed building.
- K. **Multiple Buildings on a Single Parcel:** Multiple principal buildings on a single parcel are permitted provided each principal building meets the requirements of this chapter and each principal building obtained a separate development score. New principal buildings can be located towards the rear of a parcel provided there is an existing or additional new principal building that complies with the front yard building setbacks. If one principal building receives a development score lower than other principal buildings on the site, the project shall be processed based on the lowest development score obtained.
- L. **Parking:** The purpose of this subsection is to provide locations for off street parking. All off street surface parking lots should be located so that they are compatible with pedestrian oriented streets. New uses and development or redevelopment within this district shall comply with the requirements of this subsection.
1. **Surface Parking Lots and Structures on Corner Properties:** On corner properties, surface parking lots and structures shall be located behind principal buildings or at least sixty feet (60') from a front and corner side lot lines. Only one driveway and drive aisle is permitted per street frontage and the access point shall be located a minimum of 100 feet from the intersection of the front and corner side property lines.

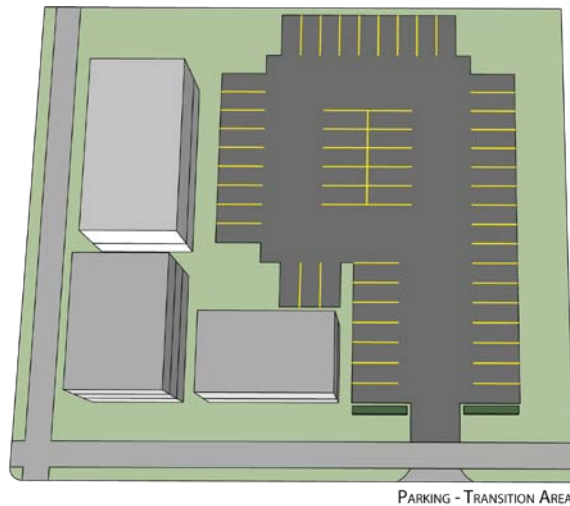
If the front or corner side property line is less than 100 feet in length, than the drive approach shall be located within 20 feet of the side or rear property line.

2. Surface Parking in the Core Area: Surface parking lots in the core area are required to be located behind the principal building or to the side of a principal building. When located to the side of a building, the parking lot shall be:
 - a. Set back a minimum of thirty feet (30') from a property line adjacent to a public street. The space between the parking lot and the property line adjacent to a public street shall be landscaped or activated with outdoor dining, plazas or similar feature.
 - b. Screened with a landscaped hedge or wall that is at least thirty six inches (36") above grade and no taller than forty two inches (42") above grade. Landscaping berms are not permitted.
 - c. The parking lot shall be no wider than what is required for two rows of parking and one drive aisle as indicated in table 21A.44.020 of this title.
 - d. Only one driveway and drive aisle is permitted per street frontage and the access point shall be located a minimum of 100 feet from the intersection of the front and corner side property lines. If the front or corner side property line is less than 100 feet in length, than the drive approach shall be located within 20 feet of the side or rear property line.
 - e. Surface parking lots as a principal use: Surface parking lots as a principal use located on a lot that has frontage on a public street are prohibited.



3. Surface Parking in the Transition Area: Surface parking lots in the transition area are required to be located behind the principal building or to the side of a principal building. When located to the side of a principal building, the parking lot shall be:

- a. Set back so that no portion of the parking area other than the driveway is closer to the street than the front wall setback of the building. In cases where the front wall of the building is located within five feet (5') of a property line adjacent to a street, the parking lot shall be set back a minimum of eight feet (8'). The space between the parking lot and the property line adjacent to a street shall be landscaped or activated with outdoor dining, plazas or similar feature.
- b. Screened with a landscaped hedge or wall that is at least thirty six inches (36") above grade and no taller than forty two inches (42") above grade. Landscaping berms are not permitted.
- c. Surface parking lots as a principal use: Surface parking lots as a principal use located on a lot that has frontage on a public street are prohibited.



4. Walkways Through Parking Lots: Parking lots with more than fifteen (15) stalls shall provide a pedestrian walkway through the parking lot to the primary building entrance or a sidewalk providing access to a primary building entrance. One walkway must be provided for every three (3) drive aisles. Walkways shall be curb separated from the parking areas and a minimum of five feet (5') wide. Vehicles shall not overhang the walkway. Parking lot landscaping requirements in chapter 21A.48 of this title shall be included on the side of the walkway. Where the walkway crosses a drive aisle, a crosswalk that is clearly identified by a change in color, material, or similar technique shall be used.
5. Surface Parking Lots as the Principal Use: Surface parking lots that are the principal use are permitted in the TSA zoning district provided the following standards are complied with:
 - a. The surface parking lot does not have street frontage on the street where the fixed rail transit is located.
 - b. The surface parking lot is setback a minimum of 15 feet from any property line

adjacent to a public street.

- c. The parking area is screened by a wall or landscaping that is a minimum of 36 inches and a maximum of 48 inches tall.
6. Other Applicable Standards: All other standards in chapter 21A.44, “Off Street Parking And Loading”, of this title shall apply.
- M. Conflicting Regulations: In cases where the regulations of this section conflict with another section of this zoning ordinance, this section shall take precedence except in situations where the conflict is related to the use of the property, in which case the more restrictive regulation takes precedence. In station areas within an overlay district, the overlay district shall take precedence.
- N. Developments Over Five Acres:
1. Intent: Large scale developments have the potential to function as a self-contained mixed use neighborhood and could have both positive and negative impacts on nearby properties. All developments over five (5) acres in size shall be designed and planned to include a series of blocks and a network of public or private streets that connects to the existing public streets in the area and to adjacent development and neighborhoods. Buildings should be oriented to this street network. Regulating block size is necessary to provide development sites that are oriented to the pedestrian while accommodating other modes of transportation. A street network is required to ensure adequate circulation for pedestrians, bicycles, automobiles and service vehicles through the site, to adjacent sites and the public streets.
 2. Application: These standards are in addition to all other applicable standards. In situations where the standards in this section conflict with a standard in another section, the standard in this section shall take precedence. A separate development score is required for each new principal building in a development over five (5) acres. A development over five (5) acres shall be subject to the applicable review process based on the lowest development score assigned to an individual building in the development.
 - a. Block Layout: The intent of regulating block size and dimension is to create a development pattern where all principal buildings have their primary facades facing a street, whether public or private. All developments over five (5) acres in size shall be designed to include a series of blocks based on the standards below:
 - (1) The maximum perimeter dimension of any block shall be one thousand six hundred feet (1,600’). The maximum length of any individual block face shall be four hundred forty feet (440’).
 - (2) The maximum perimeter dimension of a block may be increased to two thousand four hundred (2,400) linear feet, and the maximum length of any

block face increased to six hundred feet (600') provided a mid block pedestrian network is included. The mid block pedestrian network must be a minimum of twenty feet (20') wide and include pedestrian amenities such as lighting, benches, and other similar features. The mid block walkway shall connect to at least two (2) block faces or be extended to the property line to allow for future extension. The standards in subsection J2 of this section apply to building walls adjacent to a mid block walkway.

- b. **Housing Proximity to Transit:** Developments that include housing should cluster the housing so a minimum of fifty percent (50%) of the housing is located within one-fourth (1/4) mile walking distance of a transit platform.
- c. **Connectivity to Public Streets, Sidewalks, and Bicycle Lanes:** In order to ensure that the development will be fully integrated into the transit station area, that safe and efficient travelways are provided, and to limit the impact on the primary transit street and other adjacent streets, the internal circulation system, including private streets, drive aisles, sidewalks and bicycle lanes shall connect to the public street, sidewalks and bicycle lanes. All new streets shall be designed as a "complete street" defined as a street that provides dedicated space for pedestrians, bicyclists and automobiles.
- d. **Vehicle Access: Regulating access to private property from public streets** is necessary for integrating private development and public spaces. Limiting the number of access points and spacing between access points reduces areas of conflict between vehicles, pedestrians and bicycles. Maximum access widths promote a development pattern that is oriented to pedestrians and bicyclists while accommodating vehicles.
 - (1) Access points located on public streets intended for vehicles shall be spaced a minimum of one hundred feet (100') apart.
 - (2) No property shall have more than one vehicle access point for every two hundred (200) linear feet of frontage on a public street.
 - (3) No access drive shall be greater than twenty four feet (24') wide.
 - (4) The location of all vehicle access points is subject to approval from the transportation division of the city. The standards of this section may be modified by the transportation division when, in the opinion of the director of the transportation division, a different design would improve the overall safety for all modes of transportation or improve the efficiency of the transportation network.
- e. **Internal Circulation:** Internal circulation systems allow for vehicles, pedestrians and bicyclists to move safely and efficiently throughout a development site. A logical, simple and well designed internal circulation system that connects with adjacent circulation networks provides room for vehicles, safe walking paths for pedestrians through the parking lot and the site to the public way, and well

marked routes for bicycles traveling from public spaces to bicycle parking areas within a site. All new developments over five (5) acres are required to submit an internal circulation network plan.

- (1) **Travel Lanes That Connect Parking Areas with a Public Street:** All internal vehicle travel lanes that connect internal parking areas with a public street shall be designed to meet the minimum requirements in section 21A.44.020 of this title.
 - (2) **Design Speed:** The internal circulation system shall be designed to move vehicles at speeds of twenty (20) miles per hour or less.
 - (3) **Future Access to Adjacent Properties and Rights of Way:** All internal drive aisles, sidewalks, and paths shall be extended to property lines to allow for future cross access to adjacent properties when the adjacent property is undeveloped and to rights of way.
 - (4) **Centerlines:** The centerline of all internal streets shall be in line with the centerline of a street on the opposite side of an intersecting street unless the intersecting street is divided by a median. Offset streets shall be a minimum of two hundred feet (200') apart, measured from centerline to centerline.
 - (5) **Publicly Dedicated Streets:** Any street that is to be publicly dedicated shall meet the city's minimum construction and design standards (including street lighting, park strip, street trees, etc.).
 - (6) **Pedestrian Routes:** Pedestrian routes that provide safe, comfortable, clear and direct access throughout the development shall be provided. Pedestrian paths shall be bordered by residential fronts, green space, active open space, or commercial store fronts.
 - (7) **Bicycle Paths:** A coordinated system of bicycle paths should be provided.
 - (8) **Approval; Modification of Standards:** The internal circulation network is subject to approval from the transportation division of the city. The standards of this section may be modified by the transportation division when, in the opinion of the director of the transportation division, a different design would improve the overall safety for all modes of transportation or improve the efficiency of the transportation network.
- f. **Parking:** Parking may be provided along any private street within a development over five (5) acres. The parking shall be counted towards the applicable off street parking standard when provided on private streets. All parking areas and stalls must comply with the parking lane widths identified in table 21A.44.020 of this title.

g. Open Space: In order to provide space for passive and active recreation, public and private gatherings, offset storm drainage due to nonpermeable surfaces and as an amenity to individual developments and their residents, employees and customers, usable open space is required for all new developments.

(1) Required: In the core and transition areas of all station areas, a minimum of ten percent (10%) of the site, up to fifteen thousand (15,000) square feet, shall be devoted to open space. "Usable open space" is defined as landscaped areas, plazas, outdoor dining areas, terraces, rooftop gardens, stormwater retention areas, and any other similar type of area.

(2) Connectivity to Adjacent Open Space: When adjacent to public open space, parks, trails and pathways, open space on developments over five (5) acres in size are encouraged to provide access to the public open space.

h. Landscaping: All areas not occupied by buildings, plazas, terraces, patios, parking areas, or other similar feature shall be landscaped. If a project is developed in phases, only those areas in a phase that is under construction shall be landscaped. Landscaping in future phases shall be installed as those phases develop. Areas in future phases may be used as community gardens or other active open space until such time as development of that phase begins.

SECTION 4. Effective Date. This ordinance shall become effective on the date of its first publication.

Passed by the City Council of Salt Lake City, Utah, this _____ day of _____, 2012.

CHAIRPERSON

ATTEST AND COUNTERSIGN:

CITY RECORDER

Transmitted to Mayor on _____.

Mayor's Action: _____Approved. _____Vetoed.

MAYOR

CITY RECORDER
(SEAL)

Bill No. _____ of 2012.
Published: _____.

APPROVED AS TO FORM
Salt Lake City Attorney's Office

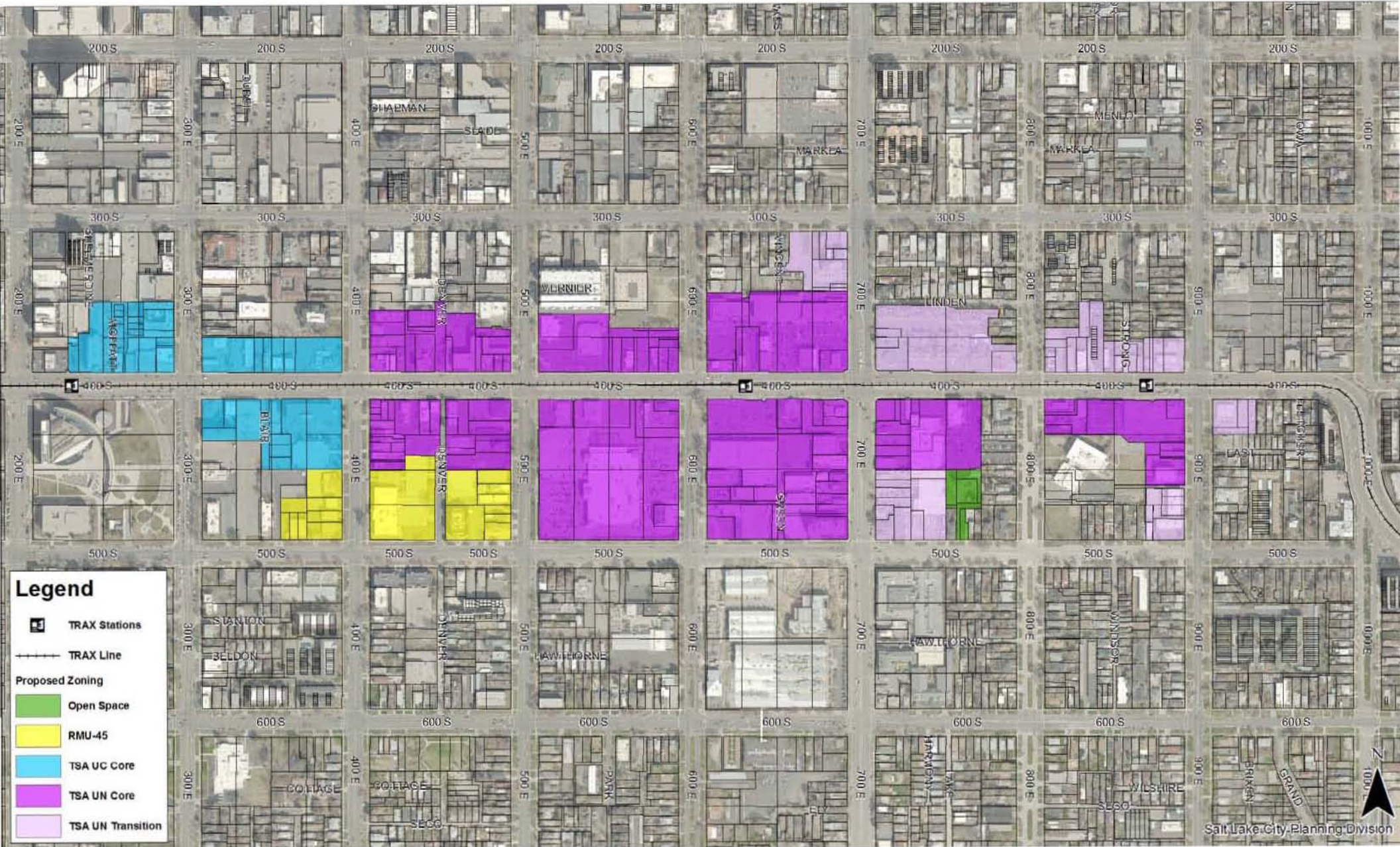
Date: _____

By: _____
Paul C. Nielson, *Senior City Attorney*

Exhibit A - 400 South Current Zoning



Exhibit A - 400 South Proposed Zoning



3. NOTICE OF CITY COUNCIL HEARING

NOTICE OF PUBLIC HEARING

The Salt Lake City Council will hold a public hearing regarding Petition PLNPCM2010-00647 a request by Salt Lake City Mayor Ralph Becker for an amendment to the Central Community Master Plan, Salt Lake City Zoning Ordinance and Zoning Map regarding transit station area plans along the 400 South transit corridor. The intent of the project is to implement livability goals for the corridor including land use policies and zoning that support mixed use development and transportation choices for current and future residents and workers. The project is located in City Council District 4 represented by Luke Garrott.

As part of its study, the City Council is holding an advertised public hearing to receive comments regarding the petition. During this hearing, anyone desiring to address the City Council concerning this issue will be given an opportunity to speak. The hearing will be held:

Date:
Time: 7:00 p.m.
Place: Room 315 (City Council Chambers)*
Salt Lake City and County Building
451 S. State Street
Salt Lake City, UT

*Please enter building from east side.

If you have any questions relating to this proposal or would like to review the petition on file, please contact Maryann Pickering, Principal Planner, at (801) 535-7660 between the hours of 8:00 a.m. and 5:00 p.m., Monday through Friday or via e-mail at maryann.pickering@slcgov.com.

People with disabilities may make requests for reasonable accommodations no later than 48 hours in advance in order to attend this public hearing. Accommodations may include alternate formats, interpreters, and other auxiliary aids. The City & County Building is an accessible facility. For questions, requests, or additional information, please contact the City Council Office at (801) 535-7600, or TDD (801) 535-6021.

4. MAILING LABELS

1415 SOUTH MAIN STREET LLC
1415 S MAIN ST
SALT LAKE CITY, UT 84115-5313

908DAU LLC
126 E BRAEWICK RD
SALT LAKE CITY, UT 84103-2201

9TH EAST PROPERTIES LLC
322 S 900 E
Salt Lake City, UT 84102

ABOELSEUD, ABDELAZIZ
721 E LINDEN AVE
SALT LAKE CITY, UT 84102-2210

ABRAMS, SAASHA S & STAMPER, SEAN M;
JT
342 S STRONGS CT
SALT LAKE CITY, UT 84102-2303

ADAMS, SUANN P; TR
325 N FEDERAL HEIGHTS CIR
SALT LAKE CITY, UT 84103-4440

AFTON OPTICAL
250 E 300 S #300
Salt Lake City, UT 84111

AI SLC 500 SOUTH PROPCO, LLC
PO BOX 17227
SALT LAKE CITY, UT 84117-0227

AIAZZI, MIRIAM
3961 S 565 E
MURRAY, UT 84107

ALL AGES HEALTH CENTER
850 E 300 S #4
Salt Lake City, UT 84102

ALMOND DENTAL
850 E 300 S #7
Salt Lake City, UT 84102

ALMOND INVESTMENTS LLC
10852 S PINE SHADOW RD
SOUTH JORDAN, UT 84095-3951

ALMOST 4TH & 4TH, LLC
170 S MAIN ST #1500
SALT LAKE CITY, UT 84101-0477

AMANO, GARY & SMILEY; JT
1523 E 1260 N
LOGAN, UT 84341-2846

AMBEL LLC
2159 S 700 E #200
SALT LAKE CITY, UT 84106-1227

AMERICA FIRST CREDIT UNION
455 E 500 S #100
Salt Lake City, UT 84111

AMERICAN APPAREL RETAIL INC
602 E 500 S #109
Salt Lake City, UT 84102

ANATHAN, SUNDAR S
1104 E 600 S
SALT LAKE CITY, UT 84102-3808

ANDERSON INVESTMENT CORP
5455 W 11000 N #202
AMERICAN FORK, UT 84003-8802

ANDERSON, DANIEL L & MARCIA H; JT
676 E MARKEA AVE #8
SALT LAKE CITY, UT 84102

ANDERSON, LEE & MARTA D; JT
922 E EAST PL
SALT LAKE CITY, UT 84102

ANDERSON, NYAL W & DONNA L (JT)
368 E 300 S
SALT LAKE CITY, UT 84111

ANNIVERSARY INN
460 S 1000 E
Salt Lake City, UT 84102

APARTMENT HOUSE
346 S 800 E
Salt Lake City, UT 84102

APRIL VAN ORMAN
440 E 300 S #25
Salt Lake City, UT 84111

ARCOM ARCHIT COMPUTER SRVC INC
332 E 500 S #200
Salt Lake City, UT 84111

ARNELL, DARIN D
4503 W 3285 S
WEST VALLEY, UT 84120-1535

ASHLEY'S PLACE, LLC
530 S 400 E #2307
Salt Lake City, UT 84111

ASSOCIATION OF UNIT OWNERS OF
INCLINE TERRACE CONDOMINIUM
1032 E 400 S #105-B
SALT LAKE CITY, UT 84102

ASTIN, MARY A & ODETTE, MICHELLE; JT
2046 E VILLAGE POINT WY
SANDY, UT 84093-2500

B&Z ENTERPRISES LLC
406 E 300 S #138
Salt Lake City, UT 84102

BALOGH, ED & WISMER, SHELLEY K; JT
1343 S 900 E
SALT LAKE CITY, UT 84105-2301

BARBER, BRIAN K
4037 SEQUOYAH AVE
KNOXVILLE, TN 37919-8352

BARDON ASSOCIATES
323 S 600 E #260
Salt Lake City, UT 84102

BARRANI PROPERTIES
328 S 800 E
Salt Lake City, UT 84102

BARRANI, KADRI O
PO BOX 581432
SALT LAKE CITY, UT 84158-1432

BEEVER, TERRENCE L
656 E MARKEA AVE
SALT LAKE CITY, UT 84102-2108

BELLAORA STUDIOS
444 S 800 E #1
Salt Lake City, UT 84102

BEMIS, BROCK R & LESLIE H; JT
420 S FLETCHER CT
SALT LAKE CITY, UT 84102-3022

BENCH, STEVEN R
432 S FLETCHER CT
SALT LAKE CITY, UT 84102-3022

BERG, GARY D & SUZANNE G; JT
1850 E 10980 S
SANDY, UT 84092-4732

BIDDULPH, ROMNEY & BETTY; JT
8177 S FRANKLIN CT
CENTENNIAL, CO 80122

BIG HORN MOUNTAIN PROPERTY
MANAGEMENT CO, LLC
21520 G YORBALINDA BLVD
YORBA LINDA, CA 92887-3232

BIG HORN PROPERTY MANAGEMENT CO
LLC
440 E 300 S
Salt Lake City, UT 84111

CASA BELLA
602 E 500 S #126A
Salt Lake City, UT 84102

CASH MONEY ENTERTAINMENT
425 S 900 E #1
Salt Lake City, UT 84102

BINGHAM, SKYLERE
415 S FLETCHER CT
SALT LAKE CITY, UT 84102-3022

BIR TOK MANAGEMENT, LLC
15709 HIGHLAND DR
SAN JOSE, CA 95127

BLACK HAUS HOLDINGS LLC
2052 E CORRIE ANN CIR
SANDY, UT 84093-6928

BLAIR HOUSE APARTMENT
947 E 400 S
Salt Lake City, UT 84102

BLEAZARD, KATHY
329 S VINCENT CT
SALT LAKE CITY, UT 84102-2109

BLEAZARD, WILLIAM J
630 E 300 S
SALT LAKE CITY, UT 84102-2104

BLUE PUZZLE PRODUCTIONS
339 E 600 S #1111
Salt Lake City, UT 84111

BOLO CORPORATION
3556 S 5600 W #121
WEST VALLEY, UT 84120

BOREGINO, MARY E
929 E EAST PL
SALT LAKE CITY, UT 84102

BRASS KEY ANTIQUES
602 E 500 S #235
Salt Lake City, UT 84102

BREWSTER, HOYT W JR & JUDITH T; TRS
7284 S PROMENADE DR
COTTONWOOD HTS, UT 84121-4057

BRIGHTON REAL ESTATE SERVICES, LLC
350 E 500 S #101
Salt Lake City, UT 84111

BROADWAY APARTMENTS
376 E 300 S
Salt Lake City, UT 84111

BUBBLE AND BEE ORGANIC
602 E 500 S #D211
Salt Lake City, UT 84102

BURGER KING #10544
217 E 400 S
Salt Lake City, UT 84111

BURGESS, ALAN G
1255 E 200 S #9
SALT LAKE CITY, UT 84102-1979

BURWELL, MICHAEL R & D'ANNE P; TC
1121 HOBART STREET
MENLO PARK, CA 94025

BUSCH, NICOLE L
3626 S 7550 W
MAGNA, UT 84044-2493

CALHOUN, CHARLES
923 E 500 S #A
SALT LAKE CITY, UT 84102-3035

CARLSTON, ELEANOR H; TR
426 S 1000 E #808
SALT LAKE CITY, UT 84102-3087

CENTURY PROP INC
730 E 300 S
Salt Lake City, UT 84102

CENTURY PROPERTIES INC
718 E 300 S
Salt Lake City, UT 84102

CH OF LIV GOD PILLAR & PROTECT OF
TRUTH "LIGHT OF WORLD"
360 E 300 S
SALT LAKE CITY, UT 84111

CHARLES, BRENDON S & HARBER WHITNEY
L; JT
439 S FLETCHER CT
SALT LAKE CITY, UT 84102-3022

CHARLIE CHOW'S DRAGON GRILL
255 E 400 S
Salt Lake City, UT 84111

CHILDREN'S CENTER
350 S 400 E
SALT LAKE CITY, UT 84111-2905

CHRISTENSEN, KAY H
925 E EAST PL
SALT LAKE CITY, UT 84102

CITY CENTRE HOLDINGS LLC
595 S RIVERWOODS PKWY
LOGAN, UT 84321

CLAIRMONT APARTMENTS
446 E 300 S
Salt Lake City, UT 84111

CLARK, KIPP H & CARRILLO, TINA; JT
637 E 300 S
SALT LAKE CITY, UT 84102-2103

COLCLOUGH, PATRICK D F
3417 S 300 E
SOUTH SALT LAKE, UT 84115-4507

COMBS, DEANNA
1338 S FOOTHILL DR #223
SALT LAKE CITY, UT 84108-1952

CONTACT BUSINESS COMMUNICATION
350 S 400 E #320
Salt Lake City, UT 84111

COOK, STEPHEN W & LYNDA; TRS
2840 E WILLOW HILLS DR
SANDY, UT 84093-1932

COSTELLO, DAVID K & JOY C; JT
323 S 800 E
SALT LAKE CITY, UT 84102-2207

COUNCIL CREST APARTMENTS
706 E 300 S
Salt Lake City, UT 84102

COUNCIL CREST APARTMENTS
722 E 300 S
Salt Lake City, UT 84102

COUNCIL CREST APARTMENTS
726 E 300 S
Salt Lake City, UT 84102

CRAIG, STUART
426 S 1000 E #404
SALT LAKE CITY, UT 84102-3097

CRUZ, JOHN F
426 S 1000 E #405
SALT LAKE CITY, UT 84102-3097

CUNDIFF, JEANNE
426 S 1000 E #506
SALT LAKE CITY, UT 84102-3085

CURVES SALT LAKE CITY AVENUES
335 S 500 E
Salt Lake City, UT 84102

D & B LLC
444 E 200 S
SALT LAKE CITY, UT 84111-2103

DALEY HOLDING COMPANY, LLC
PO BOX 27422
SALT LAKE CITY, UT 84127-0422

DAVIES, STELLA R.
648 E MARKEA AVE
SALT LAKE CITY, UT 84102-2108

DAYTON, ELIZABETH A
319 S 800 E #4
SALT LAKE CITY, UT 84102-1212

DEMURI, CHRISTOPHER R
1099 S WINDSOR ST
SALT LAKE CITY, UT 84105-1344

DENHALTER, SCOT
426 S 1000 E #301
SALT LAKE CITY, UT 84102-1474

DESIGN PLUS INC
323 S 600 E #100
Salt Lake City, UT 84102

DEWAAL, CLARK L & CANDACE L; JT
310 S 300 E #A2
SALT LAKE CITY, UT 84111-2552

DIVOT LLC
766 E 300 S
Salt Lake City, UT 84102

DIXIE CONSTRUCTION CO INC
327 S DENVER ST
Salt Lake City, UT 84111

DKL PROPERTIES LLC
5985 S HOLLADAY BLVD
HOLLADAY, UT 84121-1504

DNA VENTURES LLC
707 E WALNUT BROOK DR
MURRAY, UT 84107

DOW, REED R
618 E 300 S
SALT LAKE CITY, UT 84102-2104

DUERIG, LINDA K
350 S 900 E
SALT LAKE CITY, UT 84102-2311

EICHBAUER, JILL A
939 E 500 S #B
SALT LAKE CITY, UT 84102

ELLISON, PARKER & MERCER, VICTORIA; JT
808 E 300 S #31
SALT LAKE CITY, UT 84102

ELLSWORTH, JANE E & PURCELL, ROBERT A;
JT
2520 COLUMBIA
COSTA MESA, CA 92626

EMERSON, KEVIN & SIEGER, BEATRIX; JT
310 S 300 E #A4
SALT LAKE CITY, UT 84111-2558

EMIGRATION COURT
343 S 500 E
Salt Lake City, UT 84102

ENTRUST ADMINISTRATION TRUST (FBO
MICHAEL J MCCORMICK IRA)
555 12TH ST #1250
OAKLAND, CA 44144

ETHINGTON, NANCY K
426 S 1000 E #305
SALT LAKE CITY, UT 84102-1474

ETT, ROBERT J
426 S 1000 E #503
SALT LAKE CITY, UT 84102-3097

EVANOFF, PETE F & ARLENE; JT
426 S 1000 E #408
SALT LAKE CITY, UT 84102-3097

EYRE, HARMON J
426 S 1000 E #804
SALT LAKE CITY, UT 84102-3087

FAIRVIEW CARE CENTER EAST
455 S 900 E
Salt Lake City, UT 84102

FANTACYMODELS.COM
343 S 500 E #101
Salt Lake City, UT 84102

FARLEY, SCOTT
649 E 300 S
SALT LAKE CITY, UT 84102-2103

FAUSTINA RESTAURANT LLC
454 E 300 S
SALT LAKE CITY, UT 84111-2630

FEATHERSTONE, WRAY M
958 E 400 S
SALT LAKE CITY, UT 84102-3075

FERRELL, ALEX
334 S 900 E
SALT LAKE CITY, UT 84102-2311

FERRELL, ALEX
PO BOX 13519
ARLINGTON, TX 76094

FERRONE, MARTINE & JOHN; JT
3025 E BANBURY RD
COTTONWOOD HTS, UT 84121-5247

FETZER, GRANT J & SANDRA D; TRS
452 S 800 E
SALT LAKE CITY, UT 84102-2911

FIRST NATIONAL BANK OF LAYTON
480 E 400 S
Salt Lake City, UT 84111

FIRST TRACKS PUBLISHING INC
357 S 200 E #3RD
Salt Lake City, UT 84111

FIVE SACRED THINGS, LLC
140 S MCCLELLAND ST
SALT LAKE CITY, UT 84102-1503

FOOTE, RICHARD G
1556 S 1500 E
SALT LAKE CITY, UT 84105-2819

FORD, JAMES & BYBEE, ARIEL; JT
426 S 1000 E #PH1
SALT LAKE CITY, UT 84102

FORSGREN ASSOCIATES, P.A.
370 E 500 S #200
Salt Lake City, UT 84111

FOWLER, ROBERT A & ALLENE A; TRS
426 S 1000 E #806
SALT LAKE CITY, UT 84102-3087

FOX, KEVIN M & MARSHA; TRS
357 HAZELTINE ST
ONTARIO, CA 91761

FREEMAN, SYDNEY
319 S 800 E #6
SALT LAKE CITY, UT 84102-1212

FRODSHAM REAL ESTATE I, LC
8098 S COTTAGE PINES CV
COTTONWOOD HTS, UT 84121-5984

FULTON, BEN & PLUTYNSKI, ANYA; TC
325 S LAKER CT
SALT LAKE CITY, UT 84102-2322

GARDNER JAMES R
754 E 300 S
Salt Lake City, UT 84102

GARDNER, JAMES R & LYNN F; JT
626 E SIXTH AVE
SALT LAKE CITY, UT 84103-3044

GATEWAY BUSINESS PROPERTIES
323 S 600 E #250
Salt Lake City, UT 84102

GEORGAS, ROSE T; TR (RTGRL TRUST)
2447 E FISHER LN
SALT LAKE CITY, UT 84109-2503

GEORGE SERGAKIS
318 S 600 E
Salt Lake City, UT 84102

GNB PROPERTIES LLC
4210 SUMMER MEADOW DR
BOUNTIFUL, UT 84010

GREEK SOUVLAKI
404 E 300 S
Salt Lake City, UT 84111

GREEN TEA
602 E 500 S #KIOSK
Salt Lake City, UT 84102

GULL POINT PROPERTIES, LLC
653 E 300 S
Salt Lake City, UT 84102

GUTIERREZ, LORI J & JASON J; JT
335 S 700 E
SALT LAKE CITY, UT 84102-2112

GW PROPERTY INVESTMENTS, LC
412 N OLD OAK RD
SALT LAKE CITY, UT 84108-1685

HABITAT EAST APARTMENTS
975 E 400 S
Salt Lake City, UT 84102

HALL, RICHARD K & ELIZABETH; TC
15 EAST 500 NORTH
FARMINGTON, UT 84025

HALLIDAY, JAMES B
426 S 1000 E #401
SALT LAKE CITY, UT 84102-3045

HAMOS, BRIAN; TR (BHRL TRUST)
329 S LAKER CT
SALT LAKE CITY, UT 84102-2322

HANCOCK, JOHN B
722 E NORTHCREST DR
SALT LAKE CITY, UT 84103-3317

HANSEN, WILLIAM W & DIANNE; JT
6615 HARNESS HILL CT
CENTREVILLE, VA 20121-3817

HARDAGE SUITE HOTELS LLC
12671 HIGH BLUFF DR
SAN DIEGO, CA 92130

HAROLDS
602 E 500 S #D115
Salt Lake City, UT 84102

HARRYHAUSEN LLC
275 S WESTTEMPLE ST
SALT LAKE CITY, UT 84101

HARTMAN, EDWARD J & SANDRA M; JT
337 S LAKER CT
SALT LAKE CITY, UT 84102-2322

HAWLEY, RONALD D & NOREEN A; JT
426 S 1000 E #507
SALT LAKE CITY, UT 84102-3085

HAWTHORNE CAPITAL LLC
6875 S 900 E #100
MIDVALE, UT 84047

HAYES, JAY P & KAREN; JT
818 E 300 S #32
SALT LAKE CITY, UT 84102-2649

HELPING HAND ASSOCIATION INC
974 E SOUTHTEMPLE ST
SALT LAKE CITY, UT 84102-1413

HILL, LAEL W, TR
447 S 900 E
SALT LAKE CITY, UT 84102-2933

HIMONAS, GEORGE & CHRISSEY; JT
426 S 1000 E #505
SALT LAKE CITY, UT 84102-3085

HOLISTIC KIDS
740 E 300 S #208
Salt Lake City, UT 84102

HOLLINGSHEAD, NATE
818 E 300 S #33
SALT LAKE CITY, UT 84102-2649

HOLM, AARON L
426 S FLETCHER CT
SALT LAKE CITY, UT 84102-3022

HOOPER HOLMES PORTAMEDIC
350 E 500 S #202
Salt Lake City, UT 84111

HOWELL, ANDREW A & JULIE A; TC
906 E CROSSWIND WY
DRAPER, UT 84020-3102

HUNTSMAN, RICHARD
749 E LINDEN AVE
SALT LAKE CITY, UT 84102-2210

ICHIBAN SUSHI/IC SUSHI
336 S 400 E
Salt Lake City, UT 84111

ICROSSING INC
231 E 400 S #300
Salt Lake City, UT 84111

IHC BRYNER PHYSICAL THERAPY
850 E 300 S #6
Salt Lake City, UT 84102

INCLINE TERRACE CONDO COMMON AREA
MASTERCARD
1032 E 400 S
SALT LAKE CITY, UT 84102-3107

INLINGUA
323 S 600 E #150
Salt Lake City, UT 84102

INTEGRITY SOLUTIONS, INC
320 S 300 E #202
Salt Lake City, UT 84111

INTERNATIONAL CODE COUNCIL INC
455 E 500 S #202
Salt Lake City, UT 84111

J QUAD, LLC
2413 E ROYAL LN
COTTONWOOD HTS, UT 84093

J R PRODUCTIONS LLC
550 S 400 E #3111
Salt Lake City, UT 84102

JAMES A VALDEZ LLC
466 S 400 E #102
Salt Lake City, UT 84111

JENKINS, EMILY
808 E 300 S #30
SALT LAKE CITY, UT 84102-2648

JENSEN, MARK; ET AL
1044 E 400 S #202A
SALT LAKE CITY, UT 84102-3160

JENSON, JANET I
785 E THREE FTNS CIR #24
MURRAY, UT 84107-5251

JEPPSON, RYAN L
921 E EAST PL
SALT LAKE CITY, UT 84102

JOHANSSON, CLETO O & WALTERS,
GREGORY L; JT
352 S 900 E
SALT LAKE CITY, UT 84102-2311

JOHNSON, DAVID N
354 S 900 E
SALT LAKE CITY, UT 84102-2311

JOHNSON, GWEN W; TR (GWJ TR)
2044 N 1300 E
NORTH LOGAN, UT 84341

JOHNSON, STEPHEN L & ELIZABETH L; TRS
631 E 300 S
SALT LAKE CITY, UT 84102-2103

JOHNSON, SUSAN N
752 S 900 E
OREM, UT 84097-6551

JOHNSON, VIRGINIA H & HILL, E
MARSHALL; JT
845 SOUTHERS PLANTATION
SUWANEE, GA 30024

JOK PROPERTIES LLC
1374 W 1940 N
PROVO, UT 84604

JONES, BENJAMIN & INGRID N; JT
333 S LAKER CT
SALT LAKE CITY, UT 84102-2322

JONES, DANIEL & JANE; TRS
1317 ROYAL ST. GEORGE DR.
NAPERVILLE, IL 60586-2316

JONES, LLOYD W & CAROL J; JT
426 S 1000 E #700
SALT LAKE CITY, UT 84102-3038

JONES, ROBIN K & SUSAN D; JT
937 E 500 S #A
SALT LAKE CITY, UT 84102-3035

JOPPIEN-WU, SUSAN M & SHAUGHNESSY,
WALTER R; JT
640 E 300 S
SALT LAKE CITY, UT 84102-2104

JXV ASSET MANAGEMENT
460 E 300 S
Salt Lake City, UT 84111

JXV ASSET MANAGEMENT LLC
PO BOX 354
SEAL BEACH, CA 90740

KAO, JENNIFER PEI-CHEN; TR (JPCK LIV
TRUST)
2258 S SCENIC DR
SALT LAKE CITY, UT 84109-1431

KATHLEEN ROBISON HUNSTMAN
APARTMENTS
338 E 300 S
Salt Lake City, UT 84111

KEVIN C SULLIVAN LAW FIRM PC
357 S 200 E #105
Salt Lake City, UT 84111

KITSCHY CORNER
343 S 500 E #332
Salt Lake City, UT 84102

KLINGER, PATRICIA A; TR (PAK TRUST)
727 E LINDEN AVE
SALT LAKE CITY, UT 84102-2210

KOCH, ANNA K
808 E 300 S #29
SALT LAKE CITY, UT 84102-2648

KORISH PROPERITES, LLC
347 S 300 E
Salt Lake City, UT 84111

KORISH PROPERITES, LLC
351 S 300 E
Salt Lake City, UT 84111

KOWALCZYK, JACEK A & ALINA J; JT
739 E LINDEN AVE
SALT LAKE CITY, UT 84102-2210

KRISPEN FAMILY HOLDINGS LC
9075 S 1300 E #101
SANDY, UT 84094-1732

LAMB PROPERTY MAINTENANCE
406 E 300 S #222
Salt Lake City, UT 84102

LBJP PROPERTIES LLC
850 E 300 S #5
SALT LAKE CITY, UT 84102-2672

LEE JILL ANN DESIGN
345 E 400 S #103
Salt Lake City, UT 84111

LEISHMAN, JOANNE F; TR (2NDRD OF
TRUST)
1027 E 5285 S
OGDEN, UT 84403

LENOWITZ, HARRIS & WYER, KATHRYN; TC
328 6TH SE
WASHINGTON, DC 20003-2733

LIAN, GANG & XU, JING; JT
1357 HU QING PING RD
SHANGHAI, 20170-2

LIBERTY BANK INC
326 S 500 E
SALT LAKE CITY, UT 84102-4022

LISHA MICHELLE BROOME
338 E 300 S #303
Salt Lake City, UT 84111

LOGSDON, HOWARD M & LYNDA H; TRS
426 S 1000 E #607
SALT LAKE CITY, UT 84102-3038

LOOKINLAND, TODD & PIA, ANNE; TRS
12 BAYO VISTA WAY
SAN RAFAEL, CA 94901

LOPEZ, JOHN
276 E BROADWAY ST #S9
SALT LAKE CITY, UT 84111

LUCERO, TONY J
1061 E 500 S
SALT LAKE CITY, UT 84102-3836

LUDDINGTON, DEAN F; TR ET AL
295 E 2050 S
BOUNTIFUL, UT 84010

LUDLOW THOMAS H DDS
850 E 300 S #3
Salt Lake City, UT 84102

LUDLOW, THOMAS H & SUSAN W; TRS
2646 E HILLSIDE PINES CIR
SALT LAKE CITY, UT 84109-4016

LUMIFORM LLC
337 S 800 E
Salt Lake City, UT 84102

M & LZ PROPERTIES, LLC
319 S 800 E #5
SALT LAKE CITY, UT 84102-1212

MACKIE, MELISSA
8720 NE 17TH ST
VANCOUVER, WA 98664-6417

MALASKA COMMUNICATIONS
357 S 200 E #103
Salt Lake City, UT 84111

MALOUF, JUDY
426 S 1000 E #306
SALT LAKE CITY, UT 84102-1474

MARKETING SOLUTIONS PROMOTION
PRODUCTS
829 E 400 S #114
Salt Lake City, UT 84102

MARSHALL PROPERTIES LLC
352 S 500 E
SALT LAKE CITY, UT 84102-4022

MARTIN, BENJAMIN D & LEAH G; JT
1044 E 400 S #404A
SALT LAKE CITY, UT 84102-3120

MARUSIENSKI, JOANNA
253 E TENTH AVE
SALT LAKE CITY, UT 84103-2531

MASEK, SACHA S
644 E 300 S
SALT LAKE CITY, UT 84102-2128

MASTERS, GAYLE & LYMAN, CHARLES B; JT
426 S 1000 E #403
SALT LAKE CITY, UT 84102-3097

MATSUMORI, CAROL; TR (CM REV TR)
426 S 1000 E #705
SALT LAKE CITY, UT 84102-3048

MCALLISTER, BOYD K
426 S 1000 E #703
SALT LAKE CITY, UT 84102-3048

MCCALL, CLYDE D & DELORES H; JT
615 E 300 S
SALT LAKE CITY, UT 84102-2103

MCNEALY, JUNE C; TR JCMS TRUST
7112 S BRENT LN
COTTONWOOD HTS, UT 84121-3728

MCNELIS, DAVID
PO BOX 3064
CHAPEL HILL, NC 27515-3064

MCOWEN, LAURA J
782 E 1740 S
OREM, UT 84097-8119

MEDICAL VENTURES CONDMN COMMON
AREA MASTER CARD
6778 S 1300 E
COTTONWOOD HTS, UT 84121-2719

MESERVY, MATHEW & ELIZABETH; JT
663 E 300 S
SALT LAKE CITY, UT 84102-2103

METRO CONDOMINIUMS ASSOCIATION INC
5640 S RILEY LN
MURRAY, UT 84107-5800

MEYER GALLERY
350 S 200 E #100
Salt Lake City, UT 84111

MILES, CLARK W
PO BOX 25083
SALT LAKE CITY, UT 84125-0083

MILNER, HAROLD W; TR (SEM TRUST)
2995 CLARY HILL COURT
ROSWELL, GA 30075-5430

MJ ACCOUNTING INC
370 E 500 S #100
Salt Lake City, UT 84111

MOENCH, EMILY
276 E 300 S #S1
SALT LAKE CITY, UT 84111

MOENCH, HEATHER
276 E BROADWAY ST #S10
SALT LAKE CITY, UT 84111

MOMENTISM
653 E 300 S #5
Salt Lake City, UT 84102

NEW CONCEPT VENDING SYSTEMS
602 E 500 S #CART
Salt Lake City, UT 84102

NICHOLICH, JOSEPH A
850 E 300 S #4
SALT LAKE CITY, UT 84102-2672

NIELSEN, NICOLE A
755 E LINDEN AVE
SALT LAKE CITY, UT 84102-2210

NILES, FRANKLIN E; LIFE TRS; ET AL
2730 ATLA VISTA LANE
SAN ANGELO, TX 76904-7458

NJRA ARCHITECTS INC
370 E 500 S #101
Salt Lake City, UT 84111

OBSTETRICS & GYNECOLOGY
850 E 300 S #1
Salt Lake City, UT 84102

OCTAGON INVESTMENTS, LLC
330 S 300 E #200
SALT LAKE CITY, UT 84111-2036

OFF BROADWAY STUDIOS
316 S 400 E
Salt Lake City, UT 84111

OLIVER, MARK
808 E 300 S #27
SALT LAKE CITY, UT 84102-2648

OLSEN, JASON T P; TR (JTPO LIV TR)
2865 S STATE ST
SOUTH SALT LAKE, UT 84115-3622

OQUIRRH APARTMENTS
325 S 400 E
Salt Lake City, UT 84111

OSBORNE, DALE H; TR
426 S 1000 E #600
SALT LAKE CITY, UT 84102-3047

OSIER, JONATHAN & LAURIE; JT
636 E 300 S
SALT LAKE CITY, UT 84102-2104

OTSUKA, KOZO; ET AL
VON-STEUBEN-RING 3
STADTBERGEN, 86391

OUTDOOR RELIGION
343 S 500 E #421
Salt Lake City, UT 84102

OWENS, MARIA & MARK; JT
324 S 800 E
SALT LAKE CITY, UT 84102-2208

PACIFIC RANCH COMPANY
335 S 200 E
Salt Lake City, UT 84111

PARDEE, KATHLEEN
652 E MARKEA AVE
SALT LAKE CITY, UT 84102-2108

PAULOS, LEE F & MARIA K; JT
1153 E 4020 S
SALT LAKE CITY, UT 84124-1225

PAULOS, NIKOLE J
2720 PAINTED CAVE RD
SANTA BARBARA, CA 93105-9770

PERRY, LOWELL K; TR (P FAM REV LIV TR)
426 S 1000 E #707
SALT LAKE CITY, UT 84102-3048

PETERSON BURKE B DDS MPH
250 E 300 S #310
Salt Lake City, UT 84111

PETERSON, PAUL F & AQUINO, PEARLA U;
JT
625 E 300 S
SALT LAKE CITY, UT 84102-2103

PETERSON, SAMUEL J
676 E MARKEA AVE #6
SALT LAKE CITY, UT 84102

PITTMAN, JOEL E
336 S 800 E
SALT LAKE CITY, UT 84102-2208

PLOQUIN, PAULINE
824 E 300 S
SALT LAKE CITY, UT 84102-2320

POLJAK, ROSE MARY I
935 E 500 S #A
SALT LAKE CITY, UT 84102-3035

POTTERY BARN #362
602 E 500 S #B1026
Salt Lake City, UT 84102

POTTERY BARN KIDS #483
602 E 500 S #D108
Salt Lake City, UT 84102

POULTON, JAMES & DONNA; JT
905 N LITTLE VALLEY RD
SALT LAKE CITY, UT 84103-3733

POWER STAR ENTERPRISES, LLC
316 S VINCENT CT
Salt Lake City, UT 84102

PRATHUMMAS, PORNPIMON &
PONGSAWAN, WUTHIPHONG; JT
319 S 800 E #1
SALT LAKE CITY, UT 84102-1212

PREBLE, MYLES A & NOREE L; JT
4460 S ROGER DR
SALT LAKE CITY, UT 84124

PRENTICE, DONNA D; TR (2004 DDPRL TR)
2825 COUNTRYWOOD DR
ANTIOCH, CA 94509-5192

PRIVATE PRACTICE PSYCHOLOGY
350 S 400 E #121E
Salt Lake City, UT 84111

PROTEAN PROPERTIES INC
230 E 3900 S
MURRAY, UT 84107-1531

PROVSTGAARD, ALISHA
310 S 800 E #13
SALT LAKE CITY, UT 84102-1212

PYLE, GEORGE & REBECCA; TC
401 S 1000 E
SALT LAKE CITY, UT 84102-3030

QUILLEN, SCOTT
320 S 800 E #14
SALT LAKE CITY, UT 84102-1212

RAIKH, MIKHAIL & MARIA; JT
1575 E 900 S
SALT LAKE CITY, UT 84105-1621

RASMUSEN, EARL H & MARGARET A (JT)
1798 S PARK ST
SALT LAKE CITY, UT 84105-2937

RATLIFF, GLADYS E, TR
932 E EAST PL
SALT LAKE CITY, UT 84102

RAWCLIFFE, GILLIAN
818 E 300 S #35
SALT LAKE CITY, UT 84102-2649

REAM, L J; TR
PO BOX 510810
SALT LAKE CITY, UT 84151-0810

REHERMANN, ROBERT; ET AL
2081 E 2700 S
SALT LAKE CITY, UT 84109-1779

Residents
1044 E 400 S #206A
Salt Lake City, UT 84102-3152

Residents
1044 E 400 S #308A
Salt Lake City, UT 84102-3152

Residents
209 E 500 S
Salt Lake City, UT 84111-3203

Residents
268 E 300 S
Salt Lake City, UT 84111

Residents
276 E 300 S #S-4
Salt Lake City, UT 84111-2445

Residents
276 E 300 S #S-8
Salt Lake City, UT 84111-2445

Residents
305 S 600 E
Salt Lake City, UT 84102-4013

Residents
307 S 600 E
Salt Lake City, UT 84102-4013

Residents
310 E 300 S
Salt Lake City, UT 84111

Residents
310 S 300 E #A-4
Salt Lake City, UT 84111-2523

Residents
311 S 400 E
Salt Lake City, UT 84111-2904

Residents
315 S 700 E
Salt Lake City, UT 84102-2112

Residents
315 S VINCENT CT
Salt Lake City, UT 84102-2109

Residents
317 S VINCENT CT
Salt Lake City, UT 84102-2109

Residents
320 S 800 E
Salt Lake City, UT 84102-2229

Residents
320 S REEVES TER
Salt Lake City, UT 84102-2212

Residents
321 S REEVES TER
Salt Lake City, UT 84102-2212

Residents
322 S 900 E #2
Salt Lake City, UT 84102-2450

Residents
322 S 900 E #4
Salt Lake City, UT 84102-2450

Residents
324 S REEVES TER
Salt Lake City, UT 84102-2212

Residents
325 S 400 E #2
Salt Lake City, UT 84111-2903

Residents
325 S 400 E #4
Salt Lake City, UT 84111-2903

Residents
325 S 400 E #6
Salt Lake City, UT 84111-2903

Residents
325 S 700 E
Salt Lake City, UT 84102-2112

Residents
325 S REEVES TER
Salt Lake City, UT 84102-2212

Residents
325 S VINCENT CT
Salt Lake City, UT 84102-2109

Residents
327 S 800 E
Salt Lake City, UT 84102-2207

Residents
327 S REEVES TER
Salt Lake City, UT 84102-2212

Residents
328 S 900 E
Salt Lake City, UT 84102-2311

Residents
328 S LAKER CT
Salt Lake City, UT 84102

Residents
329 S LAKER CT
Salt Lake City, UT 84102-2322

Residents
330 S 800 E
Salt Lake City, UT 84102-2208

Residents
331 S REEVES TER
Salt Lake City, UT 84102-2212

Residents
331 S VINCENT CT
Salt Lake City, UT 84102-2109

Residents
332 S 800 E
Salt Lake City, UT 84102-2208

Residents
333 S 200 E
Salt Lake City, UT 84111-2801

Residents
333 S 300 E
Salt Lake City, UT 84111-2522

Residents
335 S 300 E
Salt Lake City, UT 84111-2522

Residents
335 S 800 E
Salt Lake City, UT 84102-2207

Residents
336 S LAKER CT
Salt Lake City, UT 84102-2322

Residents
337 S 300 E
Salt Lake City, UT 84111-2503

Residents
338 S 900 E
Salt Lake City, UT 84102-2311

Residents
338 S LAKER CT
Salt Lake City, UT 84102-2322

Residents
339 S 300 E
Salt Lake City, UT 84111-2522

Residents
341 S 400 E
Salt Lake City, UT 84111-2904

Residents
342 S 900 E
Salt Lake City, UT 84102-2311

Residents
343 S 800 E
Salt Lake City, UT 84102-2207

Residents
344 E 300 S
Salt Lake City, UT 84111

Residents
347 S STRONGS CT
Salt Lake City, UT 84102-2303

Residents
348 S 800 E
Salt Lake City, UT 84102-2260

Residents
348 S STRONGS CT
Salt Lake City, UT 84102-2303

Residents
349 S 200 E
Salt Lake City, UT 84111-2811

Residents
349 S 200 E #REAR
Salt Lake City, UT 84111-2811

Residents
349 S 300 E
Salt Lake City, UT 84111-2543

Residents
350 S 800 E
Salt Lake City, UT 84102-2260

Residents
352 E 300 S
Salt Lake City, UT 84111-2605

Residents
352 S 800 E
Salt Lake City, UT 84102-2260

Residents
353 S 300 E
Salt Lake City, UT 84111-2522

Residents
354 S 800 E
Salt Lake City, UT 84102-2260

Residents
354 S STRONGS CT
Salt Lake City, UT 84102-2303

Residents
355 S 300 E
Salt Lake City, UT 84111-2522

Residents
356 S STRONGS CT
Salt Lake City, UT 84102-2303

Residents
358 S STRONGS CT
Salt Lake City, UT 84102-2303

Residents
364 S 1000 E
Salt Lake City, UT 84102-2415

Residents
365 S STRONGS CT
Salt Lake City, UT 84102-2303

Residents
389 S 900 E
Salt Lake City, UT 84102

Residents
408 E 300 S
Salt Lake City, UT 84102

Residents
410 E 300 S
Salt Lake City, UT 84102

Residents
412 E 300 S
Salt Lake City, UT 84102

Residents
414 E 300 S
Salt Lake City, UT 84111-2607

Residents
414 S FLETCHER CT
Salt Lake City, UT 84102-3022

Residents
416 E 500 S
Salt Lake City, UT 84111-3305

Residents
418 E 300 S
Salt Lake City, UT 84111-2607

Residents
420 S 800 E
Salt Lake City, UT 84102-2911

Residents
424 E 500 S
Salt Lake City, UT 84111-3333

Residents
427 S FLETCHER CT
Salt Lake City, UT 84102-3022

Residents
429 S 800 E
Salt Lake City, UT 84102-2910

Residents
430 E 500 S
Salt Lake City, UT 84111-3305

Residents
433 S FLETCHER CT
Salt Lake City, UT 84102-3022

Residents
435 S 900 E
Salt Lake City, UT 84102-2933

Residents
443 S 900 E
Salt Lake City, UT 84102-2933

Residents
444 S 800 E #1/2
Salt Lake City, UT 84102-2911

Residents
452 S 800 E #PARK
Salt Lake City, UT 84102-2911

Residents
460 E 300 S #2
Salt Lake City, UT 84111-2669

Residents
460 E 300 S #4
Salt Lake City, UT 84111-2669

Residents
460 E 300 S #6
Salt Lake City, UT 84111-2669

Residents
460 S 1000 E #BSMT
Salt Lake City, UT 84102-3031

Residents
465 S 900 E
Salt Lake City, UT 84102-2933

Residents
475 S 300 E
Salt Lake City, UT 84111

Residents
540 E 500 S
Salt Lake City, UT 84102-2707

Residents
544 S 700 E
Salt Lake City, UT 84102

Residents
556 E 300 S
Salt Lake City, UT 84102-4032

Residents
602 E 300 S
Salt Lake City, UT 84102-2104

Residents
612 E 300 S
Salt Lake City, UT 84102-2104

Residents
614 E 300 S
Salt Lake City, UT 84102-2104

Residents
616 E 300 S
Salt Lake City, UT 84102-2104

Residents
621 E 300 S
Salt Lake City, UT 84102-2103

Residents
631 E 300 S #1/2
Salt Lake City, UT 84102-2103

Residents
633 E 300 S
Salt Lake City, UT 84102-2103

Residents
633 E 300 S #1/2
Salt Lake City, UT 84102-2103

Residents
643 E 300 S
Salt Lake City, UT 84102-2103

Residents
643 E 300 S #REAR
Salt Lake City, UT 84102-2103

Residents
659 E 300 S
Salt Lake City, UT 84102-2140

Residents
667 E 300 S
Salt Lake City, UT 84102-2103

Residents
676 E MARKEA AVE #15
Salt Lake City, UT 84102

Residents
676 E MARKEA AVE #19
Salt Lake City, UT 84102

Residents
676 E MARKEA AVE #1-9
Salt Lake City, UT 84102

Residents
710 E 300 S
Salt Lake City, UT 84102-2204

Residents
712 E 300 S
Salt Lake City, UT 84102-2204

Residents
714 E 300 S
Salt Lake City, UT 84102-2204

Residents
716 E 300 S
Salt Lake City, UT 84102-2204

Residents
722 E 300 S #2
Salt Lake City, UT 84102-2293

Residents
722 E 300 S #4
Salt Lake City, UT 84102-2293

Residents
722 E 300 S #6
Salt Lake City, UT 84102-2293

Residents
722 E 300 S #8
Salt Lake City, UT 84102-2293

Residents
726 E 300 S #2
Salt Lake City, UT 84102-2294

Residents
726 E 300 S #4
Salt Lake City, UT 84102-2294

Residents
726 E 300 S #6
Salt Lake City, UT 84102-2294

Residents
726 E 300 S #8
Salt Lake City, UT 84102-2294

Residents
729 E LINDEN AVE
Salt Lake City, UT 84102-2210

Residents
731 E LINDEN AVE
Salt Lake City, UT 84102-2210

Residents
733 E LINDEN AVE
Salt Lake City, UT 84102-2210

Residents
751 E LINDEN AVE
Salt Lake City, UT 84102-2210

Residents
752 E 300 S
Salt Lake City, UT 84102-2204

Residents
753 E LINDEN AVE
Salt Lake City, UT 84102-2210

Residents
756 E 300 S
Salt Lake City, UT 84102-2204

Residents
757 E LINDEN AVE
Salt Lake City, UT 84102-2210

Residents
759 E LINDEN AVE
Salt Lake City, UT 84102-2210

Residents
761 E LINDEN AVE
Salt Lake City, UT 84102-2259

Residents
763 E LINDEN AVE
Salt Lake City, UT 84102-2259

Residents
826 E 300 S
Salt Lake City, UT 84102-2320

Residents
834 E 300 S
Salt Lake City, UT 84102-2320

Residents
905 E 500 S
Salt Lake City, UT 84102-3017

Residents
909 E 500 S
Salt Lake City, UT 84102-3017

Residents
921 E 500 S
Salt Lake City, UT 84102-3017

Residents
921 E 500 S #B
Salt Lake City, UT 84102-3017

Residents
923 E 500 S
Salt Lake City, UT 84102-3017

Residents
924 E EAST PL
Salt Lake City, UT 84102-3021

Residents
925 E 500 S
Salt Lake City, UT 84102-3017

Residents
925 E 500 S #A
Salt Lake City, UT 84102-3017

Residents
926 E EAST PL
Salt Lake City, UT 84102-3021

Residents
927 E 500 S
Salt Lake City, UT 84102-3017

Residents
929 E 500 S
Salt Lake City, UT 84102-3017

Residents
931 E 500 S
Salt Lake City, UT 84102-3017

Residents
933 E 500 S
Salt Lake City, UT 84102-3017

Residents
933 E EAST PL
Salt Lake City, UT 84102-3021

Residents
935 E 500 S
Salt Lake City, UT 84102-3017

Residents
935 E 500 S #A
Salt Lake City, UT 84102-3017

Residents
935 E 500 S #B
Salt Lake City, UT 84102-3017

Residents
935 E EAST PL
Salt Lake City, UT 84102-3021

Residents
936 E EAST PL
Salt Lake City, UT 84102-3021

Residents
939 E 500 S
Salt Lake City, UT 84102-3017

Residents
939 E EAST PL
Salt Lake City, UT 84102-3021

Residents
951 E 400 S
Salt Lake City, UT 84102-3015

Residents
959 E 400 S
Salt Lake City, UT 84102-3015

Residents
975 E 500 S
Salt Lake City, UT 84102-3017

REX, MARK R & BARRANI, MAHA A; JT
744 E 300 S
SALT LAKE CITY, UT 84102-2204

RICE, SANDRA R
310 S 800 E #21
SALT LAKE CITY, UT 84102-1212

RICHARDSON, MARILYN S; TR
2275 E PARLEYS TER
SALT LAKE CITY, UT 84109-1537

RINNE, LEENA-MAIJA & PAYNE, DAVID C; TC
312 S 400 E
SALT LAKE CITY, UT 84111-2905

ROCKY MOUNTAIN CHOCOLATE FACTO
602 E 500 S #D-102
Salt Lake City, UT 84102

ROGERS, RICHARD B; ETAL
315 W HUENEME ROAD
CAMARILLO, CA 93012

RUBY RIVER STEAKHOUSE #404
435 S 700 E
Salt Lake City, UT 84102

RUNOLFSON, MARY
276 E 300 S #S6
SALT LAKE CITY, UT 84111

SABER, NAGI A & CAROLEE P (JT)
18344 W SANTA IRENE DR
GOODYEAR, AZ 85338-5334

SAHAMI-MALMBERG, MASOUMEH
478 S 1100 E
SALT LAKE CITY, UT 84102-3116

SAINTS PETER AND PAUL ORTHODOX
CHURCH; ET AL
406 E 300 S
SALT LAKE CITY, UT 84111-2607

SALEHI, MARYAM
1644 S MOHAWK WY
SALT LAKE CITY, UT 84108-3312

SALT LAKE COUNTY
PO BOX 144575
SALT LAKE CITY, UT 84114-4575

SALT LAKE VIOLINS LLC
364 E 300 S
Salt Lake City, UT 84111

SAMPSON ALTADENA CONDOMINIUM
HOMEOWNERS ASSOCIATION
PO BOX 510006
SALT LAKE CITY, UT 84151-0006

SARTAIN, MICHAEL
1044 E 400 S #307A
SALT LAKE CITY, UT 84102

SCOTT, EDWARD W & KATHLEEN M; JT
941 E 500 S
SALT LAKE CITY, UT 84102-3017

SCP 500 SOUTH LLC
500 N MARKET PLACE DR
CENTERVILLE, UT 84014

SEAVER, DEBORAH J
772 RIVER BIRCH
PARK CITY, UT 84060

SEELY, ROSS & LAURA M; JT
1682 E VILLAGE GREEN RD
COTTONWOOD HTS, UT 84121

SENTRY FINANCIAL CORPORATION
EMPLOYEE 401K PROFIT SHARING P
201 S MAIN ST #1400
SALT LAKE CITY, UT 84111-2217

SERGAKIS, GEORGE M & EFFROSENE K; JT
9499 S CANDLE TREE LN
SANDY, UT 84092

SHANGHAI AMERICAN MANAGEMENT AND
CONSULTING LLC
937 E 500 S #B
Salt Lake City, UT 84102

SHELTON FIRST FAMILY LTD PARTNERSHIP
641 E DRAPER VIEW RD
DRAPER, UT 84020-7693

SHIPPING CONNECTION INC
406 E 300 S
Salt Lake City, UT 84102

SHURTLEFF, GLEN C & KATHLEEN G; JT
343 S STRONGS CT
SALT LAKE CITY, UT 84102-2303

SIZZLE PARK, LLC
320 S 400 E
SALT LAKE CITY, UT 84111-2905

SMITH, BART
921 E 500 S #B
SALT LAKE CITY, UT 84102

SMITH, SAMMY W
4841 S ROCKHILL LN
TAYLORSVILLE, UT 84123-4706

SNOW, CARRIE E
320 S 800 E #22
SALT LAKE CITY, UT 84102-4145

SOTIRIOU INVESTMENT GROUP
250 E 300 S #100
Salt Lake City, UT 84111

SOTIRIOU INVESTMENT GROUP LC
242 E 300 S
SALT LAKE CITY, UT 84111

SOUND VS SILENCE
706 E 300 S #16
Salt Lake City, UT 84102

SPENCER STEWART APARTMENTS
740 E 300 S
Salt Lake City, UT 84102

SROK, NATHANIEL A
421 S FLETCHER CT
SALT LAKE CITY, UT 84102-3022

STACKHOUSE, AMBER
100 ATANTIC AVE #-808
LONG BEACH, CA 90802-5118

STARNEM GROUP LLC, THE
550 S 400 E #3206
Salt Lake City, UT 84102

STAYNER, GEORGE C
321 S 600 E
SALT LAKE CITY, UT 84102-4013

STEEL ENCOUNTERS INC
536 E 300 S
SALT LAKE CITY, UT 84102-4010

STEGEMOLLER, TYLER
319 S 800 E #3
SALT LAKE CITY, UT 84102-1212

STEPPING STONES INVESTMENT LLC
1923 E SUNNY GLEN CIR
SANDY, UT 84093

STOTT, GRANT E & HELEN C; TRS
11548 LEATHERLEAF ROAD
FONTANA, CA 92337-1016

STRADLER, REALTY, LLC
2915 E OAKHURST DR
SALT LAKE CITY, UT 84108

STRASBURG, JUSTIN B
327 S LAKER CT
SALT LAKE CITY, UT 84102-2322

STRUHS, MARK J & LAWLER, DEBRA A; JT
4222 E HELAMAN CIR
SALT LAKE CITY, UT 84124

STUDIO L DESIGN LLC
231 E 400 S #220
Salt Lake City, UT 84111

SUTTON JAMES M DDS
850 E 300 S #9
Salt Lake City, UT 84102

SWIFT, KIRSTEN M; ET AL
426 S 1000 E #508
SALT LAKE CITY, UT 84102-3085

TADPALS
829 E 400 S #103
Salt Lake City, UT 84102

TAJBAKSH, HOOMAN L
PO BOX 18765
SEATTLE, WA 98118-0765

TANNER, EARL D, JR. & TODD A; TRS (EDT
REV TR)
2818 S REDWOOD RD
WEST VALLEY, UT 84119-2375

TAYLOR JEFF
339 S 800 E
Salt Lake City, UT 84102

TAYLOR, JEFFREY S
1152 E 200 S
SALT LAKE CITY, UT 84102-2567

TEDESCO, ELLA B; TR
242 S 1200 E
SALT LAKE CITY, UT 84102-2651

TESORO
680 E 400 S
Salt Lake City, UT 84102

TGM EMIGRATION LLC
500 NORTH DEARBORN #400
CHICAGO, IL 60654-3386

THIRD SOUTH LLC
7000 SW VARNS ST
PORTLAND, OR 97233

THOMAS K DARRELL DDS
250 E 300 S #320
Salt Lake City, UT 84111

THOMPSON, ANDREW P
939 E 500 S #A
SALT LAKE CITY, UT 84102-3035

TOUCHOPIA LLC
440 S 700 E #203
Salt Lake City, UT 84102

TRACY, BETH ANNE
276 E BROADWAY ST #S8
SALT LAKE CITY, UT 84111

TRAIN SHOPPE INC
470 S 900 E
Salt Lake City, UT 84102

TROLLEY GREEN GIANT
602 E 500 S #D136
Salt Lake City, UT 84102

TROLLEY SQUARE ASSOCIATES LLC
367 TROLLEY SQUARE
SALT LAKE CITY, UT 84102

TROLLEY WING CO.
602 E 500 S #388
Salt Lake City, UT 84102

TWENTY-SEVENTH WIFE LLC
343 S 400 E
SALT LAKE CITY, UT 84111-2904

TWO PINES EAST CONDOMINIUM
HOMEOWNERS ASSOCIATION
PO BOX 571367
MURRAY, UT 84157-1367

URBAN SURF
425 S 1000 E #210C
Salt Lake City, UT 84102

UTAH WOMENS CLINIC
515 S 400 E
Salt Lake City, UT 84111

VALLEY FOOT & ANKLE CLINIC
850 E 300 S #2
Salt Lake City, UT 84102

VAN FRANK, MEGAN & HIGGINS, JOHN; JT
919 E 500 S
SALT LAKE CITY, UT 84102-3017

VAN RIJ, JOSHUA J & KATIE J; JT
566 E SECOND AVE
SALT LAKE CITY, UT 84103-2927

VAN WAGONER, ADAM & CLAWSON,
CAITLIN E; JT
320 S 800 E #20
SALT LAKE CITY, UT 84102-1212

VASS, KATHRYN S
923 E 500 S #B
SALT LAKE CITY, UT 84102

VELO-CITY BAGS
350 S 200 E #608
Salt Lake City, UT 84111

VIENNEAU, LLOYD R
2401 MINNESOTA AVE
DULUTH, MN 55802-2517

VIENNEAU:LLOYD R
356 S 900 E
Salt Lake City, UT 84102

VITAMIN WORLD INC #8203
602 E 500 S #D209
Salt Lake City, UT 84102

WADE, LLOYD W & GWENIVERE F; JT
2679 E LAMBOURNE AVE
SALT LAKE CITY, UT 84109-2742

WAGSTAFF, DAVID J; TR (DJW REV LIV TR)
4126 S PARKVIEW DR
SALT LAKE CITY, UT 84124-3422

WALKER, MICHAEL D & CARAMELLI, KIM E;
JT
358 S 1000 E
SALT LAKE CITY, UT 84102-2415

WALL, TRENT S & CAROLINE M; JT
276 E BROADWAY ST #S7
SALT LAKE CITY, UT 84111

WARREN CARMACK & ASSOCIATES
343 S 500 E #317
Salt Lake City, UT 84102

WC ADMINISTRATORS LLC
433 S 400 E #200
Salt Lake City, UT 84111

WEILER, SARAH L; ET AL
344 S STRONGS CT
SALT LAKE CITY, UT 84102-2303

WELTI & CALL ADVERTISING
376 E 400 S #307
Salt Lake City, UT 84111

WHARTON, JOHN D; ET AL
3386 S PAIGE CIR
SALT LAKE CITY, UT 84109-3178

WHITE, GARY W
341 E 2100 S
SOUTH SALT LAKE, UT 84115-2235

WILDER, ANDREW M & DANIEL A; JT
438 S FLETCHER CT
SALT LAKE CITY, UT 84102-3022

WILDHORSE COFFEE
602 E 500 S #D232
Salt Lake City, UT 84102

WILKINSON, JARED
743 E LINDEN AVE
SALT LAKE CITY, UT 84102-2210

WILLIAMS, DANIEL B, V & KIMBERLY A; JT
660 E MARKEA AVE
SALT LAKE CITY, UT 84102-2108

WILLOWS APARTMENTS
860 E 300 S
Salt Lake City, UT 84102

WILSHIRE CONDM, THE COMMON AREA
MASTER CARD
426 S 1000 E
SALT LAKE CITY, UT 84102-3031

WINTCH, DOUGLAS H
359 S STRONGS CT
SALT LAKE CITY, UT 84102-2303

WIRELESS GIANT
510 E 400 S
Salt Lake City, UT 84102

WISEGUYS COMEDY
602 E 500 S #A102
Salt Lake City, UT 84102

WOODIN, PAUL L & VICKI L; JT
6109 S 2175 E
ODGEN, UT 84403

WRAG PROPERTIES INC
1935 E VINE ST #260
SALT LAKE CITY, UT 84121-7924

WRATHALL, DEBORA J & TOBLER, JAMES; JT
331 S 800 E
SALT LAKE CITY, UT 84102-2207

XAVIER, ASHOK & ASHOK, SANDHYA C; JT
10173 S DELSEY CV
SOUTH JORDAN, UT 84095-7108

XEROX CORPORATION
675 E 500 S #100
Salt Lake City, UT 84102

YAMAGUCHI, JEAN N
1140 E EMERSON AVE
SALT LAKE CITY, UT 84105-2528

YPSILON CORPORATION
602 E 500 S #609
Salt Lake City, UT 84102

YWBUILDING HOPE QALICB
322 E 300 S
SALT LAKE CITY, UT 84111

ZHANG, HONGZHI & XIAO, XINYI; JT
PO BOX 521153
SALT LAKE CITY, UT 84152-1153

ZVONEK, BARBARA & JOE; TRS
3801 S HIGHLAND COVE #211
SALT LAKE CITY, UT 84106-4230

345 OFFICE BUILDING LC
345 E 400 S #201
SALT LAKE CITY, UT 84111-2728

400 SOUTH ENTERTAINMENT PARTNERS LC
655 E 400 S #200
SALT LAKE CITY, UT 84102-2728

400 SOUTH FOOD CORP
655 E 400 S #200
SALT LAKE CITY, UT 84102-2728

466 SOUTH ASSOCIATES LLC
466 S 400 E #200
SALT LAKE CITY, UT 84111-1840

4TH SOUTH ACE HARDWARE
612 E 400 S
Salt Lake City, UT 84102

4TH SOUTH DEL LLC
655 E 400 S
SALT LAKE CITY, UT 84102-2803

4TH ST MUSIC LLC
249 E 400 S #1
Salt Lake City, UT 84111

7-ELEVEN STORE #29514A
309 E 400 S
Salt Lake City, UT 84111

A J LURAS LEASING LC
5778 S UTAHNA DR
MURRAY, UT 84107-5928

A SMITH AND ASSOCIATES INC
455 E 500 S #201
Salt Lake City, UT 84111

A SPLASH OF BEAUTY
352 S DENVER ST #330
Salt Lake City, UT 84111

A THERAPEUTIC TOUCH MASSAGE &
BODYWORK
352 S DENVER ST #330
Salt Lake City, UT 84111

ACCESS LEGAL
455 E 400 S #100
Salt Lake City, UT 84111

ACOUSTIC MUSIC
857 E 400 S
Salt Lake City, UT 84102

ACOUSTIC MUSIC STUDIOS
845 E 400 S
Salt Lake City, UT 84102

ACRES METRO LLC
PO BOX 11491
SALT LAKE CITY, UT 84147-0491

ADAMSON, BARTON L; ET AL
423 W MELLING WY
OGDEN, UT 84404

ADEJI
366 S 500 E #4
Salt Lake City, UT 84111

ADELE CORPORATION
815 E 400 S
SALT LAKE CITY, UT 84102-2903

AKASHA SPA AND SALON
331 E 400 S
Salt Lake City, UT 84111

ALLEN SCHRENK
363 S 500 E #106
Salt Lake City, UT 84102

ALLIED PROTECTION SERVICES INC
455 E 400 S #308
Salt Lake City, UT 84111

ALPINE SECURITIES
440 E 400 S
Salt Lake City, UT 84111

ALTA MODA LLC
637 E 500 S
Salt Lake City, UT 84102

AMANDA VONK
363 S 500 E #107
Salt Lake City, UT 84102

AMAZING GRACE LC SERIES 7TH EAST
139 N VIRGINIA ST
SALT LAKE CITY, UT 84103-4316

AMERICA FIRST CREDIT UNION
455 E 500 S #100
Salt Lake City, UT 84111

AMERICAN INSURANCE & INVESTMENT
CORP
448 S 400 E
SALT LAKE CITY, UT 84111-3303

AMERICAN PROMOTIONAL EVENTS DBA
TNT FIREWORKS
455 S 500 E
Salt Lake City, UT 84102

ANN LARSEN RESIDENTIAL DESIGN, INC.
455 E 400 S #405
Salt Lake City, UT 84111

ANNA HALLOWS ESTHETICIAN
363 S 500 E #112
Salt Lake City, UT 84102

APARTMENT HOUSE
425 S 900 E
Salt Lake City, UT 84102

APARTMENT HOUSE
454 S 500 E
Salt Lake City, UT 84111

APARTMENT HOUSE
448 S 500 E #REAR
Salt Lake City, UT 84111

AQUA NAILS
363 S 500 E #107
Salt Lake City, UT 84102

ARBYS ROAST BEEF #174
420 E 400 S
Salt Lake City, UT 84111

ARNOLD R CLARK
425 S 400 E
Salt Lake City, UT 84111

ARNOLD, R CLARK; TR ET AL
PO BOX 1588
SALT LAKE CITY, UT 84110-1588

ASAP QUALITY CLEANING
366 S 500 E #208
Salt Lake City, UT 84111

AT & T CORP
675 E 500 S #600
Salt Lake City, UT 84102

AUBREY F CONNER INC
352 S DENVER ST #102
Salt Lake City, UT 84111

AVENUES INVESTMENT GROUP LLC
470 S 700 E #REAR
Salt Lake City, UT 84102

AXIS PROPERTIES, LLC
352 S DENVER ST #205
Salt Lake City, UT 84111

AZURE DAY SPA
363 S 500 E #108
Salt Lake City, UT 84102

B AND R FINANCIAL INC
455 E 500 S #400
Salt Lake City, UT 84111

BARNES BANKING COMPANY
431 S 300 E #100
Salt Lake City, UT 84111

BASKIN ROBBINS
576 E 400 S
Salt Lake City, UT 84102

BEACHED WHALE ENTERPRISES
345 S MOFFATT CT
Salt Lake City, UT 84111

BEMENT & COMPANY P.C.
448 E 400 S #204
Salt Lake City, UT 84111

BENCH CRAFT COMPANY
352 S DENVER ST #202
Salt Lake City, UT 84111

BERCEAU DAVID J P.C.
340 E 400 S #50
Salt Lake City, UT 84111

BERGER KAY INC
425 S 400 E
Salt Lake City, UT 84111

BETOS MEXICAN FOOD
435 E 400 S
Salt Lake City, UT 84111

BIG DADDYS PIZZA DOWNTOWN
470 S 700 E
Salt Lake City, UT 84102

BIG J'S
455 E 400 S #30
Salt Lake City, UT 84111

BLACKHAWK PROPERTIES, LLC
539 E BLACKHAWK LN
ALPINE, UT 84004-1825

BLDG CAT, LLC
343 E 500 S
SALT LAKE CITY, UT 84111-3315

BLOCKBUSTER VIDEO #49013
510 E 400 S
Salt Lake City, UT 84102

BNY YOUKSTETTER INVESTMENTS LC; ET AL
PO BOX 2973
MCCALL, ID 83638

BOLTON, JOHN
320 E 400 S
SALT LAKE CITY, UT 84111-2902

BOMAN INVESTMENTS
314 S 700 E
Salt Lake City, UT 84102

BOMAN INVESTMENTS, LLC
11711 S SILVER SPUR LN
DRAPER, UT 84020-4515

BOMAN INVESTMENTS, LLC
222 E TWIN RIDGE DR
DRAPER, UT 84020

BOWNCO
940 E 400 S #B
Salt Lake City, UT 84102

BOYER - MADSEN SEVENTH EAST LC
90 S 400 W #200
SALT LAKE CITY, UT 84101-1200

BRANDEN T BURNINGHAM ESQ
455 E 500 S #205
Salt Lake City, UT 84111

BROWN RICHARD APARTMENTS
448 S 500 E
Salt Lake City, UT 84111

BROWN, RICHARD W & SEOKIM C; TRS
1969 E SOUTHBRIDGE WY
SANDY, UT 84093-2547

BURGER KING 5224
575 E 400 S
Salt Lake City, UT 84102

BURNINGHAM LEONARD W ESQ
455 E 500 S #205
Salt Lake City, UT 84111

BYTHEWAY HOLDINGS, LLC
1338 S FOOTHILL DR #311
SALT LAKE CITY, UT 84108-1953

BYTHEWAY HOLDINGS, LLC
363 N MIDDLE OAK LN
SALT LAKE CITY, UT 84108-1649

CABZ LLC
450 S 900 E
SALT LAKE CITY, UT 84102-2944

CAFE RIO MEXICAN GRILL
532 E 400 S
Salt Lake City, UT 84102

CAFFE' D'BOLLA
249 E 400 S #1ST WS
Salt Lake City, UT 84111

CAL TEMPLE PROPERTIES, LLC
PO BOX 71590
SALT LAKE CITY, UT 84171-0590

CANNON ASSOCIATES
455 E 400 S #400
Salt Lake City, UT 84111

CANYON COUNSELING GROUP
352 S DENVER ST #318
Salt Lake City, UT 84111

CARMON BLACK MANAGEMENT CO; ET AL
1010 E PEAKS CIR
SALT LAKE CITY, UT 84117

CAROL JACQUES
363 S 500 E #113
Salt Lake City, UT 84102

CARROLL JAMES L & ASSOCIATES
455 E 400 S #403
Salt Lake City, UT 84111

CASH MONEY ENTERTAINMENT
425 S 900 E #1
Salt Lake City, UT 84102

CELTIC BANK CORPORATION
340 E 400 S
Salt Lake City, UT 84111

CENTRAL CHRISTIAN CHURCH
370 S 300 E
SALT LAKE CITY, UT 84111-2504

CHARLES L DIVINEY III MC LPC NCC CCMHC
LLC
352 S DENVER ST #360
Salt Lake City, UT 84111

CHASE SUITE HOTEL BY WOODFIN
765 E 400 S
Salt Lake City, UT 84102

CHILIS SOUTHWEST GRILL
668 E 400 S
Salt Lake City, UT 84102

CHRISTENSEN MARK P.C.
455 E 500 S #207
Salt Lake City, UT 84111

CHRISTOFFERSEN ACCOUNTANCY COR
448 E 400 S #204
Salt Lake City, UT 84111

CINEGRILL
344 S 300 E
Salt Lake City, UT 84111

CLARK INVESTIGATIVE SERVICES
358 S 700 E #B113
Salt Lake City, UT 84102

COLE WG SALT LAKE CITY UT DST
104 WILMOT RD
DEERFIELD, IL 60015

COLLUM ENTERPRISES INC, ET AL
3007 S STATE ST
SOUTH SALT LAKE, UT 84115-3820

COMBAT FILMS AND RESEARCH INC
455 E 400 S #205
Salt Lake City, UT 84111

CONGER CHIROPRACTIC CLINIC
479 E 500 S
Salt Lake City, UT 84111

CONSTRUCTION CONTROL CORP
460 S 400 E
Salt Lake City, UT 84111

CORDRAY, CATHRYN R & SMITH, NATHAN
N; JT
411 S 800 E
SALT LAKE CITY, UT 84102-2910

COSTUME JEWELRY INC
470 S 900 E
Salt Lake City, UT 84102

COURTRIGHT, NANDRA J
829 E 400 S #114
SALT LAKE CITY, UT 84102-2728

CURT WOODLAND
455 E 400 S #30
Salt Lake City, UT 84111

D & J SONNTAG INVESTMENT COMPANY
LP, THE
2402 E 900 S
SALT LAKE CITY, UT 84108-1441

D E MOSS FAMILY LIMITED PARTNERSHIP
744 E 400 S
SALT LAKE CITY, UT 84102-2902

DAISY PROPERTIES
5544 S GREEN ST
MURRAY, UT 84123-5798

DAVIS, HARDIN K & OLSON, SANDRA L; JT
1538 E LOGAN AVE
SALT LAKE CITY, UT 84105-2808

DDR FAMILY CENTERS LP
455 E 500 S #GDLV
Salt Lake City, UT 84111

DEANS JAMES H, ATTNY AT LAW
440 S 700 E #101
Salt Lake City, UT 84102

DEMERS MENTAL HEALTH, PLLC
231 E 400 S #208
Salt Lake City, UT 84111

DEVELOPMENT SPECIALTIES INC
455 E 400 S #101
Salt Lake City, UT 84111

DLB UTAH, LLC
PO BOX 3176
SALT LAKE CITY, UT 84110-3176

DON C HALE INVESTMENT COMPANY
8160 S HIGHLAND DR #210
SANDY, UT 84093-4347

DON C HALE INVESTMENT COMPANY LLC
2431 E LYNWOOD DR
SALT LAKE CITY, UT 84109-1213

DONALD J WINDER PROPERTIES
1500 E ARLINGTON DR
SALT LAKE CITY, UT 84103-4428

DUMONT MINING COMPANY
352 S DENVER ST #370
Salt Lake City, UT 84111

EAST DOWNTOWN SPE LLC
1014 VINE ST
CINCINNATI, OH 45202-1100

EAST DOWNTOWN SPE LLC
PO BOX 712139
COTTONWOOD HEIGHTS, UT 84171-2139

ELM ENTERPRISES LLC
675 E 500 S #340
Salt Lake City, UT 84102

EMA ARCHITECTS, LLC
460 S 400 E
Salt Lake City, UT 84111

EMMALY ADAMS
363 S 500 E #103
Salt Lake City, UT 84102

ENSIGN WHOLESALE FLORAL
461 S 600 E
Salt Lake City, UT 84102

ETTORI INVESTMENTS LLC
352 S MOFFATT CT
SALT LAKE CITY, UT 84111-2805

EVERY BLOOMING THING INC
444 S 700 E
Salt Lake City, UT 84102

EXAMONE WORLD WIDE, INC
448 E 400 S #302
Salt Lake City, UT 84111

EYEMASTERS #004
635 E 400 S
Salt Lake City, UT 84102

EYRING, DANIEL M
829 E 400 S #107
SALT LAKE CITY, UT 84102-2728

EZ LOAN SERVICES
820 E 400 S
Salt Lake City, UT 84102

FALECIA WILSON
815 E 400 S
Salt Lake City, UT-84102

FENDALLS ICE CREAM
470 S 700 E
Salt Lake City, UT 84102

FIRST MUTUAL CAPITAL MARKETS
433 S 400 E #200
Salt Lake City, UT 84111

FIRST NATIONAL BANK OF LAYTON
PO BOX 248
LAYTON, UT 84041

FIRST NATIONAL BANK OF LAYTON
480 E 400 S
Salt Lake City, UT 84111

FISCHER, THIERRY & CATHERINE R; JT
123 W SOUTHTEMPLE ST
SALT LAKE CITY, UT 84101-1403

FOOTE, LARRY J
406 E 300 S #100
SALT LAKE CITY, UT 84111

FRAME, SUE; TR
77 N 'J' ST
SALT LAKE CITY, UT 84103-3422

FRED MEYER JEWELERS #00094
455 S 500 E
Salt Lake City, UT 84102

FRESHMAN ENTERPRISES
353 E 500 S
SALT LAKE CITY, UT 84111-3315

FRONTIER
942 E 400 S
SALT LAKE CITY, UT 84102-3016

G & S PROPERTIES, LLC
PO BOX 9069
SALT LAKE CITY, UT 84109-0069

GILLMAN, JOHN K & TINA B; JT
829 E 400 S #111
SALT LAKE CITY, UT 84102-2728

GILLMOR, FLORENCE J
201 S MAIN ST #1800
SALT LAKE CITY, UT 84111-0458

GOVTEK LLC
352 S DENVER ST #230
Salt Lake City, UT 84111

GREY OAK LLC
2157 S LINCOLN ST
SALT LAKE CITY, UT 84106-2306

GREY OAK LLC
455 E 400 S #101
Salt Lake City, UT 84111

H B BOYS INC; ET AL
2280 S MAIN ST
SOUTH SALT LAKE, UT 84115-2629

HALLEY'S HIGH MOUNTAIN MASSAGE
455 E 400 S #10
Salt Lake City, UT 84111

HALLOWEEN USA
634 E 400 S
Salt Lake City, UT 84102

HANDS OF LIGHT TRIO
455 E 400 S #50
Salt Lake City, UT 84111

HARDAGE SUITE HOTELS, LLC
12671 HIGH BLUFF DR #300
SAN DIEGO, CA 92130

HARMAN MANAGEMENT CORPORATION
5544 S GREEN ST
MURRAY, UT 84123-5798

HARMONY MASSAGE, LLC
363 S 500 E #109
Salt Lake City, UT 84102

HARRIS JEFFREY D
366 S 500 E #205
Salt Lake City, UT 84111

HARRIS TRENT
366 S 500 E #204
Salt Lake City, UT 84111

HATCH REAL ESTATE LLC
4571 S HOLLADAY BLVD
HOLLADAY, UT 84117-4425

HEALING WATERS
455 E 400 S #207
Salt Lake City, UT 84111

HEAVENLY HANDS FOR MASSAGE
475 E 500 S #250
Salt Lake City, UT 84111

HERMES ASSOCIATES LTD
3300 ENTERPRISE PARK WAY
BEACHWOOD, OH 44122

HERMES BUILDING ANNEX LLC
3300 ENTERPRISE PARK WAY
BEACHWOOD, OH 44122

HEVENLY HANDS
455 E 400 S #30
Salt Lake City, UT 84111

HIRES DRIVE IN
425 S 700 E
Salt Lake City, UT 84102

HOLLBERG DEVELOPMENT &
CONSTRUCTION LLC
448 E 400 S #303
Salt Lake City, UT 84111

HOLLYWOOD VIDEO #044-608
613 E 400 S
Salt Lake City, UT 84102

HOME CAREGIVERS
455 E 400 S #410
Salt Lake City, UT 84111

HUTCHINGS BRENT DDS
440 S 700 E #305
Salt Lake City, UT 84102

ILLUSIONS HAIR STUDIO
363 S 500 E #101
Salt Lake City, UT 84102

INTERGLOBAL SLC LLC
455 E 400 S #202
Salt Lake City, UT 84111

INTERMOUNTAIN COMMERCIAL REAL
ESTATE LLC
376 E 400 S #120
SALT LAKE CITY, UT 84111-2915

INTERNATIONAL CODE COUNCIL INC
455 E 500 S #202
Salt Lake City, UT 84111

INTERNEXUS SOLUTIONS
455 E 400 S #202
Salt Lake City, UT 84111

INTERSTATE BRANDS CORPORATION
734 E 400 S
Salt Lake City, UT 84102

INTERSTATE BRANDS CORPORATION ET AL
PO BOX 419627
KANSAS CITY, MO 64141-6627

INVESTRIO INC
455 E 400 S #203
Salt Lake City, UT 84111

J.MATHIS STYLIST
363 S 500 E
Salt Lake City, UT 84102

JACOBY GROUP LLC
5790 S WHITEWATER DR
HOLLADAY, UT 84121-1541

JAMBA JUICE
613 E 400 S
Salt Lake City, UT 84102

JANDA, LISA & MARK; JT
2851 E ALEXANDER CT
GILBERT, AZ 85298-4297

JENNIFER BILLINGSLEY, LMT
352 S DENVER ST #320
Salt Lake City, UT 84111

JENNIFER CHRISTENSEN
363 S 500 E
Salt Lake City, UT 84102

JENSEN, JEFFREY J; TR
1338 S FOOTHILL DR #311
SALT LAKE CITY, UT 84108-1953

JIFFY LUBE #1224
804 E 400 S
Salt Lake City, UT 84102

JIMMY JOHN'S
605 E 400 S
Salt Lake City, UT 84102

JORDAN D HARWARD
363 S 500 E
Salt Lake City, UT 84102

K S G ROY-FOURTH SOUTH, LLC
748 HERITAGE PRK BLVD 20
LAYTON, UT 84041-5725

KENTUCKY FRIED CHICKEN
555 E 400 S
Salt Lake City, UT 84102

KEY BANK OF UTAH
410 E 400 S
Salt Lake City, UT 84111

KLTMD, LLC
173 HILLSIDE LN
NORTH SALT LAKE, UT 84054

KNUCKLEHEADS
443 E 400 S
Salt Lake City, UT 84111

KNUDSEN, DARWIN C & LENORE W; TRS
7177 S HIGHLAND DR
COTTONWOOD HTS, UT 84121-3703

KNUDSEN, LENORE W & WAYNE E & CRAIG
W; TRS ET AL
7177 S HIGHLAND DR
COTTONWOOD HTS, UT 84121-3703

KRISTINA'S HANDS
455 E 400 S #50-2
Salt Lake City, UT 84111

KULA YOGA STUDIO INC.
823 E 400 S
Salt Lake City, UT 84102

L & L HAWAIIAN BARBECUE
358 S 700 E #A3
Salt Lake City, UT 84102

LARSEN RALPH B CPA
448 E 400 S #204
Salt Lake City, UT 84111

LAW OFFICE OF TIMOTHY C ALLEN
352 S DENVER ST #240
Salt Lake City, UT 84111

LHR INC
PO BOX 171003
SALT LAKE CITY, UT 84117-1003

LIBERTY TAX SERVICE - SUGARHOUSE
366 S 500 E #101
Salt Lake City, UT 84111

LIBRARY SQUARE ANNEX LLC
231 E 400 S #380
SALT LAKE CITY, UT 84111-2831

LIBRARY SQUARE CENTRE, LLC
1462 E FEDERAL HEIGHTS DR
SALT LAKE CITY, UT 84103-4444

LIN FAMILY HPJ LLC
255 E 400 S
SALT LAKE CITY, UT 84111-2810

LITZAS PIZZA
716 E 400 S
Salt Lake City, UT 84102

LOTUS BODYWORKS
363 S 500 E #112
Salt Lake City, UT 84102

LRH INC
PO BOX 171003
SALT LAKE CITY, UT 84117-1003

LUNABERRY
358 S 700 E #D
Salt Lake City, UT 84102

LYNNE WHITESIDES
455 E 400 S #405
Salt Lake City, UT 84111

M E DANCY CONSULTING SERVICES
455 E 500 S #205
Salt Lake City, UT 84111

MANA DEVELOPMENT LLC
790 N NORTHSHORE ST
SALT LAKE CITY, UT 84103

MANN, WILLARD C.
353 E 500 S
SALT LAKE CITY, UT 84111-3315

MANO A MANO MASSAGE
455 E 400 S #10
Salt Lake City, UT 84111

MARJI HANSON P.C.
352 S DENVER ST #240
Salt Lake City, UT 84111

MARSH, CARYL ANN; TR
829 E 400 S #110
SALT LAKE CITY, UT 84102-2728

MARTIN D. OLSON FINANCIAL LLC
455 E 500 S #400
Salt Lake City, UT 84111

MASSAGE ACADEMY REIKI AND MASSAGE
ROOM RENTALS
455 E 400 S #30
Salt Lake City, UT 84111

MATTHEW W DRIGGS FAMILY LIMITED
PARTNERSHIP, THE
331 S 600 E
SALT LAKE CITY, UT 84102-4013

MAXINE'S BARBER SHOP
366 S 500 E #105
Salt Lake City, UT 84111

MBA REALTY
340 E 400 S #175
Salt Lake City, UT 84111

MCCANDLESS, GLORIA D & FRANKIE; JT
330 S 700 E
SALT LAKE CITY, UT 84102-2113

MELONY BEVER
455 E 400 S #50
Salt Lake City, UT 84111

MERCURY INVESTMENTS LP; ET AL
PO BOX 3208
PARK CITY, UT 84060

MFJF SALT LAKE LLC
810 SEVENTH AVE
NEW YORK, NY 10019

MICELLI GROUP LLC
30699 RUSSELL RANCH RD
WESTLAKE VILLAGE, CA 91362

MILLER, MARIAN K; TR
1338 S FOOTHILL DR #311
SALT LAKE CITY, UT 84108-1953

MINDI STAY
363 S 500 E #107
Salt Lake City, UT 84102

MISS KITTY'S
455 E 400 S #306
Salt Lake City, UT 84111

MOSCA VISCA
448 S 500 E #D
Salt Lake City, UT 84111

MOTLEY TECHNICAL SUPPORT
366 S 500 E #104
Salt Lake City, UT 84111

MOUNTAIN COURTYARD SUITES
350 S 300 E #224
Salt Lake City, UT 84111

MOUNTAIN COURTYARD SUITES LLC
PO BOX 58324
SALT LAKE CITY, UT 84158-0324

MOUNTAIN CREEK BUILDERS LLC
455 E 400 S #400
Salt Lake City, UT 84111

MY MASSAGE PARADISE
455 E 400 S #20
Salt Lake City, UT 84111

MY PRO AGENT INC
352 S DENVER ST #222
Salt Lake City, UT 84111

N B INC
455 E 500 S #203
Salt Lake City, UT 84111

NACEY INVESTMENTS LLC
50 W BROADWAY ST #1000
SALT LAKE CITY, UT 84101-2005

NAI UTAH COMMERCIAL REAL ESTATE
CORPORATE SERVICES
375 E 500 S
Salt Lake City, UT 84111

NAVALLA LLC
358 S 700 E #248
Salt Lake City, UT 84102

NESS, ERIC
3050 W 2995 S
WEST VALLEY, UT 84119-1728

NEW IMAGES DAY SPA
363 S 500 E #112
Salt Lake City, UT 84102

NEW IMAGES LAB AND SPA
363 S 500 E #112
Salt Lake City, UT 84102

NEWPORT DEVCOR / LIBRARY SQUARE, LLC
231 E 400 S #100
Salt Lake City, UT 84111

NEXSTEP GROUP, LLC
176 N 2200 W #200
SALT LAKE CITY, UT 84116-2983

NIKKO ENTERPRISES CORP / NIKKO SUSHI
645 E 400 S
Salt Lake City, UT 84102

NOODLES & COMPANY
358 S 700 E
Salt Lake City, UT 84102

NY URBAN COUTURE
363 S 500 E #111
Salt Lake City, UT 84102

OFFICEMAX #425
410 S 900 E
Salt Lake City, UT 84102

OILWELL PROPERTIES LC
7430 S CREEK RD #200
SANDY, UT 84093-6113

OPPORTUNITY KNOCKS INC
358 S 700 E #B123
Salt Lake City, UT 84102

ORTON, KIRK G & GAYLA V; JT
10617 S 455 E
SANDY, UT 84070

OWEN, GAIL M
829 E 400 S #106
SALT LAKE CITY, UT 84102-2728

OXFORD FINANCIAL GROUP INC
455 E 500 S #305
Salt Lake City, UT 84111

PACE, DEAN G; ET AL
3550 S 2175 E
SALT LAKE CITY, UT 84109-2903

PACE, DEAN; ET AL
4300 S HIGHLAND DR
SALT LAKE CITY, UT 84124-3504

PALACE MEAT COMPANY
715 E 500 S
Salt Lake City, UT 84102

PAPA JOHNS PIZZA
369 S 400 E
Salt Lake City, UT 84111

PAPA MURPHY'S UNIVERSITY
302 S 700 E
Salt Lake City, UT 84102

PARADISE BAKERY FOUR SOUTH, LLC
618 E 400 S
Salt Lake City, UT 84102

PAUL NICHOLS FINANCIAL SERVICES LLC
455 E 500 S #4TH FL
Salt Lake City, UT 84111

PEAK HEALTH DEVELOPMENT REIKI
INDEPENDENT CONTRACT
455 E 400 S #50
Salt Lake City, UT 84111

PETERSEN R K & MARIE
347 S 800 E
Salt Lake City, UT 84102

PETERSEN, REED K.
11484 S 1380 E
SANDY, UT 84092-5366

PETTY INVESTMENT CO
2001 S WINDSOR ST
SALT LAKE CITY, UT 84105-3223

PETTY, IREVA G, ET AL
2001 S WINDSOR ST
SALT LAKE CITY, UT 84105-3223

PIZZA HUT - #745114
775 E 400 S
Salt Lake City, UT 84102

PORTFOLIO MANAGER INTERNATIONAL
353 E 400 S #301
Salt Lake City, UT 84111

POTTER, JOHN M
829 E 400 S #103
SALT LAKE CITY, UT 84102-2728

PRECISION DENTAL LABORATORY
440 S 700 E #306
Salt Lake City, UT 84102

PROGENEALOGISTS, INC.
455 E 400 S #300
Salt Lake City, UT 84111

PURE BODYWORK, LLC
352 S DENVER ST #223
Salt Lake City, UT 84111

Q LUBE #2038
677 E 400 S
Salt Lake City, UT 84102

QUICK GYM UTAH LLC
363 S 500 E #110
Salt Lake City, UT 84102

QUICK LOAN
464 S 600 E #C
Salt Lake City, UT 84102

RADIANCE INSIDE OUT
363 S 500 E
Salt Lake City, UT 84102

RADIANCE MEDSPA
363 S 500 E
Salt Lake City, UT 84102

RAPID ORTHOPEDIC CARE CLINIC
821 E 400 S
Salt Lake City, UT 84102

RB&K LLC
333 E 400 S
SALT LAKE CITY, UT 84111-2901

RED CANYON PICTURES LLC
455 E 400 S #102
Salt Lake City, UT 84111

REED, DAVID L; TR (DLR TRUST)
829 E 400 S #108
SALT LAKE CITY, UT 84102-2728

REMINGTON RESEARCH
455 E 400 S #209
Salt Lake City, UT 84111

RENT A CENTER INC #02313
409 E 400 S
Salt Lake City, UT 84111

Residents
666 E 300 S
Salt Lake City, UT 84102-2128

Residents
328 S 700 E
Salt Lake City, UT 84102-2113

Residents
353 S 800 E
Salt Lake City, UT 84102-2207

Residents
436 S 700 E
Salt Lake City, UT 84102-2806

Residents
360 S STRONGS CT
Salt Lake City, UT 84102-2303

Residents
362 S STRONGS CT
Salt Lake City, UT 84102-2303

Residents
853 E 400 S
Salt Lake City, UT 84102-2928

Residents
440 S 900 E
Salt Lake City, UT 84102-2915

Residents
928 E 400 S
Salt Lake City, UT 84102-3016

Residents
928 E 400 S #BLDG 2
Salt Lake City, UT 84102-3016

Residents
940 E 400 S #REAR
Salt Lake City, UT 84102-3016

Residents
946 E 400 S
Salt Lake City, UT 84102-3016

Residents
352 S 300 E
Salt Lake City, UT 84111-2504

Residents
356 S 300 E
Salt Lake City, UT 84111-2504

Residents
358 S 300 E
Salt Lake City, UT 84111-2504

Residents
341 E 400 S
Salt Lake City, UT 84111-2901

Residents
365 S DENVER ST
Salt Lake City, UT 84111-3002

Residents
367 S DENVER ST
Salt Lake City, UT 84111-3002

Residents
369 S DENVER ST
Salt Lake City, UT 84111-3002

Residents
371 S DENVER ST
Salt Lake City, UT 84111-3002

Residents
373 S DENVER ST
Salt Lake City, UT 84111-3002

Residents
375 S DENVER ST
Salt Lake City, UT 84111-3002

Residents
477 E 400 S
Salt Lake City, UT 84111-3005

Residents
338 E 400 S
Salt Lake City, UT 84111-2902

Residents
437 S 400 E
Salt Lake City, UT 84111-3320

Residents
461 S 400 E
Salt Lake City, UT 84111-3302

Residents
465 S 400 E
Salt Lake City, UT 84111-3302

Residents
469 S 400 E
Salt Lake City, UT 84111-3302

Residents
452 S 500 E
Salt Lake City, UT 84111-2705

Residents
329 S 600 E
Salt Lake City, UT 84102-4013

Residents
512 E 400 S
Salt Lake City, UT 84102

Residents
443 S 600 E
Salt Lake City, UT 84102-2708

Residents
851 E 400 S
Salt Lake City, UT 84102-2928

Residents
490 S 400 E
Salt Lake City, UT 84111-3303

Residents
625 E 500 S
Salt Lake City, UT 84102-2808

Residents
639 E 400 S
Salt Lake City, UT 84102

Residents
651 E 400 S
Salt Lake City, UT 84102-

Residents
227 E 400 S
Salt Lake City, UT 84111-2803

RESORT APPRAISAL SERVICE - SLC
352 S DENVER ST #310
Salt Lake City, UT 84111

RICHARD P SMOOT FAMILY LLC
847 E 400 S
SALT LAKE CITY, UT 84102-2928

RICHINS
479 S 700 E
Salt Lake City, UT 84102

ROBBINS, WILSON K; TR
3730 E MILLSTREAM DR
SALT LAKE CITY, UT 84109-3221

ROTHMAN, NOEL
311 S WACKER DRIVE #4190
CHICAGO, IL 60606-6618

RUBIOS BAJA GRILL
358 S 700 E #F
Salt Lake City, UT 84102

RUBY RIVER STEAKHOUSE #404
435 S 700 E
Salt Lake City, UT 84102

RUMBI ISLAND GRILL
358 S 700 E #E
Salt Lake City, UT 84102

RUMEL PLACE LLC
358 S 500 E
SALT LAKE CITY, UT 84102-4022

RYAN CONSELING LPPC
352 S DENVER ST #360
Salt Lake City, UT 84111

SALLY O'MARRA
363 S 500 E #107
Salt Lake City, UT 84102

SALT LAKE CITY BOARD OF EDUCATION
440 E 100 S
SALT LAKE CITY, UT 84111-1898

SALT LAKE CITY CORPORATION
PO BOX 145460
SALT LAKE CITY, UT 84114-5460

SALT LAKE COFFEE BREAK
430 E 400 S
Salt Lake City, UT 84111

SAMUELS, GARTH S; ET AL
829 E 400 S #112
SALT LAKE CITY, UT 84102-2728

SCHWARTZ, DEVON
934 E 400 S
SALT LAKE CITY, UT 84102-3016

SENIOR HELPERS OF SALT LAKE CITY
352 S DENVER ST #101
Salt Lake City, UT 84111

SEREBRYAKOV, ALEXANDER
11054 S TOPVIEW RD
SOUTH JORDAN, UT 84095-7719

SEVEN65 LOFTS SALT LAKE CITY LLC
1301 DOVE ST #1080
NEWPORT BEACH, CA 92660

SIZZLER #321
371 E 400 S
Salt Lake City, UT 84111

SMITH'S FOOD & DRUG CENTERS INC
1550 S REDWOOD RD
SALT LAKE CITY, UT 84104-5105

SMITH'S MARKETPLACE #94
455 S 500 E
Salt Lake City, UT 84102

SMOOT, BRIAN M
837 E 400 S
SALT LAKE CITY, UT 84102-2903

SMS GROUP, LLC
455 E 400 S #309
Salt Lake City, UT 84111

SNYDER, STEPHANIE A
829 E 400 S #104
SALT LAKE CITY, UT 84102-2728

SOUTHRIDGE FINANCIAL INC
466 S 500 E
SALT LAKE CITY, UT 84102-2705

SOVEREIGN FINANCIAL GROUP
455 E 500 S #400
Salt Lake City, UT 84111

SPOON ME
532 E 400 S
Salt Lake City, UT 84102

SPRINT
665 E 400 S
Salt Lake City, UT 84102

STACY TOOL CREATIONS
455 E 500 S #205
Salt Lake City, UT 84111

STANFORD COURT OWNERS ASSOCIATION
6792 S 1300 E
COTTONWOOD HTS, UT 84121-2721

STAPLES THE OFFICE SUPERSTORE
624 E 400 S
Salt Lake City, UT 84102

STEWART & CHRISTENSEN LLC
455 E 500 S #207
Salt Lake City, UT 84111

STONEGROUND
249 E 400 S
Salt Lake City, UT 84111

STUDIO SALONS 4TH SOUTH LLC
363 S 500 E
Salt Lake City, UT 84102

SUBWAY SANDWICHES
421 E 400 S
Salt Lake City, UT 84111

SUPERCUTS #90203
358 S 700 E #C
Salt Lake City, UT 84102

SYCAMORE HOLDINGS, LC
1462 E FEDERAL HEIGHTS DR
SALT LAKE CITY, UT 84103-4444

SYLVIA JOY NOON
455 E 400 S #50
Salt Lake City, UT 84111

T H A INVESTMENTS, LTD
424 S 700 E
SALT LAKE CITY, UT 84102-2864

TACO BELL #16181
621 E 400 S
Salt Lake City, UT 84102

TAG STATIONERY
433 S 400 E #100
Salt Lake City, UT 84111

TERRAPIN ENTERPRISES OF UTAH
PARTNERSHIP
426 S 500 E
SALT LAKE CITY, UT 84102-2705

TESORO
680 E 400 S
Salt Lake City, UT 84102

TESORO WEST COAST COMPANY
13111 NORTHWEST FWY #125
HOUSTON, TX 77040-6321

THA INVESTMENTS LTD
424 S 700 E
SALT LAKE CITY, UT 84102-2864

THE LAW OFFICE OF EWARD AND FLETT
455 E 400 S #101
Salt Lake City, UT 84111

THE LIFTED LIFE HEALING HANDS
455 E 400 S #BSMT
Salt Lake City, UT 84111

THE REIKI SPECIALISTS
243 E 400 S #200
Salt Lake City, UT 84111

THE RELAXATION BODYWORK
455 E 400 S #30
Salt Lake City, UT 84111

THOR B ROUNDY PC
448 E 400 S #100
Salt Lake City, UT 84111

T-MOBILE
613 E 400 S #D
Salt Lake City, UT 84102

TOUCHOPIA LLC
440 S 700 E #203
Salt Lake City, UT 84102

TRAIN SHOPPE INC
470 S 900 E
Salt Lake City, UT 84102

TRAINING TABLE #2
809 E 400 S
Salt Lake City, UT 84102

TRANSWESTERN GENERAL AGENCY INC
440 S 500 E
SALT LAKE CITY, UT 84102-2705

TRIPP, N JOSH L
976 E FIRST AVE
SALT LAKE CITY, UT 84103-3924

TRUE REFLECTIONS
363 S 500 E #102
Salt Lake City, UT 84102

UNIVERSITY PHILLIPS 66
873 E 400 S
Salt Lake City, UT 84102

UPPER COLORADO RIVER COM.
355 S 400 E
SALT LAKE CITY, UT 84111-2904

UPS STORE #3851, THE
358 S 700 E #B
Salt Lake City, UT 84102

UTAH POWER & LIGHT CO CARTER, JOSEPH
T
825 NE MULTNOMAH ST #1900
PORTLAND, OR 97232

UTAH PRIDE CENTER
355 N 300 W
SALT LAKE CITY, UT 84103-1215

UTAH TRANSIT AUTHORITY
3600 S 700 W
WEST VALLEY, UT 84119-4122

UTAH VALLEY TIRE INC
420 S 500 E
Salt Lake City, UT 84111

VALLEY MENTAL HEALTH INCORPORATED
5965 S 900 E
MURRAY, UT 84121-1720

VILLAGE INN PANCAKE HOUSE #076
910 E 400 S
Salt Lake City, UT 84102

VINCENT COURT LLC
655 E 400 S #200
SALT LAKE CITY, UT 84102-2870

VINCENT COURT LLC
320 S 700 E
Salt Lake City, UT 84102

VIXENS BODY & ENERGY WORK
455 E 400 S #70
Salt Lake City, UT 84111

WAGSTAFF, DAVID B
1061 S CRESTVIEW CIR
SALT LAKE CITY, UT 84108-2077

WALGREENS #09238
531 E 400 S
Salt Lake City, UT 84102

WASATCH CNA CENTER
455 E 400 S #410
Salt Lake City, UT 84111

WASATCH PIZZA
820 E 400 S
Salt Lake City, UT 84102

WASATCH VISION CLINIC LLC
849 E 400 S
Salt Lake City, UT 84102

WAX ME TOO
363 S 500 E #107
Salt Lake City, UT 84102

WC ADMINISTRATORS LLC
433 S 400 E #200
Salt Lake City, UT 84111

WENDYS OLD FASHIONED HAMBURGER
562 E 400 S
Salt Lake City, UT 84102

WENDYS OLD FASHIONED HAMBURGERS
OF NEW YORK INC
1155 PERIMETER CNTR WE ST
ATLANTA, GA 30338

WENDYS OLD FASHIONED HAMBURGERS
OF NEW YORK, INC
1155 PERIMETER CNTR WE ST
ATLANTA, GA 30338

WHITNEY INVESTMENT PROPERTIES LLC
435 S 400 E
SALT LAKE CITY, UT 84111-3302

WHOLE FOODS MARKET
645 E 400 S
Salt Lake City, UT 84102

WILLIAMSEN SOUTH JORDAN INC
655 E 400 S #200
SALT LAKE CITY, UT 84102-2728

WINGERS DINER
613 E 400 S #B
Salt Lake City, UT 84102

WINKEL GENERAL REMODELING
460 S 400 E
Salt Lake City, UT 84111

WIRELESS GIANT
510 E 400 S
Salt Lake City, UT 84102

WOMANS DIV CHRISTN SERV BD OF
MISSION & CH EXT METHDST CH
347 S 400 E
SALT LAKE CITY, UT 84111-2904

WRIGHT, JEFFREY E
159 W BROADWAY ST #200
SALT LAKE CITY, UT 84101-2079

WRIGHT, MICHAEL J & CYNTHIA L; JT
1335 S COLONIAL CIR
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**5A. PUBLIC MEETINGS AND PARTICIPATION
SUMMARY DOCUMENT**

Salt Lake City Planning Division

400 South Livability Project Public and Stakeholder Outreach



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Maryann Pickering

Project Team

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Daniel Echeverria

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Daunte Rushton

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Salt Lake City Planning

400 South Livability Project Public and Stakeholder Outreach

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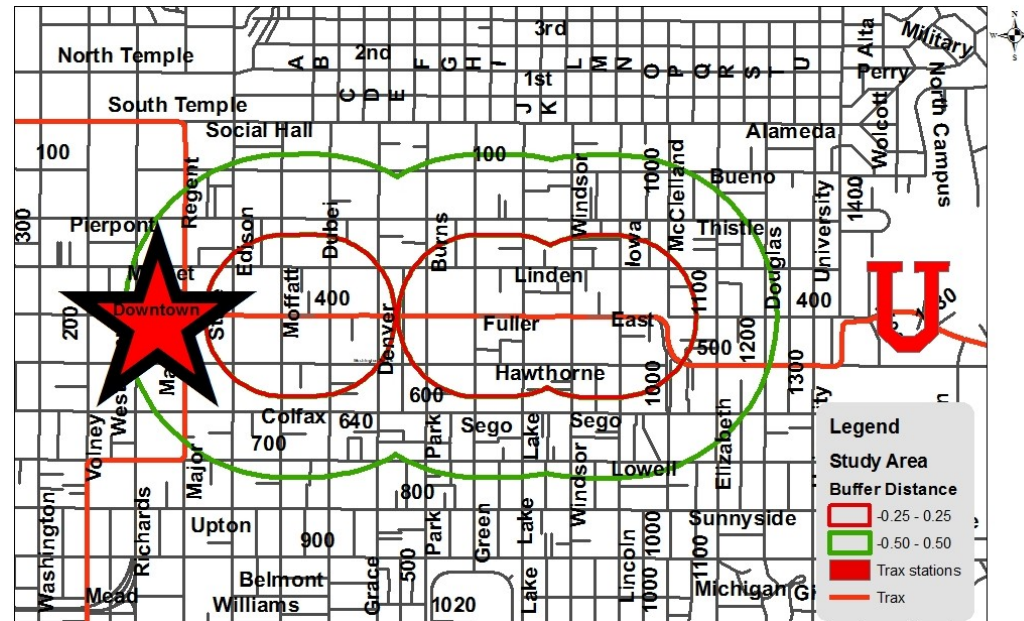
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Purpose of Meeting

The purpose of the May 19th Community Workshop was to identify those things about the 400 South corridor and adjacent neighborhoods that people liked, did not like and what they wanted to see happen in the future. For the Planning Division, this was an opportunity to listen to feedback and to facilitate discussions on a wide range of issues related to the corridor. For the public, it was an opportunity to discuss the issues with each other without being directed by the City.

This report is a summary of what was heard by the City. The comments from the meeting will be used, along with comments and input from future meetings, will be used to create guiding principles and proposed City policies to help guide future decision making.



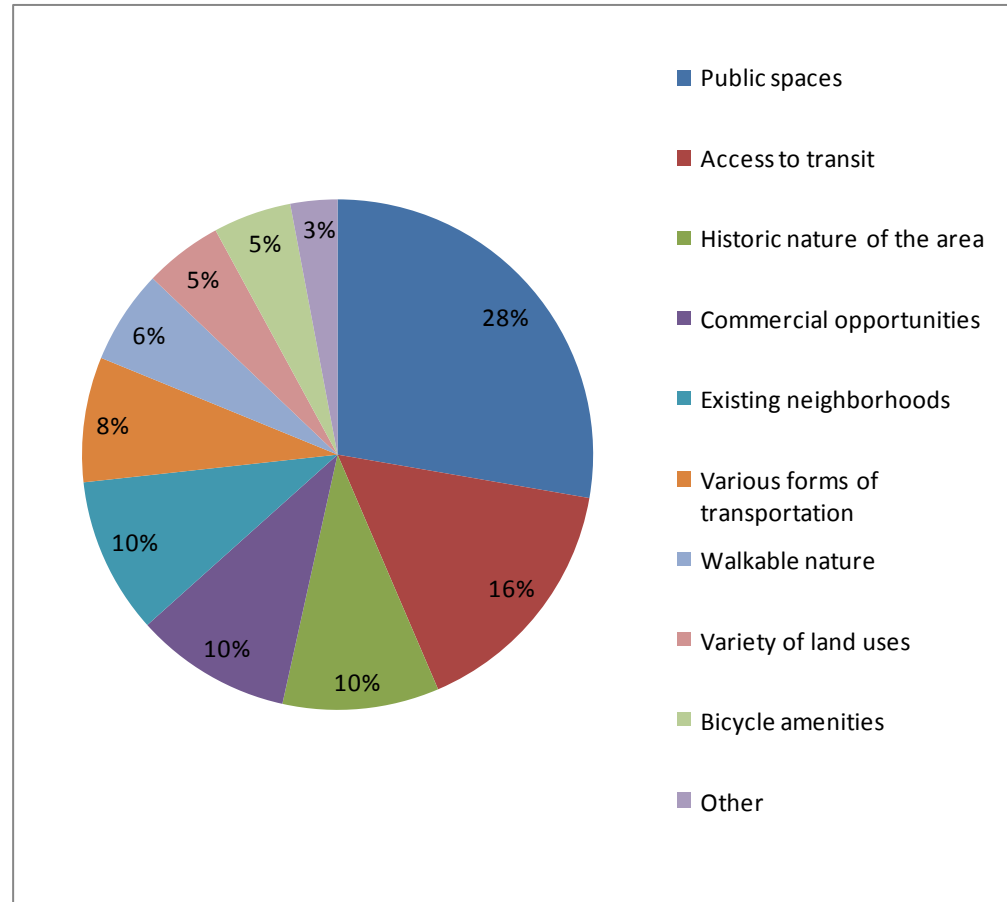
The 400 South Livability Project study area is generally the area within a $\frac{1}{2}$ mile radius of each transit station, shown by the green line on the above map. The study area was established based on a number of studies and best practices from across the country that indicate light rail transit benefits are mostly found within a $\frac{1}{2}$ mile walking distance of a transit station. Those areas that within $\frac{1}{4}$ mile (the red circles on the above map) see the greatest benefits and the benefits decrease the further away one travels from a transit station.

The study area is being used in a number of ways. First, it is used to identify stakeholders. For the May 19 Workshop, all property owners and building tenants (both residential tenants and businesses) were sent notices of the workshops. Second, the study area will be used by the Planning Division to analyze development related issues based on feedback received during the May 19th workshop and other future workshops related to the project.

What the Community Likes

Those items that the community likes generally fit into the following categories:

- Public spaces, including parks, open space, landscaping, etc
- Access to transit
- Historic nature of the area
- The commercial opportunities in the area
- The existing neighborhoods
- The various forms of transportation that are available in the area
- The walkable nature of the area
- The variety of land uses in the area
- Bicycle amenities



The general categories of things that people like about the corridor, shown as a percentage of the total number of comments received.

Specific comments about what people like (comments are in no particular order)

Access to Transit

- Access to transit
- Proximity to transit
- Minimal distance between stations
- TRAX
- Frequency of Tax stops
- Transit access to larger region
- Likes no Tax parking
- Hybrid electric buses

Bicycle Amenities

- Bike lanes
- 800 East should be a bicycle street
- Bicycle amenities

Historic Nature of Area

- Encourage adaptive reuse of buildings
- Historic buildings
- Neighborhoods adjacent to 400 South
- Historic nature of neighborhoods
- Historic markers
- Exchange Place
- Washington Square

Public Spaces, Parks, Open Space, Landscaping

- Trees
- Green spaces
- 600 East medians
- Smaller parks; such as Fault Line, Senior Center, Gilgal
- Landscaping in Tax corridor
- 800 East medians
- Pedestrian friendly green buffers
- Washington Square and the festivals
- Public safety building
- Library
- Station art
- Street art
- Gallivan Center

Commercial

- Easy access to commercial spaces
- Trolley square renovation
- Fast food and drive thru restaurants
- Diverse nature of commercial
- 400 South commercial
- Mix of retail uses

Walkability

- Countdown clocks
- Urban feel of library station, lots of pedestrians
- Walkable neighborhoods
- Pedestrian amenities
- Sidewalks
- Crossing flags
- Locations where building are built up to the sidewalk
- Walkability

Neighborhoods

- Quiet, safe neighborhoods
- High density
- Buffers to neighborhood
- Diverse characteristics
- Existing mixed use neighborhoods
- Protection of single family neighborhoods
- Limited negative impacts from Tax on neighborhoods
- Integrated neighborhoods
- Mix of housing

Variety

- Density of uses
- Variety
- Ethnic/age mix
- Mix of uses
- University students

Transportation

- Uncongested streets
- U-Car share program
- A good travel way for cars between downtown, east bench with little impact to neighborhoods
- Access to university
- Drivable nature of corridor
- Regional location to mountains, recreation, etc.
- Access to downtown

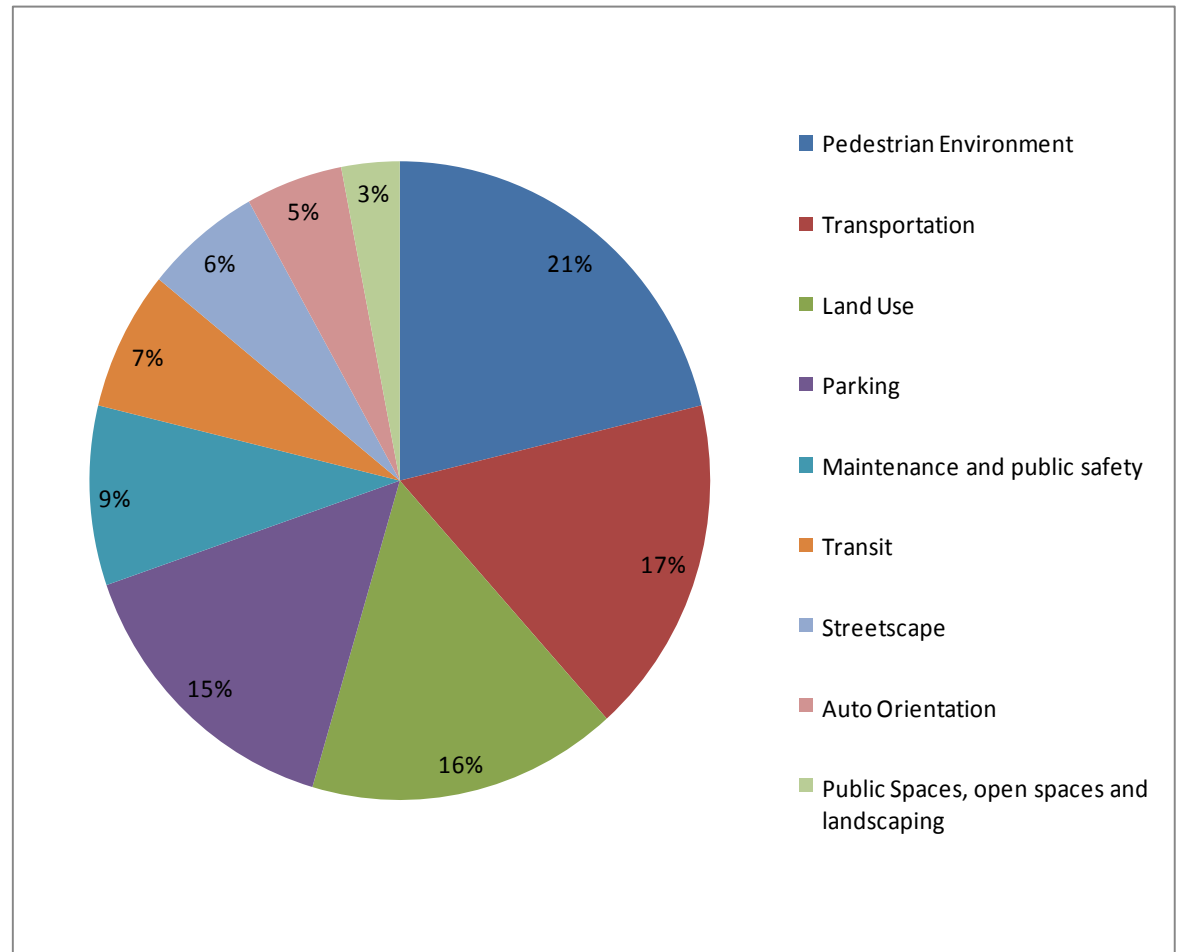
Miscellaneous

- SL Regional Hospital
- Cleanliness
- New architecture downtown
- Potential of area

What the Community Does Not Like

Those items that the community likes generally fit into the following categories:

- Pedestrian environment
- Transportation
- Land Use
- Parking
- Maintenance and public safety
- Transit
- Streetscape
- Auto orientation
- Public spaces, open spaces and landscaping



The general categories of things that people do not like about the corridor, shown as a percentage of the total number of comments received.

Specific comments about what people do not like (comments are in no particular order)

Pedestrian Environment

- Sidewalks are not wide enough or are in disrepair
- Unfriendly/unsafe to walk (cluttered, narrow, no buffer from traffic, blind driveways, etc)
- Limited pedestrian access to Tax is not conducive to walking and supports jaywalking
- Pedestrian conflicts with auto infrastructure (wide roads, driveways, right turns, etc.)
- Pedestrian walkways have no amenities (shade, benches, garbage cans, etc.)
- Mid block walkways are not safe or well taken care of
- Feel out of place walking on 400 South, it is not pedestrian friendly

Parking

- Middle of the street parking
- Lack of pedestrian access through parking lots
- Tax riders parking in commercial lots
- Parking is a nightmare in the eastern neighborhoods
- Most annoying and arrogant thing the City says is you should not have any expectation that you can park in front of your house

Public Spaces, Open Spaces, and Landscaping

- Not enough open space, green space, community space, trees, landscaping

Auto Orientation

- Auto oriented uses/dependency
- Three lanes of auto traffic in each direction on 400 South
- Auto oriented destination

Transportation

- Traffic flow, congestion, speeding
- Cars using side streets to cut through busy intersections
- Stoplights are not synchronized
- Dangerous intersections (unsignalized crossing on 800 East, two way stops, etc.)
- 800 East medians block trash and truck access
- Traffic noise, especially from buses
- Lack of traffic calming
- More access to car share program
- U-Car share pays high taxes, should be exempt
- Difficult to ride bike on transit streets

Transit

- Bus fares are not conducive for short trips (too expensive, need distance faced fares, expand free fare zone, etc)
- Lack of bus service
- Bus infrastructure is not synced to Tax
- Tax
- Bus stops in residential areas have no amenities and often damage landscape from people waiting, littering, etc.
- Need mid block connection to Tax stations
- noise from buses and trains
- Some of the bus routes that support Tax are going away, limiting the effectiveness of Tax
- Bus stops and pull outs do not work well

Specific comments about what people do not like (comments are in no particular order)

Land Use

- Public safety building should not be across from library
- Center of block development
- Not livable
- Circular nature of TOD, needs to be linear along corridor
- Lack of protection for single family neighborhoods
- 400 South has not encouraged local business, not evolved organically
- Housing on 400 South is not compatible due to the noise
- Empty buildings
- More core services accessible
- Land uses need to relate better to the transportation infrastructure
- No incentives to encourage redevelopment in the right places
- Too many chains and fast food restaurants
- Woefully underused
- Loss of housing to other uses
- Conflicts between zoning and historic district overlay
- Redundant land uses (too many of one land use, like medical offices)
- Tax has not encouraged organic development; 400 South is still mostly big box, fast food and rundown offices
- Do not like high density in the middle of the blocks
- Many people living in high density still do not use TRAX, they drive
- Industry does not belong (Wonder Bread factory)

Streetscape

- Lack of aesthetics
- 400 south feels like anywhere else in America
- Needs to be beautified so it is more like a boulevard
- 400 South is not transit oriented
- 600 East between Trolley Square and the 600 East station
- Resembles State Street between 300 East and 900 East
- No recreational opportunities for kids, either indoor or outdoor.

Maintenance and Public Safety

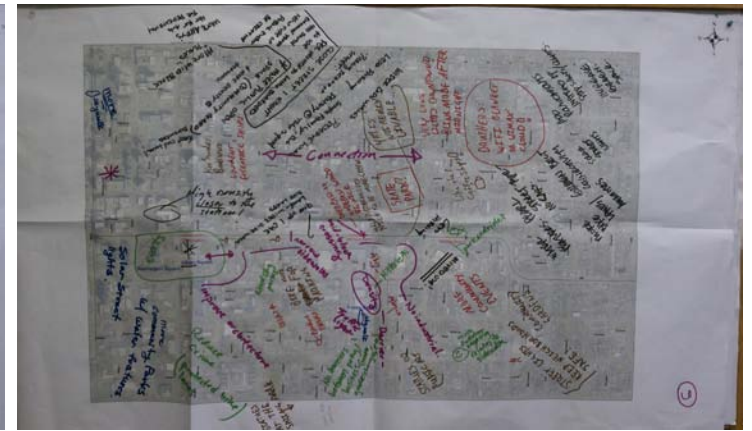
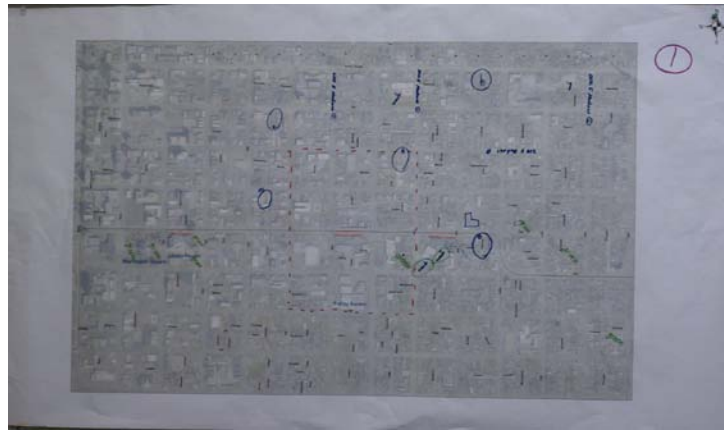
- Loud motorcycles
- Rental units are a business and need to be treated like one to address maintenance issues
- Cats and dogs roaming free
- Not enough yard waste containers
- Boarded houses
- Long term vacancies
- Absentee landlords, both residential and commercial
- Derelict properties
- Curbs not painted red in front of fire hydrants

Visions

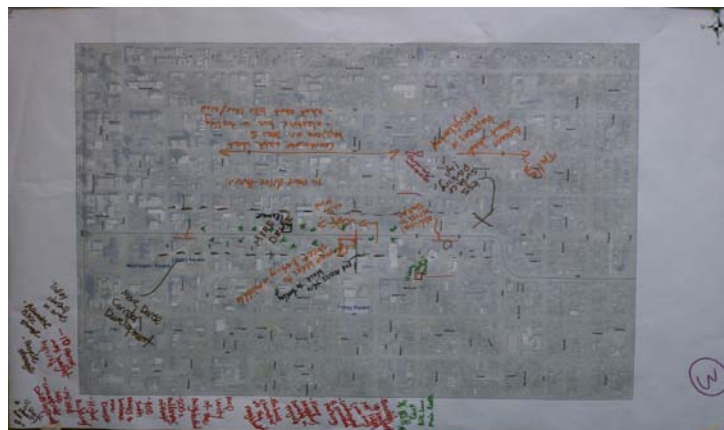
The community workshop provided some very helpful insight as to what the 400 South corridor should look like in the future. This is important because it helps the Planning Division continue to work with the community and stakeholders to craft a long term vision for the area and then create land use policies and development regulations to implement the vision. Most of the visions received during the workshop fall into these general categories:

- Land Use and Development
- Transportation
- Public Spaces
- Bicycles
- Walkability
- Neighborhood Character
- Policy

The visions were collected from the note cards handed out as people arrived, on the flip charts where the facilitators were capturing the discussions at each table and from the information written on the maps at each table.



Images of the maps from the May 19, 2011 Community Workshop



Staff Summary of Vision Comments

Below is a list of all of the vision comments received during the Workshop. Similar comments have been grouped into categories.

Land Use	Number of comments	Transportation	Number of comments
Higher densities closer to Downtown and the stations, with lower densities adjacent to existing neighborhoods.	21	Work with UTA to come up with alternate fare options for transit. Options may include an expanded free fare zone, distance base fares, etc.	2
Provide a wide range of housing options that complement the existing housing types and add new housing types to the area.	7	Provide more transit options that interface with the University TRAX line, such as future street car lines, bus routes, etc. Streets to consider include 600 East to Liberty Park, 900 East, and 200 South.	6
Develop design guidelines that bring buildings closer to the street, address building setbacks, parking locations, landscaping, green roofs and buildings, etc. to make a more pedestrian oriented community.	18	Add park and ride lots at 900 East and the Stadium Stations to serve the residential neighborhoods.	4
More mixed use in appropriate locations.	6	Use roundabouts, traffic lights and stop signs to improve safety at intersections located at 200 South 800 East, 300 South 800 East, 500 South 800 East and 600 East 600 South.	6
Ensure that zoning regulations support appropriate land use, design and scale for historic areas.	5	Synchronize traffic lights and adjust the traffic lights so they that switch to flashing red after midnight.	2
Create incentives for appropriate development and redevelopment of undesirable land uses (surface parking lots, drive-thru restaurants, etc).	18	Coordinate with the 200 South Project to create a multi-modal street along 200 South and use east-west connections to make connections to the University TRAX line	2
Promote Trolley Square as a neighborhood anchor by providing more residential density and other supportive uses around it.	2	Provide funds to fix up interior court like public streets, such as Denver Street.	1
Promote more locally owned businesses along the corridor.	2	Extend the University TRAX line west on 400 South to connect to Salt Lake Central Station	1
		Use visual enhancements to slow traffic.	1
		Eliminate one vehicle lane in each direction on 400 South and replace it with bike lanes and wider sidewalks.	1

Staff Summary of Vision Comments

Public Spaces	Number of comments	General Policy (continued)	Number of comments
Protect the existing green spaces in the area and increase the amount of landscaping in the area by adding pocket parks and greenways throughout the area and extending green islands on 200 South.	8	Take local action to do our part to improve air quality.	1
Add more smaller scale community spaces to complement the large spaces at Library Square and Washington Square and have more events in the spaces.	4	Make transit fares more conducive to short trips and more family friendly.	4
Add more community gardens throughout the area.	2	Innovative community events that utilize the public spaces more, such as bike races, longboarding events, etc.	2
Provide more recreational opportunities	1	Improve the technological infrastructure in the area.	2
Beautify the boulevard	1	Walkability	Number of comments
A wider variety of public spaces are needed, including skate parks and dog parks and public spaces need to include more water features	3	Make 400 South and the adjacent neighborhoods more pedestrian friendly by widening sidewalks, adding buffers between sidewalk and the street, adding safety measures and increasing landscaping.	10
Bicycling	Number of comments	Add more mid block walkways.	4
Provide bike amenities, including sufficient bike parking and secure bike storage.	5	Find ways to improve access and decrease jaywalking to the TRAX stations by adding midblock crosswalks or pedestrian bridges.	6
More bike lanes throughout the area that connect major destinations, such as downtown and the University of Utah, to the University TRAX line stations.	8	Create a greenbelt between downtown and the University of Utah for pedestrians. Consider both 200 South and 300 South.	4
Keep bike lanes on the side of the road, but look at alternatives to make them safer.	1	Require accessible entrances to all businesses.	1
General Policy	Number of comments	Neighborhood Character	Number of comments
Protect property values	2	Preserve neighborhood history, character and variety and protect it from inappropriate development.	12
Improve public safety	3	Promote more locally owned business and less national chains.	2
Improve the public process and keep stakeholders involved.	2	Make the area a welcoming and livable destination.	4
		Provide a consistent look to the corridor.	1
		Control the gentrification of the area.	1

Purpose of Stakeholder and Community Organization Meetings

The purpose of the meetings with stakeholders, was to learn about the future intentions of business and property owners, the potential of developed and undeveloped sites and how they envisioned the future land uses along the corridor.

Meetings were also held with various community organizations to learn more about how their groups and their members interact with the 400 South corridor, how the members of these groups are utilizing the corridor, their views of current and future transit options and land uses along the corridor and how we could find ways to include these groups and their individual members in future public meetings.

This report is a summary of what was heard by the City. The comments from the meetings will be used, along with comments and input from future meetings, will be used to create guiding principles and proposed City policies to help guide future decision making.



Specific comments from stakeholders and community organizations

YWCA Meeting (July 7, 2011)

Transit Related Comments

Do away with tokens for bus and trains, TRAX doesn't use tokens.

Very difficult to get on TRAX and bus transfers with children and strollers.

Bus drivers don't realize how heavy strollers are, please help more.

Bus schedules are always gone.

The Free Zone is good.

Need temporary cards instead of tokens for ease of use on all transit.

Land Use Related Comments

Need more drugstores like Walgreens.

Need more clothing stores like Ross.

Need more grocery stores like Fresh Market.

Need more retail stores offering electronics.

Need book stores.

Need more affordable housing near transit.

Need more urban recreational opportunities.

Need more daycare centers near transit.

Too many fast food chains restaurants (but still convenient).

Odyssey House Meeting (August 26, 2011)

Transit Related Comments

New trains are nice, with more space and bigger seats.

Land Use Related Comments

Need to focus on lots between 9th E and Library - it is "deadsville" for a pedestrian.

Area SE of 9th E stop could have more density and apartments, redevelop as they are in terrible condition.

Odyssey House Meeting (continued)

Need a definition in Zoning Ordinance for "Smoke Shops" as these are unwanted land uses and bring in unwanted people and drug activity which is not good for the corridor or neighborhood

Prohibit "pawn shops" along 4th South.

Put a great gym with indoor recreation opportunities, pool in the old "Whole Foods" space.

Put a little Walmart into "Whole Foods." Something 24 hour and to serve those living around neighborhood.

Need more pedestrian alleys and created spaces on and around the corridor planning area, these urban walkways add interest and give the community a place to meet friends and shop. Would help make it more walkable and wouldn't have to walk along 4th South as sidewalk isn't that pleasant.

Want more cafes and outdoor seating areas, place to meet friends and enjoy the area.

Food carts are fun and add interest and social activity along the street.

Limit drive-thrus as they are hard to walk past and just don't create an interesting place.

Need more mixed-use like the Broadway, with apartments above and small grocery and retail below, put these by the stations. Need a lot of density so that more uses will stay open longer.

Need new apartments or condos by pocket parks so that the park clientele are "cleaned up."

Need more drugstores like Walgreens, especially 24 hour pharmacies.

Need more clothing stores like Ross.

Need more grocery stores like Fresh Market.

Need more retail stores offering electronics like Apple or Best Buy.

Need book stores.

Need more affordable housing near transit, but also want high-end housing to balance out socio-economic groups and to provide enough foot traffic and populations with higher disposable income so that better retailers will choose to locate on the corridor.

Need a mix of housing types to bring in both the bachelor and a large family.

Need to allow higher density so that the old and decrepit apartment complexes are redeveloped, thus offering a higher standard of living around the corridor.

A hotel or small hotels or B&B type places on the corridor or around stations would be nice, and would bring in more nightlife and pedestrian activity and possibly nicer retail stores.

Specific comments from stakeholders and community organizations

Odyssey House Meeting (continued)

Need more urban recreational opportunities, parks, bowling alley, arcade or pool halls. More family oriented recreation is especially needed. Another Gallivan Center-type of place would be welcome.

Need more destination lounges, the Green Pig is only place open on Sundays.

Need more daycare centers or charter schools near transit, this would liven the place with young people.

Parking is fine as long as located underground.

Too many fast food chains restaurants (but still convenient).

Property Owner Meeting (August 10, 2011)

Center was made to be walkable to the north.

Owners feel like they are shoehorned with the current zoning.

Would like to be able to go up more or build taller buildings.

Suggest that this project look further back than one half of a block.

Varied setback is possible. For example, buildings directly on the street would be limited to three stories in height and it would setback up to seven stories the further away from the street.

Right now, market is for rental residential units, very tough to do sales.

Would like the flexibility to expand the former Whole Foods building. Expansion would take place to then north. Right now it is 25,000 square feet and 40,000 would make it more attractive to potential tenants

Difficult to do one building mixed use projects these days because of financing. Lenders prefer to lend on mixed use projects when the residential and commercial uses are in separate buildings, but they can be on the same lot or part of the same project. This is national, not just a local trend.

Most mixed use projects (in a traditional single building) are into their second or third owners because it is just hard to do in most areas unless the project is located in a very dense downtown area.

Tenants prefer to lease a project when parking is available in front of the storefront. Creating separate areas for parking is not desirable.

Owners feel that the city is short sighted in wanting a walkable community, but then density is limited.

Property Owner Meeting (continued)

Being in a historic district is a hindrance. Tenants do not want to have to go through two processed to obtain signs or to make minor modifications to their structure. It's also hard when modern signs are preferred on a faux historic building (Chipotle in Sugarhouse example).

RMU (with no density restriction) is the type of zoning designation they feel would work for the site.

University of Utah Transportation Meeting (June 27, 2011)

Transit Related Comments

Students should be encouraged to use transit more. This could be accomplished by adding more density along TRAX line.

University has a significant transit system, with busses and proposed light rail spurs, please keep them informed as to future developments.

What impact, if any, will this study have on proposed bicycle lanes on 200 south and in other areas?

Land Use Related Comments

Please densify the area west of 700 East with mixed use.

Please keep the area east of 700 East with residential.

It would be great to use the parking lot at the corner of 900 East and 400 south as a park and ride for students.

Please find a way to keep student traffic away from residential areas west of the University.

Refugee Services Meeting (July 21, 2011)

Transit Related Comments

Refugees heavily utilize the public transportation system in the valley, especially TRAX.

Schedules and information are only found in English along the route.

Land Use Related Comments

Concerns were mentioned about redeveloping properties for refugees in the historic district.

Open houses and other meetings that can be held outside of the City & County Building may bring in more refugees. Many of the refugees may find the City & County Building or other large government buildings intimidating.

Specific comments from stakeholders and community organizations

Refugee Services Meeting (continued)

There are a large number of refugees living at Park Plaza Apartments which is located between 700 South and 800 South and between 300 East and 400 East.

The Office of Diversity may be a good contact as they work with Refugee Services a lot.

Difficult to find affordable housing near transit lines.

Because many refugees have large families affordable housing with 2 or 3 bedrooms is important.

Open Houses at Trolley Square and the Library

A summary of the comments received from the public that attended one of the open houses held at Trolley Square on February 29, 2012 or at the City Library on March 15, 2012.

Bennion Elementary School

Protect the environment around the school. Keep it free from being over developed.

10th Ward LDS Chapel

The historic 10th Ward LDS Chapel is a landmark in the neighborhood and should be protected.

Gilgal Gardens

The area around Gilgal Garden should be kept free from development.

The property connecting Gilgal Gardens to 600 South should be zoned as Open Space to ensure that it remains as it is the only access.

Block south of Trolley Square along 600 South between 600 East and 700 East

The historic structures should be preserved along 600 South. The area should not be upzoned.

The city has great foresight allowing development on the empty parking lot.

Central City Historic District

The entire city right-of-way along 600 East should be kept inside the historic district to ensure that the medians as well as the sidewalks and park strips are protected.

Western Garden 550 South 600 East

If the zoning is changed, allow office on the site and also off site parking for Trolley Square.

Miscellaneous Comments

There should be more neighborhood businesses.

Would like to see more mixed use development.

Concerned about limiting vehicle access along 400 South.

Single-family homes and neighborhoods should be protected.

TRAX is for the people and the neighborhood not vice versa.



Community Council Meetings — East Central and Central Community

A summary of the comments received from the public that attended one of the community council meetings held in March.

Keep density closer to downtown

Is changing the boundaries of the Central City Historic District being driven by developers?

Along 500 South, south of Smiths Marketplace there should be more residential.

There should be density added south of Trolley Square

Concerned about view corridors looking east towards downtown.

Appreciated the proposed changes, forward thinking.

Single-family residences should be allowed and not be banned from the proposed zoning.

The Historic Tenth Ward Building should be protected.

How many more people are expected to be in this corridor in so how many years?

Feedback Received from the Official Website

Feedback Left on the Official Website <http://www.slcgov.com/ced/planning/400South>

Here's what I want to see...

- 1) An urban corridor along 4th with 3-5 story buildings fronting directly onto 4th. Parking should be accommodated behind buildings, not in front.
- 2) Wider sidewalks in front of those buildings, similar to what's on Main Street from S. Temple to 4th South. Right now the sidewalks are too narrow.
- 3) Much better access control. There are WAY too many driveways all over that street. Taking care of Item #1 should largely solve this problem, too.
- 4) Have a high-quality bicycle facility. It could be a regular bike lane, buffered bike lane, or a curb-separated bike lane. Any of those things would work. I LOVE having TRAX down this street, but adding a high-quality bike facility would make it even better.

Each TRAX station should have a "you are here" map that shows businesses and destinations within a short-radius walking distance. The maps would help give transit riders a sense of the neighborhood and promote the small businesses that give the neighborhood a unique character.

New development should take place on the surface parking lots that front 400 South..

This idea is key to improving 400 S. Since people walk to the TRAX stops the strip-mall like street has become into an accidental pedestrian corridor but pedestrians are stranded between a busy street and a parking lot.

In particular, there is a huge parking lot by the OfficeMax near the 900 E. TRAX Stop that is off-limits for TRAX parking but always empty.

Focus on locally owned businesses and limit national chains.

More housing options along the corridor, with high density closer to stations.

Protect the historic buildings in the Central City Historic Districts.

Tear it all down, 400 S between 900 East and the Library is a monstrosity. I can't tell if I'm in a city or if I'm at Jordan Landing. I can't get over how aesthetically unpleasing the whole stretch is.

For this project to be successful, development shouldn't be stimulated beyond demand. There are currently dozens of properties east of 2nd extending from 300 South to 500 South that could renovated to meet the design requirements this project suggests.

It will be easier to extend from this area, as many buildings already meet the distance-to-sidewalk requirements, etc, than those properties east of 400 East.

**6.A. HISTORIC LANDMARK COMMISSION
ORIGINAL HEARING NOTICE**



SALT LAKE CITY PLANNING DIVISION
 451 S STATE STREET ROOM 406
 PO BOX 145480

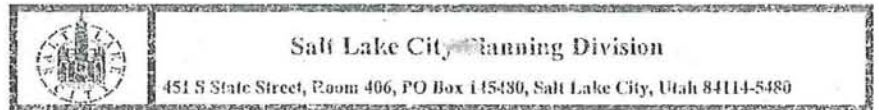
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Salt Lake City Planning Division
 Maryann Pickering
 451 South State Street Ste 406
 PO Box 145480
 Salt Lake City, Utah 84114



Salt Lake City Planning Division

451 S State Street, Room 406, PO Box 145480, Salt Lake City, Utah 84114-5480

Salt Lake City Historic Landmark Commission
 Thursday, May 17, 2012 5:30 p.m.
 Room 326 of the City and County Building

PLNFCM2010-00647 - 400 South Livable Communities Station Area Plans - A request by Salt Lake City Mayor Ralph Becker for an amendment to the Central Community Master Plan, Salt Lake City Zoning Ordinance and Zoning Map regarding transit station area plans along the 400 South transit corridor. The intent of the project is to implement livability goals for the corridor including land use policies and zoning that support mixed use development and transportation choices for current and future residents and workers. The Historic Landmark Commission will review and make a recommendation on the proposed zoning changes within the Central City Historic District. The project is located in City Council District 4 represented by Luke Garrett. (Staff contact: Maryann Pickering at 801-535-7660 or maryann.pickering@slcgov.com)

Salt Lake City Corporation complies with all ADA guidelines. People with disabilities may make requests for reasonable accommodations no later than 15 hours in advance in order to attend this meeting. Accommodations may include: alternative formats, interpreters, and other auxiliary aids. This is an accessible facility. For questions, requests, or additional information, please contact the Planning Office at 535-7757, TDD 535-6220.

**6.B. HISTORIC LANDMARK COMMISSION
APRIL 5, 2012 BRIEFING**



Briefing for Historic Landmark Commission

**Planning Division
Community & Economic Development Department**

To: Historic Landmark Commission
From: Maryann Pickering, Principal Planner
Date: April 5, 2012
Re: 400 South Livable Communities Update

On April 5, Planning Division staff will provide a briefing for the Historic Landmark Commission on the current status of the 400 South Livable Communities project. Staff will present a summary of the public outreach that has occurred to this point and will also provide a short summary of proposed changes to both the Central Community Master Plan and the Central City Historic District.

Background

In the fall of 2010, the Community & Economic Development Department was awarded a matching grant from the Department of Housing and Urban Development (HUD) to undertake a rezoning process for a portion of the University TRAX line. The amount of the grant from HUD was \$22,620 with a commitment from the city to provide an additional \$10,000 in the form of staff resources for the project.

Starting in March of 2011, Planning Division staff began working on a master plan amendment and rezoning project for the University TRAX line. Three stations were identified to be included as part of the rezoning process. Those stations are: Library, Trolley and 900 East. The goal of the proposed project is to create station area plans and zoning regulations for the three stops that:

- a. Provide more transportation choices.
- b. Promote equitable, affordable housing.
- c. Enhance economic competitiveness.
- d. Support existing communities.
- e. Coordinate policies and leverage investments.
- f. Value communities and neighborhoods.

In addition to the above, this project assists in the implementation of the Wasatch Choices 2040 Plan, which is a regional plan created by residents, businesses, and other stakeholders to manage future growth in the region by focusing it on certain nodes, particularly those where transit facilities already are in place. Further, Planning Division staff felt that this area needed to be studied for a potential change at the current zoning because despite the current zoning of TC-75, very little private investment has been made in the area since the TC-75 zoning was created in 2005.

Public Outreach

When the grant application was submitted to HUD, one of the project outcomes was to increase participation in the public planning process for this project. A goal of reaching at least 60 individuals (or approximately 30% of the total participants) from traditionally under-represented groups that do not normally participate in the planning process was included. Through all the different types of outreach completed this goal has been met and has been exceeded in terms of the number of participants.

The first public workshop in the project was held on May 19, 2011. Staff sent notices of the workshop to all property owners within a half mile radius of each TRAX station. The purpose of this workshop was to hear from the residents and property owners within this area to find out what they like about the area, what works, what doesn't work and what they would like to see different.

Over the summer months of 2011, members of the project team met with various local community groups and developers to get their feedback on the area. Similar to the May workshop, we wanted to know what was currently working in the area and what needed to be changed. A summary document was prepared that included all the comments that were received at the May workshop and from the meetings that were held in the summer. A copy of that report can be found in Attachment A.

An additional public workshop was held in October of 2011. The purpose of this meeting was to present to the public some of the information that had been gathered since the process began. There was not a lot of feedback received from this meeting.

The next series of meetings were public workshops held in February and March of 2012. Notices were mailed to all property owners whose property was proposed to be changed and all those located within 300 feet of those properties. Workshops were held at Trolley Square and the main City library. There were several people who participated as part of this process and the level of feedback has been high. The feedback is currently being compiled and input into the summary report so we have a complete record of our outreach efforts.

We have also attended the meeting of the applicable community councils whose boundaries include the station areas. An initial meeting was held with each group when the process started and an additional meeting was attended within the last month.

Public Outreach Feedback

Overall, the response to the project has been positive. There have been a few negative comments received at this point, but staff feels that with some modifications, additional education or materials,

most of those concerns can be reduced or alleviated. We do recognize that this is a large master plan amendment and rezoning project and those who own property will have varied opinions on the best way to go about this process or if it is even necessary. The Planning Division has offered to meet individually with any stakeholder who may have issues with the proposal and have done so throughout the process.

Proposed Central Community Master Plan Changes

The proposed changes to the Central Community Master Plan as part of this project can be found within both the 'Transit Oriented Development (TOD)' and the 'Central Community Master Plan Committee Goals and Recommendations' sections of the plan.

The existing Central Community Master Plan was adopted in 2005. Some of the policies within the TOD section were written in anticipation of the North Temple TRAX line and the expansion to the airport. Staff has updated this section to reflect the current development and conditions along the North Temple TRAX line and also added a section specific to the 400 South Livable Communities project.

In addition to updating the TOD section, some new goals and recommendations that are specific to the 400 South Livable Communities project have been included. The new ones are categorized by each of the three station areas. Some of the goals and recommendations are the same for each of the stations areas and some are unique to each area. It has been organized in this manner for each of implementation by the Planning staff and so that the public can easily see what policies are applicable to area that they are interested in.

Attachment C is an excerpt of the existing Central Community Master Plan with all of proposed changes noted above.

Proposed Zoning Changes

There are two types of zoning changes proposed as part of this project. One is to modify the existing Transit Station Area (TSA) zoning district and the other is to make zoning map amendments in order to implement this project.

Changes to the TSA zoning district as a result of this project are minimal. The TSA zoning district was adopted a few years ago as a result of the work done on the North Temple TRAX line project and the rezoning process that was part of that project. A series of different types of station areas were created and zoning regulations were put in place for each of those station areas in the North Temple area. The 400 South project and station area plans are being modeled after the existing categories and zoning regulations that are currently in place for the North Temple area. This is being done so that there is consistency between the regulations that are in place for the transit corridors throughout the city. If approved, this will be the second area in the City subject to the TSA regulations.

There have been approximately five projects reviewed for the North Temple area with the current TSA standards and for process is working well, though a few modifications are necessary. A few of the standards that currently exist need to be clarified now that staff has had the opportunity to review

projects based on those standards. We will be making those modifications and also adding some standards that are specific to the 400 South corridor.

- In addition to changing some of the standards in the TSA zoning district, staff is also proposing to make some modifications to the Residential Mixed Use (RMU) zoning districts. The two districts proposed to be modified are RMU-35 and RMU-45. A summary of the proposed changes are as follows:
- Increase maximum allowable height five feet in both zones. Traditionally, mixed-use buildings are designed with a distinct base, which is larger and designed to accommodate commercial or office uses, a middle which is generally residential in nature, and a top, which is also residential but designed with architecturally appealing features such as parapets and cornices. In order to ensure proper scale between the first floor and the upper floors, staff is proposing that the first floor ceiling height of a mixed use building be at least 14 feet. This will ensure that retail/commercial space on the first floor is properly designed to accommodate uses that serve the community.
- Elimination of minimum lot area requirement for multi-family dwellings. This provision will promote the development of smaller lots that may accommodate appropriately scaled development but do not meet the minimum lot size requirements.
- Elimination of qualifying provisions for multi-family units as it relates to density calculations, (i.e., 9,000 square feet of lot area for first three units and 1,000 square feet for each additional unit). Development constraints would be primarily based on the petitioner's ability to meet maximum height limits, setbacks, and parking.
- Creation of building design standards. These standards are based on the Small Neighborhood Business standards created by the Planning Commission last August. Their purpose is to promote human scaled massing, varied articulation and traditional façade design.
- Creation of site specific standards for 500 South corridor between State Street and 700 East. These standards are intended to maintain the existing pedestrian/low impact design of the street while allowing the increased density that would come with the RMU zones.

In addition to text changes noted above, this project is also a zoning map amendment to change some of the zoning designations within the 400 South corridor. Most the changes shown in the dark purple, light purple and blue colors are changes that would rezone these properties to the different TSA zoning district categories. You will see that most of these changes are directly along the corridor as we feel this is the best location for more intense development with the intensity lessening on the north and south sides of the corridor.

The balance of the zoning changes (those shown in a yellow hue) are proposed to be an RMU zoning district. After an analysis by staff, it was determined that the RMU categories would be more appropriate for the properties identified than their current zoning designation.

One item we would like the Historic Landmark Commission to discuss during this briefing is the parking lot located to the south of Trolley Square and currently connected by the sky bridge. This lot is currently zoned RMF-45 (Moderate/High Density Multi-Family Residential District). The parking lot is a non-conforming use in this zoning district. In the future, an expansion of the parking lot may be necessary, but it could not be done with the current zoning designation of RMF-45. Staff has proposed to rezone this property as part of this project in order to allow a mixed use development, including an expansion of the existing parking lot. There has been some concerns expressed about changing this zoning and staff would like to have a discussion about possible options or other zoning designations for this site. If after the discussion by the Historic Landmark Commission and Planning Commission it is determined that the best course of action would be to leave the zoning designation as it is, we will remove this area from the proposed zoning map amendments.

Central City Historic District Boundaries

As part of this master plan amendment and rezoning process, staff is recommending that the boundaries of the Central City Historic District be modified. An analysis has been completed that shows within the blocks on the north and south side of 400 South between 500 East and 700 East, there are few historic resources remaining and therefore the Historic Overlay may not be the most appropriate tool to regulate new development on these blocks. The entire two blocks bounded by 400 and 500 South between 500 and 700 East are proposed to be removed. The two blocks between 300 and 400 South and 500 and 700 East will be partially removed. We have found that there are some historic resources along the 300 South frontage that would be better served remaining in the historic district.

The three potential historic resources that staff found when doing a field inspection are listed below. They are all in the same block in close proximity of each other.

444 South 700 East – Every Blooming Thing flower shop
Appears to be a significant structure.

470 South 700 East – Fendall's/Pacesetters
Appears to be contributory.

637 East 500 South – Bridal shop
Appears to be possibly contributory, but has been; modified heavily.

The 600 East landscaped median are an important element of the historic district and need to be preserved. Policies will be included within the Central Community Master Plan that preserve these medians in their current state and minimize future impacts from development. This means that breaks in the median to accommodate turn around areas for vehicular traffic will be discouraged. We will also include policies that the sidewalks and other areas of the public right-of-way remain in the historic district so that the City can main design control of features (streetlights, seating, etc.) that is

located within the right-of-way. Please see Attachment B which contains a map of the modified historic district.

Prior to a public hearing on this master plan amendment and rezoning petition and the subsequent change of the boundaries of the historic district, staff will hold a briefing and a public hearing with the Historic Landmark Commission for their input and direction. We expect the briefing to take place in April and a public hearing in May.

Discussion for Briefing

Staff would like to have a discussion at the briefing this evening at the briefing:

1. Potential modifications to the Central City Historic District boundary.
2. 400 South project in general.

Next Steps

It is anticipated that after the direction from Historic Landmark Commission is received, we will begin the public hearing process for this petition. At this time, we expect that those hearings will begin in mid-May.

Attachments

- A. 400 South Livability Project – Public and Stakeholder Outreach Summary Report
- B. Map of Proposed Modifications of the Central City Historic District
- C. Draft Changes to an Excerpt of the Central Community Master Plan
- D. Library Station Summary Handout
- E. Trolley Station Summary Handout
- F. 900 Station Summary Handout
- G. Map of Proposed Zoning Changes

Attachment A
400 South Livability Project – Public and Stakeholder Outreach Summary Report

Salt Lake City Planning Division

400 South Livability Project

Public and Stakeholder Outreach



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Salt Lake City Planning

400 South Livability Project Public and Stakeholder Outreach

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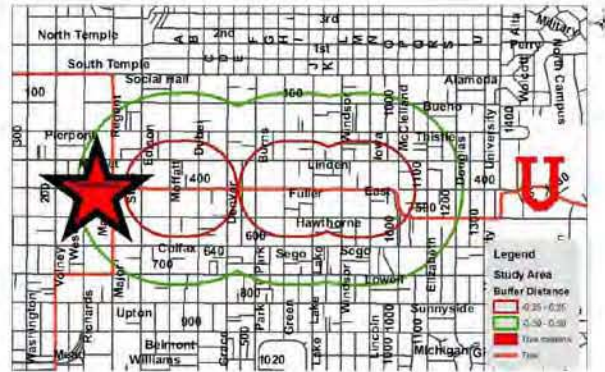


451 South State Street Room 406 PO BOX 145480 Salt Lake City, UT 84114-5480 801 535-7757 www.slcgov.com/CED/planning/

Purpose of Meeting

The purpose of the May 19th Community Workshop was to identify those things about the 400 South corridor and adjacent neighborhoods that people liked, did not like and what they wanted to see happen in the future. For the Planning Division, this was an opportunity to listen to feedback and to facilitate discussions on a wide range of issues related to the corridor. For the public, it was an opportunity to discuss the issues with each other without being directed by the City.

This report is a summary of what was heard by the City. The comments from the meeting will be used, along with comments and input from future meetings, will be used to create guiding principles and proposed City policies to help guide future decision making.



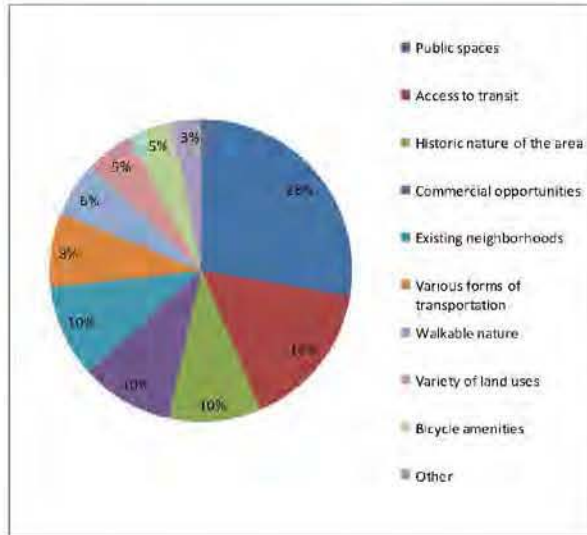
The 400 South Livability Project study area is generally the area within a 1/2 mile radius of each transit station, shown by the green line on the above map. The study area was established based on a number of studies and best practices from across the country that indicate light rail transit benefits are mostly found within a 1/2 mile walking distance of a transit station. Those areas that within 1/4 mile (the red circles on the above map) see the greatest benefits and the benefits decrease the further away one travels from a transit station.

The study area is being used in a number of ways. First, it is used to identify stakeholders. For the May 19 Workshop, all property owners and building tenants (both residential tenants and businesses) were sent notices of the workshops. Second, the study area will be used by the Planning Division to analyze development related issues based on feedback received during the May 19th workshop and other future workshops related to the project.

What the Community Likes

Those items that the community likes generally fit into the following categories:

- Public spaces, including parks, open space, landscaping, etc
- Access to transit
- Historic nature of the area
- The commercial opportunities in the area
- The existing neighborhoods
- The various forms of transportation that are available in the area
- The walkable nature of the area
- The variety of land uses in the area
- Bicycle amenities



The general categories of things that people like about the corridor, shown as a percentage of the total number of comments received.

Specific comments about what people like (comments are in no particular order)

Access to Transit

- Access to transit
- Proximity to transit
- Minimal distance between stations
- TRAX
- Frequency of Tax stops
- Transit access to larger region
- Likes no Tax parking
- Hybrid electric buses

Bicycle Amenities

- Bike lanes
- 800 East should be a bicycle street
- Bicycle amenities

Historic Nature of Area

- Encourage adaptive reuse of buildings
- Historic buildings
- Neighborhoods adjacent to 400 South
- Historic nature of neighborhoods
- Historic markers
- Exchange Place
- Washington Square

Public Spaces, Parks, Open Space, Landscaping

- Trees
- Green spaces
- 800 East medians
- Smaller parks, such as Fault Line, Senior Center, Gilgal
- Landscaping in Tax corridor
- 800 East medians
- Pedestrian friendly green buffers
- Washington Square and the festivals
- Public safety building
- Library
- Station art
- Street art
- Gallivan Center

Commercial

- Easy access to commercial spaces
- Trolley square renovation
- Fast food and drive thru restaurants
- Diverse nature of commercial
- 100 South commercial
- Mix of retail uses

Walkability

- Countdown clocks
- Urban feel of library station, lots of pedestrians
- Walkable neighborhoods
- Pedestrian amenities
- Sidewalks
- Crossing flags
- Locations where building are built up to the sidewalk
- Walkability

Neighborhoods

- Quiet, safe neighborhoods
- High density
- Buffers to neighborhood
- Diverse characteristics
- Existing mixed use neighborhoods
- Protection of single family neighborhoods
- Limited negative impacts from Tax on neighborhoods
- Integrated neighborhoods
- Mix of housing

Variety

- Density of uses
- Variety
- Ethnic/age mix
- Mix of uses
- University students

Transportation

- Uncongested streets
- U-Car share program
- A good travel way for cars between downtown, east bench with little impact to neighborhoods
- Access to university
- Driveable nature of corridor
- Regional location to mountains, recreation, etc.
- Access to downtown

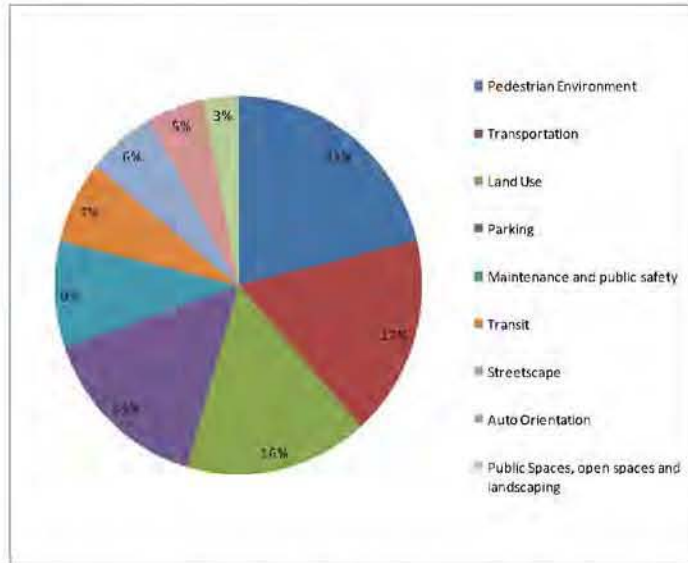
Miscellaneous

- SL Regional Hospital
- Cleanliness
- New architecture downtown
- Potential of area

What the Community Does Not Like

Those items that the community likes generally fit into the following categories:

- Pedestrian environment
- Transportation
- Land Use
- Parking
- Maintenance and public safety
- Transit
- Streetscape
- Auto orientation
- Public spaces, open spaces and landscaping



The general categories of things that people do not like about the corridor, shown as a percentage of the total number of comments received.

Specific comments about what people do not like (comments are in no particular order)

Pedestrian Environment

- Sidewalks are not wide enough or are in disrepair
- Unfriendly/unsafe to walk (cluttered, narrow, no buffer from traffic, blind driveways, etc)
- Limited pedestrian access to Tax is not conducive to walking and supports jaywalking
- Pedestrian conflicts with auto infrastructure (wide roads, driveways, right turns, etc.)
- Pedestrian walkways have no amenities (shade, benches, garbage cans, etc.)
- Mid block walkways are not safe or well taken care of
- Feel out of place walking on 400 South, it is not pedestrian friendly.

Parking

- Middle of the street parking
- Lack of pedestrian access through parking lots
- Tax riders parking in commercial lots
- Parking is a nightmare in the eastern neighborhoods
- Most annoying and arrogant thing the City says is you should not have any expectation that you can park in front of your house

Public Spaces, Open Spaces, and Landscaping

- Not enough open space, green space, community space, trees, landscaping

Auto Orientation

- Auto oriented uses/dependency
- Three lanes of auto traffic in each direction on 400 South
- Auto oriented destination

Transportation

- Traffic flow, congestion, speeding
- Cars using side streets to cut through busy intersections
- Stoplights are not synchronized
- Dangerous intersections (un-signalized crossing on 800 East, two way stop, etc.)
- 800 East medians block trash and truck access
- Traffic noise, especially from buses
- Lack of traffic calming
- More access to car share program
- U-Car share pays high taxes, should be exempt
- Difficult to ride bike on transit streets

Transit

- Bus fares are not conducive for short trips (too expensive, need distance faced fares, expand free fare zone, etc)
- Lack of bus service
- Bus infrastructure is not synced to Tax
- Tax
- Bus stops in residential areas have no amenities and often damage landscape from people waiting, littering, etc.
- Need mid block connection to Tax stations
- noise from buses and trains
- Some of the bus routes that support Tax are going away, limiting the effectiveness of Tax
- Bus stops and pull outs do not work well

Specific comments about what people do not like (comments are in no particular order)

Land Use

Public safety building should not be across from library
 Center of block development
 Not livable
 Circular nature of TOD, needs to be linear along corridor
 Lack of protection for single family neighborhoods
 400 South has not encouraged local business, not evolved organically
 Housing on 400 South is not compatible due to the noise
 Empty buildings
 More core services accessible
 Land uses need to relate better to the transportation infrastructure
 No incentives to encourage redevelopment in the right places
 Too many chains and fast food restaurants
 Woefully underused
 Loss of housing to other uses
 Conflicts between zoning and historic district overlay
 Redundant land uses (too many of one land use, like medical offices)
 Tax has not encouraged organic development, 400 South is still mostly big box, fast food and rundown offices
 Do not like high density in the middle of the blocks
 Many people living in high density still do not use TRAX, they drive
 Industry does not belong (Wonder Bread factory)

Streetscape

Lack of aesthetics
 400 south feels like anywhere else in America
 Needs to be beautified so it is more like a boulevard
 400 South is not transit oriented
 600 East between Trolley Square and the 600 East station
 Resembles State Street between 300 East and 500 East
 No recreational opportunities for kids, either indoor or outdoor.

Maintenance and Public Safety

Loud motorcycles
 Rental units are a business and need to be treated like one to address maintenance issues
 Cats and dogs roaming free
 Not enough yard waste containers
 Boarded houses
 Long term vacancies
 Absentee landlords, both residential and commercial
 Derelict properties
 Curbs not painted red in front of fire hydrants

Visions

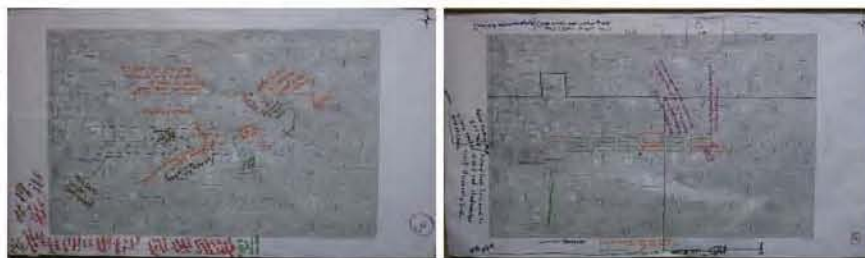
The community workshop provided some very helpful insight as to what the 400 South corridor should look like in the future. This is important because it helps the Planning Division continue to work with the community and stakeholders to craft a long term vision for the area and then create land use policies and development regulations to implement the vision. Most of the visions received during the workshop fall into these general categories:

- Land Use and Development
- Transportation
- Public Spaces
- Bicycles
- Walkability
- Neighborhood Character
- Policy

The visions were collected from the note cards handed out as people arrived, on the flip charts where the facilitators were capturing the discussions at each table and from the information written on the maps at each table.



Images of the maps from the May 19, 2011 Community Workshop



Staff Summary of Vision Comments

Below is a list of all of the vision comments received during the Workshop. Similar comments have been grouped into categories.

Land Use	Number of comments	Transportation	Number of comments
Higher densities closer to Downtown and the stations, with lower densities adjacent to existing neighborhoods.	21	Work with UTA to come up with alternate fare options for transit. Options may include an expanded free fare zone, distance base fares, etc.	2
Provide a wide range of housing options that complement the existing housing types and add new housing types to the area.	7	Provide more transit options that interface with the University TRAX line, such as future street car lines, bus routes, etc. Streets to consider include 600 East to Liberty Park, 900 East, and 200 South.	6
Develop design guidelines that bring buildings closer to the street, address building setbacks, parking locations, landscaping, green roofs and buildings, etc. to make a more pedestrian oriented community.	18	Add park and ride lots at 900 East and the Stadium Stations to serve the residential neighborhoods.	4
More mixed use in appropriate locations.	6	Use roundabouts, traffic lights and stop signs to improve safety at intersections located at 200 South 800 East, 300 South 800 East, 500 South 800 East and 600 East 600 South.	6
Ensure that zoning regulations support appropriate land use, design and scale for historic areas.	5	Synchronize traffic lights and adjust the traffic lights so they that switch to flashing red after midnight.	2
Create incentives for appropriate development and redevelopment of undesirable land uses (surface parking lots, drive-thru restaurants, etc).	18	Coordinate with the 200 South Project to create a multi-modal street along 200 South and use east-west connections to make connections to the University TRAX line.	2
Promote Trolley Square as a neighborhood anchor by providing more residential density and other supportive uses around it.	2	Provide funds to fix up interior court like public streets, such as Denver Street.	1
Promote more locally owned businesses along the corridor.	2	Extend the University TRAX line west on 400 South to connect to Salt Lake Central Station.	1
		Use visual enhancements to slow traffic.	1
		Eliminate one vehicle lane in each direction on 400 South and replace it with bike lanes and wider sidewalks.	1

Purpose of Stakeholder and Community Organization Meetings

The purpose of the meetings with stakeholders, was to learn about the future intentions of business and property owners, the potential of developed and undeveloped sites and how they envisioned the future land uses along the corridor.

Meetings were also held with various community organizations to learn more about how their groups and their members interact with the 400 South corridor, how the members of these groups are utilizing the corridor, their views of current and future transit options and land uses along the corridor and how we could find ways to include these groups and their individual members in future public meetings.

This report is a summary of what was heard by the City. The comments from the meetings will be used, along with comments and input from future meetings, will be used to create guiding principles and proposed City policies to help guide future decision making.



Specific comments from stakeholders and community organizations

YWCA Meeting (July 7, 2011)

Transit Related Comments

- Do away with tokens for bus and trains, TRAX doesn't use tokens.
- Very difficult to get on TRAX and bus transfers with children and strollers.
- Bus drivers don't realize how heavy strollers are, please help more.
- Bus schedules are always gone.
- The Free Zone is good.
- Need temporary cards instead of tokens for ease of use on all transit.

Land Use Related Comments

- Need more drugstores like Walgreens.
- Need more clothing stores like Ross.
- Need more grocery stores like Fresh Market.
- Need more retail stores offering electronics.
- Need book stores.
- Need more affordable housing near transit.
- Need more urban recreational opportunities.
- Need more daycare centers near transit.
- Too many fast food chains restaurants (but still convenient).

Odyssey House Meeting (August 26, 2011)

Transit Related Comments

- New trains are nice, with more space and bigger seats.

Land Use Related Comments

- Need to focus on lots between 9th E and Library - it is "deadsville" for a pedestrian.
- Area SE of 9th E stop could have more density and apartments, redevelop as they are in

Odyssey House Meeting (continued)

- Need a definition in Zoning Ordinance for "Smoke Shops" as these are unwanted land uses and bring in unwanted people and drug activity which is not good for the corridor or neighborhood.
- Prohibit "pawn shops" along 4th South.
- Put a great gym with indoor recreation opportunities, pool in the old "Whole Foods" space.
- Put a little Walmart into "Whole Foods." Something 24 hour and to serve those living around neighborhood.
- Need more pedestrian alleys and created spaces on and around the corridor planning area, these urban walkways add interest and give the community a place to meet friends and shop. Would help make it more walkable and wouldn't have to walk along 4th South as sidewalk isn't that pleasant.
- Want more cafes and outdoor seating areas, place to meet friends and enjoy the area.
- Food carts are fun and add interest and social activity along the street.
- Limit drive-thrus as they are hard to walk past and just don't create an interesting place.
- Need more mixed-use like the Broadway, with apartments above and small grocery and retail below, put these by the stations. Need a lot of density so that more uses will stay open longer.
- Need new apartments or condos by pocket parks so that the park clientele are "cleaned up."
- Need more drugstores like Walgreens, especially 24 hour pharmacies.
- Need more clothing stores like Ross.
- Need more grocery stores like Fresh Market.
- Need more retail stores offering electronics like Apple or Best Buy.
- Need book stores.
- Need more affordable housing near transit, but also want high-end housing to balance out socio-economic groups and to provide enough foot traffic and populations with higher disposable income so that better retailers will choose to locate on the corridor.
- Need a mix of housing types to bring in both the bachelor and a large family.
- Need to allow higher density so that the old and decrepit apartment complexes are redeveloped, thus offering a higher standard of living around the corridor.
- A hotel or small hotels or B&B type places on the corridor or around stations would be nice, and would bring in more nightlife and pedestrian activity and possibly nicer retail stores.

Specific comments from stakeholders and community organizations

Odyssey House Meeting (continued)

Need more urban recreational opportunities, parks, bowling alley, arcade or pool halls.
More family oriented recreation is especially needed. Another Gallivan Center-type of place would be welcome.

Need more destination lounges, the Green Pig is only place open on Sundays.

Need more daycare centers or charter schools near transit, this would liver the place with young people.

Parking is fine as long as located underground.

Too many fast food chains restaurants (but still convenient).

Property Owner Meeting (August 10, 2011)

Center was made to be walkable to the north.

Owners feel like they are shoehorned with the current zoning.

Would like to be able to go up more or build taller buildings.

Suggest that this project look further back than one half of a block.

Varied setback is possible. For example, buildings directly on the street would be limited to three stories in height and it would setback up to seven stories the further away from the street.

Right now, market is for rental residential units, very tough to do sales.

Would like the flexibility to expand the former Whole Foods building. Expansion would take place to then north. Right now it is 25,000 square feet and 40,000 would make it more attractive to potential tenants.

Difficult to do one building mixed use projects these days because of financing. Lenders prefer to lend on mixed use projects when the residential and commercial uses are in separate buildings, but they can be on the same lot or part of the same project. This is national, not just a local trend.

Most mixed use projects (in a traditional single building) are into their second or third owners because it is just hard to do in most areas unless the project is located in a very dense downtown area.

Tenants prefer to lease a project when parking is available in front of the storefront. Creating separate areas for parking is not desirable.

Owners feel that the city is short sighted in wanting a walkable community, but then density is limited.

Property Owner Meeting (continued)

Being in a historic district is a hindrance. Tenants do not want to have to go through two processes to obtain signs or to make minor modifications to their structure. It's also hard when modern signs are preferred on a faux historic building (Chipotle in Sugarhouse example).

RMU (with no density restriction) is the type of zoning designation they feel would work for the

University of Utah Transportation Meeting (June 27, 2011)

Transit Related Comments

Students should be encouraged to use transit more. This could be accomplished by adding more density along TRAX line.

University has a significant transit system, with buses and proposed light rail spur; please keep them informed as to future developments.

What impact, if any, will this study have on proposed bicycle lanes on 200 south and in other areas?

Land Use Related Comments

Please density the area west of 700 East with mixed use.

Please keep the area east of 700 East with residential.

It would be great to use the parking lot at the corner of 900 East and 400 south as a park and ride for students.

Please find a way to keep student traffic away from residential areas west of the University.

Refugee Services Meeting (July 21, 2011)

Transit Related Comments

Refugees heavily utilize the public transportation system in the valley, especially TRAX.

Schedules and information are only found in English along the route.

Land Use Related Comments

Concerns were mentioned about redeveloping properties for refugees in the historic district.

Open houses and other meetings that can be held outside of the City & County Building may bring in more refugees. Many of the refugees may find the City & County Building or other large government buildings intimidating.

Specific comments from stakeholders and community organizations

Refugee Services Meeting (continued)

There are a large number of refugees living at Park Plaza Apartments which is located between 700 South and 800 South and between 300 East and 400 East.

The Office of Diversity may be a good contact as they work with Refugee Services a lot.

Difficult to find affordable housing near transit lines.

Because many refugees have large families affordable housing with 2 or 3 bedrooms is important.

Feedback Received from the Official Website

Feedback Left on the Official Website <http://www.sl.gov.com/ced/planning/400South>

Here's what I want to see...

- 1) An urban corridor along 4th with 3-5 story buildings fronting directly onto 4th. Parking should be accommodated behind buildings, not in front.
- 2) Wider sidewalks in front of those buildings, similar to what's on Main Street from S. Temple to 4th South. Right now the sidewalks are too narrow.
- 3) Much better access control. There are WAY too many driveways all over that street. Taking care of Item #1 should largely solve this problem, too.
- 4) I have a high-quality bicycle facility. It could be a regular bike lane, buffered bike lane, or a curb-separated bike lane. Any of those things would work. I LOVE having TRAX down this street, but adding a high-quality bike facility would make it even better.

Each TRAX station should have a "you are here" map that shows businesses and destinations within a short radius walking distance. The maps would help give transit riders a sense of the neighborhood and promote the small businesses that give the neighborhood a unique character.

New development should take place on the surface parking lots that front 400 South.

This idea is key to improving 400 S. Since people walk to the TRAX stops the strip-mall like street has become into an accidental pedestrian corridor but pedestrians are stranded between a busy street and a parking lot.

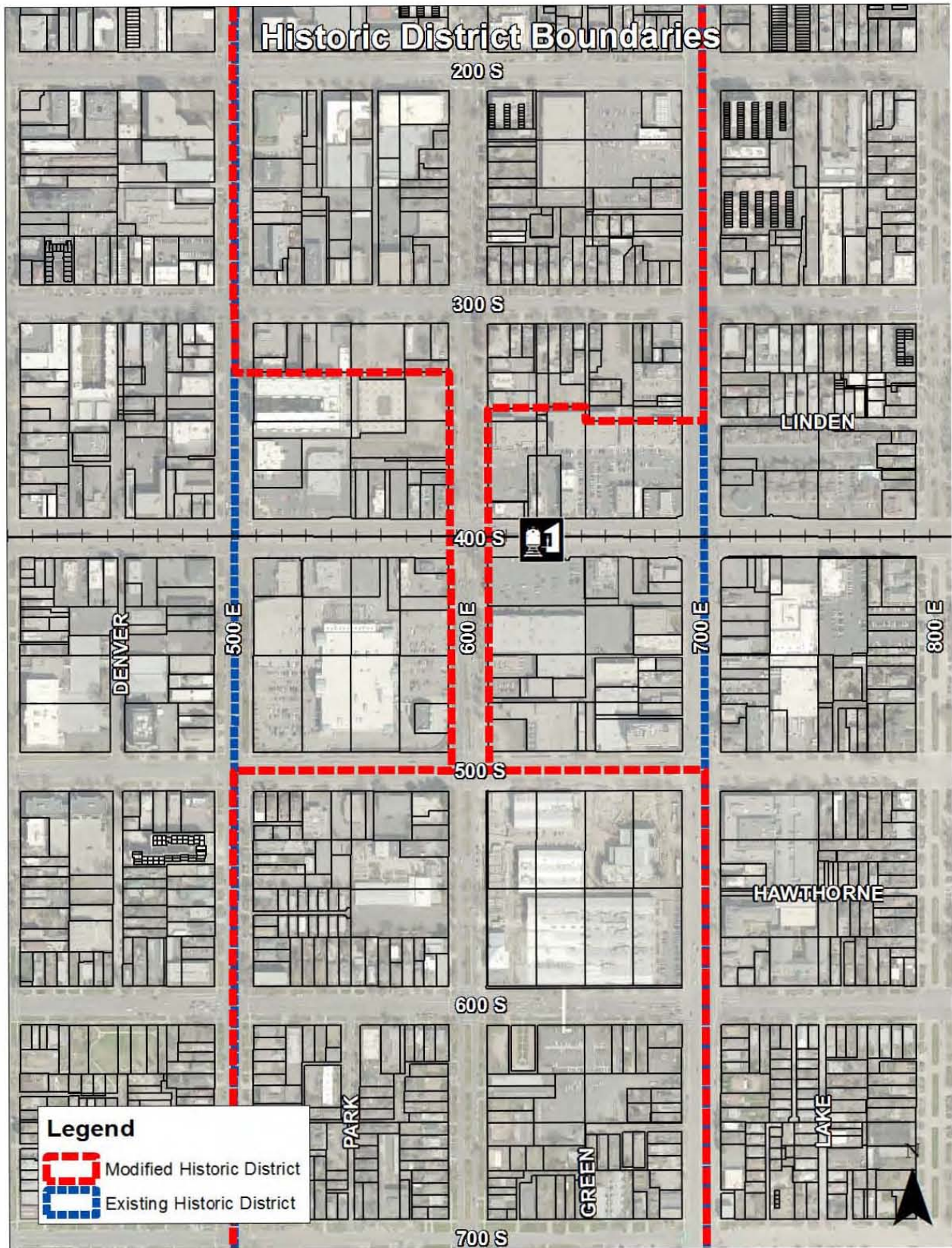
In particular, there is a huge parking lot by the Office/Inn near the 900 E. TRAX Stop that is off-limits for TRAX parking but always empty.

Focus on locally owned businesses and limit national chains.

More housing options along the corridor, with high density closer to station.

Protect the historic buildings in the Central City Historic Districts.

Attachment B
Map of Proposed Modifications of the Central City Historic District



Attachment C
Draft Changes to an Excerpt of the Central Community Master Plan



Please note that the following Transit Oriented Development (TOD) section will replace the text found on page 15 for the current Central Community Master Plan.

Transit-Oriented Development (TOD)

Introduction

Transit-Oriented-Development (TOD) is a land use and urban design concept that mixes land use near transit to maximize transportation options and provides people with choices about living near transit. A TOD neighborhood typically has a center with a light rail station or bus stop surrounded by relatively high-density development with progressively lower-density development spreading outward from the transit center. TOD's generally are located within a radius of one-quarter to one-half mile from a transit stop, as this is considered an acceptable walking distance.

TOD areas offer choices in housing, commerce and transportation. They are designed to be flexible so they can respond to changing economic and social conditions and endeavor to make development economically viable from a number of perspectives (city, transit agency, developer, resident, employer, etc.). TOD districts encourage the stabilization and revitalization of existing neighborhoods, as new uses are designed to support existing neighborhood characteristics.

TOD districts improve the quality of life in urban areas by:

- Including mixed-use development that will use transit at all times of day.
- Creation of excellent pedestrian facilities such as high quality pedestrian crossings, narrow streets, and tapering of buildings as they become more distant from the public transport node.
- Ensuring compatibility and connectivity with surrounding neighborhoods.
- Creating compact development within an easy walk of public transit and with sufficient density to support transit ridership.
- To establish a hierarchy of transportation which places the pedestrian first, bicycle second and auto third.
- Creating active places and livable communities that service daily needs and where people feel a sense of belonging and ownership.
- Including engaging, high quality public spaces (e.g. small parks or plazas) as organizing features and gathering places for the neighborhood.
- Encouraging a variety of housing types near transit facilities.
- Providing housing choices for a wide range of ages and incomes.
- Incorporating retail into a development if it is a viable use at the location. Ideally drawing customers both from both the TOD and a major street.



- Introducing creative parking strategies that integrate, rather than divide a site and reduce the presence of the auto.
- Creating TOD plans that are flexible so they can respond to changing conditions.
- Recognizing that all TOD's are not the same; each development is located within its own unique context and serves a specific purpose in the larger context.

TOD strengthens urban development but also helps manage future regional growth by encouraging growth to occur where the existing infrastructure is best suited to address it. The benefits of this type of development include:

- Opportunities for affordable and accessible living by incentivizing affordable and accessible housing goals, providing a range of housing options, and allows people to spend less of their income on the combined expenses related to housing and transportation.
- Reduction of the long term expenditures of tax dollars to build and maintain public utilities, roads and other auto-oriented infrastructure.
- More residents living close to commercial areas to support a strong, local oriented economy.

This reduces the overall cost of development and reduces negative environmental impacts on air and water quality while creating community oriented public places.

Transit-oriented development designations

Transit-oriented development districts within the Central Community have three designations: low-density, medium-density, and high-density. The Future Land Use map shows locations where these districts are supported by this master plan. Where conflicts between TOD and historic district overlay regulations occur, the historic overlay requirements govern.

Low-density transit-oriented development (light sage green on map): Low-density TOD supports residential uses with a density ranging from 1-20 dwellings per acre. The emphasis of low-density TOD design and land use relates to existing lower density residential and neighborhood commercial land uses. Low intensive development should be designed to assure compatibility in neighborhoods with established low-density characteristics by focusing on the massing and scale of the existing structures. Medium intensive land uses, such as a coffee shop, town house or daycare center, may be appropriate near the light rail station or busier intersections. Implementing low-density TOD areas may include development of accessory units in the rear yards of low-density residential land uses as well as small businesses that can be operated out of a residential structure. New structures should be in scale with the low-density neighborhood with modest increases in building height being appropriate if it provides opportunity for increasing the residential density. Zoning designations should require compatibility in these areas.



Medium-density transit-oriented development (medium sage green on map): Medium-density TOD supports residential land uses with a density range of 20-50 dwelling units per acre. The design emphasis for medium-density TOD is compatibility with existing medium and low-density residential and commercial development. Higher intensive uses may be located near light rail stations where applicable. Medium-density TOD areas include four to five story buildings with a mix of ground level retail or office space components with multi-story residential development above. These areas should have limits on the amount of space allocated for non-residential land uses by locating them closer to transit and at busier intersections. Individual residential land uses could remain within the TOD area. Zoning districts that allow four to five story building, including pedestrian oriented design standards and allow 20-50 dwelling units per acre are appropriate in areas with this designation.

High-density transit-oriented development (dark sage green on map): High-density TOD is similar to medium-density TOD except at a greater scale. These areas should be centers of high population with a concentration of pedestrians in close proximity to transit stations. These areas will be more successful in places that are well served by rail transit. Building heights are established for high density residential and higher intensity office or commercial uses. The ideal building type in these areas are vertical mixed use structures tall than five stories. High-density TOD supports the development of compact urban centers with 50 or more dwelling units per acre. These centers can provide accessibility and a multitude of benefits (efficiencies that result when many activities are physically close together). Design standards are critical in these areas in order to create people oriented spaces. Auto oriented development should be prohibited. Standards and processes should be put in place to allow for the easy and financially viable transition of auto oriented developments to people oriented development.

The transit-oriented development land use designations are shown on the Central Community TOD map and on the Future Land Use map.

Community input on Transit-Oriented Development

This section will be further modified after input from the next community open houses in February and March of 2012.

Future Transit-Oriented Development land use changes

TOD's are implemented by the public and private sectors. The public sector sets the stage by providing the transit, investing in public infrastructure and applying appropriate zoning regulations near the transit stations. The private sector generally finances, constructs and markets TOD's. When based on a sound and agreed upon vision, TOD implementation should be a simple and straightforward process. To ensure this is the case, the TOD station area plans intend to create the vision and set the stage for the private sector. The City has developed certain zoning districts to implement the various intensity levels of TOD that are based on input from a broad range of stakeholders. The areas listed below have gone through a community visioning process that had identified the desired development characteristics and therefore, support zoning changes based on the specific goals for each area.



400 South and University Boulevard

In June of 2011, the City received a grant from the Housing and Urban Development Department (HUD) to initiate the 400 South and University Boulevard Livable Communities project. The focus of the project was to enhance the Community's vision of the corridor and make it a place where people could live, work and shop. Inherent in this process was the inclusion of people who rely on the corridor for their daily needs, but for various reasons do not normally participate. The group included those who are traditionally under-represented in the planning process. The project determined that future development at all 400 South stations should be compatible with the existing neighborhood scale and that development should not supplant or compete with the central business district regional services (beyond the existing Trolley Square development). Development should enhance existing urban neighborhoods within walking distance of the transit line to create areas where pedestrian activity is the focus of daily transportation needs, without excluding the automobile. These concepts are critical to groups who rely on public transportation to access housing, employment, education, health care, and other daily needs.

The 400 South and University Boulevard Trax lines run from downtown to the University of Utah. The corridor incorporates a mix of transportation options, including light rail, bus and automobile. There are several bicycle lanes that parallel and intersect the street to create a complete transportation network through the area. When the light rail line opened in 2001, the pedestrian environment improved by adding street trees and widening the sidewalk. Unfortunately, the parking lane was removed, placing a travel lane immediately adjacent to the sidewalk. In order to make the corridor more pedestrian oriented, sidewalk improvements are necessary.

The Liveable Communities project identified specific land use goals and policies that can be found in the "400 South and University Boulevard Station Area Plans" in the addendum of this Central Community Master Plan. The station area plans include specific land use policies and a future land use map that should be used to help guide future land use, capital improvements and budget decisions.

West Temple Gateway

The West Temple Gateway area extends from 700 South to the 900 South Interstate off ramp and from 300 West to West Temple and includes the 200 West / 900 South future light rail stop. The West Temple Gateway area is part of a redevelopment project area created in 1987, which included two revitalization concept plans. These are not adopted policy plans but resource documents. The 1994 plan identified alternative concepts ranging from low-density residential infill to Big Box retail uses. A second analysis in 2001, after the light rail line was constructed, provided an Illustrative Plan that proposes a mixed use transit-oriented neighborhood containing residential, retail, office, and industrial land uses. Development of a West Temple Gateway small area master plan will provide detailed development guidelines for this area.



Transit Oriented Development goal

To create TOD development with a balanced mix of uses that generates 24-hour transit ridership. This development will have places to work, to live, to learn, to relax and to shop for daily needs. The goal will be achieved through land use designations, development guidelines, zoning, and both public and private funding.

Transit Oriented Development policies

Transit-Oriented Development policies fall into these general categories: location and variety of land use.

Location

Policy TOD-1.0 Based on the Future Land Use map and specific station area plans, establish Transit-Oriented Districts that will provide residents with housing, transportation and employment options at various densities near transit stations.

TOD-1.1 Develop station area plans for each transit station within the Central Community Planning Community.

TOD-1.2 Utilize a broad community involvement approach to identify appropriately located and scaled transit oriented development that put people first.

Variety of Land Use

Policy TOD-2.0 Encourage the development of mixed-use projects near light rail stations to create a livable, walkable urban environment.

TOD-2.1 Support a variety of low-, medium- and high-density residential uses around light rail stations in TOD districts, based on the Future Land Use map designations.

TOD-2.2 At light rail stations in TOD districts, establish a centralized core of land uses that support transit ridership. Anchor transit centers with land uses that act as destination points.

TOD-2.3 Encourage a variety of commercial uses that share the same clientele and patrons. For example, movie theaters provide a clientele to patronize restaurants, arcades, and retail businesses.



Central Community Master Plan Goals and Recommendations Addendum

Please note that the following station area plans will be added into the addendum located on pages 22-23 of the current Central Community Master Plan. These items will be added right after #15 of the 'Environment' section.

400 South and University Boulevard Station Area Plans

Each of the three station area plans include subareas as identified below:

Core Area: The purpose of the core area is to provide areas for comparatively intense land development with a mix of land uses incorporating the principles of sustainable, transit oriented development and to enhance the area closest to a transit station as a lively, people oriented place. The core area is generally within a one-fourth ($\frac{1}{4}$) mile walk of a transit station platform. The core area may mix ground floor retail, office, commercial and residential space in order to activate the public realm. Buildings in this area should have minimal setbacks to encourage active outdoor use adjacent to the sidewalk, such as outdoor dining and patios that reflect the desired character of the area. Building facades should be varied and articulated, include storefronts adjacent to the street, windows on the street level, and have clearly defined entrances to provide visual interest to pedestrians. Buildings should be a minimum of two (2) or three (3) stories in height, depending on location, in order to define the street edge. Arcades, bays, and balconies are encouraged. The configuration of buildings must balance the needs of all modes of circulation with the safety and comfort of pedestrians and bicyclists. A vertical mix of uses, with office and residential above ground floor commercial uses is encouraged. A minimum of thirty (30) dwelling units per acre is encouraged within the core.

Transition Area: The purpose of the transition area is to provide a moderate level of land development intensity that incorporates the principles of sustainable transit oriented development. The transition area is intended to provide an important support base to the core area and transit ridership as well as buffer surrounding neighborhoods from the intensity of the core area. These areas reinforce the viability of the core area and provide opportunities for a range of housing types at different densities. Transition areas are generally located within a one-half ($\frac{1}{2}$) mile from the station platform, but may vary based on the character of the area. Transition areas typically serve the surrounding neighborhood; include a broad range of building forms that house a mix of compatible land uses. The minimum desired density is ten (10) dwelling units per acre. Commercial uses may include office, retail, restaurant and other



commercial land uses that are necessary to create mixed use neighborhoods. Commercial uses can be clustered around intersections and along block faces to create neighborhood nodes.

Library Station Area

The Library Urban Center Station has the highest intensity level and mix of uses along 400 South. It supports Salt Lake City's central business district in terms of building scale and use. The area has a twenty-four-hour population, active streetscapes, defined street walls and multiple types of public transit. Development generally occurs on surface parking lots or through redevelopment of underutilized parcels.

The street frontages in the Library Station area have a compact, dense, interconnected and walkable development pattern, while the interior of blocks are open generally with parking lots. Large scale development is closer to the station, and is scaled back as it moves to the areas away from 400 South. Building forms vary, but are typically oriented to the pedestrian, are multiple stories in height, and contain a horizontal and vertical mix of land uses. Buildings up to thirty stories in height are allowed within the D-1 zoned sections, while buildings in the transition zone can be approximately three to four stories in height.

The area features a variety of dense, mixed use commercial and housing developments, providing residents with a number of housing options. The station area contains important regional attractions, such as the Library, City & County Building, Leonardo Museum, and the Public Safety Building. These buildings are a regional draw, bringing thousands of people to the area each day. The area also features office, dining and entertainment options with a high level of pedestrian activity. The Library and Washington Square feature a number of civic and cultural festivals during the summer.

The station area comprises of core and transition areas. The purpose of creating the different areas is to recognize the scale and nature of existing development patterns and identify the appropriate locations for growth. The general concept is that bigger buildings with the most dwelling units and a higher intensity level of commercial space should be located closest to the station in the core. The transition area reduces the scale, mass and intensity of new development as it moves away from the core.

In addition to the civic uses, and high density development surrounding the Library Station core, there is a significant amount of underutilized land. The Library Station Area Plan encourages the development of these areas to create medium to high density housing, mixed use development and appropriate support commercial along 400 South. By doing this, the area would further its role as a vibrant and contributory feature of Salt Lake City, while maintaining its function as a support to the downtown area.

The "Transit Station Area (TSA) Development Guidelines" will be used along 400 South to encourage a walkable urban neighborhood compatible with the adjacent historic district.



Library Station Area Goals:

1. Rezone properties fronting 400 South so that the zoning reflects the vision for the station area. Rezone the properties that are in the vicinity to a mix of zoning districts that promote high density residential development while allowing limited commercial types of uses.
2. Coordinate with Utah Department of Transportation (UDOT) to improve the functionality of 400 South for pedestrians. This could be accomplished by adding additional midblock crosswalks to the transit stations, allowing on-street parking during off peak travel hours, studying the impact of reducing the road width, whether in key spots or for longer stretches and other changes to the street that are aimed at creating a safe and functional transportation corridor for all users.
3. Work with the City Transportation Division to find appropriate bicycle routes that run on or are parallel to 400 South and that connect to appropriately located north/south bike routes.
4. Require midblock connections from 300 South and 500 South that connect to 400 South.
5. Incorporate way-finding features at the station area so people can easily locate station area assets such as Library, Library Plaza, City & County Building, Leonardo Museum and the Public Safety Building.
6. Maintain the original 15-foot landscaped setback along the 400 South street frontage.
7. Require properties to share driveway access as properties are redeveloped to reduce the number of drive approaches in the station area.
8. Minimize the number of driveways, garage entrances and dedicated turning lanes on all major pedestrian routes.
9. Encourage community services, including schools, childcare and museums with pedestrian connections to transit and other land uses.
10. Extend 450 South midblock walkway through to 650 East.
11. Develop and enhance existing public gathering spaces, including parks, plazas and courtyards to attract people and transform 300 South and 500 South into active pedestrian places.
12. Increase building intensity and residential densities closest to the transit station and gradually step down further away. Parking requirements should be lower closer to the station.



13. Discourage additional or expanded stand alone automobile oriented uses such as: large surface parking lots, fuel stations, auto repair shops, auto dealerships, large scale retail development, car washes and drive-thru facilities.
14. Mid-block crossings across 400 South should be considered adjacent to the Trax stop to help pedestrians cross safely and to discourage jaywalking.

Trolley Station Area

The Trolley Station is defined as an Urban Neighborhood Station Area. Urban Neighborhoods are places that have an established development pattern that contain a mix of uses and can support an increase in residential density and supporting commercial activities. New development generally occurs as infill, occurring on undeveloped or underutilized properties. Redevelopment of surface parking lots that front on 400 South is a priority. A compact development pattern is desired in order to focus new growth at the station and respect the existing scale and intensity of the surrounding neighborhood. The highest residential density and intensity of commercial land use occur closest to the transit station and are scaled down the further one moves from the station.

The station area comprises of core and transition areas. The purpose of creating the different areas is to recognize the scale and nature of existing development patterns and identify the appropriate locations for growth. The general concept is that bigger buildings with the most dwelling units and a higher intensity level of commercial space should be located closest to the station in the core. The transition area reduces the scale, mass and intensity of new development as it moves away from the core area.

Trolley Station is a unique Transit Station Area because it is located within the Central City Historic District. The Central City Historic District is centered on the 600 East landscaped medians, which are a character defining feature of the historic district. Over time, the 600 East medians have been degraded by the continual pressure from commercial development to allow vehicle access through the medians. The policy of the Trolley Station Area is to prohibit further dissection of the 600 East medians for vehicular traffic and to maintain the historical 15 foot landscaped setback of building along 600 East.

Despite being located within the Central City Historic District, the blocks that front on 400 South have lost most, if not all, of their historic character. Without the historic character being present, the boundaries of the Central City Historic District should be reviewed and amendments to the district boundaries should be proposed.

The "TSA Development Guidelines" will be used along 400 South to encourage a walkable urban neighborhood compatible with the adjacent historic district.



Trolley Station Area Goals:

1. The primary purpose of the Trolley Station Area is to provide housing and access to higher intensity employment, commercial centers, downtown, and to the University of Utah.
2. Rezone properties fronting 400 South so that the zoning reflects the vision for the station area. Rezone the properties that are in the vicinity to a mix of zoning districts that promote high density residential development, while allowing limited commercial types of uses in appropriate places. The properties fronting 400 South should be the focus of the station area and development should focus on creating an urban neighborhood in scale and purpose, and is not intended to supplant or compete with the much higher density central business district. Regional scale development beyond the existing Trolley Square commercial development is not encouraged.
3. Coordinate with the Utah Department of Transportation (UDOT) to improve the functionality of 400 South for pedestrians. This could be accomplished by adding additional midblock crosswalks to the transit stations, allowing on street parking during off peak travel hours, studying the impact of reducing the road width, whether in key spots or for longer stretches, and other changes to the street that are aimed at creating a safe and functional transportation corridor for all users.
4. Work with the Transportation Division to find appropriate bicycle routes that run on or parallel 400 South and that connect to appropriately located north/south bike routes.
5. Require midblock connections from 300 South and 500 South that connect to 400 South.
6. Incorporate way-finding features at the station area so people can easily locate station area assets such as Trolley Square, Gilgal Gardens and Liberty Park.
7. Maintain the original 15-foot landscaped setback along the 400 South street frontage.
8. Review the appropriateness of and consider amendments to the boundaries of the Central City Historic District to remove those blocks and portions of blocks that front on 400 South from the historic district due to the lack of remaining historic character. See the "....." map to determine the areas that should be considered for removal from the Central City Historic District.
9. Preserve the 600 East medians and prohibit further bisections of the medians for the purpose of allowing vehicular access and left turns to private property or streets.
10. Extend 450 South midblock walkway through to 650 East.



11. Further multi-modal solutions to change the way 600 South is utilized between 500 East and 700 East. Possible solutions include adding middle of the street parking, midblock crosswalks, medians, or removing a lane of traffic in each direction to add bike lanes, larger park strips or angled parking.
12. Identify zoning solutions for the block faces across from Trolley Square on 600 East and 600 South. The focus should be to encourage development on vacant parcels, increase residential density and promote the preservation and adaptive reuse of contributing structures. The surface parking lot south of Trolley Square should be rezoned to allow Trolley Square to building a parking structure, retain the historic structures fronting on 600 South and build housing.
13. Adopt zoning that increases housing potential, but does not introduce extensive commercial development on the south side of 500 South between 500 and 600 East and both sides of the street between 400 and 500 East.
14. Encourage redevelopment of strip centers into higher density with structured parking with a potential park and ride. The block bounded by 400 and 500 South and 600 and 700 East should have the highest commercial densities of the area.
15. Encourage development that is compatible with the historic development pattern in the Central City Historic District where appropriate.
16. Reevaluate the densities on the northwest quadrant of the block bounded by 300 South and 400 South and 600 East and 700 East, to allow higher densities and design that is compatible with the adjacent historic character.
17. Mid-block crossings across 400 South should be considered adjacent to the Trax stop to help pedestrians to cross safely and to discourage jaywalking.

900 East Station Area

The 900 East Station is an Urban Neighborhood Station Area due to the established and predominating residential character and the potential for infill development along 400 South which adds variations of density and intensity of building forms that blend in and complement the existing residential character of the area. The development strategy of an urban neighborhood station may include the mixing of building types and uses, including the allocation of commercial or office uses to the ground floor, and residential uses above these floors. The highest residential densities and most intense land uses are generally located closest to the station platform along 400 South between 700 East and 900 East, particularly on the south side of 400 South.

The station area comprises of core and transition areas. The purpose of creating the different areas is to recognize the scale and nature of existing development patterns and identify the appropriate locations for growth. The general concept is that bigger buildings with the most



dwelling units and a higher intensity level of commercial space should be located closest to the station in the core. The transition area reduces the scale, mass and intensity of new development as it moves away from the core area. The north side of 400 South is part of the transition area due to the close proximity of the relatively low scale nature of the residential area to the north and the desire to maintain that character and the impacts that taller buildings on 400 South would have on privacy and solar access.

Contributing landmarks and existing neighborhood characteristics will influence future development in and around the 900 East Station. These existing traits include institutional uses such as the Intermountain Health Care facility, Bennion Elementary School, and religious structures. There are also various types of commercial and a strong mixture of single and multi-family residential building types.

900 East Station Area Goals:

1. Protect historic landmark sites that currently exist in the Station Area.
2. Coordinate with UDOT to improve the functionality of 400 South for pedestrians. This could be accomplished by adding additional midblock crosswalks to the transit stations, allowing on street parking during off peak travel hours, studying the impact of reducing the road width, whether in key spots or for longer stretches, and other changes to the street that are aimed at creating a safe and functional transportation corridor for all users.
3. Maintain original 15-foot landscaped setback along the 400 South street frontage.
4. Incorporate way-finding features at the station area so people can easily locate station area assets such as Gilgal Gardens and health care facilities.
5. Protect the 800 East medians by not allowing vehicular access cuts through the existing landscaped area.
6. Encourage infill uses that will eliminate surface parking lots with an emphasis on those that front 400 South or areas that can easily access the station platform.
7. Encourage shared parking facilities between uses where applicable.
8. Focus on mixed-use development with commercial and residential uses along 400 South and the east side of 700 East. In remaining areas the primary redevelopment focus should be on residential uses.
9. If the Bennion Elementary School discontinues to be utilized as a school, the property should be redeveloped with an emphasis on residential uses along the frontages but much of the open space behind should remain, and be enhanced and available to the public.



10. Create mid-block connections that draw pedestrians to 400 South and specifically to the 900 East TRAX station. This should be emphasized between 300 South and 400 South where connections could more easily be created by linking existing rights-of-way such as, Laker Court and Strongs Court.
11. Encourage nonconforming manufacturing or warehousing uses to relocate to appropriate locations in the city.
12. Adopt zoning that promotes high density housing with appropriate design standards adjacent to Gilgal Gardens in order to activate and provide security for the park.
13. Mid-block crossings across 400 South should be considered adjacent to the Trax stop to help pedestrians to cross safely and to discourage jaywalking.

DRAFT

Attachment D
Library Station Summary Handout




400 SOUTH

LIVABLE COMMUNITIES

LIBRARY STATION



LOCATION	BOUNDARY
<p>The Library Station Trax platform is located in the middle of 400 South. The platform is located just to the east of 200 East and to the north of the Salt Lake City Main library.</p>	<p>The general boundaries of the Library Station are as follows:</p>
	<p>North: 300 South East: 400 East South: 500 South West: 200 East</p>



February 2012 400 South Livable Communities

Library Station Area

The Library Station area is divided into two subareas as follows:

CORE

The purpose of the core area is to provide areas for comparatively intense land development with a mix of land uses incorporating the principles of sustainable, transit oriented development and to enhance the area closest to a transit station as a lively, people oriented place. The core area is generally within a ¼ mile walk of a transit station platform. The core area may mix ground floor retail, office, commercial and residential space in order to activate the public realm.

Buildings in this area should have minimal setbacks to encourage active outdoor use adjacent to the sidewalk, such as outdoor dining and patios that reflect the desired character of the area. Building facades should be varied and articulated, include storefronts adjacent to the street, windows on the street level, and have clearly defined entrances to provide visual interest to pedestrians. Buildings should be a minimum of two or three stories in height, depending on location, in order to define the street edge. Arcades, bays, and balconies are encouraged. The configuration of buildings must balance the needs of all modes of circulation with the safety and comfort of pedestrians and bicyclists. A minimum of 30 dwelling units per acre is encouraged within the core.

TRANSITION

The purpose of the transition area is to provide a moderate level of land development intensity that incorporates the principles of sustainable transit oriented development. The transition area is intended to provide an important support base to the core area and transit ridership as well as buffer surrounding neighborhoods from the intensity of the core area. These areas reinforce the viability of the core area and provide opportunities for a range of housing types at different densities.

Transition areas are generally located within a ½ mile from the station platform, but may vary based on the character of the area. Transition areas typically serve the surrounding neighborhood; include a broad range of building forms that house a mix of compatible land uses. The minimum desired density is 10 dwelling units per acre. Commercial uses may include office, retail, restaurant and other commercial land uses that are necessary to create mixed use neighborhoods. Commercial uses can be clustered around intersections and along block faces to create neighborhood nodes.



CENTRAL COMMUNITY MASTER PLAN *Transit Oriented Development*

Area Summary

- The Library Urban Center Station has the highest intensity level and mix of uses along 400 South.
- Large scale development is closer to the station; and is scaled back as it moves to the areas away from 400 South.
- The station area contains important regional attractions, such as the Library, City & County Building, Leonardo Museum, and the Public Safety Building
- Policy Highlights
- Rezone properties fronting 400 South so that the zoning reflects the vision for the station area.
- Work with the City Transportation Division to find appropriate bicycle routes that run on or are parallel to 400 South and that connect to appropriately located north/south bike routes.
- Require midblock connections from 300 South and 500 South that connect to 400 South.
- Require properties to share driveway access as properties are redeveloped to reduce the number of drive approaches in the station area.
- Extend 450 South midblock walkway through to 650 East.
- Increase building intensity and residential densities closest to the transit station and gradually step down further away. Parking requirements should be lower closer to the station.
- Discourage additional or expanded stand alone automobile oriented uses such as: large surface parking lots, fuel stations, auto repair shops, auto dealerships, large scale retail development, car washes and drive-thru facilities.
- Mid-block crossings across 400 South should be considered adjacent to the Trax stop to help pedestrians cross safely and to discourage jaywalking.

February 2011 400 South Livable Communities

TSA Design Guidelines



What are Development Guidelines?

Development Guidelines are a list of recommendations that should be used to guide new development within a transit station area. The guidelines are an integral part of the review process set forth in Chapter 21A.26. The intent of the TSADG is to reward high quality, desired development through the use of incentives (such as increased building height) and by allowing for a quicker review process.

How to use the Development Guidelines

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The Development Score Thresholds

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February 2012 400 South Livable Communities

Transit Station Area Zoning

New Zoning- Transit Station Area



Salt Lake City Zoning Ordinance Section 21A.26.070

The purpose of the TSA zoning designation is to provide an environment for efficient and attractive transit and pedestrian oriented commercial, residential and mixed use development around transit stations. Redevelopment, infill development and increased development on underutilized parcels should include uses that allow them to function as part of a walkable, mixed use district. Existing uses that are complementary to the district, and economically and physically viable, should be integrated into the form and function of a compact, mixed use pedestrian oriented neighborhood. Each transit station is categorized into a station type. These typologies are used to establish appropriate zoning regulations for similar station areas. Each station area will typically have two subsections: the core area and the transition area.

Within the TSA zone, uses are listed as prohibited, rather than a list of permitted uses. The reason is to identify those land uses that are not compatible with transit oriented development due to the nature of the use, the land requirements of the use, or the potential impacts of the use. Any use not listed, but is substantially similar to a use listed, is prohibited. All other uses would be permitted. Some examples of uses that are currently in Chapter 21A.26.070 as prohibited in a TSA zone:

- Ambulance services, dispatching, staging and maintenance
- Animal pound
- Auto salvage and recycling
- Auto, truck, and RV repair, sales and/or service (minor or major)
- Car washes
- Cemeteries, any type
- Commercial parking lots not located in a parking structure
- Community correction facilities, any size
- Detached single-family dwelling
- Drive-through windows and service
- Flammable liquids or gases, heating fuel distribution
- Gas stations
- Heavy equipment rental, repair and sales
- Homeless shelters
- Impound lot
- Incinerators, for any purpose
- Kennel
- Manufactured/mobile home sales and service
- Ministorage facilities
- Outdoor storage including storage or staging of fleet vehicles associated with a business, but not intended to include outdoor retail sales areas or outdoor storage as an accessory and incidental use to a principal use
- Pawnshops
- Sexually oriented business
- Stable, public or private
- Wholesale distributors

For a complete list, please refer to Table 21A.26.078E in the Zoning Ordinance.

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A Housing & Urban Development Liveable Communities Grant Project

400 SOUTH



February 2012 400 South Livable Communities

Attachment E
Trolley Station Summary Handout



400 SOUTH

LIVABLE COMMUNITIES

TROLLEY STATION



LOCATION	BOUNDARY
<p>The Trolley Station Trax platform is located in the middle of 400 South. The platform is located just to the east of 600 East and provides easy walking access to Trolley Square and other retail shopping locations in the area.</p>	<p>The general boundaries of the Trolley Station are as follows:</p> <p>North: 300 South East: 900 East South: 500 South West: 400 East</p> 

February 2012 400 South Livable Communities

Trolley Station Area

The Trolley Station area is divided into two subareas as follows:

CORE ●

The purpose of the core area is to provide areas for comparatively intense land development with a mix of land uses incorporating the principles of sustainable, transit oriented development and to enhance the area closest to a transit station as a lively, people oriented place. The core area is generally within a ¼ mile walk of a transit station platform. The core area may mix ground floor retail, office, commercial and residential space in order to activate the public realm.

Buildings in this area should have minimal setbacks to encourage active outdoor use adjacent to the sidewalk, such as outdoor dining and patios that reflect the desired character of the area. Building facades should be varied and articulated, include storefronts adjacent to the street, windows on the street level, and have clearly defined entrances to provide visual interest to pedestrians. Buildings should be a minimum of two or three stories in height, depending on location, in order to define the street edge. Arcades, bays, and balconies are encouraged. The configuration of buildings must balance the needs of all modes of circulation with the safety and comfort of pedestrians and bicyclists. A minimum of 30 dwelling units per acre is encouraged within the core.

TRANSITION ○

The purpose of the transition area is to provide a moderate level of land development intensity that incorporates the principles of sustainable transit oriented development. The transition area is intended to provide an important support base to the core area and transit ridership as well as buffer surrounding neighborhoods from the intensity of the core area. These areas reinforce the viability of the core area and provide opportunities for a range of housing types at different densities.

Transition areas are generally located within a ½ mile from the station platform, but may vary based on the character of the area. Transition areas typically serve the surrounding neighborhood; include a broad range of building forms that house a mix of compatible land uses. The minimum desired density is 10 dwelling units per acre. Commercial uses may include office, retail, restaurant and other commercial land uses that are necessary to create mixed use neighborhoods. Commercial uses can be clustered around intersections and along block faces to create neighborhood nodes.



CENTRAL COMMUNITY MASTER PLAN *Transit Oriented Development*

Area Summary

- The primary purpose of the Trolley Station Area is to provide housing and access to higher intensity employment, commercial centers, downtown, and to the University of Utah.
- Work with the Transportation Division to find appropriate bicycle routes that run on or parallel 400 South and that connect to appropriately located north/south bike routes.
- Require midblock connections from 300 South and 500 South that connect to 400 South.
- Review the appropriateness of and consider amendments to the boundaries of the Central City Historic District to remove those blocks and portions of blocks that front on 400 South from the historic district due to the lack of remaining historic character.
- Preserve the 600 East medians and prohibit further bisections of the medians for the purpose of allowing vehicular access and left turns to private property or streets.
- Identify zoning solutions for the block faces across from Trolley Square on 600 East and 600 South. The focus should be to encourage development on vacant parcels, increase residential density and promote the preservation and adaptive reuse of contributing structures. The surface parking lot south of Trolley Square should be rezoned to allow Trolley Square to building a parking structure, retain the historic structures fronting on 600 South and build housing.
- Adopt zoning that increases housing potential, but does not introduce extensive commercial development on the south side of 500 South between 500 and 600 East and both sides of the street between 400 and 500 East.
- Encourage development that is compatible with the historic development pattern in the Central City Historic District where appropriate.

February 2012 400 South Livable Communities

TSA Design Guidelines



What are Development Guidelines?

Development Guidelines are a list of recommendations that should be used to guide new development within a transit station area. The guidelines are an integral part of the review process set forth in Chapter 21A.26. The intent of the TSADG is to reward high quality, desired development through the use of incentives (such as increased building height) and by allowing for a quicker review process.

How to use the Development Guidelines

These development guidelines are to be used by the City in the review process for each project located in the TSA zoning district. While the guidelines are voluntary, compliance with them entitles applicants to faster review and can guide new development and redevelopment towards implementing the vision in the station area plans. Designers and developers are encouraged to incorporate as many of the development guidelines as they reasonably can, which in turn will lessen the review process timeframe. Each Guideline includes a value. The assigned value is based on several factors, including the importance of the guideline in terms of implementing the vision of a Station Area Plan or other applicable land use related policies of the City and the cost of addressing the guideline. Development Guidelines should be addressed in all new projects. The Development Score is established by adding up the values of each guideline that has been incorporated into a project. Applicants have the ability to determine City review time of their proposal by achieving a high development score. This type of point and reward system allows desired development to be processed quicker.

The Development Score Thresholds

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standards. The purpose of the Conditional Building and Site Design Review Process is to provide flexible implementation of the specific design requirements found in individual zoning districts. It is intended to help ensure that newly developed properties and redeveloped properties are designed to encourage pedestrian access, circulation and orientation while acknowledging the need for transit and automobile access. This process also requires public input prior to scheduling a public hearing with the Planning Commission. Projects that are in Tier 2 require an Administrative Hearing prior to an Administrative Decision. This process allows for a relatively quick review and approval process, but still requires a public hearing. Projects that achieve a development score in Tier 3 are rewarded by allowing administrative approval without a public hearing. Tier 3 was established so that those projects that are highly compliant with the vision of a station area plan have a streamlined approval process. The decision made in each Tier is subject to appeal as outlined in the Zoning Ordinance.

Review Process

Tier	Approval Process	Estimated Time Frame*
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February 2012 400 South Livable Communities

Transit Station Area Zoning

New Zoning- Transit Station Area



Salt Lake City Zoning Ordinance Section 21A.26.070

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- Animal pound
- Auto salvage and recycling
- Auto, truck, and RV repair, sales and/or service (minor or major)
- Car washes
- Cemeteries, any type
- Commercial parking lots not located in a parking structure
- Community correction facilities, any size
- Detached single-family dwelling
- Drive-through windows and service
- Flammable liquids or gases, heating fuel distribution
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- Incinerators, for any purpose
- Kennel
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- Outdoor storage including storage or staging of fleet vehicles associated with a business, but not intended to include outdoor retail sales areas or outdoor storage as an accessory and incidental use to a principal use
- Pawnshops
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- Wholesale distributors

For a complete list, please refer to Table 21A.26.078E in the Zoning Ordinance.

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A Housing & Urban Development Liveable Communities Grant Project

400 SOUTH



February 2012 400 South Livable Communities

Attachment F
900 Station Summary Handout



400 SOUTH

LIVABLE COMMUNITIES

900 EAST STATION



LOCATION	BOUNDARY
<p>The 900 East Station Trax platform is located in the middle of 400 South. The platform is located just to the west of 900 East and provides easy walking access to various retail shopping and residential locations in the area.</p>	<p>The general boundaries of the 900 East Station are as follows:</p> <p>North: 300 South East: 1100 East South: 500 South West: 700 East</p>



February 2012 400 South Livable Communities

900 East Station Area

The 900 East Station area is divided into two subareas as follows:

CORE ●

The purpose of the core area is to provide areas for comparatively intense land development with a mix of land uses incorporating the principles of sustainable, transit oriented development and to enhance the area closest to a transit station as a lively, people oriented place. The core area is generally within a ¼ mile walk of a transit station platform. The core area may mix ground floor retail, office, commercial and residential space in order to activate the public realm.

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TRANSITION ○

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CENTRAL COMMUNITY MASTER PLAN *Transit Oriented Development*

Area Summary

- Protect historic landmark sites that currently exist in the Station Area.
- Protect the 800 East medians by not allowing vehicular access cuts through the existing landscaped area.
- Encourage infill uses that will eliminate surface parking lots with an emphasis on those that front 400 South or areas that can easily access the station platform.
- Focus on mixed-use development with commercial and residential uses along 400 South and the east side of 700 East. In remaining areas the primary redevelopment focus should be on residential uses.
- If the Bennion Elementary School discontinues to be utilized as a school, the property should be redeveloped with an emphasis on residential uses along the frontages but much of the open space behind should remain, and be enhanced and available to the public.
- Encourage nonconforming manufacturing or warehousing uses to relocate to appropriate locations in the city.
- Adopt zoning that promotes high density housing with appropriate design standards adjacent to Gilgal Gardens in order to activate and provide security for the park.

February 2012 400 South Livable Communities

TSA Design Guidelines



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February 2012 400 South Livable Communities

Transit Station Area Zoning

New Zoning- Transit Station Area



Salt Lake City Zoning Ordinance Section 21A.26.070

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- Car washes
- Cemeteries, any type
- Commercial parking lots not located in a parking structure
- Community correction facilities, any size
- Detached single-family dwelling
- Drive-through windows and service
- Flammable liquids or gases, heating fuel distribution
- Gas stations
- Heavy equipment rental, repair and sales
- Homeless shelters
- Impound lot
- Incinerators, for any purpose
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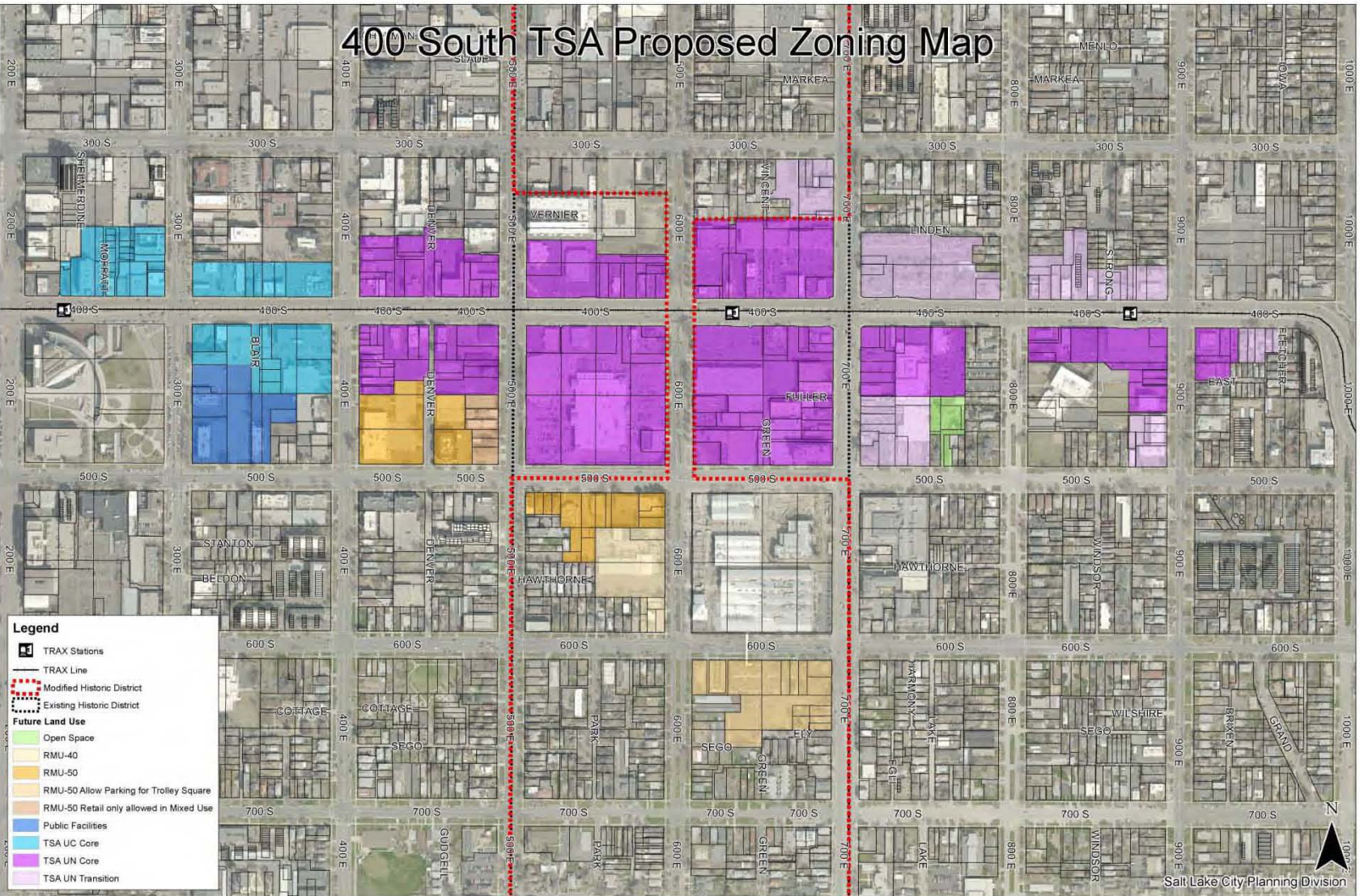
A Housing & Urban Development Liveable Communities Grant Project

400 SOUTH



February 2012 400 South Livable Communities

Attachment G
Map of Proposed Zoning Changes



**Excerpt of
SALT LAKE CITY
HISTORIC LANDMARK COMMISSION
Minutes of the Meeting
Room 326, 451 South State Street
April 5, 2012**

This document along with the digital recording constitute the official minutes of the Historic Landmark Commission regular session meeting held on April 5, 2012.

Historic Landmark Commission Meetings are televised on SLCTV 17. Archived video of this meeting can be found at the following link under, "Historic Landmark Commission and RDA": http://www.slctv.com/vid_demand.htm,

A regular meeting of the Historic Landmark Commission was called to order on Thursday, April 5, 2012, in Room 326 of the City and County Building, located at 451 South State Street, Salt Lake City, Utah, [5:36:47 PM](#) Commissioners present for the meeting were Chairperson Anne Oliver, Vice Chair Polly Hart, Earle Bevins III, Arla Funk, Sheleigh Harding, Stephen James and Dave Richards. Commissioner Bill Davis was excused.

Planning Staff members present for the meeting were Cheri Coffey, Assistant Planning Director, Janice Lew, Senior Planner, Michaela Oktay, Principal Planner, Maryann Pickering, Principal Planner, Katia Pace, Principal Planner, and Michelle Moeller, Senior Secretary. City Attorney Lynn Pace was also present.

[6:31:48 PM](#)

PLNPCM2010-00647 – 400 South Livable Communities Station Area Plans - A request by Salt Lake City Mayor Ralph Becker for an amendment to the Central Community Master Plan, Salt Lake City Zoning Ordinance and Zoning Map regarding transit station area plans along the 400 South transit corridor. The intent of the project is to implement livability goals for the corridor including land use policies and zoning that support mixed use development and transportation choices for current and future residents and workers. The project is located in City Council District 4 represented by Luke Garrott. (Staff contact: Maryann Pickering at 801-535-7660 or maryann.pickering@slcgov.com).

Ms. Maryann Pickering, Principal Planner, reviewed the petition as presented in the Staff Report (located in the case file).

Commissioner Hart stated Ensign Floral was a contributing building and asked if the grey apartment building located at 318 South 600 East was going to be in the Historic District or not as it was not clear.

Ms. Pickering stated it was on the proposed block therefore, it would be removed.

Commissioner Hart stated it should be added to the list of contributing buildings.

Ms. Pickering stated she would recheck the area and make sure the historic buildings were left within the district.

Commissioner James asked Staff to summarize the core characteristics of the intent for zone and what was the zone would accomplish in the particular area.

Ms. Pickering reviewed the zoning and core specifications for each area as outlined in the TSA Zoning Ordinance.

Commissioner James asked what the zone yielded in terms of streetscape character and potential compatibility with the traditional context.

Ms. Pickering stated the overall intent of the TSA district would encourage more mixed use development and push buildings up to the street to create a pedestrian environment with parking in the rear making it more of a pedestrian core.

Commissioner James stated the idea was the functional urbanism that was established through the new zone actually reflected the traditional urbanism of the original neighborhoods eroded over time.

Ms. Pickering stated that was correct.

Commissioner Funk stated she was concerned with the height allowance and asked why seventy five feet was determined to be an appropriate height. She stated if a seventy five foot building was placed on the face of the block what would it do to the residential area behind the TSA zone.

Ms. Pickering stated part of the requirement was that any core area, adjacent to a single family residential zone, would have to step back from the residential zone so the highest buildings would be allowed along 400 South or any other major street. She stated the minimum setback would be twenty five feet and the higher the building the larger the setback. Ms. Pickering stated through the design standards the residential areas would be protected by pushing the intensive height away from the residential areas.

Commissioner Funk asked if North Temple and 400 South were considered comparable.

Ms. Pickering stated with the Trax line they were but, there were different components involved on 400 South.

Commissioner Funk stated she had concerns, because of past planning, that the City may be currently over building and not thinking what the City's needs would be in the future.

Ms. Pickering stated the predictions show the City's population would probably double in the next fifty years and instead of sprawling out to the edges the City was trying to concentrate the development. She stated last summer, Staff met with the individual community groups where many people expressed a need for more apartments in this area therefore, the goal would be mixed use with retail on the first and second floors and apartments above.

Commissioner Funk asked how the height limit was determined.

Ms. Pickering stated it was currently seventy five feet including the TC-75 Zoning District that is in the area.

Commissioner Funk asked what drove that height allowance.

Ms. Pickering stated it was a standard used throughout the country.

Chairperson Oliver stated maybe the question was if the height should be allowed or if it was too high for the area. She asked by removing the historic overlay how would it impede the proposal and could both exist at the same time or could the height be lowered in the section to help reunite the two sections of the Historic District. Chairperson Oliver stated the question could be asked if the historic overlay would even need to be removed.

Ms. Pickering stated the historic resources remaining in the identified areas were extremely limited.

Chairperson Oliver stated she agreed but by removing the historic overlay requirement there would be less control over what was allowed. She stated if it were kept more direction could be given. Chairperson Oliver stated it seemed there was a danger in splitting up the historic district.

Commissioner Funk expressed her concern on the impact of the zoning around the stadium neighborhood and setting a precedent in the area.

Ms. Pickering stated currently the TSA zone did not reach the University and if it was to expand it most likely would be towards downtown.

Commissioner James stated he felt the compact development around transit stops would ultimately be a tool to help preserve traditional neighborhoods because there would be a lot less development pressure to expand horizontally. He stated the seventy five feet in context to the width of the right of way was a good thing because in space and scale it was proportionate. Commissioner James said in order to achieve the sense of community and walkability there needed to be a bit of enclosure to the space. Commissioner James stated he likes the idea but the area should retain its historic character.

The Commissioners discussed the ideal height in respect to the neighborhood and 400 South.

Ms. Coffey stated the areas being discussed were only those in the historic district. She said as Staff stated there was not a lot of historic fabric left in the area. Ms. Coffey explained Staff and the Commission needed to see how well the zoning guidelines worked with the preservation guidelines in the area.

Chairperson Oliver stated the careful consideration of what is allowed in the TSA zoning and the compatibility with the Commercial Design Guidelines needed to be considered.

Ms. Pickering stated the guidelines would be sent to the Commissioners before the next meeting.

Chairperson Oliver stated the proposal referred to the parking lot south of Trolley Square and asked Ms. Pickering to discuss what was being proposed.

Ms. Pickering stated the property was currently zoned RMF and the owners of the parking lot have requested expansion which is not permitted because it would expand a non-conforming

use. She stated because of the property's location to 400 South Staff decided to add it to the proposed project as it would be an ideal time to rezone the parcel. Ms. Pickering stated Staff had received negative feedback indicating the RMU zoning would be too intense and possibly cause disregard for the existing historic structures. She stated this could be addressed at the Public Hearing if the Commission wanted or it could be removed from the proposal.

Commissioner Hart asked if this was a transit orientated zone why Staff would want to expand the parking lot.

Ms. Pickering stated the subject section was not transit orientated development. She stated it was just in the area and under the same historic district therefore, Staff included it in the proposal.

Chairperson Oliver stated there was a clear delineation between residential and commercial on the street and she would agree that it would not be a good idea to rezone the parcel and add mixed use in the area.

Commissioner James stated it didn't make sense to change the zoning for the parcel.

Commissioner Bevins asked if parking was part of the Trolley Square expansion requirements.

Ms. Pickering stated it may have been part of the requirements but due to the store vacancies in Trolley Square the parking lot was not currently utilized. She explained if Trolley Square were to be fully leased the parking would be utilized more frequently.

Commissioner Bevins asked if the parking would have been a condition to the expansion of Trolley Square.

Ms. Pickering stated no not under the current zoning because it was a non-conforming use.

Commissioner Hart asked if the owners of the lot requested the zoning change or was it the City's suggestion.

Ms. Pickering stated the owner approached the City a few years ago and it was something that had been put on the back burner. She explained the owner had not come forward as part of the current proposal.

Commissioner Hart asked if a zoning change had been previously requested.

Ms. Pickering stated no.

Ms. Coffey stated over the years the development of the parcel had been reviewed and with the start of the proposed project it seemed like the time to look at the idea of changing the zoning and possibly sparking the use of the boarded up buildings.

Chairperson Oliver asked Staff to give their thoughts on rezoning the parcel and what the benefits would be.

Ms. Pickering stated development in the parking lot and on the vacant corner lot with other development in years to come would most likely be the result.

Commissioner James asked if the City was providing to many development opportunities at once and would it not make sense to focus on one area.

Commissioner Hart stated she did not understand why the City would offer up a zoning change to a parcel when other property owners have asked for rezoning and been denied. She stated if the owner wanted a zoning change they should go through the process.

**6.C. HISTORIC LANDMARK COMMISSION
MAY 17, 2012 PUBLIC HEARING**

HISTORIC LANDMARK COMMISSION STAFF REPORT



Planning and Zoning Division
Department of Community and
Economic Development

400 South Livable Communities Master Plan, Zoning Map and Text Amendments PLNPCM2010-00647 May 17, 2012

Applicant: Mayor Ralph
Becker

Staff: Maryann Pickering
801-535-7660 or
maryann.pickering@slcgov.com

Tax ID: N/A

Current Zone: Various – see
page 3 for current zoning map

Council District: District 4
represented by Luke Garrott

Community Council: Central
City and East Central

Lot Size: N/A

Current Use: N/A

Request

A request by Salt Lake City Mayor Ralph Becker for an amendment to the Central Community Master Plan, Salt Lake City Zoning Ordinance and Zoning Map regarding transit station area plans along the 400 South transit corridor. The intent of the project is to implement livability goals for the corridor including land use policies and zoning that support mixed use development and transportation choices for current and future residents and workers. The Planning Commission is required to transmit a recommendation to the City Council for Master Plan and Zoning Map Amendment Requests.

Recommendation

That the Historic Landmark Commission discusses the proposed changes as part of the 400 South Livable Communities project and provide comments and a recommendation to the Planning Commission.

Background

Starting in March of 2011, Planning Division staff began working on a master plan amendment and rezoning project for the University TRAX line. Three stations were identified to be included as part of the rezoning process. Those stations are: Library, Trolley and 900 East. The goal of the proposed project is to create station area plans and zoning regulations for the three stops that:

- a. Provide more transportation choices.
- b. Promote equitable, affordable housing.
- c. Enhance economic competitiveness.
- d. Support existing communities.
- e. Coordinate policies and leverage investments.
- f. Value communities and neighborhoods.

In addition to the above, this project assists in the implementation of the Wasatch Choices 2040 Plan, which is a regional plan created by residents, businesses, and other stakeholders to manage future growth in the region by focusing it on certain nodes, particularly those where transit facilities already are in place. Further, Planning Division staff felt that this area needed to be studied for a potential change at the current zoning because despite the current zoning of TC-75, very little private investment has been made in the area since the TC-75 zoning was created in 2005.

Responses to April 5 Historic Landmark Commission Briefing Central City Historic District Boundaries

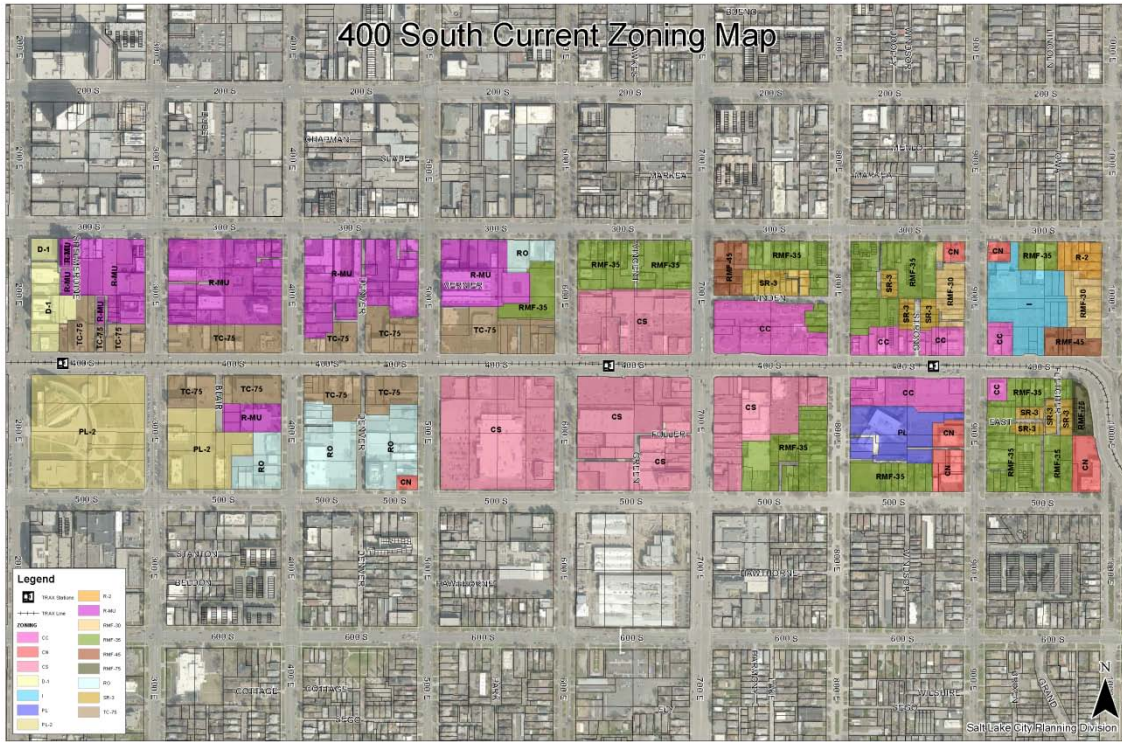
As part of this master plan amendment and rezoning process for the 400 South Livable Communities Project, staff was recommending that the boundaries of the Central City Historic District be modified. After the discussion with the Historic Landmark Commission last month, staff is no longer recommending that the boundaries of the Central City Historic District be modified at this time. Staff would suggest that in the future a proper survey of the existing Central City Historic District is conducted and that once that survey is complete, staff should evaluate modifying the boundaries of the Central City Historic District.

Proposed Zoning Changes

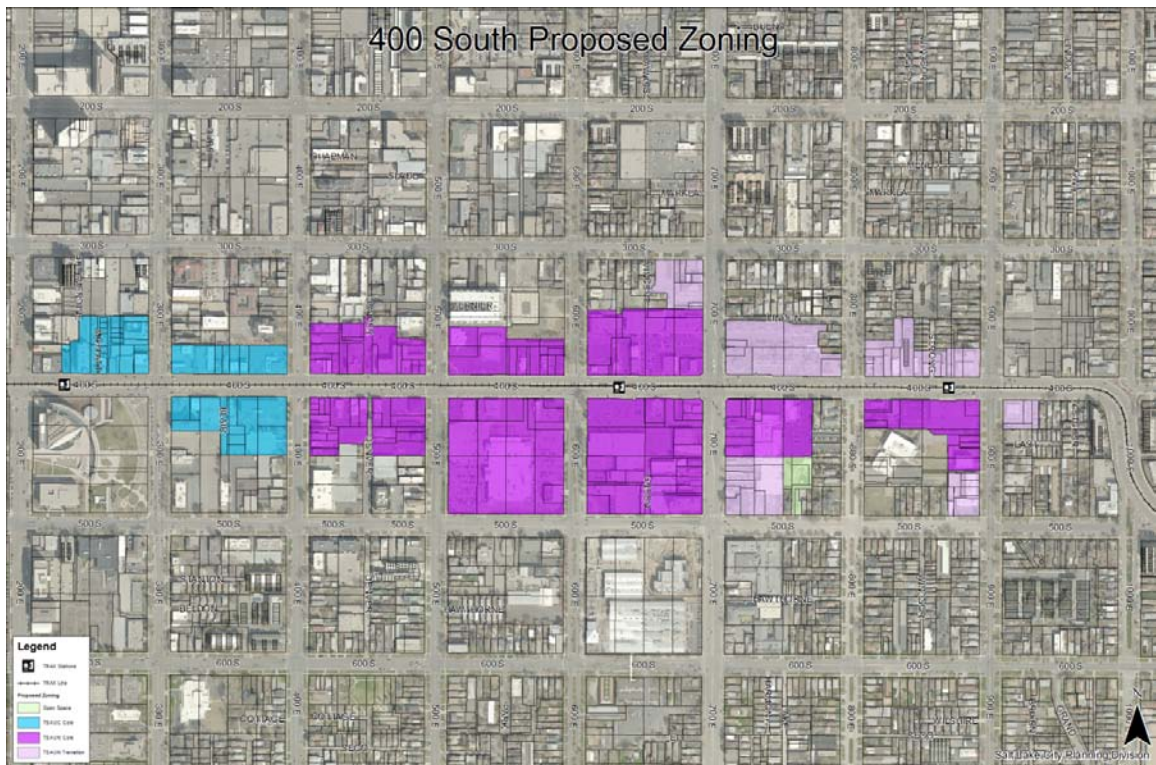
At the last meeting in April, there was discussion about the proposed zoning changes to the west and south of Trolley Square. The proposed zoning changes were within the boundaries of the Central City Historic District. These areas have now been removed and staff feels that similar to the boundaries of the historic district, rezoning of these parcels should be evaluated after a survey is completed.

Staff continues to recommend zoning changes directly along the 400 South corridor and for a portion of 700 East. We feel this is the best location for more intense development with the intensity lessening on the north and south sides of the corridor. The existing and proposed zoning is noted below.

EXISTING ZONING



PROPOSED ZONING

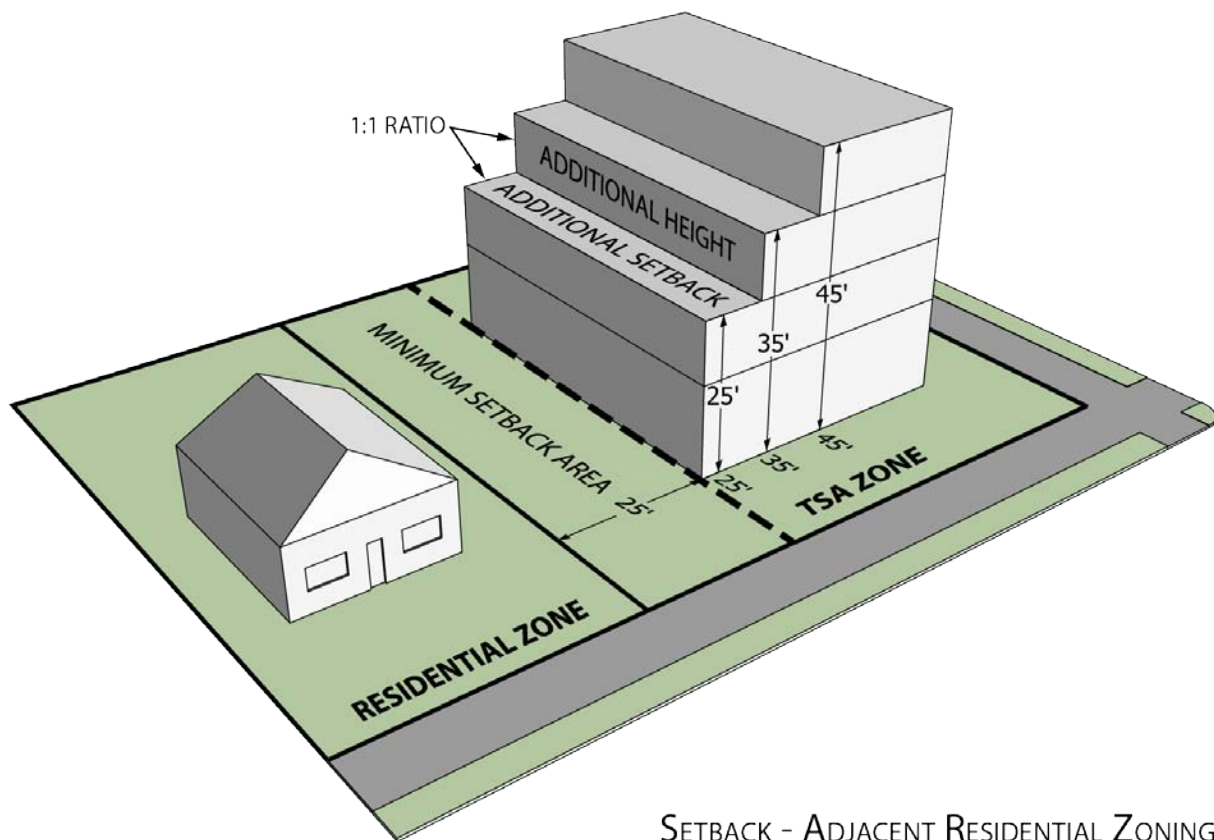


Analysis of Existing and Proposed Zoning Changes

The current zoning along 400 South between 200 and 600 East is primarily Transit Corridor District (TC-75). This zoning designation was put in place in 2005 and allowed a maximum building height of 75 feet, but up to 125 feet could be approved as part of the Conditional Building and Site Design review process. 400 South between 600 and 900 East is currently zoned either Community Shopping (CS) or Corridor Commercial (CC). These two zoning designations do have a lower maximum building height than what is currently being proposed but with the standards and development guidelines of the proposed TSA district, the adjoining neighborhoods will be buffered from the more intensive development.

The proposed zoning changes within the historic district will primarily be located along 400 South and there will be some changes along the west side of 700 East. The core area, with the taller building heights will be located directly on or fronting along 400 South and the two blocks south of 400 South between 500 and 700 East. The transition area with less building height will be located along 700 East between 300 and 400 South.

Any new development proposed within the TSA zoning designation would need to be setback at a 1:1 ratio when it is adjacent to certain residential zoning districts and when a street is less than 50 feet in width. The reasoning behind this additional setback is to provide a buffer to the existing residential areas. An example would be if a building was proposed to be 45 feet tall. The height of 45 feet would need to be at least 45 feet from the property line adjacent to the residential zoning or the narrow street.



SETBACK - ADJACENT RESIDENTIAL ZONING

Existing Building Heights

Within the boundaries of the Central City Historic District, there are currently some buildings that are similar in height to the proposed standards in the TSA zoning designation.



Xerox/IBM Building (northwest corner of 500 South 700 East) – approximately 70 feet tall



Emigration Court (east side of 500 East) – approximately 80 feet tall along 500 East



New housing on 300 South (between 500 and 600 East) – approximately 50 feet



Eastside Apartments (west side of 600 East) – range in height from approximately 35 feet tall to 75 feet tall.

Transit Station Area Development Guidelines

In addition to the additional setback requirement noted above for any new development, the TSA zoning district includes Development Guidelines. The intent of the Development Guidelines is to reward high quality, desired development through the use of incentives. The Development

Guidelines are extensive and a full copy can be found by clicking on this link: <http://www.slcclassic.com/CED/planning/400South/Docs/DDG.pdf>.

As stated, in addition to encouraging a high quality development, the Development Guidelines also contain point values for different components or amenities incorporated into projects. Although compliance with the guidelines is voluntary, compliance with them entitles applicants to a faster review and can guide new development and redevelopment in implementing the vision in the station area plans. Some examples of the Development Guidelines include:

Number 6 – Redevelopment of Surface Parking Lots

A project that includes the redevelopment of an existing surface parking lot to an active use or structured parking shall have the following number of points added to the development score:

1. 50% or more of the existing surface parking lot is covered by new buildings: 15 points.
2. 35% or more of the existing surface parking lot is covered by new buildings: 10 points.
3. 25% or more of the existing surface parking lot is covered by new buildings: 5 points.



The parking lot to the left was redeveloped into a mixed use building with residential on the upper floors and commercial space on the ground floor.

Number 12 – 360-Degree Architecture

A project that incorporates architecture features on a building façade that are not adjacent to the street shall have the following number of points added to the development score:

1. Architectural detailing is wrapped on all four sides: 20 points.
2. Architectural detailing is wrapped around both side façades, but not on the rear façade: 15 points.



Prominent cornices, belt courses, and a rhythm of voids (windows) to solids (walls) are included on the sides of these buildings that are not adjacent to a street but are still visible from public spaces.

Number 13 – Historic Preservation

Projects that preserve, rehabilitate, restore, reuse a historic property or new construction that contributes to the character of a historic property or district shall have the following points added to the development score:

1. Local Register: New construction, major alterations and additions that are approved by the Historic Landmark Commission that include reuse of the site: 40 points.
2. National Register: State Historic Preservation Office review and approval of projects with exterior alterations not locally designated and seeking federal tax credits: 20 points.
3. Local Register: Projects that receive administrative approval in accordance with Zoning Ordinance Section 21A.34.020: 5 points.
4. Projects that add historically significant sites to the Salt Lake City Register of Cultural Resources if they qualify as defined in Zoning Ordinance Section 21A.34. 5 points



Every effort should be made to preserve historic buildings. This picture shows the preservation of a historic structure where damaged or missing historic materials have been replaced with matching materials.

Number 23 – Connections and Walkways

Projects that include connections and walkways from buildings, parking lots and private open space to public spaces, shall have the following number of points added to their development score:

1. Projects that include a minimum six foot wide ADA accessible walkway through a parking lot that is separated from vehicle drive aisles: 4 points.
2. Projects that include a minimum six foot wide ADA accessible sidewalk from private property to public open spaces: 4 points.



Walkways can connect development to open spaces.

Walkways can connect development to open spaces.

Number 25 – Access to Transit

A project located within close proximity to a transit station shall have the following number of points added to the development score.

1. Within 750 feet, measured along the most direct, legal walking path: 8 points.
2. Within 1500 feet, measured along the most direct, legal walking path: 4 points.



Projects that are closer to a station platform are more likely to increase pedestrian, bicycle and transit use and are more desirable.

Again, the examples above represent only a few of the development guidelines. There are a total of 29 potential guidelines that an applicant can choose to integrate into a development. Staff

would encourage that you look at the complete development guidelines document online to gain an understanding of how the process works.

Existing Historic District Regulations

In addition to the TSA Development Guidelines, any project within the existing Central City Historic District would still need to comply with all the regulations of Chapter 21A.34 of the Zoning Ordinance and a Certificate of Appropriateness.

Existing Landmark Sites

Within the entire boundaries of the 400 South Livable Communities project, but outside of the Central City Historic District boundaries, there are two landmark buildings. One is the LDS Tenth Ward House at the southwest corner of 400 South and 800 East. The current zoning for this site is CS and the site has not been recommended to be rezoned to a TSA designation.

The other landmark site is the Peck House located near the northwest corner of 500 South and 500 East. This property is currently zoned Residential Office (RO) and is proposed to be rezoned to Moderate/High Density Multi-Family Residential (RMF-45).

It is not anticipated that any negative impact to these existing landmark sites will occur as a result of the proposed TSA zoning designations.

Findings

21A.50.050 Standards for general amendments.

A decision to amend the text of this title or the zoning map by general amendment is a matter committed to the legislative discretion of the city council and is not controlled by any one standard.

A. In making its decision concerning a proposed text amendment, the city council should consider the following factors:

- 1. Whether a proposed text amendment is consistent with the purposes, goals, objectives, and policies of the city as stated through its various adopted planning documents;**

Analysis: In reviewing the station area plans, several adopted master plans were considered, including the Urban Design Element, the Salt Lake Futures Commission Report, the Central Community Master Plan, and the Wasatch Choices 2040 Plan. The City's adopted Housing Plan and Transportation Plan also call for the type of development supported in the station area plans. The analysis of the station area plans indicated that they were generally consistent with these plans or explained a change in policy to those plans.

Finding: The proposed zoning text changes are consistent with the goals and policies identified in the companion station area plans and several other adopted master plans.

2. Whether a proposed text amendment furthers the specific purpose statements of the zoning ordinance;

Analysis: The proposed changes enhance an existing chapter of the zoning ordinance, with a specific purpose statement. The general purpose statement of the zoning ordinance is to promote the health, safety, morals, convenience, order, prosperity and welfare of the present and future inhabitants of the City. In addition, the zoning ordinance is intended to lessen congestion in the streets, secure safety from fire and other dangers, provide adequate light and air, classify land uses and distribute land development and utilization, protect the tax base, secure economy in government expenditures, foster the City's industrial, business and residential development and protect the environment.

Finding: The proposed zoning ordinance furthers the specific purpose statements of the zoning ordinance.

3. Whether a proposed text amendment is consistent with the purposes and provisions of any applicable overlay zoning districts which may impose additional standards; and

Analysis: The existing zoning ordinance includes a statement that any applicable overlay zone supersedes the standards in the proposed ordinance. This section is not proposed to be modified as part of this petition. This is particularly relevant to the Central City Historic District, which includes 400 South between 500 and 700 East. Although the proposal includes changing the base zoning from TC-75 and CS in this area, the properties within the Historic District would still be subject to the H Historic Overlay regulations.

Finding: The existing zoning ordinance is consistent with the purposes of any applicable overlay zoning district.

4. The extent to which a proposed text amendment implements best current, professional practices of urban planning and design.

Analysis: The proposed changes continue to represent a new approach to zoning for Salt Lake City. This approach recognizes the value and importance of community input, the needs of developers and establishes an opportunity for the City, through private investment and development, to promote sustainable development practices, increase the housing stock, promote the business community, increase the use of alternative forms of transportation and improve public spaces.

Finding: The proposed changes continue to show how Salt Lake City is one of the few cities in the nation to implement this type of zoning, rather than the traditional Euclidean zoning that is widely used.

B. In making a decision to amend the zoning map, the city council should consider the following factors

- 1. Whether a proposed map amendment is consistent with the purposes, goals, objectives, and policies of the City as stated through its various adopted planning documents;**

Analysis: In reviewing the proposed zoning map changes, several adopted master plans were considered, including the Urban Design Element, the Salt Lake Futures Commission Report, the Central Community Master Plan, and the Wasatch Choices 2040 Plan. The City's adopted Housing Plan and Transportation Plan also call for the type of development supported in the station area plans. The analysis of the station area plans indicated that they were generally consistent with these plans or explained a change in policy to those plans.

Finding: The proposed zoning map amendments are consistent with the goals and policies identified in the station area plans and several other adopted master plans.

- 2. Whether a proposed map amendment furthers the specific purpose statements of the zoning ordinance;**

Analysis: The proposed zoning map amendment includes provisions for reducing the impact new development may have on existing areas. The boundaries of the proposed zoning district correspond to the boundaries in the station area plans. The station area plans identify the vision for what the areas around the transit stations should look like, how they work, what types of uses there are, etc. The existing character of the subject areas differs from what is identified in the long term vision for the area. Therefore, the important aspect to consider is the impact on those areas that are adjacent to the proposed zoning district boundaries.

Finding: The proposed zoning map amendments further the specific purpose statements of the zoning ordinance.

- 3. The extent to which a proposed map amendment will affect adjacent properties;**

Analysis: The proposed amendment would affect those properties that are within the boundaries of the TC-75, CS, CC and RMF-35 zoning districts by rezoning some of these properties to TSA-UC and TSA-UN. The proposed zoning district would decrease some of the development potential of some properties and for others only increase it slightly. However, this should not be viewed as an adverse impact because the proposed regulations that allow similar or decreased scale development are consistent with what was identified through the public planning process as desirable development. As properties redevelop, there will be instances where a new project is considerable larger than what may be adjacent to

it. The adverse impacts are more relevant where the proposed zoning district is adjacent to an area that will not be rezoned and has smaller mass and scale regulations than the proposed ordinance. The proposed ordinance contains provisions to reduce the impacts in these situations, such as increased setbacks than what currently exists, stepping of certain setbacks as the building height increases and more design standards than the current zoning requires. The intent of the proposal is to allow more building density and intensity along 400 South and step that density and intensity down as one moves closer to lower density residentially zoned areas.

Finding: The proposed zoning map amendment will have a minimal affect on adjacent properties due to the proposed zoning district containing provisions to reduce to impacts of the scale and mass of potential adjacent development.

4. **Whether a proposed map amendment is consistent with the purposes and provisions of any applicable overlay zoning districts which may impose additional standards; and**

Analysis: The proposed zoning map amendment is consistent with the purposes of any applicable overlay zoning district. The proposed zoning ordinance includes a statement that any applicable overlay zone supersedes the standards in the proposed ordinance. The Historic Preservation Overlay District is the only district within the boundaries of the proposed map amendments. Because the H Historic Preservation Overlay District applies, the Historic Landmark Commission has the authority to approve the overall design of new construction and major additions when they occur between 500 East and 700 East and they have the authority to modify certain bulk and mass regulations to insure that the proposals do not negatively impact the historic character of the Historic District.

Finding: The proposed map amendments will be consistent with any existing or future overlay district within the boundaries of the changes.

5. The adequacy of public facilities and services intended to serve the subject property, including but not limited to roadways, parks and recreational facilities, police and fire protection, schools, stormwater drainage systems, water supplies, and wastewater and refuse collection.

Analysis: The project area is located within areas that are already served by public facilities and services. However, the proposed ordinance does increase the development potential of the area in some instances and decreases it others. Population, employment and household projections for the corridor indicate an increase in all three categories. These projections were done under the current zoning regulations. The capacity of the road is not anticipated to be greatly impacted, at least initially, due to the change in zoning. The desired type of development and the development promoted by the proposed ordinance is considered transit oriented development, which can reduce the need to use private

automobiles. Other measures in the ordinance create incentives, primarily through a quicker review process, for other measures that reduce the energy use of new buildings, storm water runoff, and other sustainable measures. The proposed ordinance has been routed to other Departments and Divisions for comments. No comments were received that would indicate that the City would not be able to serve new development.

Finding: There appear to be adequate facilities in place to serve the boundaries of the proposed project.

Project Analysis

The proposed 400 South Livable Communities Station Area Plans project is a reflection of the community's vision for 400 South. The creation of the plan was done with the intent of incorporating major themes identified through the public process. Once these items were identified, a series of best practices that were applicable to the community's vision were incorporated into the plan to guide future development in a manner that can help turn the community vision into reality.

After analyzing the comments from the community, the desire for a different type of development along 400 South eliminated the option to make no changes. If the proposed station area plans are not adopted, the existing policies and regulations would remain in effect. Community input and existing conditions indicate that there are unique situation and characteristics area each station and that a one size fits all approach could not capitalize on the unique assets at each station. Making limited changes near the station areas would not provide enough land area to accommodate future projected growth.

Therefore, based on the information provided in this staff report, it is the Planning Staff's opinion that overall the project generally meets the applicable standards of the Zoning Ordinance and therefore, recommends the Historic Landmark Commission provide a favorable recommendation on this project.

**Excerpt of
SALT LAKE CITY
HISTORIC LANDMARK COMMISSION
Minutes of the Meeting
Room 326, 451 South State Street
May 17, 2012**

This document along with the digital recording constitute the official minutes of the Historic Landmark Commission regular session meeting held on May 17, 2012.

Historic Landmark Commission Meetings are televised on SLCTV 17. Archived video of this meeting can be found at the following link under, "Historic Landmark Commission and RDA": http://www.slctv.com/vid_demand.htm,

A regular meeting of the Historic Landmark Commission was called to order on Thursday, May 17, 2012, in Room 326 of the City and County Building, located at 451 South State Street, Salt Lake City, Utah, [5:33:28 PM](#) Commissioners present for the meeting were, Vice Chair Polly Hart, Earle Bevins III, Bill Davis, Arla Funk, Stephen James and Dave Richards. Chairperson Anne Oliver and Commissioner Sheleigh Harding were excused.

Planning Staff members present for the meeting were Cheri Coffey, Assistant Planning Director; Joel Paterson, Planning Manager; Thomas Irvin, Principal Planner; Maryann Pickering, Principal Planner; Michaela Oktay, Principal Planner; and Michelle Moeller, Senior Secretary. City Attorney Paul Neilson was also present.

FIELD TRIP [5:33:39 PM](#)

The Commissioner visited the following properties:

153 North C Street- Suggested 36" railing. Mr. Irvin explained the Applicant's optimal location for the fence. He explained the previous rail designs and Staff's recommendation with building code issues identified.

Commissioner Bevins asked for other examples or if a rail is allowed for Greek Revival.

Mr. Irvin had one example, where it doesn't block the front door. He said most porches on Greek Revival do not have railings characteristic to the style. Mr. Irvin stated the Applicant

had dogs and would like to have a gate for them.

400 South – Ms. Pickering explained the Rumel house and reviewed the discussions with Kirk Huffaker (UHF) regarding a preservation easement. She stated Mr. Huffaker agreed with the rezone and they decided that TSA was the best.

Landmark site R/O Peak House to RMU-45 biggest difference. You can't build a big office building. Commissioner Hart asked what the height in RMU-45 was. Ms. Pickering said it was 45 feet. Why not zone for another zone? Commissioner Hart said it encouraged a tear down and she discussed how churches like to tear things down. Want to keep Village Inn site and parking lot into TSA, only properties to rezone east of 900 East.

600 East Medians – Option C is the preferred choice.

[7:15:19 PM](#)

PLNPCM2010-00647 - 400 South Livable Communities Station Area Plans - A request by Salt Lake City Mayor Ralph Becker for an amendment to the Central Community Master Plan, Salt Lake City Zoning Ordinance and Zoning Map regarding transit station area plans along the 400 South transit corridor. The intent of the project is to implement livability goals for the corridor including land use policies and zoning that support mixed use development and transportation choices for current and future residents and workers. The Historic Landmark Commission will review and make a recommendation on the proposed zoning changes within the Central City Historic District. The project is located in City Council District 4 represented by Luke Garrott. (Staff contact: Maryann Pickering at 801-535-7660 or maryann.pickering@slcgov.com)

Ms. Maryann Pickering, Principal Planner, reviewed the petition as presented in the Staff Report (located in the Case File). She stated Staff recommended approval of the petition as presented.

The Commissioners discussed the color code on the maps and what was TSA and Historic Districts.

Commissioner James asked if the proposed setback was appropriate for the area. He stated it created gaps in the layout.

Ms. Pickering stated the proposal had been very well received by the public and explained the proposal was to help with the shadowing in the areas.

The Commissioners and Staff discussed the layout of the area, the current development and the maximum footprint that could occur with the transition. Staff explained any proposal in the Historic District would be reviewed prior to being approved.

The Commissioners and Staff discussed the step back design abutting residential areas that allowed for light, and where it applied in the proposed zone. Staff explained the area was specified within the Zoning Ordinance and was specifically in the RMF zoning district and some of the RMU to protect those areas. The Commissioners asked if there was a commercial building behind a larger building would the same standard apply. Staff stated it would not apply. It was stated that if the idea was to promote a transit orientated development then from the stand point of a developer there was less leasable space.

Commissioner James stated there needed to be continuity on the street with no gaps.

Vice Chairperson Hart asked why and when the Historic District line adjustments had been taken out of the proposal.

Ms. Pickering stated the reason for the removal of the boundary change language, was because the temporary ordinance had been adopted by the City Council. She explained Staff needed to take the proposal to the City Council for action within the next few months. She stated Staff understood the importance and sensitivity of the Historic District therefore, in order for the proposal to move forward the language was removed. Ms. Pickering stated as a result of the last meeting Staff discussed and determined that because the survey for the Central City Historic District was not thorough or proper it needed to be redone, after which the boundaries could be reevaluated.

Vice Chairperson Hart asked if the Administration planned on coming back and asking to remove that portion of the Historic District.

Ms. Pickering stated it was her understanding that there was grant money that would be used to conduct the survey.

Vice Chairperson Hart stated, regarding the Peck house and the Ward house, she could not understand why the Administration would want to rezone the house to RMF-45 if they thought there was no intention of tearing it down. She asked why it would be up zoned if it would not be allowed to be demolished.

Ms. Coffey explained the proposal was actually a down zone from RO to RMF-45.

Vice Chairperson Hart asked what RO allowed.

Ms. Coffey explained it allowed residential and offices up to sixty feet high.

Ms. Pickering stated the rezone would change the subject area to RMU-45. She reviewed the changes on the map and explained it was an older district where primarily offices were constructed. She explained there are always vacancies but because the offices would be allowed in the RMU Staff was trying to bring in the residential component because of the proximity to Trax.

Vice Chairperson Hart asked if there was a residential office zoning that was less than forty five feet or was that the minimum.

Ms. Coffey stated there was an RMU-35.

Vice Chairperson Hart asked why that was not chosen for the property.

Ms. Pickering stated Staff felt the 45 was more comparable to the transition zoning and because there is core right next to it making it more compatible.

The Commission discussed the zoning for the area and why it would not be good for the historic district. They stated it made sense to them to take it to the lowest zoning that still allowed for it to retain the commercial use.

Staff stated the structure had been used for offices in the past and was currently being used as an office.

The Commissioners asked if the Peck house property was part of the land that would be removed from the Historic District.

Ms. Pickering stated the Peck House was not currently within the Historic District it was registered as a City Landmark site.

Vice Chairperson asked why not rezone the ward house to what it is.

Ms. Coffee Stated it was zoned institutional. She stated places of worship are allowed in

residential zones and the City did not usually zone properties institutional because it could be a fairly intense zone unless it was four acres or more. She explained the institutional zoning was more for hospitals, or places like the LDS Temple and the Cathedral of the Madeline. Ms. Coffey stated places for worship were allowed in many residential zones.

Vice Chairperson stated correct and her concern was not about allowing it to be used as an institution but was that the current structure could be demolished and something constructed under what the zoning allowed.

Ms. Coffey stated if it was rezoned to institutional it could become a much larger structure than what the current zoning allowed.

Commissioner Funk asked why Staff was not considering Historic Properties out of the Central City Historic District that were affected by the changes. She stated the proposal extends to 900 East and there were historic properties in the area that would be affected. Commissioner Funk stated she felt those properties needed to be addressed before the proposal was put in place. She asked if Gilgal gardens were within the proposed area.

Ms. Pickering stated Staff was proposing to rezone the garden to open space because it had residential zoning designation. She stated with the rezone to open space the garden would not be allowed to be developed.

Commissioner Funk stated she was concerned with backing the TOD zone to the property line of the garden.

Ms. Pickering stated any development around the open space would be required to have the proposed setbacks protecting the open space areas.

The Commissioners and Staff discussed the open space areas and the shadow lines that became a problem in the garden areas.

The Commissioners and Staff discussed the point system as outlined in the proposed development guidelines. They discussed the pros and cons of the new point system and how it would improve the overall process. The Commissioners and Staff discussed what other cities have done and how the process was reviewed. They reviewed the parking requirements as outlined in the proposal. Staff explained a future ordinance would address parking and outline the limits to parking in the proposed zone. They reviewed where parking was required and not required.

PUBLIC HEARING [7:53:42 PM](#)

Vice Chairperson Hart opened the Public Hearing.

Ms. Judi Short, Resident, stated she was generally in favor of the proposal. She stated she agreed with conducting a survey and preserving the history of the area. Ms. Short spoke of the issues surrounding Gilgal Garden and what would happen if larger building were built around the open space. She asked the Commission to protect the historic fabric in the area.

Ms. Cindy Cromer, Resident, stated there were national register structures in the area as well as the Central City Historic District. She stated her suggestion would be to allow the transfer of development rights within a site. Ms. Cromer explained how that would work and explained there should be more points allotted for historic preservation as it was important for the City. She stated the Commission needed to address the point system before approving the proposal.

Mr. Robert Bliss, Resident, stated Gilgal Garden was a city park and it needed to be treated as such. He stated there was an importance in keeping open spaces as they were intended to be kept. Mr. Bliss stated he was disappointed in the architecture of the proposal and suggested having design competitions to help improve the quality of work done in Salt Lake City.

Ms. Pickering stated Ms. Short was correct in referring to the shadowing at Gilgal Garden but Staff felt the proposal adequately addressed the issue. She explained the problems with TDR and that they are typically used for agriculture land.

Commissioner Richards asked if there had been discussion regarding increasing setbacks around park areas.

Ms. Pickering explained the changes that lead up to adding open space in the current proposal.

The Commissioners and Staff discussed the open space and the amount of historic properties affected by the TSA zoning. Staff stated, currently there were three or four historic properties in the area however, the survey needed to be complete for a clearer picture. The Commission asked if there was a way to require development around and in the historic area to be reviewed by the Historic Landmark Commission in order to get the points. Staff explained regardless of the point system, if a project of any significance was to be constructed in the Historic District it would be reviewed by the Historic Landmark Commission.

The Commissioners stated they were concerned that the Mayor would ask for the current Historic District to be removed in the next few months. They stated they wanted to ensure the Historic Landmark Commission had purview over any development surrounding a historic site. Staff stated they would look into whether or not that was a possibility and return with the information.

Vice Chairperson Hart closed the Public Hearing

COMMISSIONERS COMMENTS [8:09:57 PM](#)

The Commissioners made the following comments.

- The proposal would take away the historic urbanism of the area.
- Taking away the fabric of the area would be detrimental.
- A uniform development plan was needed to keep people thinking about what made the area great to start with.
- Open space and set backs are necessary to protect the gardens and parks that are in place.
- Should not let the Historic District be removed.
- TSA zoning will impact the remaining Historic District negatively.
- The center of the Central City Historic District does not have contributing structures.
- New point system has potential but needs to be fine tuned.
- Intention of the proposal was good but needed to be refined.

The Commissioners discussed the effects of the TSA zoning on the Historic Landmark Commissions' authority within the existing Historic District. Staff explained it did not change the overall authority of the Historic Landmark Commission or the requirement for Applicants to acquire approval for projects in the Historic District.

Vice Chairperson Hart stated she would like to know who was in favor of the petition and who was not in favor prior to a motion being made.

Commissioner James asked if the desired outcome was consistent and if the center part of the Historic District was not really relevant why the Historic Landmark Commission would worry about the development in the center of the Historic District.

Vice Chairperson Hart stated because the Commission should be commenting on how it was going to effect the existing Historic resources. She stated she did not think the Commission needed to be bothered with the rest of the 400 South zoning because it was not within the Commissions purview. Vice Chairperson Hart explained the TSA zoning would be the base zoning and the Historic District would be the overlay.

Commissioner James asked why the historic boundary review was being taken out of the proposal.

Mr. Paterson stated it was not being taken out of the proposal. He stated what the Commission was indicating was that at some point the City may look at modifying the Central City Historic District boundaries, which could take a portion of the corridor out of the Historic District. Mr. Paterson stated in the corridor that was proposed for TSA zoning, that goes through the Central City Historic District, basically between 500 South and 300 South and within those four blocks of the Historic District there are three of four contributing buildings combined.

Vice Chairperson Hart asked if all new construction would come to The Landmark Commission.

Mr. Paterson stated yes all new construction, all external modification required a certificate of appropriateness from Staff or to come to the Historic Landmark Commission for approval. He stated the City hears a number of complaints from business owners and property owners along 400 South regarding new development and that it is not historical in nature.

Commissioner Funk asked if rezoning could be approved without Historic Landmark Commission approval.

Mr. Paterson stated any boundary change or modification to the Historic District required the recommendation of the Historic Landmark Commission. He stated the subject proposal did not contain language regarding changing the historic district therefore, the base zone was being considered and brought to the HLC as a courtesy so Staff could gather comments on the base zoning and its impact on historic resources in the Historic District. Mr. Paterson stated the Historic Landmark Commission was not required to review the proposal however, by state law the Planning Commission was.

The Commission discussed who was ready to make a motion and who felt they had all the information they needed to make an appropriate recommendation on the proposal.

MOTION [8:27:58 PM](#)

Commissioner James stated in regard to PLNPCM2010-00647, he moved that the Historic Landmark Commission forward a positive recommendation to the Planning Commission for the 400 South Livable Communities Master Plan and to encourage the Landmarks Commission to participate in the developments along the corridor in order to reinforce and strengthen the historic links that are currently between the northern and southern ends of the Central City Historic District

Mr. Paul Neilson, City Attorney, asked for clarification on what was meant by the Landmark Commission participation.

Commissioner James stated through public process and review.

Mr. Neilson clarified maintaining the areas currently within the Historic District and preserving those with the H overlay district.

Commissioner James stated yes, so they are able to maintain traditional urbanism through the corridor.

Commissioner Bevins seconded the motion.

Commissioner James stated the Commission was fully anticipating participating in the review of new development through the corridor in order to strengthen the link between the north and south ends of the Central City Historic District.

Vice Chairperson Hart stated she thought what Commissioner James was saying was that he would like to maintain the current Historic District Boundaries.

Commissioner James stated that was correct and asked if the motion needed to be amended.

Mr. Neilson stated he felt Staff understood what was being asked and could forward the information.

The Commissioners discussed the lack of historic structures in the center of the Historic District and that the motion was in a sense pretending that there were historic materials present. They clarified that the motion was for a favorable recommendation for the TSA zone. The Commissioners discussed the pros and cons of the TSA zoning within the Historic District and how it would affect the future of the area.

[8:38:26 PM](#)

The motion passed 3-2.

Vice Chairperson Hart read the appeal process

MOTION

Commissioner Funk moved in the case of PLNHLC2010-00674 that the Historic Landmark Commission recommend, specifically, that the City implement the survey to determine significant historic sites within the TSA zone and protect those individual historic sites with historic value and that the Historic Landmark Commission be given the purview to review the immediate surrounding developments. Commissioner James seconded the motion.

Vice Chairperson Hart asked Commissioner Funk if she wished to included both A and B buildings.

Commissioner Funk stated yes both A and B buildings should be included.

Commissioner Davis questioned the validity of the motion since the Historic Landmark Commission all ready has purview over the area.

Commissioner Funk stated the point was that the Historic District may be removed and with that the historic buildings would go away.

Vice Chairperson Hart suggested amending the motion to include the language "should the district boundaries be changed".

Mr. Paterson stated it was not Staff's recommendation to eliminate the Historic District but to conduct a survey and consider the results prior to deciding what to do with the District.

Commissioner Funk stated that was understood but the properties needed to be protected.

[8:45:48 PM](#)

The motion passed unanimously.

The meeting stood adjourned at [8:45:58 PM](#)

**7.A. PLANNING COMMISSION
ORIGINAL HEARING NOTICE**

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P.O. BOX 704005
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PLANNING DIVISION, 451 SOUTH STATE STREET, ROOM 4 SALT LAKE CITY UT 84111	9001394298	4/16/2012

ACCOUNT NAME	
PLANNING DIVISION,	
TELEPHONE	ADORDER# / INVOICE NUMBER
8015357759	0000783617 /
SCHEDULE	
Start 04/13/2012	End 04/13/2012
CUST. REF. NO.	
Public Hearing	
CAPTION	
NOTICE OF PUBLIC HEARING On April 25, 2012, the Salt Lake City Planning Comm	
SIZE	
50 Lines	2.00 COLUMN
TIMES	RATE
3	
MISC. CHARGES	AD CHARGES
TOTAL COST	
130.00	

NOTICE OF PUBLIC HEARING

On April 25, 2012, the Salt Lake City Planning Commission will hold a public hearing to consider making recommendations to the City Council regarding the following petitions:

PLNPCM2011-00451 Localized Alternative Sign Overlay District at Library Square - A request by Mayor Ralph Becker to analyze the appropriateness of amending the Salt Lake City Zoning Ordinance to create a localized alternative sign overlay district at Library Square. This proposed overlay district would allow for more signage and different types of signage than is currently allowed in the PL-2 Public Lands District. Library Square encompasses the entire city block between 400 South and 500 South and 200 East and 300 East and is located in City Council District 4, represented by Luke Garrott. (Staff contact: John Anderson at 801-535-7214 or john.anderson@slcgov.com)

PLNPCM2011-00472 Historic District Design Guidelines for Signs - A request by Mayor Ralph Becker to create historic district design guidelines relating to signs. Guidelines will be for commercial signs located in an area with a local historic designation. Policies, definitions and guidelines for maintaining existing signs and new construction will be included in the document. No sections of the Zoning Ordinance will be modified or affected by this petition. (Staff contact: Ray Milliner, 801-535-7645 ray.milliner@slcgov.com)

PLNPCM2010-00647 - 400 South Livable Communities Station Area Plans - A request by Salt Lake City Mayor Ralph Becker for an amendment to the Central Community Master Plan, Salt Lake City Zoning Ordinance and Zoning Map regarding transit station area plans along the 400 South transit corridor. The intent of the project is to implement livability goals for the corridor including land use policies and zoning that support mixed use development and transportation choices for current and future residents and workers. The project is located in City Council District 4 represented by Luke Garrott. (Staff contact: Maryann Pickering at 801-535-7660 or maryann.pickering@slcgov.com)

The public hearing will begin at 5:30 p.m. in Room 326 of the City & County Building, 451 South State Street, Salt Lake City, UT. For more information or for special ADA accommodations, which may include, alternate formats, interpreters, and other auxiliary aids or additional information, please contact Michael Stoff at 801-535-7976 or call TDD 801-535-6220.
783617

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AS NEWSPAPER AGENCY COMPANY, LLC dba MEDIAONE OF UTAH LEGAL BOOKER, I CERTIFY THAT THE ATTACHED ADVERTISEMENT OF **NOTICE OF PUBLIC HEARING On April 25, 2012, the Salt Lake City Planning Commission will hold a public hearing to consider making recommendations to the City Co** FOR **PLANNING DIVISION**, WAS PUBLISHED BY THE NEWSPAPER AGENCY COMPANY, LLC dba MEDIAONE OF UTAH, AGENT FOR THE SALT LAKE TRIBUNE AND DESERET NEWS, DAILY NEWSPAPERS PRINTED IN THE ENGLISH LANGUAGE WITH GENERAL CIRCULATION IN UTAH, AND PUBLISHED IN SALT LAKE CITY, SALT LAKE COUNTY IN THE STATE OF UTAH. NOTICE IS ALSO POSTED ON UTAHLEGALS.COM ON THE SAME DAY AS THE FIRST NEWSPAPER PUBLICATION DATE AND REMAINS ON UTAHLEGALS.COM INDEFINATELY.

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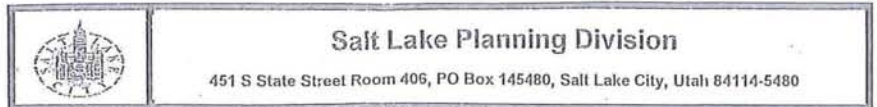


SALT LAKE CITY PLANNING DIVISION
 451 S STATE STREET ROOM 406
 PO BOX 145480
 SALT LAKE CITY UT 84114-5480

RETURN SERVICE REQUESTED



Maryann Pickering - Salt Lake City Planning Division
 PO Box 145480
 Salt Lake City, Utah 84114-5480



Salt Lake Planning Division

451 S State Street Room 406, PO Box 145480, Salt Lake City, Utah 84114-5480

Planning Commission

Wednesday, May 23, 2012 at 5:30 p.m.

City & County Building at 451 S State Street in Room 326

PLNPCM2010-00647 - 400 South Livable Communities Station Area Plans - A request by Salt Lake City Mayor Ralph Becker for an amendment to the Central Community Master Plan, Salt Lake City Zoning Ordinance and Zoning Map regarding transit station area plans along the 400 South transit corridor. The intent of the project is to implement livability goals for the corridor including land use policies and zoning that support mixed use development and transportation choices for current and future residents and workers. The project is located in City Council District 4 represented by Luke Garrott. (Staff contact: Maryann Pickering at 801-535-7660 or maryann.pickering@slcgov.com)

Salt Lake City Corporation complies with all ADA guidelines. People with disabilities may make requests for reasonable accommodations no later than 48 hours in advance in order to attend this meeting. Accommodations may include: alternative formats, interpreters, and other auxiliary aids. This is an accessible facility. For questions requests, or additional information, please contact the Planning Office at 535-7757; TDD 535-6220.

For additional meeting guidelines please see www.slcgov.com or call 801-535-7757

**7.B. PLANNING COMMISSION
MARCH 28, 2012 BRIEFING**



Briefing for Planning Commission

**Planning Division
Community & Economic Development Department**

To: Planning Commission
From: Maryann Pickering, Principal Planner
Date: March 28, 2012
Re: 400 South Livable Communities Update

On March 28, Planning Division staff will provide a briefing for the Planning Commission on the current status of the 400 South Livable Communities project. Staff will present a summary of the public outreach that has occurred to this point and will also provide a summary of proposed changes to both the Central Community Master Plan and the Transit Station Area (TSA) chapter of the Zoning Ordinance.

Background

In the fall of 2010, the Community & Economic Development Department was awarded a matching grant from the Department of Housing and Urban Development (HUD) to undertake a rezoning process for a portion of the University TRAX line. The amount of the grant from HUD was \$22,620 with a commitment from the city to provide an additional \$10,000 in the form of staff resources for the project.

Starting in March of 2011, Planning Division staff began working on a master plan amendment and rezoning project for the University TRAX line. Three stations were identified to be included as part of the rezoning process. Those stations are: Library, Trolley and 900 East. The goal of the proposed project is to create station area plans and zoning regulations for the three stops that:

- a. Provide more transportation choices.
- b. Promote equitable, affordable housing.
- c. Enhance economic competitiveness.
- d. Support existing communities.
- e. Coordinate policies and leverage investments.
- f. Value communities and neighborhoods.

In addition to the above, this project assists in the implementation of the Wasatch Choices 2040 Plan, which is a regional plan created by residents, businesses, and other stakeholders to manage future growth in the region by focusing it on certain nodes, particularly those where transit facilities already are in place. Further, Planning Division staff felt that this area needed to be studied for a potential change at the current zoning because despite the current zoning of TC-75, very little private investment has been made in the area since the TC-75 zoning was created in 2005.

Public Outreach

When the grant application was submitted to HUD, one of the project outcomes was to increase participation in the public planning process for this project. A goal of reaching at least 60 individuals (or approximately 30% of the total participants) from traditionally under-represented groups that do not normally participate in the planning process was included. Through all the different types of outreach completed this goal has been met and has been exceeded in terms of the number of participants.

The first public workshop in the project was held on May 19, 2011. Staff sent notices of the workshop to all property owners within a half mile radius of each TRAX station. The purpose of this workshop was to hear from the residents and property owners within this area to find out what they like about the area, what works, what doesn't work and what they would like to see different.

Over the summer months of 2011, members of the project team met with various local community groups and developers to get their feedback on the area. Similar to the May workshop, we wanted to know what was currently working in the area and what needed to be changed. A summary document was prepared that included all the comments that were received at the May workshop and from the meetings that were held in the summer. A copy of that report can be found in Attachment A.

An additional public workshop was held in October of 2011. The purpose of this meeting was to present to the public some of the information that had been gathered since the process began. There was not a lot of feedback received from this meeting.

The next series of meetings were public workshops held in February and March of 2012. Notices were mailed to all property owners whose property was proposed to be changed and all those located within 300 feet of those properties. Workshops were held at Trolley Square and the main City library. There were several people who participated as part of this process and the level of feedback has been high. The feedback is currently being compiled and input into the summary report so we have a complete record of our outreach efforts.

We have also attended the meeting of the applicable community councils whose boundaries include the station areas. An initial meeting was held with each group when the process started and an additional meeting was attended within the last month.

Public Outreach Feedback

Overall, the response to the project has been positive. There have been a few negative comments received at this point, but staff feels that with some modifications, additional education or materials,

most of those concerns can be reduced or alleviated. We do recognize that this is a large master plan amendment and rezoning project and those who own property will have varied opinions on the best way to go about this process or if it is even necessary. The Planning Division has offered to meet individually with any stakeholder who may have issues with the proposal and have done so throughout the process.

Proposed Central Community Master Plan Changes

The proposed changes to the Central Community Master Plan as part of this project can be found within both the ‘Transit Oriented Development (TOD)’ and the ‘Central Community Master Plan Committee Goals and Recommendations’ sections of the plan.

The existing Central Community Master Plan was adopted in 2005. Some of the policies within the TOD section were written in anticipation of the North Temple TRAX line and the expansion to the airport. Staff has updated this section to reflect the current development and conditions along the North Temple TRAX line and also added a section specific to the 400 South Livable Communities project.

In addition to updating the TOD section, some new goals and recommendations that are specific to the 400 South Livable Communities project have been included. The new ones are categorized by each of the three station areas. Some of the goals and recommendations are the same for each of the stations areas and some are unique to each area. It has been organized in this manner for each of implementation by the Planning staff and so that the public can easily see what policies are applicable to area that they are interested in.

Attachment C is an excerpt of the existing Central Community Master Plan with all of proposed changes noted above.

Proposed Zoning Changes

There are two types of zoning changes proposed as part of this project. One is to modify the existing Transit Station Area (TSA) zoning district and the other is to make zoning map amendments in order to implement this project.

Changes to the TSA zoning district as a result of this project are minimal. The TSA zoning district was adopted a few years ago as a result of the work done on the North Temple TRAX line project and the rezoning process that was part of that project. A series of different types of station areas were created and zoning regulations were put in place for each of those station areas in the North Temple area. The 400 South project and station area plans are being modeled after the existing categories and zoning regulations that are currently in place for the North Temple area. This is being done so that there is consistency between the regulations that are in place for the transit corridors throughout the city. If approved, this will be the second area in the City subject to the TSA regulations.

There have been approximately five projects reviewed for the North Temple area with the current TSA standards and for process is working well, though a few modifications are necessary. A few of the standards that currently exist need to be clarified now that staff has had the opportunity to review

projects based on those standards. We will be making those modifications are also adding some standards that are specific to the 400 South corridor.

- In addition to changing some of the standards in the TSA zoning district, staff is also proposing to make some modifications to the Residential Mixed Use (RMU) zoning districts. The two districts proposed to be modified are RMU-35 and RMU-45. A summary of the proposed changes are as follows:
- Increase maximum allowable height five feet in both zones. Traditionally, mixed-use buildings are designed with a distinct base, which is larger and designed to accommodate commercial or office uses, a middle which is generally residential in nature, and a top, which is also residential but designed with architecturally appealing features such as parapets and cornices. In order to ensure proper scale between the first floor and the upper floors, staff is proposing that the first floor ceiling height of a mixed use building be at least 14 feet. This will ensure that retail/commercial space on the first floor is properly designed to accommodate uses that serve the community.
- Elimination of minimum lot area requirement for multi-family dwellings. This provision will promote the development of smaller lots that may accommodate appropriately scaled development but do not meet the minimum lot size requirements.
- Elimination of qualifying provisions for multi-family units as it relates to density calculations, (i.e., 9,000 square feet of lot area for first three units and 1,000 square feet for each additional unit). Development constraints would be primarily based on the petitioner's ability to meet maximum height limits, setbacks, and parking.
- Creation of building design standards. These standards are based on the Small Neighborhood Business standards created by the Planning Commission last August. Their purpose is to promote human scaled massing, varied articulation and traditional façade design.
- Creation of site specific standards for 500 South corridor between State Street and 700 East. These standards are intended to maintain the existing pedestrian/low impact design of the street while allowing the increased density that would come with the RMU zones.

In addition to text changes noted above, this project is also a zoning map amendment to change some of the zoning designations within the 400 South corridor. Most the changes shown in the dark purple, light purple and blue colors are changes that would rezone these properties to the different TSA zoning district categories. You will see that most of these changes are directly along the corridor as we feel this is the best location for more intense development with the intensity lessening on the north and south sides of the corridor.

The balance of the zoning changes (those shown in a yellow hue) are proposed to be an RMU zoning district. After an analysis by staff, it was determined that the RMU categories would be more appropriate for the properties identified than their current zoning designation.

One item we would like the Planning Commission to discuss during this briefing is the parking lot located to the south of Trolley Square and currently connected by the sky bridge. This lot is currently zoned RMF-45 (Moderate/High Density Multi-Family Residential District). The parking lot is a non-conforming use in this zoning district. In the future, an expansion of the parking lot may be necessary, but it could not be done with the current zoning designation of RMF-45. Staff has proposed to rezone this property as part of this project in order to allow a mixed use development, including an expansion of the existing parking lot. There has been some concerns expressed about changing this zoning and staff would like to have a discussion about possible options or other zoning designations for this site. If after the discussion it is determined that the best course of action would be to leave the zoning designation as it is, we will remove this area from the proposed zoning map amendments.

Central City Historic District Boundaries

As part of this master plan amendment and rezoning process, staff is recommending that the boundaries of the Central City Historic District be modified. An analysis has been completed that shows within the blocks on the north and south side of 400 South between 500 East and 700 East, there are few historic resources remaining and therefore the Historic Overlay may not be the most appropriate tool to regulate new development on these blocks. The entire two blocks bounded by 400 and 500 South between 500 and 700 East are proposed to be removed. The two blocks between 300 and 400 South and 500 and 700 East will be partially removed. We have found that there are some historic resources along the 300 South frontage that would be better served remaining in the historic district.

The 600 East landscaped median are an important element of the historic district and need to be preserved. Policies will be included within the Central Community Master Plan that preserve these medians in their current state and minimize future impacts from development. This means that breaks in the median to accommodate turn around areas for vehicular traffic will be discouraged. We will also include policies that the sidewalks and other areas of the public right-of-way remain in the historic district so that the City can maintain design control of features (streetlights, seating, etc.) that is located within the right-of-way. Please see Attachment B which contains a map of the modified historic district.

Prior to a public hearing on this master plan amendment and rezoning petition and the subsequent change of the boundaries of the historic district, staff will hold a briefing and a public hearing with the Historic Landmark Commission for their input and direction. We expect the briefing to take place in April and a public hearing in May.

Discussion for Briefing

Staff would like to have a discussion at the briefing this evening at the briefing:

1. Changes to RMU zoning district.
2. Potential rezoning of the parking lot south of Trolley Square.
3. 400 South project in general.

Next Steps

It is anticipated that after the direction from Historic Landmark Commission is received, we will begin the public hearing process for this petition. At this time, we expect that those hearings will begin in mid-May.

Attachments

- A. 400 South Livability Project – Public and Stakeholder Outreach Summary Report
- B. Map of Proposed Modifications of the Central City Historic District
- C. Draft Changes to an Excerpt of the Central Community Master Plan
- D. Library Station Summary Handout
- E. Trolley Station Summary Handout
- F. 900 Station Summary Handout
- G. Map of Proposed Zoning Changes

Attachment A
400 South Livability Project – Public and Stakeholder Outreach Summary Report

Salt Lake City Planning Division

400 South Livability Project

Public and Stakeholder Outreach



Project Manager

Maryann Pickering

Project Team

John Anderson

Doug Dansie

Ray Milliner

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Daunte Rushton

Nole Walkingshaw



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Salt Lake City Planning

400 South Livability Project Public and Stakeholder Outreach

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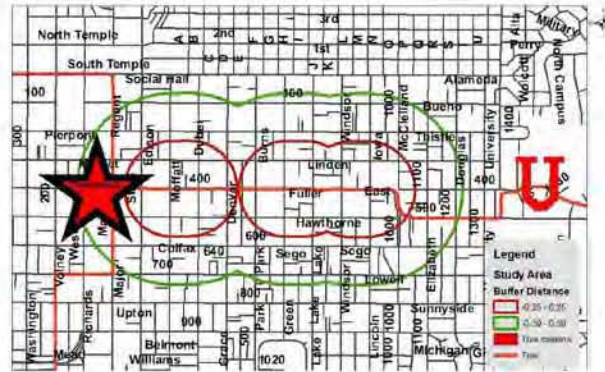


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Purpose of Meeting

The purpose of the May 19th Community Workshop was to identify those things about the 400 South corridor and adjacent neighborhoods that people liked, did not like and what they wanted to see happen in the future. For the Planning Division, this was an opportunity to listen to feedback and to facilitate discussions on a wide range of issues related to the corridor. For the public, it was an opportunity to discuss the issues with each other without being directed by the City.

This report is a summary of what was heard by the City. The comments from the meeting will be used, along with comments and input from future meetings, will be used to create guiding principles and proposed City policies to help guide future decision making.



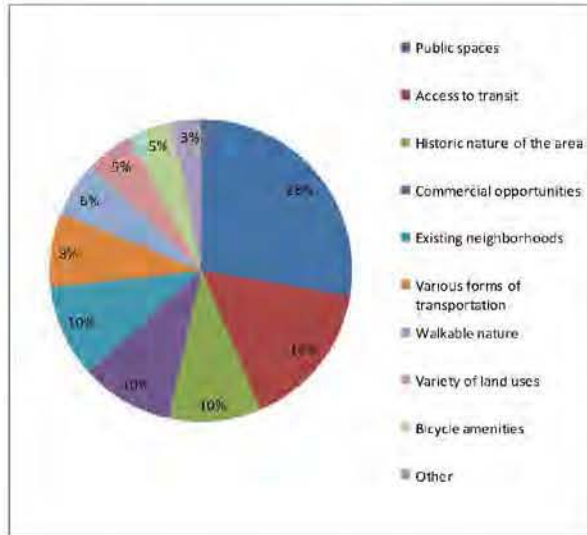
The 400 South Livability Project study area is generally the area within a 1/2 mile radius of each transit station, shown by the green line on the above map. The study area was established based on a number of studies and best practices from across the country that indicate light rail transit benefits are mostly found within a 1/2 mile walking distance of a transit station. Those areas that within 1/4 mile (the red circles on the above map) see the greatest benefits and the benefits decrease the further away one travels from a transit station.

The study area is being used in a number of ways. First, it is used to identify stakeholders. For the May 19 Workshop, all property owners and building tenants (both residential tenants and businesses) were sent notices of the workshops. Second, the study area will be used by the Planning Division to analyze development related issues based on feedback received during the May 19th workshop and other future workshops related to the project.

What the Community Likes

Those items that the community likes generally fit into the following categories:

- Public spaces, including parks, open space, landscaping, etc
- Access to transit
- Historic nature of the area
- The commercial opportunities in the area
- The existing neighborhoods
- The various forms of transportation that are available in the area
- The walkable nature of the area
- The variety of land uses in the area
- Bicycle amenities



The general categories of things that people like about the corridor, shown as a percentage of the total number of comments received.

Specific comments about what people like (comments are in no particular order)

Access to Transit

- Access to transit
- Proximity to transit
- Minimal distance between stations
- TRAX
- Frequency of Tax stops
- Transit access to larger region
- Likes no Tax parking
- Hybrid electric buses

Bicycle Amenities

- Bike lanes
- 800 East should be a bicycle street
- Bicycle amenities

Historic Nature of Area

- Encourage adaptive reuse of buildings
- Historic buildings
- Neighborhoods adjacent to 400 South
- Historic nature of neighborhoods
- Historic markers
- Exchange Place
- Washington Square

Public Spaces, Parks, Open Space, Landscaping

- Trees
- Green spaces
- 800 East medians
- Smaller parks, such as Fault Line, Senior Center, Gilgal
- Landscaping in Tax corridor
- 800 East medians
- Pedestrian friendly green buffers
- Washington Square and the festivals
- Public safety building
- Library
- Station art
- Street art
- Gallivan Center

Commercial

- Easy access to commercial spaces
- Trolley square renovation
- Fast food and drive thru restaurants
- Diverse nature of commercial
- 100 South commercial
- Mix of retail uses

Walkability

- Countdown clocks
- Urban feel of library station, lots of pedestrians
- Walkable neighborhoods
- Pedestrian amenities
- Sidewalks
- Crossing flags
- Locations where building are built up to the sidewalk
- Walkability

Neighborhoods

- Quiet, safe neighborhoods
- High density
- Buffers to neighborhood
- Diverse characteristics
- Existing mixed use neighborhoods
- Protection of single family neighborhoods
- Limited negative impacts from Tax on neighborhoods
- Integrated neighborhoods
- Mix of housing

Variety

- Density of uses
- Variety
- Ethnic/age mix
- Mix of uses
- University students

Transportation

- Uncongested streets
- U-Car share program
- A good travel way for cars between downtown, east bench with little impact to neighborhoods
- Access to university
- Driveable nature of corridor
- Regional location to mountains, recreation, etc.
- Access to downtown

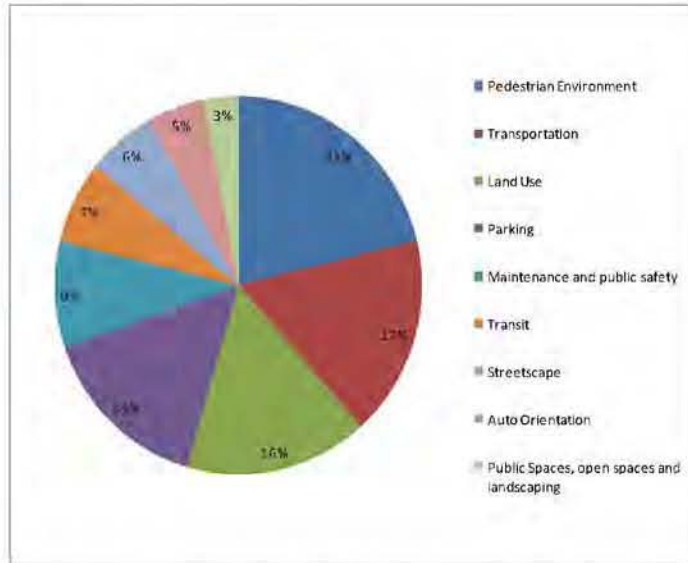
Miscellaneous

- SL Regional Hospital
- Cleanliness
- New architecture downtown
- Potential of area

What the Community Does Not Like

Those items that the community likes generally fit into the following categories:

- Pedestrian environment
- Transportation
- Land Use
- Parking
- Maintenance and public safety
- Transit
- Streetscape
- Auto orientation
- Public spaces, open spaces and landscaping



The general categories of things that people do not like about the corridor, shown as a percentage of the total number of comments received.

Specific comments about what people do not like (comments are in no particular order)

Pedestrian Environment

- Sidewalks are not wide enough or are in disrepair
- Unfriendly/unsafe to walk (cluttered, narrow, no buffer from traffic, blind driveways, etc)
- Limited pedestrian access to Tax is not conducive to walking and supports jaywalking
- Pedestrian conflicts with auto infrastructure (wide roads, driveways, right turns, etc.)
- Pedestrian walkways have no amenities (shade, benches, garbage cans, etc.)
- Mid block walkways are not safe or well taken care of
- Feel out of place walking on 400 South, it is not pedestrian friendly.

Parking

- Middle of the street parking
- Lack of pedestrian access through parking lots
- Tax riders parking in commercial lots
- Parking is a nightmare in the eastern neighborhoods
- Most annoying and arrogant thing the City says is you should not have any expectation that you can park in front of your house

Public Spaces, Open Spaces, and Landscaping

- Not enough open space, green space, community space, trees, landscaping

Auto Orientation

- Auto oriented uses/dependency
- Three lanes of auto traffic in each direction on 400 South
- Auto oriented destination

Transportation

- Traffic flow, congestion, speeding
- Cars using side streets to cut through busy intersections
- Stoplights are not synchronized
- Dangerous intersections (un-signalized crossing on 800 East, two way stop, etc.)
- 800 East medians block trash and truck access
- Traffic noise, especially from buses
- Lack of traffic calming
- More access to car share program
- U-Car share pays high taxes, should be exempt
- Difficult to ride bike on transit streets

Transit

- Bus fares are not conducive for short trips (too expensive, need distance faced fares, expand free fare zone, etc)
- Lack of bus service
- Bus infrastructure is not synced to Tax
- Tax
- Bus stops in residential areas have no amenities and often damage landscape from people waiting, littering, etc.
- Need mid block connection to Tax stations
- noise from buses and trains
- Some of the bus routes that support Tax are going away, limiting the effectiveness of Tax
- Bus stops and pull outs do not work well

Specific comments about what people do not like (comments are in no particular order)

Land Use

Public safety building should not be across from library
 Center of block development
 Not livable
 Circular nature of TOD, needs to be linear along corridor
 Lack of protection for single family neighborhoods
 400 South has not encouraged local business, not evolved organically
 Housing on 400 South is not compatible due to the noise
 Empty buildings
 More core services accessible
 Land uses need to relate better to the transportation infrastructure
 No incentives to encourage redevelopment in the right places
 Too many chains and fast food restaurants
 Woefully underused
 Loss of housing to other uses
 Conflicts between zoning and historic district overlay
 Redundant land uses (too many of one land use, like medical offices)
 Tax has not encouraged organic development, 400 South is still mostly big box, fast food and rundown offices
 Do not like high density in the middle of the blocks
 Many people living in high density still do not use TRAX, they drive
 Industry does not belong (Wonder Bread factory)

Streetscape

Lack of aesthetics
 400 south feels like anywhere else in America
 Needs to be beautified so it is more like a boulevard
 400 South is not transit oriented
 600 East between Trolley Square and the 600 East station
 Resembles State Street between 300 East and 500 East
 No recreational opportunities for kids, either indoor or outdoor.

Maintenance and Public Safety

Loud motorcycles
 Rental units are a business and need to be treated like one to address maintenance issues
 Cats and dogs roaming free
 Not enough yard waste containers
 Boarded houses
 Long term vacancies
 Absentee landlords, both residential and commercial
 Derelict properties
 Curbs not painted red in front of fire hydrants

Visions

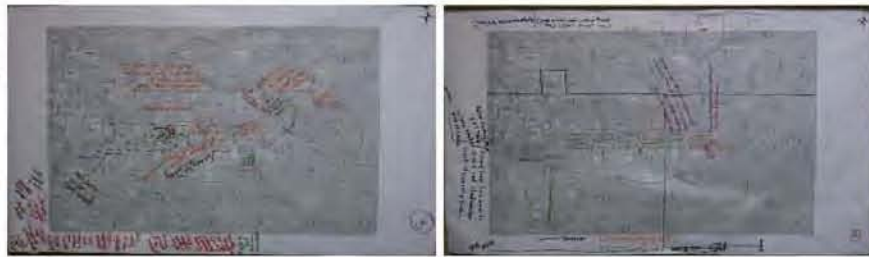
The community workshop provided some very helpful insight as to what the 400 South corridor should look like in the future. This is important because it helps the Planning Division continue to work with the community and stakeholders to craft a long term vision for the area and then create land use policies and development regulations to implement the vision. Most of the visions received during the workshop fall into these general categories:

- Land Use and Development
- Transportation
- Public Spaces
- Bicycles
- Walkability
- Neighborhood Character
- Policy

The visions were collected from the note cards handed out as people arrived, on the flip charts where the facilitators were capturing the discussions at each table and from the information written on the maps at each table.



Images of the maps from the May 19, 2011 Community Workshop



Staff Summary of Vision Comments

Below is a list of all of the vision comments received during the Workshop. Similar comments have been grouped into categories.

Land Use	Number of comments	Transportation	Number of comments
Higher densities closer to Downtown and the stations, with lower densities adjacent to existing neighborhoods.	21	Work with UTA to come up with alternate fare options for transit. Options may include an expanded free fare zone, distance base fares, etc.	2
Provide a wide range of housing options that complement the existing housing types and add new housing types to the area.	7	Provide more transit options that interface with the University TRAX line, such as future street car lines, bus routes, etc. Streets to consider include 600 East to Liberty Park, 900 East, and 200 South.	6
Develop design guidelines that bring buildings closer to the street, address building setbacks, parking locations, landscaping, green roofs and buildings, etc. to make a more pedestrian oriented community.	18	Add park and ride lots at 900 East and the Stadium Stations to serve the residential neighborhoods.	4
More mixed use in appropriate locations.	6	Use roundabouts, traffic lights and stop signs to improve safety at intersections located at 200 South 800 East, 300 South 800 East, 500 South 800 East and 600 East 600 South.	6
Ensure that zoning regulations support appropriate land use, design and scale for historic areas.	5	Synchronize traffic lights and adjust the traffic lights so they that switch to flashing red after midnight.	2
Create incentives for appropriate development and redevelopment of undesirable land uses (surface parking lots, drive-thru restaurants, etc).	18	Coordinate with the 200 South Project to create a multi-modal street along 200 South and use east-west connections to make connections to the University TRAX line.	2
Promote Trolley Square as a neighborhood anchor by providing more residential density and other supportive uses around it.	2	Provide funds to fix up interior court like public streets, such as Denver Street.	1
Promote more locally owned businesses along the corridor.	2	Extend the University TRAX line west on 400 South to connect to Salt Lake Central Station.	1
		Use visual enhancements to slow traffic.	1
		Eliminate one vehicle lane in each direction on 400 South and replace it with bike lanes and wider sidewalks.	1

Purpose of Stakeholder and Community Organization Meetings

The purpose of the meetings with stakeholders, was to learn about the future intentions of business and property owners, the potential of developed and undeveloped sites and how they envisioned the future land uses along the corridor.

Meetings were also held with various community organizations to learn more about how their groups and their members interact with the 400 South corridor, how the members of these groups are utilizing the corridor, their views of current and future transit options and land uses along the corridor and how we could find ways to include these groups and their individual members in future public meetings.

This report is a summary of what was heard by the City. The comments from the meetings will be used, along with comments and input from future meetings, will be used to create guiding principles and proposed City policies to help guide future decision making.



Specific comments from stakeholders and community organizations

YWCA Meeting (July 7, 2011)

Transit Related Comments

- Do away with tokens for bus and trains, TRAX doesn't use tokens.
- Very difficult to get on TRAX and bus transfers with children and strollers.
- Bus drivers don't realize how heavy strollers are, please help more.
- Bus schedules are always gone.
- The Free Zone is good.
- Need temporary cards instead of tokens for ease of use on all transit.

Land Use Related Comments

- Need more drugstores like Walgreens.
- Need more clothing stores like Ross.
- Need more grocery stores like Fresh Market.
- Need more retail stores offering electronics.
- Need book stores.
- Need more affordable housing near transit.
- Need more urban recreational opportunities.
- Need more daycare centers near transit.
- Too many fast food chains restaurants (but still convenient).

Odyssey House Meeting (August 26, 2011)

Transit Related Comments

- New trains are nice, with more space and bigger seats.

Land Use Related Comments

- Need to focus on lots between 9th E and Library - it is "deadsville" for a pedestrian.
- Area SE of 9th E stop could have more density and apartments, redevelop as they are in

Odyssey House Meeting (continued)

Need a definition in Zoning Ordinance for "Smoke Shops" as these are unwanted land uses and bring in unwanted people and drug activity which is not good for the corridor or neighborhood.

Prohibit "pawn shops" along 4th South.

Put a great gym with indoor recreation opportunities, pool in the old "Whole Foods" space.

Put a little Walmart into "Whole Foods." Something 24 hour and to serve those living around neighborhood.

Need more pedestrian alleys and created spaces on and around the corridor planning area, these urban walkways add interest and give the community a place to meet friends and shop. Would help make it more walkable and wouldn't have to walk along 4th South as sidewalk isn't that pleasant.

Want more cafes and outdoor seating areas, place to meet friends and enjoy the area.

Food carts are fun and add interest and social activity along the street.

Limit drive-thrus as they are hard to walk past and just don't create an interesting place.

Need more mixed-use like the Broadway, with apartments above and small grocery and retail below, put these by the stations. Need a lot of density so that more uses will stay open longer.

Need new apartments or condos by pocket parks so that the park clientele are "cleaned up."

Need more drugstores like Walgreens, especially 24 hour pharmacies.

Need more clothing stores like Ross.

Need more grocery stores like Fresh Market.

Need more retail stores offering electronics like Apple or Best Buy.

Need book stores.

Need more affordable housing near transit, but also want high-end housing to balance out socio-economic groups and to provide enough foot traffic and populations with higher disposable income so that better retailers will choose to locate on the corridor.

Need a mix of housing types to bring in both the bachelor and a large family.

Need to allow higher density so that the old and decrepit apartment complexes are redeveloped, thus offering a higher standard of living around the corridor.

A hotel or small hotels or B&B type places on the corridor or around stations would be nice, and would bring in more nightlife and pedestrian activity and possibly nicer retail stores.

Specific comments from stakeholders and community organizations

Odyssey House Meeting (continued)

Need more urban recreational opportunities, parks, bowling alley, arcade or pool halls.
More family oriented recreation is especially needed. Another Gallivan Center-type of place would be welcome.

Need more destination lounges, the Green Pig is only place open on Sundays.

Need more daycare centers or charter schools near transit, this would liver the place with young people.

Parking is fine as long as located underground.

Too many fast food chains restaurants (but still convenient).

Property Owner Meeting (August 10, 2011)

Center was made to be walkable to the north.

Owners feel like they are shoehorned with the current zoning.

Would like to be able to go up more or build taller buildings.

Suggest that this project look further back than one half of a block.

Varied setback is possible. For example, buildings directly on the street would be limited to three stories in height and it would setback up to seven stories the further away from the street.

Right now, market is for rental residential units, very tough to do sales.

Would like the flexibility to expand the former Whole Foods building. Expansion would take place to the north. Right now it is 25,000 square feet and 40,000 would make it more attractive to potential tenants.

Difficult to do one building mixed use projects these days because of financing. Lenders prefer to lend on mixed use projects when the residential and commercial uses are in separate buildings, but they can be on the same lot or part of the same project. This is national, not just a local trend.

Most mixed use projects (in a traditional single building) are into their second or third owners because it is just hard to do in most areas unless the project is located in a very dense downtown area.

Tenants prefer to lease a project when parking is available in front of the storefront. Creating separate areas for parking is not desirable.

Owners feel that the city is short sighted in wanting a walkable community, but then density is limited.

Property Owner Meeting (continued)

Being in a historic district is a hindrance. Tenants do not want to have to go through two processes to obtain signs or to make minor modifications to their structure. It's also hard when modern signs are preferred on a faux historic building (Chipotle in Sugarhouse example).

RMU (with no density restriction) is the type of zoning designation they feel would work for the

University of Utah Transportation Meeting (June 27, 2011)

Transit Related Comments

Students should be encouraged to use transit more. This could be accomplished by adding more density along TRAX line.

University has a significant transit system, with buses and proposed light rail spur; please keep them informed as to future developments.

What impact, if any, will this study have on proposed bicycle lanes on 200 south and in other areas?

Land Use Related Comments

Please densify the area west of 700 East with mixed use.

Please keep the area east of 700 East with residential.

It would be great to use the parking lot at the corner of 900 East and 400 south as a park and ride for students.

Please find a way to keep student traffic away from residential areas west of the University.

Refugee Services Meeting (July 21, 2011)

Transit Related Comments

Refugees heavily utilize the public transportation system in the valley, especially TRAX.

Schedules and information are only found in English along the route.

Land Use Related Comments

Concerns were mentioned about redeveloping properties for refugees in the historic district.

Open houses and other meetings that can be held outside of the City & County Building may bring in more refugees. Many of the refugees may find the City & County Building or other large government buildings intimidating.

Specific comments from stakeholders and community organizations

Refugee Services Meeting (continued)

There are a large number of refugees living at Park Plaza Apartments which is located between 700 South and 800 South and between 300 East and 400 East.

The Office of Diversity may be a good contact as they work with Refugee Services a lot.

Difficult to find affordable housing near transit lines.

Because many refugees have large families affordable housing with 2 or 3 bedrooms is important.

Feedback Received from the Official Website

Feedback Left on the Official Website <http://www.sl.gov.com/ced/planning/400South>

Here's what I want to see...

- 1) An urban corridor along 4th with 3-5 story buildings fronting directly onto 4th. Parking should be accommodated behind buildings, not in front.
- 2) Wider sidewalks in front of those buildings, similar to what's on Main Street from S. Temple to 4th South. Right now the sidewalks are too narrow.
- 3) Much better access control. There are WAY too many driveways all over that street. Taking care of Item #1 should largely solve this problem, too.
- 4) I have a high-quality bicycle facility. It could be a regular bike lane, buffered bike lane, or a curb-separated bike lane. Any of those things would work. I LOVE having TRAX down this street, but adding a high-quality bike facility would make it even better.

Each TRAX station should have a "you are here" map that shows businesses and destinations within a short radius walking distance. The maps would help give transit riders a sense of the neighborhood and promote the small businesses that give the neighborhood a unique character.

New development should take place on the surface parking lots that front 400 South.

This idea is key to improving 400 S. Since people walk to the TRAX stops the strip-mall like street has become into an accidental pedestrian corridor but pedestrians are stranded between a busy street and a parking lot.

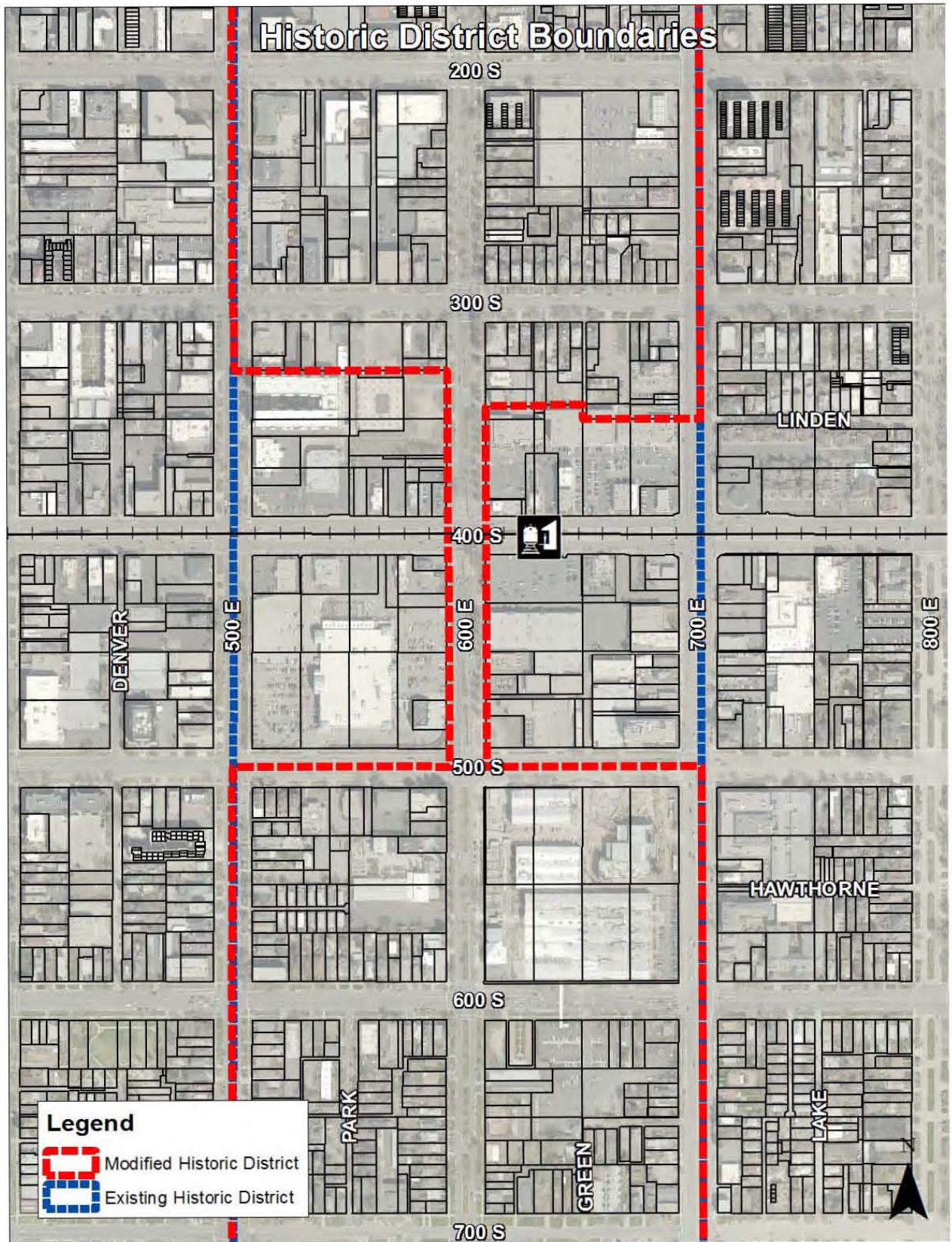
In particular, there is a huge parking lot by the Office/Inn near the 900 E. TRAX Stop that is off-limits for TRAX parking but always empty.

Focus on locally owned businesses and limit national chains.

More housing options along the corridor, with high density closer to stations.

Protect the historic buildings in the Central City Historic Districts.

Attachment B
Map of Proposed Modifications of the Central City Historic District



Attachment C
Draft Changes to an Excerpt of the Central Community Master Plan



Please note that the following Transit Oriented Development (TOD) section will replace the text found on page 15 for the current Central Community Master Plan.

Transit-Oriented Development (TOD)

Introduction

Transit-Oriented-Development (TOD) is a land use and urban design concept that mixes land use near transit to maximize transportation options and provides people with choices about living near transit. A TOD neighborhood typically has a center with a light rail station or bus stop surrounded by relatively high-density development with progressively lower-density development spreading outward from the transit center. TOD's generally are located within a radius of one-quarter to one-half mile from a transit stop, as this is considered an acceptable walking distance.

TOD areas offer choices in housing, commerce and transportation. They are designed to be flexible so they can respond to changing economic and social conditions and endeavor to make development economically viable from a number of perspectives (city, transit agency, developer, resident, employer, etc.). TOD districts encourage the stabilization and revitalization of existing neighborhoods, as new uses are designed to support existing neighborhood characteristics.

TOD districts improve the quality of life in urban areas by:

- Including mixed-use development that will use transit at all times of day.
- Creation of excellent pedestrian facilities such as high quality pedestrian crossings, narrow streets, and tapering of buildings as they become more distant from the public transport node.
- Ensuring compatibility and connectivity with surrounding neighborhoods.
- Creating compact development within an easy walk of public transit and with sufficient density to support transit ridership.
- To establish a hierarchy of transportation which places the pedestrian first, bicycle second and auto third.
- Creating active places and livable communities that service daily needs and where people feel a sense of belonging and ownership.
- Including engaging, high quality public spaces (e.g. small parks or plazas) as organizing features and gathering places for the neighborhood.
- Encouraging a variety of housing types near transit facilities.
- Providing housing choices for a wide range of ages and incomes.
- Incorporating retail into a development if it is a viable use at the location. Ideally drawing customers both from both the TOD and a major street.



- Introducing creative parking strategies that integrate, rather than divide a site and reduce the presence of the auto.
- Creating TOD plans that are flexible so they can respond to changing conditions.
- Recognizing that all TOD's are not the same; each development is located within its own unique context and serves a specific purpose in the larger context.

TOD strengthens urban development but also helps manage future regional growth by encouraging growth to occur where the existing infrastructure is best suited to address it. The benefits of this type of development include:

- Opportunities for affordable and accessible living by incentivizing affordable and accessible housing goals, providing a range of housing options, and allows people to spend less of their income on the combined expenses related to housing and transportation.
- Reduction of the long term expenditures of tax dollars to build and maintain public utilities, roads and other auto-oriented infrastructure.
- More residents living close to commercial areas to support a strong, local oriented economy.

This reduces the overall cost of development and reduces negative environmental impacts on air and water quality while creating community oriented public places.

Transit-oriented development designations

Transit-oriented development districts within the Central Community have three designations: low-density, medium-density, and high-density. The Future Land Use map shows locations where these districts are supported by this master plan. Where conflicts between TOD and historic district overlay regulations occur, the historic overlay requirements govern.

Low-density transit-oriented development (light sage green on map): Low-density TOD supports residential uses with a density ranging from 1-20 dwellings per acre. The emphasis of low-density TOD design and land use relates to existing lower density residential and neighborhood commercial land uses. Low intensive development should be designed to assure compatibility in neighborhoods with established low-density characteristics by focusing on the massing and scale of the existing structures. Medium intensive land uses, such as a coffee shop, town house or daycare center, may be appropriate near the light rail station or busier intersections. Implementing low-density TOD areas may include development of accessory units in the rear yards of low-density residential land uses as well as small businesses that can be operated out of a residential structure. New structures should be in scale with the low-density neighborhood with modest increases in building height being appropriate if it provides opportunity for increasing the residential density. Zoning designations should require compatibility in these areas.



Medium-density transit-oriented development (medium sage green on map): Medium-density TOD supports residential land uses with a density range of 20-50 dwelling units per acre. The design emphasis for medium-density TOD is compatibility with existing medium and low-density residential and commercial development. Higher intensive uses may be located near light rail stations where applicable. Medium-density TOD areas include four to five story buildings with a mix of ground level retail or office space components with multi-story residential development above. These areas should have limits on the amount of space allocated for non-residential land uses by locating them closer to transit and at busier intersections. Individual residential land uses could remain within the TOD area. Zoning districts that allow four to five story building, including pedestrian oriented design standards and allow 20-50 dwelling units per acre are appropriate in areas with this designation.

High-density transit-oriented development (dark sage green on map): High-density TOD is similar to medium-density TOD except at a greater scale. These areas should be centers of high population with a concentration of pedestrians in close proximity to transit stations. These areas will be more successful in places that are well served by rail transit. Building heights are established for high density residential and higher intensity office or commercial uses. The ideal building type in these areas are vertical mixed use structures tall than five stories. High-density TOD supports the development of compact urban centers with 50 or more dwelling units per acre. These centers can provide accessibility and a multitude of benefits (efficiencies that result when many activities are physically close together). Design standards are critical in these areas in order to create people oriented spaces. Auto oriented development should be prohibited. Standards and processes should be put in place to allow for the easy and financially viable transition of auto oriented developments to people oriented development.

The transit-oriented development land use designations are shown on the Central Community TOD map and on the Future Land Use map.

Community input on Transit-Oriented Development

This section will be further modified after input from the next community open houses in February and March of 2012.

Future Transit-Oriented Development land use changes

TOD's are implemented by the public and private sectors. The public sector sets the stage by providing the transit, investing in public infrastructure and applying appropriate zoning regulations near the transit stations. The private sector generally finances, constructs and markets TOD's. When based on a sound and agreed upon vision, TOD implementation should be a simple and straightforward process. To ensure this is the case, the TOD station area plans intend to create the vision and set the stage for the private sector. The City has developed certain zoning districts to implement the various intensity levels of TOD that are based on input from a broad range of stakeholders. The areas listed below have gone through a community visioning process that had identified the desired development characteristics and therefore, support zoning changes based on the specific goals for each area.



400 South and University Boulevard

In June of 2011, the City received a grant from the Housing and Urban Development Department (HUD) to initiate the 400 South and University Boulevard Livable Communities project. The focus of the project was to enhance the Community's vision of the corridor and make it a place where people could live, work and shop. Inherent in this process was the inclusion of people who rely on the corridor for their daily needs, but for various reasons do not normally participate. The group included those who are traditionally under-represented in the planning process. The project determined that future development at all 400 South stations should be compatible with the existing neighborhood scale and that development should not supplant or compete with the central business district regional services (beyond the existing Trolley Square development). Development should enhance existing urban neighborhoods within walking distance of the transit line to create areas where pedestrian activity is the focus of daily transportation needs, without excluding the automobile. These concepts are critical to groups who rely on public transportation to access housing, employment, education, health care, and other daily needs.

The 400 South and University Boulevard Trax lines run from downtown to the University of Utah. The corridor incorporates a mix of transportation options, including light rail, bus and automobile. There are several bicycle lanes that parallel and intersect the street to create a complete transportation network through the area. When the light rail line opened in 2001, the pedestrian environment improved by adding street trees and widening the sidewalk. Unfortunately, the parking lane was removed, placing a travel lane immediately adjacent to the sidewalk. In order to make the corridor more pedestrian oriented, sidewalk improvements are necessary.

The Liveable Communities project identified specific land use goals and policies that can be found in the "400 South and University Boulevard Station Area Plans" in the addendum of this Central Community Master Plan. The station area plans include specific land use policies and a future land use map that should be used to help guide future land use, capital improvements and budget decisions.

West Temple Gateway

The West Temple Gateway area extends from 700 South to the 900 South Interstate off ramp and from 300 West to West Temple and includes the 200 West / 900 South future light rail stop. The West Temple Gateway area is part of a redevelopment project area created in 1987, which included two revitalization concept plans. These are not adopted policy plans but resource documents. The 1994 plan identified alternative concepts ranging from low-density residential infill to Big Box retail uses. A second analysis in 2001, after the light rail line was constructed, provided an Illustrative Plan that proposes a mixed use transit-oriented neighborhood containing residential, retail, office, and industrial land uses. Development of a West Temple Gateway small area master plan will provide detailed development guidelines for this area.



Transit Oriented Development goal

To create TOD development with a balanced mix of uses that generates 24-hour transit ridership. This development will have places to work, to live, to learn, to relax and to shop for daily needs. The goal will be achieved through land use designations, development guidelines, zoning, and both public and private funding.

Transit Oriented Development policies

Transit-Oriented Development policies fall into these general categories: location and variety of land use.

Location

Policy TOD-1.0 Based on the Future Land Use map and specific station area plans, establish Transit-Oriented Districts that will provide residents with housing, transportation and employment options at various densities near transit stations.

TOD-1.1 Develop station area plans for each transit station within the Central Community Planning Community.

TOD-1.2 Utilize a broad community involvement approach to identify appropriately located and scaled transit oriented development that put people first.

Variety of Land Use

Policy TOD-2.0 Encourage the development of mixed-use projects near light rail stations to create a livable, walkable urban environment.

TOD-2.1 Support a variety of low-, medium- and high-density residential uses around light rail stations in TOD districts, based on the Future Land Use map designations.

TOD-2.2 At light rail stations in TOD districts, establish a centralized core of land uses that support transit ridership. Anchor transit centers with land uses that act as destination points.

TOD-2.3 Encourage a variety of commercial uses that share the same clientele and patrons. For example, movie theaters provide a clientele to patronize restaurants, arcades, and retail businesses.



Central Community Master Plan Goals and Recommendations Addendum

Please note that the following station area plans will be added into the addendum located on pages 22-23 of the current Central Community Master Plan. These items will be added right after #15 of the 'Environment' section.

400 South and University Boulevard Station Area Plans

Each of the three station area plans include subareas as identified below:

Core Area: The purpose of the core area is to provide areas for comparatively intense land development with a mix of land uses incorporating the principles of sustainable, transit oriented development and to enhance the area closest to a transit station as a lively, people oriented place. The core area is generally within a one-fourth ($\frac{1}{4}$) mile walk of a transit station platform. The core area may mix ground floor retail, office, commercial and residential space in order to activate the public realm. Buildings in this area should have minimal setbacks to encourage active outdoor use adjacent to the sidewalk, such as outdoor dining and patios that reflect the desired character of the area. Building facades should be varied and articulated, include storefronts adjacent to the street, windows on the street level, and have clearly defined entrances to provide visual interest to pedestrians. Buildings should be a minimum of two (2) or three (3) stories in height, depending on location, in order to define the street edge. Arcades, bays, and balconies are encouraged. The configuration of buildings must balance the needs of all modes of circulation with the safety and comfort of pedestrians and bicyclists. A vertical mix of uses, with office and residential above ground floor commercial uses is encouraged. A minimum of thirty (30) dwelling units per acre is encouraged within the core.

Transition Area: The purpose of the transition area is to provide a moderate level of land development intensity that incorporates the principles of sustainable transit oriented development. The transition area is intended to provide an important support base to the core area and transit ridership as well as buffer surrounding neighborhoods from the intensity of the core area. These areas reinforce the viability of the core area and provide opportunities for a range of housing types at different densities. Transition areas are generally located within a one-half ($\frac{1}{2}$) mile from the station platform, but may vary based on the character of the area. Transition areas typically serve the surrounding neighborhood; include a broad range of building forms that house a mix of compatible land uses. The minimum desired density is ten (10) dwelling units per acre. Commercial uses may include office, retail, restaurant and other



commercial land uses that are necessary to create mixed use neighborhoods. Commercial uses can be clustered around intersections and along block faces to create neighborhood nodes.

Library Station Area

The Library Urban Center Station has the highest intensity level and mix of uses along 400 South. It supports Salt Lake City's central business district in terms of building scale and use. The area has a twenty-four-hour population, active streetscapes, defined street walls and multiple types of public transit. Development generally occurs on surface parking lots or through redevelopment of underutilized parcels.

The street frontages in the Library Station area have a compact, dense, interconnected and walkable development pattern, while the interior of blocks are open generally with parking lots. Large scale development is closer to the station, and is scaled back as it moves to the areas away from 400 South. Building forms vary, but are typically oriented to the pedestrian, are multiple stories in height, and contain a horizontal and vertical mix of land uses. Buildings up to thirty stories in height are allowed within the D-1 zoned sections, while buildings in the transition zone can be approximately three to four stories in height.

The area features a variety of dense, mixed use commercial and housing developments, providing residents with a number of housing options. The station area contains important regional attractions, such as the Library, City & County Building, Leonardo Museum, and the Public Safety Building. These buildings are a regional draw, bringing thousands of people to the area each day. The area also features office, dining and entertainment options with a high level of pedestrian activity. The Library and Washington Square feature a number of civic and cultural festivals during the summer.

The station area comprises of core and transition areas. The purpose of creating the different areas is to recognize the scale and nature of existing development patterns and identify the appropriate locations for growth. The general concept is that bigger buildings with the most dwelling units and a higher intensity level of commercial space should be located closest to the station in the core. The transition area reduces the scale, mass and intensity of new development as it moves away from the core.

In addition to the civic uses, and high density development surrounding the Library Station core, there is a significant amount of underutilized land. The Library Station Area Plan encourages the development of these areas to create medium to high density housing, mixed use development and appropriate support commercial along 400 South. By doing this, the area would further its role as a vibrant and contributory feature of Salt Lake City, while maintaining its function as a support to the downtown area.

The "Transit Station Area (TSA) Development Guidelines" will be used along 400 South to encourage a walkable urban neighborhood compatible with the adjacent historic district.



Library Station Area Goals:

1. Rezone properties fronting 400 South so that the zoning reflects the vision for the station area. Rezone the properties that are in the vicinity to a mix of zoning districts that promote high density residential development while allowing limited commercial types of uses.
2. Coordinate with Utah Department of Transportation (UDOT) to improve the functionality of 400 South for pedestrians. This could be accomplished by adding additional midblock crosswalks to the transit stations, allowing on-street parking during off peak travel hours, studying the impact of reducing the road width, whether in key spots or for longer stretches and other changes to the street that are aimed at creating a safe and functional transportation corridor for all users.
3. Work with the City Transportation Division to find appropriate bicycle routes that run on or are parallel to 400 South and that connect to appropriately located north/south bike routes.
4. Require midblock connections from 300 South and 500 South that connect to 400 South.
5. Incorporate way-finding features at the station area so people can easily locate station area assets such as Library, Library Plaza, City & County Building, Leonardo Museum and the Public Safety Building.
6. Maintain the original 15-foot landscaped setback along the 400 South street frontage.
7. Require properties to share driveway access as properties are redeveloped to reduce the number of drive approaches in the station area.
8. Minimize the number of driveways, garage entrances and dedicated turning lanes on all major pedestrian routes.
9. Encourage community services, including schools, childcare and museums with pedestrian connections to transit and other land uses.
10. Extend 450 South midblock walkway through to 650 East.
11. Develop and enhance existing public gathering spaces, including parks, plazas and courtyards to attract people and transform 300 South and 500 South into active pedestrian places.
12. Increase building intensity and residential densities closest to the transit station and gradually step down further away. Parking requirements should be lower closer to the station.



13. Discourage additional or expanded stand alone automobile oriented uses such as: large surface parking lots, fuel stations, auto repair shops, auto dealerships, large scale retail development, car washes and drive-thru facilities.
14. Mid-block crossings across 400 South should be considered adjacent to the Trax stop to help pedestrians cross safely and to discourage jaywalking.

Trolley Station Area

The Trolley Station is defined as an Urban Neighborhood Station Area. Urban Neighborhoods are places that have an established development pattern that contain a mix of uses and can support an increase in residential density and supporting commercial activities. New development generally occurs as infill, occurring on undeveloped or underutilized properties. Redevelopment of surface parking lots that front on 400 South is a priority. A compact development pattern is desired in order to focus new growth at the station and respect the existing scale and intensity of the surrounding neighborhood. The highest residential density and intensity of commercial land use occur closest to the transit station and are scaled down the further one moves from the station.

The station area comprises of core and transition areas. The purpose of creating the different areas is to recognize the scale and nature of existing development patterns and identify the appropriate locations for growth. The general concept is that bigger buildings with the most dwelling units and a higher intensity level of commercial space should be located closest to the station in the core. The transition area reduces the scale, mass and intensity of new development as it moves away from the core area.

Trolley Station is a unique Transit Station Area because it is located within the Central City Historic District. The Central City Historic District is centered on the 600 East landscaped medians, which are a character defining feature of the historic district. Over time, the 600 East medians have been degraded by the continual pressure from commercial development to allow vehicle access through the medians. The policy of the Trolley Station Area is to prohibit further dissection of the 600 East medians for vehicular traffic and to maintain the historical 15 foot landscaped setback of building along 600 East.

Despite being located within the Central City Historic District, the blocks that front on 400 South have lost most, if not all, of their historic character. Without the historic character being present, the boundaries of the Central City Historic District should be reviewed and amendments to the district boundaries should be proposed.

The “TSA Development Guidelines” will be used along 400 South to encourage a walkable urban neighborhood compatible with the adjacent historic district.



Trolley Station Area Goals:

1. The primary purpose of the Trolley Station Area is to provide housing and access to higher intensity employment, commercial centers, downtown, and to the University of Utah.
2. Rezone properties fronting 400 South so that the zoning reflects the vision for the station area. Rezone the properties that are in the vicinity to a mix of zoning districts that promote high density residential development, while allowing limited commercial types of uses in appropriate places. The properties fronting 400 South should be the focus of the station area and development should focus on creating an urban neighborhood in scale and purpose, and is not intended to supplant or compete with the much higher density central business district. Regional scale development beyond the existing Trolley Square commercial development is not encouraged.
3. Coordinate with the Utah Department of Transportation (UDOT) to improve the functionality of 400 South for pedestrians. This could be accomplished by adding additional midblock crosswalks to the transit stations, allowing on street parking during off peak travel hours, studying the impact of reducing the road width, whether in key spots or for longer stretches, and other changes to the street that are aimed at creating a safe and functional transportation corridor for all users.
4. Work with the Transportation Division to find appropriate bicycle routes that run on or parallel 400 South and that connect to appropriately located north/south bike routes.
5. Require midblock connections from 300 South and 500 South that connect to 400 South.
6. Incorporate way-finding features at the station area so people can easily locate station area assets such as Trolley Square, Gilgal Gardens and Liberty Park.
7. Maintain the original 15-foot landscaped setback along the 400 South street frontage.
8. Review the appropriateness of and consider amendments to the boundaries of the Central City Historic District to remove those blocks and portions of blocks that front on 400 South from the historic district due to the lack of remaining historic character. See the “.....” map to determine the areas that should be considered for removal from the Central City Historic District.
9. Preserve the 600 East medians and prohibit further bisections of the medians for the purpose of allowing vehicular access and left turns to private property or streets.
10. Extend 450 South midblock walkway through to 650 East.



11. Further multi-modal solutions to change the way 600 South is utilized between 500 East and 700 East. Possible solutions include adding middle of the street parking, midblock crosswalks, medians, or removing a lane of traffic in each direction to add bike lanes, larger park strips or angled parking.
12. Identify zoning solutions for the block faces across from Trolley Square on 600 East and 600 South. The focus should be to encourage development on vacant parcels, increase residential density and promote the preservation and adaptive reuse of contributing structures. The surface parking lot south of Trolley Square should be rezoned to allow Trolley Square to building a parking structure, retain the historic structures fronting on 600 South and build housing.
13. Adopt zoning that increases housing potential, but does not introduce extensive commercial development on the south side of 500 South between 500 and 600 East and both sides of the street between 400 and 500 East.
14. Encourage redevelopment of strip centers into higher density with structured parking with a potential park and ride. The block bounded by 400 and 500 South and 600 and 700 East should have the highest commercial densities of the area.
15. Encourage development that is compatible with the historic development pattern in the Central City Historic District where appropriate.
16. Reevaluate the densities on the northwest quadrant of the block bounded by 300 South and 400 South and 600 East and 700 East, to allow higher densities and design that is compatible with the adjacent historic character.
17. Mid-block crossings across 400 South should be considered adjacent to the Trax stop to help pedestrians to cross safely and to discourage jaywalking.

900 East Station Area

The 900 East Station is an Urban Neighborhood Station Area due to the established and predominating residential character and the potential for infill development along 400 South which adds variations of density and intensity of building forms that blend in and complement the existing residential character of the area. The development strategy of an urban neighborhood station may include the mixing of building types and uses, including the allocation of commercial or office uses to the ground floor, and residential uses above these floors. The highest residential densities and most intense land uses are generally located closest to the station platform along 400 South between 700 East and 900 East, particularly on the south side of 400 South.

The station area comprises of core and transition areas. The purpose of creating the different areas is to recognize the scale and nature of existing development patterns and identify the appropriate locations for growth. The general concept is that bigger buildings with the most



dwelling units and a higher intensity level of commercial space should be located closest to the station in the core. The transition area reduces the scale, mass and intensity of new development as it moves away from the core area. The north side of 400 South is part of the transition area due to the close proximity of the relatively low scale nature of the residential area to the north and the desire to maintain that character and the impacts that taller buildings on 400 South would have on privacy and solar access.

Contributing landmarks and existing neighborhood characteristics will influence future development in and around the 900 East Station. These existing traits include institutional uses such as the Intermountain Health Care facility, Bennion Elementary School, and religious structures. There are also various types of commercial and a strong mixture of single and multi-family residential building types.

900 East Station Area Goals:

1. Protect historic landmark sites that currently exist in the Station Area.
2. Coordinate with UDOT to improve the functionality of 400 South for pedestrians. This could be accomplished by adding additional midblock crosswalks to the transit stations, allowing on street parking during off peak travel hours, studying the impact of reducing the road width, whether in key spots or for longer stretches, and other changes to the street that are aimed at creating a safe and functional transportation corridor for all users.
3. Maintain original 15-foot landscaped setback along the 400 South street frontage.
4. Incorporate way-finding features at the station area so people can easily locate station area assets such as Gilgal Gardens and health care facilities.
5. Protect the 800 East medians by not allowing vehicular access cuts through the existing landscaped area.
6. Encourage infill uses that will eliminate surface parking lots with an emphasis on those that front 400 South or areas that can easily access the station platform.
7. Encourage shared parking facilities between uses where applicable.
8. Focus on mixed-use development with commercial and residential uses along 400 South and the east side of 700 East. In remaining areas the primary redevelopment focus should be on residential uses.
9. If the Bennion Elementary School discontinues to be utilized as a school, the property should be redeveloped with an emphasis on residential uses along the frontages but much of the open space behind should remain, and be enhanced and available to the public.



10. Create mid-block connections that draw pedestrians to 400 South and specifically to the 900 East TRAX station. This should be emphasized between 300 South and 400 South where connections could more easily be created by linking existing rights-of-way such as, Laker Court and Strongs Court.
11. Encourage nonconforming manufacturing or warehousing uses to relocate to appropriate locations in the city.
12. Adopt zoning that promotes high density housing with appropriate design standards adjacent to Gilgal Gardens in order to activate and provide security for the park.
13. Mid-block crossings across 400 South should be considered adjacent to the Trax stop to help pedestrians to cross safely and to discourage jaywalking.

DRAFT

Attachment D
Library Station Summary Handout



400 SOUTH

LIVABLE COMMUNITIES

LIBRARY STATION



LOCATION	BOUNDARY
<p>The Library Station Trax platform is located in the middle of 400 South. The platform is located just to the east of 200 East and to the north of the Salt Lake City Main library.</p>	<p>The general boundaries of the Library Station are as follows:</p>
	<p>North: 300 South East: 400 East South: 500 South West: 200 East</p>



February 2012 400 South Livable Communities

Library Station Area

The Library Station area is divided into two subareas as follows:

CORE

The purpose of the core area is to provide areas for comparatively intense land development with a mix of land uses incorporating the principles of sustainable, transit oriented development and to enhance the area closest to a transit station as a lively, people oriented place. The core area is generally within a ¼ mile walk of a transit station platform. The core area may mix ground floor retail, office, commercial and residential space in order to activate the public realm.

Buildings in this area should have minimal setbacks to encourage active outdoor use adjacent to the sidewalk, such as outdoor dining and patios that reflect the desired character of the area. Building facades should be varied and articulated, include storefronts adjacent to the street, windows on the street level, and have clearly defined entrances to provide visual interest to pedestrians. Buildings should be a minimum of two or three stories in height, depending on location, in order to define the street edge. Arcades, bays, and balconies are encouraged. The configuration of buildings must balance the needs of all modes of circulation with the safety and comfort of pedestrians and bicyclists. A minimum of 30 dwelling units per acre is encouraged within the core.

TRANSITION

The purpose of the transition area is to provide a moderate level of land development intensity that incorporates the principles of sustainable transit oriented development. The transition area is intended to provide an important support base to the core area and transit ridership as well as buffer surrounding neighborhoods from the intensity of the core area. These areas reinforce the viability of the core area and provide opportunities for a range of housing types at different densities.

Transition areas are generally located within a ½ mile from the station platform, but may vary based on the character of the area. Transition areas typically serve the surrounding neighborhood; include a broad range of building forms that house a mix of compatible land uses. The minimum desired density is 10 dwelling units per acre. Commercial uses may include office, retail, restaurant and other commercial land uses that are necessary to create mixed use neighborhoods. Commercial uses can be clustered around intersections and along block faces to create neighborhood nodes.



CENTRAL COMMUNITY MASTER PLAN *Transit Oriented Development*

Area Summary

- The Library Urban Center Station has the highest intensity level and mix of uses along 400 South.
- Large scale development is closer to the station; and is scaled back as it moves to the areas away from 400 South.
- The station area contains important regional attractions, such as the Library, City & County Building, Leonardo Museum, and the Public Safety Building
- Policy Highlights
- Rezone properties fronting 400 South so that the zoning reflects the vision for the station area.
- Work with the City Transportation Division to find appropriate bicycle routes that run on or are parallel to 400 South and that connect to appropriately located north/south bike routes.
- Require midblock connections from 300 South and 500 South that connect to 400 South.
- Require properties to share driveway access as properties are redeveloped to reduce the number of drive approaches in the station area.
- Extend 450 South midblock walkway through to 650 East.
- Increase building intensity and residential densities closest to the transit station and gradually step down further away. Parking requirements should be lower closer to the station.
- Discourage additional or expanded stand alone automobile oriented uses such as: large surface parking lots, fuel stations, auto repair shops, auto dealerships, large scale retail development, car washes and drive-thru facilities.
- Mid-block crossings across 400 South should be considered adjacent to the Trax stop to help pedestrians cross safely and to discourage jaywalking.

February 2011 400 South Livable Communities

TSA Design Guidelines



What are Development Guidelines?

Development Guidelines are a list of recommendations that should be used to guide new development within a transit station area. The guidelines are an integral part of the review process set forth in Chapter 21A.26. The intent of the TSADG is to reward high quality, desired development through the use of incentives (such as increased building height) and by allowing for a quicker review process.

How to use the Development Guidelines

These development guidelines are to be used by the City in the review process for each project located in the TSA zoning district. While the guidelines are voluntary, compliance with them entitles applicants to faster review and can guide new development and redevelopment towards implementing the vision in the station area plans. Designers and developers are encouraged to incorporate as many of the development guidelines as they reasonably can, which in turn will lessen the review process timeframe. Each Guideline includes a value. The assigned value is based on several factors, including the importance of the guideline in terms of implementing the vision of a Station Area Plan or other applicable land use related policies of the City and the cost of addressing the guideline. Development Guidelines should be addressed in all new projects. The Development Score is established by adding up the values of each guideline that has been incorporated into a project. Applicants have the ability to determine City review time of their proposal by achieving a high development score. This type of point and reward system allows desired development to be processed quicker.

The Development Score Thresholds

The approval process for all new development in the TSA District is based on the development score. A series of tiers has been established, indicating which approval process is required. The higher the Development Score, the more streamlined the approval process. Those projects that are in Tier 1 require Planning Commission approval through the Conditional Building and Site Design Review Process and

standards. The purpose of the Conditional Building and Site Design Review Process is to provide flexible implementation of the specific design requirements found in individual zoning districts. It is intended to help ensure that newly developed properties and redeveloped properties are designed to encourage pedestrian access, circulation and orientation while acknowledging the need for transit and automobile access. This process also requires public input prior to scheduling a public hearing with the Planning Commission. Projects that are in Tier 2 require an Administrative Hearing prior to an Administrative Decision. This process allows for a relatively quick review and approval process, but still requires a public hearing. Projects that achieve a development score in Tier 3 are rewarded by allowing administrative approval without a public hearing. Tier 3 was established so that those projects that are highly compliant with the vision of a station area plan have a streamlined approval process. The decision made in each Tier is subject to appeal as outlined in the Zoning Ordinance.

Review Process

Tier	Approval Process	Estimated Time Frame*
Tier 1- Projects with a development score of less than 50 points	Planning Commission	3-6 months
Tier 2- Projects with a development score between 50 and 99 points	Administrative Hearing	1-3 months
Tier 3- Projects with a development score more than 100 points	Administrative Review	No additional time required after Development Score review

* The timeframes are estimates. Variables such as an incomplete application, lack of information, time until next available community council meeting or public hearing, and appeals may affect the estimated time frame for approval.

February 2012 400 South Livable Communities

Transit Station Area Zoning

New Zoning- Transit Station Area



Salt Lake City Zoning Ordinance Section 21A.26.070

The purpose of the TSA zoning designation is to provide an environment for efficient and attractive transit and pedestrian oriented commercial, residential and mixed use development around transit stations. Redevelopment, infill development and increased development on underutilized parcels should include uses that allow them to function as part of a walkable, mixed use district. Existing uses that are complementary to the district, and economically and physically viable, should be integrated into the form and function of a compact, mixed use pedestrian oriented neighborhood. Each transit station is categorized into a station type. These typologies are used to establish appropriate zoning regulations for similar station areas. Each station area will typically have two subsections: the core area and the transition area.

Within the TSA zone, uses are listed as prohibited, rather than a list of permitted uses. The reason is to identify those land uses that are not compatible with transit oriented development due to the nature of the use, the land requirements of the use, or the potential impacts of the use. Any use not listed, but is substantially similar to a use listed, is prohibited. All other uses would be permitted. Some examples of uses that are currently in Chapter 21A.26.070 as prohibited in a TSA zone:

- Ambulance services, dispatching, staging and maintenance
- Animal pound
- Auto salvage and recycling
- Auto, truck, and RV repair, sales and/or service (minor or major)
- Car washes
- Cemeteries, any type
- Commercial parking lots not located in a parking structure
- Community correction facilities, any size
- Detached single-family dwelling
- Drive-through windows and service
- Flammable liquids or gases, heating fuel distribution
- Gas stations
- Heavy equipment rental, repair and sales
- Homeless shelters
- Impound lot
- Incinerators, for any purpose
- Kennel
- Manufactured/mobile home sales and service
- Ministorage facilities
- Outdoor storage including storage or staging of fleet vehicles associated with a business, but not intended to include outdoor retail sales areas or outdoor storage as an accessory and incidental use to a principal use
- Pawnshops
- Sexually oriented business
- Stable, public or private
- Wholesale distributors

For a complete list, please refer to Table 21A.26.078E in the Zoning Ordinance.

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A Housing & Urban Development Liveable Communities Grant Project

400 SOUTH



February 2012 400 South Livable Communities

Attachment E
Trolley Station Summary Handout



400 SOUTH

LIVABLE COMMUNITIES

TROLLEY STATION



LOCATION	BOUNDARY
<p>The Trolley Station Trax platform is located in the middle of 400 South. The platform is located just to the east of 600 East and provides easy walking access to Trolley Square and other retail shopping locations in the area.</p>	<p>The general boundaries of the Trolley Station are as follows:</p> <ul style="list-style-type: none">North: 300 SouthEast: 900 EastSouth: 500 SouthWest: 400 East



February 2012 400 South Livable Communities

Trolley Station Area

The Trolley Station area is divided into two subareas as follows:

CORE ●

The purpose of the core area is to provide areas for comparatively intense land development with a mix of land uses incorporating the principles of sustainable, transit oriented development and to enhance the area closest to a transit station as a lively, people oriented place. The core area is generally within a ¼ mile walk of a transit station platform. The core area may mix ground floor retail, office, commercial and residential space in order to activate the public realm.

Buildings in this area should have minimal setbacks to encourage active outdoor use adjacent to the sidewalk, such as outdoor dining and patios that reflect the desired character of the area. Building facades should be varied and articulated, include storefronts adjacent to the street, windows on the street level, and have clearly defined entrances to provide visual interest to pedestrians. Buildings should be a minimum of two or three stories in height, depending on location, in order to define the street edge. Arcades, bays, and balconies are encouraged. The configuration of buildings must balance the needs of all modes of circulation with the safety and comfort of pedestrians and bicyclists. A minimum of 30 dwelling units per acre is encouraged within the core.

TRANSITION ○

The purpose of the transition area is to provide a moderate level of land development intensity that incorporates the principles of sustainable transit oriented development. The transition area is intended to provide an important support base to the core area and transit ridership as well as buffer surrounding neighborhoods from the intensity of the core area. These areas reinforce the viability of the core area and provide opportunities for a range of housing types at different densities.

Transition areas are generally located within a ½ mile from the station platform, but may vary based on the character of the area. Transition areas typically serve the surrounding neighborhood; include a broad range of building forms that house a mix of compatible land uses. The minimum desired density is 10 dwelling units per acre. Commercial uses may include office, retail, restaurant and other commercial land uses that are necessary to create mixed use neighborhoods. Commercial uses can be clustered around intersections and along block faces to create neighborhood nodes.



CENTRAL COMMUNITY MASTER PLAN *Transit Oriented Development*

Area Summary

- The primary purpose of the Trolley Station Area is to provide housing and access to higher intensity employment, commercial centers, downtown, and to the University of Utah.
- Work with the Transportation Division to find appropriate bicycle routes that run on or parallel 400 South and that connect to appropriately located north/south bike routes.
- Require midblock connections from 300 South and 500 South that connect to 400 South.
- Review the appropriateness of and consider amendments to the boundaries of the Central City Historic District to remove those blocks and portions of blocks that front on 400 South from the historic district due to the lack of remaining historic character.
- Preserve the 600 East medians and prohibit further bisections of the medians for the purpose of allowing vehicular access and left turns to private property or streets.
- Identify zoning solutions for the block faces across from Trolley Square on 600 East and 600 South. The focus should be to encourage development on vacant parcels, increase residential density and promote the preservation and adaptive reuse of contributing structures. The surface parking lot south of Trolley Square should be rezoned to allow Trolley Square to building a parking structure, retain the historic structures fronting on 600 South and build housing.
- Adopt zoning that increases housing potential, but does not introduce extensive commercial development on the south side of 500 South between 500 and 600 East and both sides of the street between 400 and 500 East.
- Encourage development that is compatible with the historic development pattern in the Central City Historic District where appropriate.

February 2012 400 South Livable Communities

TSA Design Guidelines



What are Development Guidelines?

Development Guidelines are a list of recommendations that should be used to guide new development within a transit station area. The guidelines are an integral part of the review process set forth in Chapter 21A.26. The intent of the TSADG is to reward high quality, desired development through the use of incentives (such as increased building height) and by allowing for a quicker review process.

How to use the Development Guidelines

These development guidelines are to be used by the City in the review process for each project located in the TSA zoning district. While the guidelines are voluntary, compliance with them entitles applicants to faster review and can guide new development and redevelopment towards implementing the vision in the station area plans. Designers and developers are encouraged to incorporate as many of the development guidelines as they reasonably can, which in turn will lessen the review process timeframe. Each Guideline includes a value. The assigned value is based on several factors, including the importance of the guideline in terms of implementing the vision of a Station Area Plan or other applicable land use related policies of the City and the cost of addressing the guideline. Development Guidelines should be addressed in all new projects. The Development Score is established by adding up the values of each guideline that has been incorporated into a project. Applicants have the ability to determine City review time of their proposal by achieving a high development score. This type of point and reward system allows desired development to be processed quicker.

The Development Score Thresholds

The approval process for all new development in the TSA District is based on the development score. A series of tiers has been established, indicating which approval process is required. The higher the Development Score, the more streamlined the approval process. Those projects that are in Tier 1 require Planning Commission approval through the Conditional Building and Site Design Review Process and

standards. The purpose of the Conditional Building and Site Design Review Process is to provide flexible implementation of the specific design requirements found in individual zoning districts. It is intended to help ensure that newly developed properties and redeveloped properties are designed to encourage pedestrian access, circulation and orientation while acknowledging the need for transit and automobile access. This process also requires public input prior to scheduling a public hearing with the Planning Commission. Projects that are in Tier 2 require an Administrative Hearing prior to an Administrative Decision. This process allows for a relatively quick review and approval process, but still requires a public hearing. Projects that achieve a development score in Tier 3 are rewarded by allowing administrative approval without a public hearing. Tier 3 was established so that those projects that are highly compliant with the vision of a station area plan have a streamlined approval process. The decision made in each Tier is subject to appeal as outlined in the Zoning Ordinance.

Review Process

Tier	Approval Process	Estimated Time Frame*
Tier 1- Projects with a development score of less than 50 points	Planning Commission	3-6 months
Tier 2- Projects with a development score between 50 and 99 points	Administrative Hearing	1-3 months
Tier 3- Projects with a development score more than 100 points	Administrative Review	No additional time required after Development Score review

* The timeframes are estimates. Variables such as an incomplete application, lack of information, time until next available community council meeting or public hearing, and appeals may affect the estimated time frame for approval.

February 2012 400 South Livable Communities

Transit Station Area Zoning

New Zoning- Transit Station Area



Salt Lake City Zoning Ordinance Section 21A.26.070

The purpose of the TSA zoning designation is to provide an environment for efficient and attractive transit and pedestrian oriented commercial, residential and mixed use development around transit stations. Redevelopment, infill development and increased development on underutilized parcels should include uses that allow them to function as part of a walkable, mixed use district. Existing uses that are complementary to the district, and economically and physically viable, should be integrated into the form and function of a compact, mixed use pedestrian oriented neighborhood. Each transit station is categorized into a station type. These typologies are used to establish appropriate zoning regulations for similar station areas. Each station area will typically have two subsections: the core area and the transition area.

Within the TSA zone, uses are listed as prohibited, rather than a list of permitted uses. The reason is to identify those land uses that are not compatible with transit oriented development due to the nature of the use, the land requirements of the use, or the potential impacts of the use. Any use not listed, but is substantially similar to a use listed, is prohibited. All other uses would be permitted. Some examples of uses that are currently in Chapter 21A.26.070 as prohibited in a TSA zone:

- Ambulance services, dispatching, staging and maintenance
- Animal pound
- Auto salvage and recycling
- Auto, truck, and RV repair, sales and/or service (minor or major)
- Car washes
- Cemeteries, any type
- Commercial parking lots not located in a parking structure
- Community correction facilities, any size
- Detached single-family dwelling
- Drive-through windows and service
- Flammable liquids or gases, heating fuel distribution
- Gas stations
- Heavy equipment rental, repair and sales
- Homeless shelters
- Impound lot
- Incinerators, for any purpose
- Kennel
- Manufactured/mobile home sales and service
- Ministorage facilities
- Outdoor storage including storage or staging of fleet vehicles associated with a business, but not intended to include outdoor retail sales areas or outdoor storage as an accessory and incidental use to a principal use
- Pawnshops
- Sexually oriented business
- Stable, public or private
- Wholesale distributors

For a complete list, please refer to Table 21A.26.078E in the Zoning Ordinance.

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A Housing & Urban Development Liveable Communities Grant Project

400 SOUTH



February 2012 400 South Livable Communities

Attachment F
900 Station Summary Handout



400 SOUTH

LIVABLE COMMUNITIES

900 EAST STATION



LOCATION	BOUNDARY
<p>The 900 East Station Trax platform is located in the middle of 400 South. The platform is located just to the west of 900 East and provides easy walking access to various retail shopping and residential locations in the area.</p>	<p>The general boundaries of the 900 East Station are as follows:</p> <p>North: 300 South East: 1100 East South: 500 South West: 700 East</p>



February 2012 400 South Livable Communities

900 East Station Area

The 900 East Station area is divided into two subareas as follows:

CORE

The purpose of the core area is to provide areas for comparatively intense land development with a mix of land uses incorporating the principles of sustainable, transit oriented development and to enhance the area closest to a transit station as a lively, people oriented place. The core area is generally within a ¼ mile walk of a transit station platform. The core area may mix ground floor retail, office, commercial and residential space in order to activate the public realm.

Buildings in this area should have minimal setbacks to encourage active outdoor use adjacent to the sidewalk, such as outdoor dining and patios that reflect the desired character of the area. Building facades should be varied and articulated, include storefronts adjacent to the street, windows on the street level, and have clearly defined entrances to provide visual interest to pedestrians. Buildings should be a minimum of two or three stories in height, depending on location, in order to define the street edge. Arcades, bays, and balconies are encouraged. The configuration of buildings must balance the needs of all modes of circulation with the safety and comfort of pedestrians and bicyclists. A minimum of 30 dwelling units per acre is encouraged within the core.

TRANSITION

The purpose of the transition area is to provide a moderate level of land development intensity that incorporates the principles of sustainable transit oriented development. The transition area is intended to provide an important support base to the core area and transit ridership as well as buffer surrounding neighborhoods from the intensity of the core area. These areas reinforce the viability of the core area and provide opportunities for a range of housing types at different densities.

Transition areas are generally located within a ½ mile from the station platform, but may vary based on the character of the area. Transition areas typically serve the surrounding neighborhood; include a broad range of building forms that house a mix of compatible land uses. The minimum desired density is 10 dwelling units per acre. Commercial uses may include office, retail, restaurant and other commercial land uses that are necessary to create mixed use neighborhoods. Commercial uses can be clustered around intersections and along block faces to create neighborhood nodes.



CENTRAL COMMUNITY MASTER PLAN *Transit Oriented Development*

Area Summary

- Protect historic landmark sites that currently exist in the Station Area.
- Protect the 800 East medians by not allowing vehicular access cuts through the existing landscaped area.
- Encourage infill uses that will eliminate surface parking lots with an emphasis on those that front 400 South or areas that can easily access the station platform.
- Focus on mixed-use development with commercial and residential uses along 400 South and the east side of 700 East. In remaining areas the primary redevelopment focus should be on residential uses.
- If the Bennion Elementary School discontinues to be utilized as a school, the property should be redeveloped with an emphasis on residential uses along the frontages but much of the open space behind should remain, and be enhanced and available to the public.
- Encourage nonconforming manufacturing or warehousing uses to relocate to appropriate locations in the city.
- Adopt zoning that promotes high density housing with appropriate design standards adjacent to Gilgal Gardens in order to activate and provide security for the park.

February 2012 400 South Livable Communities

TSA Design Guidelines



What are Development Guidelines?

Development Guidelines are a list of recommendations that should be used to guide new development within a transit station area. The guidelines are an integral part of the review process set forth in Chapter 21A.26. The intent of the TSADG is to reward high quality, desired development through the use of incentives (such as increased building height) and by allowing for a quicker review process.

How to use the Development Guidelines

These development guidelines are to be used by the City in the review process for each project located in the TSA zoning district. While the guidelines are voluntary, compliance with them entitles applicants to faster review and can guide new development and redevelopment towards implementing the vision in the station area plans. Designers and developers are encouraged to incorporate as many of the development guidelines as they reasonably can, which in turn will lessen the review process timeframe. Each Guideline includes a value. The assigned value is based on several factors, including the importance of the guideline in terms of implementing the vision of a Station Area Plan or other applicable land use related policies of the City and the cost of addressing the guideline. Development Guidelines should be addressed in all new projects. The Development Score is established by adding up the values of each guideline that has been incorporated into a project. Applicants have the ability to determine City review time of their proposal by achieving a high development score. This type of point and reward system allows desired development to be processed quicker.

The Development Score Thresholds

The approval process for all new development in the TSA District is based on the development score. A series of tiers has been established, indicating which approval process is required. The higher the Development Score, the more streamlined the approval process. Those projects that are in Tier 1 require Planning Commission approval through the Conditional Building and Site Design Review Process and

standards. The purpose of the Conditional Building and Site Design Review Process is to provide flexible implementation of the specific design requirements found in individual zoning districts. It is intended to help ensure that newly developed properties and redeveloped properties are designed to encourage pedestrian access, circulation and orientation while acknowledging the need for transit and automobile access. This process also requires public input prior to scheduling a public hearing with the Planning Commission. Projects that are in Tier 2 require an Administrative Hearing prior to an Administrative Decision. This process allows for a relatively quick review and approval process, but still requires a public hearing. Projects that achieve a development score in Tier 3 are rewarded by allowing administrative approval without a public hearing. Tier 3 was established so that those projects that are highly compliant with the vision of a station area plan have a streamlined approval process. The decision made in each Tier is subject to appeal as outlined in the Zoning Ordinance.

Review Process

Tier	Approval Process	Estimated Time Frame*
Tier 1- Projects with a development score of less than 50 points	Planning Commission	3-6 months
Tier 2- Projects with a development score between 50 and 99 points	Administrative Hearing	1-3 months
Tier 3- Projects with a development score more than 100 points	Administrative Review	No additional time required after Development Score review

* The timeframes are estimates. Variables such as an incomplete application, lack of information, time until next available community council meeting or public hearing, and appeals may affect the estimated time frame for approval.

February 2012 400 South Livable Communities

Transit Station Area Zoning

New Zoning- Transit Station Area



Salt Lake City Zoning Ordinance Section 21A.26.070

The purpose of the TSA zoning designation is to provide an environment for efficient and attractive transit and pedestrian oriented commercial, residential and mixed use development around transit stations. Redevelopment, infill development and increased development on underutilized parcels should include uses that allow them to function as part of a walkable, mixed use district. Existing uses that are complementary to the district, and economically and physically viable, should be integrated into the form and function of a compact, mixed use pedestrian oriented neighborhood. Each transit station is categorized into a station type. These typologies are used to establish appropriate zoning regulations for similar station areas. Each station area will typically have two subsections: the core area and the transition area.

Within the TSA zone, uses are listed as prohibited, rather than a list of permitted uses. The reason is to identify those land uses that are not compatible with transit oriented development due to the nature of the use, the land requirements of the use, or the potential impacts of the use. Any use not listed, but is substantially similar to a use listed, is prohibited. All other uses would be permitted. Some examples of uses that are currently in Chapter 21A.26.070 as prohibited in a TSA zone:

- Ambulance services, dispatching, staging and maintenance
- Animal pound
- Auto salvage and recycling
- Auto, truck, and RV repair, sales and/or service (minor or major)
- Car washes
- Cemeteries, any type
- Commercial parking lots not located in a parking structure
- Community correction facilities, any size
- Detached single-family dwelling
- Drive-through windows and service
- Flammable liquids or gases, heating fuel distribution
- Gas stations
- Heavy equipment rental, repair and sales
- Homeless shelters
- Impound lot
- Incinerators, for any purpose
- Kennel
- Manufactured/mobile home sales and service
- Ministorage facilities
- Outdoor storage including storage or staging of fleet vehicles associated with a business, but not intended to include outdoor retail sales areas or outdoor storage as an accessory and incidental use to a principal use
- Pawnshops
- Sexually oriented business
- Stable, public or private
- Wholesale distributors

For a complete list, please refer to Table 21A.26.078E in the Zoning Ordinance.

Salt Lake City Planning Division
451 South State Street, Room 406
PO Box 145480
Salt Lake City, Utah 84111-5480
(801) 535-7757

<http://www.slcclassic.com/CED/planning/400South/>

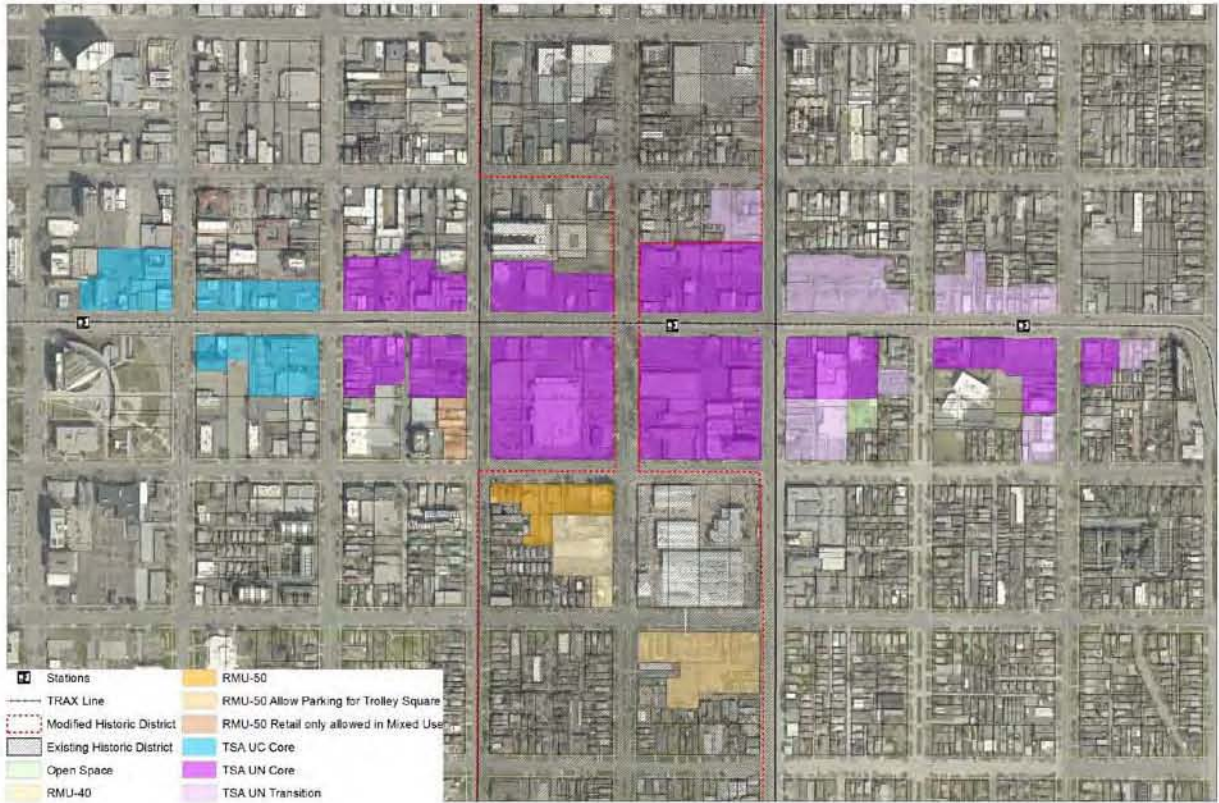
A Housing & Urban Development Liveable Communities Grant Project

400 SOUTH



February 2012 400 South Livable Communities

Attachment G
Map of Proposed Zoning Changes



**Excerpt of
SALT LAKE CITY PLANNING COMMISSION MEETING
In Room 326 of the City & County Building
451 South State Street, Salt Lake City, Utah
Wednesday, March 28, 2012**

Present for the Planning Commission meeting were: Chairperson Angela Dean, Vice Chair Michael Gallegos and Commissioners Lisa Adams, Emily Drown, Bernardo Flores-Sahagun, Michael Fife, Kathleen Hill, Clark Ruttinger, Marie Taylor, Matthew Wirthlin, and Mary Woodhead. A field trip was held prior to the meeting. Planning Commissioners present were: Commissioners Emily Drown, Bernardo Flores-Sahagun, Clark Ruttinger, Marie Taylor, Mary Woodhead and Lisa Adams. Staff members in attendance were Joel Paterson, Casey Stewart, and Katia Pace.

A roll is being kept of all who attended the Planning Commission Meeting. The meeting was called to order at [5:35:38 PM](#). Audio recordings of the Planning Commission meetings are retained in the Planning Office for an indefinite period of time. Planning staff members present at the meeting were: Cheri Coffey, Assistant Planning Director; Joel Paterson, Planning Manager, Casey Stewart, Senior Planner; Maryann Pickering, Principal Planner; Katia Pace, Principal Planner; Ana Valdemoros, Principal Planner; Paul Nielson, Senior City Attorney ; and Angela Hasenberg, Senior Secretary.

[5:35:51 PM](#)

Briefing:

PLNPCM2010-00647 - 400 South Livable Communities Station Area Plans - A request by Salt Lake City Mayor Ralph Becker for an amendment to the Central Community Master Plan, Salt Lake City Zoning Ordinance and Zoning Map regarding transit station area plans along the 400 South transit corridor (approximately 200 East to 1100 East). The intent of the project is to implement livability goals for the corridor including land use policies and zoning that support mixed use development and transportation choices for current and future residents and workers. The project is located in City Council District 4 represented by Luke Garrott. (Staff contact: Maryann Pickering at 801-535-7660 or maryann.pickering@slcgov.com)

Chairperson Dean recognized Maryann Pickering as staff representative.

Ms. Pickering stated that this was a briefing on the current status of the 400 South Livable Communities project.

Ms. Pickering gave a background on the project outlining that this was a project that received a grant from the Department of Housing and Urban Development (HUD) to undertake a rezoning process for a portion of the University TRAX line. The amount of the grant was approximately

\$23,000.00 with a commitment from the City to provide and an additional \$10,000.00 in the form of staff resources for the project.

As Part of the project three stations were identified. Those stations are Liberty, Trolley and 900 East. The goal was to create stationary plains that promote more mixed use development in the area.

Ms. Pickering described the public outreach efforts, stating that there were four public workshops and staff met with various local community groups and developers to gain input. Staff attended four Community Council meetings, two with the Central City group and two with the East Central group. Over all feedback had been positive with only a few negative comments.

Ms. Pickering stated that the proposed changes to the Central Community Masterplan were minimal and the changes were to the existing Transit Oriented Development (TOD) section. The 400 South project and station area plans were being modeled after the existing categories and zoning regulations that are currently in place for the North Temple area. This was being done so that there was consistency between the regulations that were in place for the transit corridors throughout the City.

Ms. Pickering noted that staff was recommending that as part of the master plan amendment and rezoning process, staff was recommending that the boundaries of the Central City Historic District be modified. An analysis had been completed that showed within the blocks on the north and south side of 400 South between 500 East and 700 East, there were few historic resources remaining and therefore the Historic Overlay may not be the most appropriate tool to regulate new development on these blocks. The entire two blocks bounded by 400 and 500 South between 500 and 700 East are proposed to be removed. The two blocks between 300 and 400 South and 500 and 700 East will be partially removed. This will be presented to the Historic Landmark Commission for their input.

Ms. Pickering asked for input from the Planning Commission regarding the modification of some of the RMU district standards and would like feedback on the following:

- Increase maximum allowable height five feet in both zones. Traditionally, mixed-use buildings are designed with a distinct base, which is larger and designed to accommodate commercial or office uses, a middle which is generally residential in nature, and a top, which is also residential but designed with architecturally appealing features such as parapets and cornices. In order to ensure proper scale between the first floor and the upper floors, staff is proposing that the first floor ceiling height of a mixed use building be at least 14 feet. This will ensure that retail/commercial space on the first floor is properly designed to accommodate uses that serve the community.

- Elimination of minimum lot area requirement for multi-family dwellings. This provision will promote the development of smaller lots that may accommodate appropriately scaled development but do not meet the minimum lot size requirements.
- Elimination of qualifying provisions for multi-family units as it relates to density calculations, (i.e., 9,000 square feet of lot area for first three units and 1,000 square feet for each additional unit). Development constraints would be primarily based on the petitioner's ability to meet maximum height limits, setbacks, and parking.
- Creation of building design standards. These standards are based on the Small Neighborhood Business standards created by the Planning Commission last August. Their purpose is to promote human scaled massing, varied articulation and traditional façade design.
- Creation of site specific standards for 500 South corridors between State Street and 700 East. These standards are intended to maintain the existing pedestrian/low impact design of the street while allowing the increased density that would come with the RMU zones.

[5:43:38 PM](#)

Questions from the Commissioners:

Commissioner Adams asked about the elimination of minimum lot area requirement for multi-family dwellings and asked for an example and why it was a good idea.

Ms. Pickering responded that some of the properties that have the RMU zoning are small and there is a minimum for each of the dwelling units in the zoning designation, lots need to be a minimum size to be developed.

Commissioner Woodhead stated that she felt that the items presented for discussion were generally good ideas she like the idea of the height increase and density, that it made sense for the area and was headed in the right direction.

Ms. Pickering asked for input regarding the area surrounding Trolley Square. She noted that there had been negative input from the community in the area, and the intent was to help Trolley Square if they should ever have plans to expand or improve the parking lot. It could not currently happen because it does not have the proper zoning.

Commissioner Gallegos asked if Trolley Square wanted to develop in that area, or if there was concern about developing near single family housing to the south.

Ms. Pickering responded that the question was whether or not they should change the zoning or leave it as it was today, which was Residential Multi-family zoning district which would not allow any retail or mixed use.

Commissioner Hill stated that it was apparent that there was a need for high density there and a zoning change, and wondering what the negative feedback was.

Ms. Pickering responded that the concerns were with the proposals south of Trolley Square, that the zoning change would derogate the historic resources that were there because of the nature of the development. Other feedback was near Gilgal Gardens and concerns about the density classifications proposed.

**7.C. PLANNING COMMISSION
APRIL 25, 2012 PUBLIC HEARING**

PLANNING COMMISSION STAFF REPORT
Legislative Item



Planning Division
Department of Community
and Economic Development

400 South Livable Communities
Master Plan, Zoning Map and Text Amendments
PLNPCM2010-00647
April 25, 2012

Applicant: Mayor Ralph
Becker

Staff: Maryann Pickering
801-535-7660 or
maryann.pickering@slcgov.com

Tax ID: N/A

Current Zone: Various – see
page 2 for current zoning map

Master Plan Designation:
Various – see Attachment B

Council District: District 4
represented by Luke Garrott

Community Council: Central
City and East Central

Lot Size: N/A

Current Use: N/A

Applicable Land Use
Regulations:

- List all applicable regulation citations

Attachments:

- Existing and Proposed Zoning Map
- Existing Master Plan Map Designations
- Proposed Zoning Text Amendment Changes
- Proposed Master Plan Amendment Changes
- Public Input

Request

A request by Salt Lake City Mayor Ralph Becker for an amendment to the Central Community Master Plan, Salt Lake City Zoning Ordinance and Zoning Map regarding transit station area plans along the 400 South transit corridor. The intent of the project is to implement livability goals for the corridor including land use policies and zoning that support mixed use development and transportation choices for current and future residents and workers. The Planning Commission is required to transmit a recommendation to the City Council for Master Plan and Zoning Map Amendment Requests.

Recommendation

Discuss the proposed changes and continue the public hearing to a future meeting date.

Background Project Description

In the fall of 2010, the Community & Economic Development Department was awarded a matching grant from the Department of Housing and Urban Development (HUD) to undertake a planning process for a portion of the University TRAX line. The amount of the grant from HUD was \$22,620 with a commitment from the city to provide an additional \$10,000 in the form of staff resources for the project. The primary purpose of the grant was to reach out to individuals and groups who do not normally participate in the planning process and to determine if the existing policies and regulations were capable of fulfilling the goals listed below or if changes were needed:

- a. Provide more transportation choices.
- b. Promote equitable, affordable housing.
- c. Enhance economic competitiveness.
- d. Support existing communities.
- e. Coordinate policies and leverage investments.
- f. Value communities and neighborhoods.

Starting in March of 2011, Planning Division staff began working on a master plan amendment and rezoning project for the University TRAX line. Three stations were identified to be included as part of the rezoning process. Those stations are: Library, Trolley and 900 East.

In addition to the above, this project assists in the implementation of the Wasatch Choices 2040 Plan, which is a regional plan created by residents, businesses, and other stakeholders to manage future growth in the region by focusing it on certain nodes, particularly those where transit facilities already are in place. Further, Planning Division staff felt that this area needed to be studied for a potential change at the current zoning because despite the current zoning of Transit Corridor District (TC-75), very little private investment has been made in the area since the TC-75 zoning was created in 2005 and existing zoning east of 600 East was not consistent with the Central Community Master Plan.

Public Notice, Meetings and Comments

The following is a list of public meetings that have been held related to the proposed project. A summary report that contains comments from most of the public meetings can be found on the projects webpage at <http://www.slcclassic.com/CED/planning/400South/>.

- Community Council meetings held on the following dates:
 - Central Community – April 6, 2011 and March 7, 2012.
 - East Central Community – May 12, 2011 and March 8, 2012.
 - Comments and notes can be found in attachment E.
- Community Workshops held on the following dates:
 - Salt Lake City Main Library – May 19, 2011
Approximately 5,400 owners and tenants within a ½ radius of the three transit stops by a postcard mailed to their address. Various flyers were posted throughout the area also.

- Salt Lake City Main Library – October 3, 2011
An email notice was sent through the listserve and also to anyone who had attended the first community workshop.
- Trolley Square – February 29, 2012
Approximately 1,350 postcard notices were mailed to the owners and occupants whose property was proposed to be changed or was located within 300 feet of a property proposed to be changed for the February and March open houses.
- Planning Division Open House – March 15, 2012
- Comments and notes can be found in attachment E.
- Stakeholder and Community Organization Meetings on the following dates:
 - University of Utah Transportation Meeting – June 27, 2011
 - YWCA – July 7, 2011
 - Refugee Services – July 21, 2011
 - Property Owner/Developer – August 10, 2011
 - Odyssey House – August 26, 2011
 - East High School AP US Government Class – April 11, 2012
 - Several requests were made to Bennion Elementary school about having a presentation. The school was not interested in having a presentation.
- Public comments have been left on the project’s webpage during the course of May 2011 up until today.
- Various one-on-one meetings with local community members who have requested a meeting.
- Planning Commission briefing on March 29, 2012.
- Historic Landmark Commission briefing on April 5, 2012. The purpose of the briefing was to introduce the overall project to the Historic Landmark Commission and to obtain their feedback on the proposed zoning map and Central City Historic District boundary changes. There was much discussion about the former and it was decided by staff that it this time it would be best to only change the zoning in the historic district for properties that are located along 400 South and a few along 700 East/300 South. The Commission also had concerns about the potential height of buildings in the historic district. Staff has scheduled a follow up meeting to present further information and graphics regarding the proposed changes in the Central City Historic District.

In addition to the above public meetings or workshops, the item was placed on the City’s webpage in the ‘Open City Hall’ section and various comments were made about the project. The summary report from Open City Hall is included in Attachment E as well as the emails and letters we have received since the project was first introduced last May.

Notice of this public hearing for the proposal includes are noted below. A second public hearing will be held in ensuring all notification requirements are met and to give every affected property owner the opportunity to address the Planning Commission.

- Public hearing notice posted in newspaper on April 13, 2012.
- Public hearing notice posted on City and State websites on April 12, 2012.
- Public hearing notice emailed to the Planning Division listserve on April 12, 2012.

City Department Comments

Only three comments were received from pertinent City Departments / Divisions. The Planning Division has not received comments from the applicable City Departments / Divisions that cannot reasonably be fulfilled or that warrant denial of the petition.

Master Plan Findings

Findings

The City does not have specific standards for Master Plan Amendments. The Central Community Master Plan already includes a section on transit oriented development (TOD) and most of the corridor within the study area has a TOD future land use designation. The purpose of reviewing the Master Plan was to insure that the vision for the corridor was still valid, identify what transit oriented development means for the corridor and make necessary modifications to strengthen the vision identified during the public process.

In considering an amendment to the Central Community Master Plan as part of the 400 South Livable Communities project, Planning staff also analyzed the following documents related to land use:

- Salt Lake City Futures Commission Report (1998)
- Salt Lake City Urban Design Element (1990)
- Salt Lake City Community Housing Plan (2010 – currently under review by City Council)
- Salt Lake City Transportation Plan (1996)
- Central Community Master Plan (2005)
- Wasatch Choices 2040 (2011)

Salt Lake City Futures Commission Report

The Salt Lake City Futures Commission report is a citywide document that is general in nature. It includes a number of recommendations grouped by category. Those relevant to the project include:

- Arts and Culture
- Built Environment
- Economics
- Natural Environment
- Neighborhoods
- Social Environment

This document recommends providing adequate public spaces that are equipped to handle gatherings of various sizes at different locations throughout the City. Providing live/work space for artists is also recommended. The 400 South Livable Community Station Area Plans recommends a broad range of housing types, including live/work units near the light rail stations.

The Built Environment section identifies a number of key recommendations that are relevant to the 400 South Livable Communities Station Area Plan. Assertion A states:

“An integrated transportation system, including alternative modes of transportation such as pedestrian ways, bicycles, mass transit, freight vehicles and personal automobiles ensure the enjoyable movement of people and products within the City.”

The Station Area Plans identify all these modes of travel and recognize the importance of effectively managing each mode. The success of each area depends on the efficient movement of people and goods.

Assertion B creates a hierarchy upon which urban design should be based:

1. Focus on the needs of the pedestrians and bicycles first;
2. Focus on mass transit second; and
3. Focus on the automobile third.

This section continues by saying: “public transit systems such as light rail are user friendly and designed with the pedestrian in mind; and all citizens have access to public transit within 1,200 feet of their homes.” By strategically focusing future growth and development around transit stations, current and future residences and workers will have better access to transit.

This section introduces the importance of design and mentions that high aesthetic standards, integrating urban design and building design, having streets with character and unique neighborhoods contribute to a more livable City and nurture a strong community. The Station Area Plans include a number of policies and strategies that attempt each of the assertions in the Built Environment section.

An important aspect of the Futures Commission report is identifying that all people have a stake in the planning and building of the City. From the beginning of the planning process for 400 South, Planning Division staff has intended for this plan to be based on community input, especially from those who are traditionally under-represented in the planning process. The broad participation that was received in this process is an important aspect of the plan and important to the long term success of the plan. It should be noted that the Planning Division set a goal of engaging at least 200 people as part of the outreach project with at least 60 participants being from groups who are traditionally under-represented in the planning process. The Planning Team exceeded this goal. While the numbers are modest, they establish a baseline for future planning activities in the City and hopefully a turning point in how the City engages its residents, property owners, business owners, and service providers.

The Economics section of the Futures Commission report identifies that planning and zoning are important economic development tools. Many of the policies, strategies and key projects are aimed at promoting economic development along the corridor to support the business community, enhance the neighborhoods, project the City’s tax base, and improve the economic condition of the neighborhoods along the corridor and the City as a whole. Other parts of the Economic Development section discuss:

- Light rail being critical to the transportation system as well as improving air quality; and
- Promoting housing and mixed use development throughout the City.

The Natural Environment section focuses on air and water quality, solid waste management, open space, and gateways. The 400 South Livable Communities Station Area Plans addresses these issues by promoting compact development that uses less land and provides people with options on how they move, where they live, and where they shop, dine, work and play. The plans also allow for the creation of new open spaces.

Neighborhoods are the backbone of any city and the neighborhoods along 400 South are no exception. Preserving the neighborhoods in the area provide a foundation for future development in the area. With the anticipated growth in Salt Lake City, future development and residential density should be strategically located so that the existing neighborhoods are preserved. By concentrating new development near the existing transit stations, the City can adequately provide services to new development and preserve the neighborhoods at the same time. Providing a range of housing options

helps to create diverse neighborhoods and provides people with different need options as to where they live.

The Social Environment section defines itself as “everything in our society that improves our lives, expands our minds, and helps us to be healthy, caring, educated and productive citizens”. This section has recommendations related to promoting community involvement, expanding recreational opportunities, and addressing issues that impact our community. The station area plans project on some of these principles and has had an extensive public involvement process. Providing adequate housing for people with different needs, providing transportation options and enchaining our open spaces and access to our trail system improves our community.

Salt Lake City Urban Design Element

The purpose of the Urban Design Element is to define urban design objectives for the City and illustrate a process for making decisions regarding the City’s future character. To that end, the Urban Design Element recommends a number of policies and strategies. A key strategy is to recognize that land use intensities and building heights should reflect relationship between the district that they are located within and adjacent neighborhoods and their respective role in the City. The document also states “indiscriminate high rise construction outside of the downtown core adversely affects the strong downtown development concentration characteristic of the City.” The station area plans recognize this concept by limiting building height to a level that transitions from the core of a stations down towards existing neighborhoods. In addition, the building height complements Downtown by having lower building heights and continues the downward progression of building height between Downtown and the 900 East Station while allowing adequate development potential to accommodate future growth within the corridor. The Urban Design Element lists many other policies and strategies that are relative to 400 South and addressed in the station area plans, including:

- Allowing individual districts to develop in response to their unique characteristics within the overall urban design scheme for the City;
- Treat building height, scale and character of significant features of a districts image;
- Ensure that features of building design such as color, detail, materials and scale are responsive to district character, neighborhood buildings and the pedestrian;
- Maintain a pedestrian-oriented environment at the ground floor of buildings;
- Introduce pedestrian-oriented elements such as landscaping, sidewalk lighting, pedestrian oriented building and site design into neighborhood commercial centers;
- Use street spaces, patterns and rhythms to unify the image of a district;
- Preserve prominent view corridors and city vistas. Prominent land forms, buildings and monuments should remain clearly visible as city landmarks. Special attention should be given to the design of building adjacent to prominent streets and vista corridors.
- Encourage pedestrian walkways networks that connect individual buildings, blocks, groups of blocks and entire districts; and
- Require new buildings to respect the pedestrian elements of the street.

The 400 South Livable Communities Station Area Plans provide additional guidance for land use decisions and includes policies which complement the Urban Design Element. The Station Area Plans provide more focus of the urban design concepts because there are specific urban design policies related to each station area.

Salt Lake City Community Housing Plan

The goal of the Community Housing Plan is to enhance, maintain and sustain a livable community that includes a vibrant downtown integrated with surrounding neighborhoods that offer a wide range of housing choices, mixed uses and transit oriented design. The key concepts addressed in the Housing Plan include:

- Foster and celebrate the urban residential tradition;
- Respect the character and charm of predominantly residential districts, including those with historic character and qualities, while also providing opportunities for the provision of local goods and services easily accessed by neighborhoods;
- Promote a diverse and balanced community by ensuring that a wide range of housing types and choices exists for all income levels, age groups, and types of households;
- Develop new housing opportunities throughout the City;
- Ensure that affordable housing is available in all neighborhoods and not concentrated in a few areas of the City;
- Emphasize the value of transit-oriented development, transit accessibility and proximity to services;
- Recognize that residents, business owners, and local government all have a role to play in creating and sustaining healthy neighborhoods;
- Create an appropriate balance of rental and ownership opportunities in neighborhoods without jeopardizing an adequate supply of affordable housing; and
- Strongly incentivize or require the use of green building techniques and sustainability practices in public and private housing developments.

The Station Area Plans include a number of policies that support the above concepts. The development concepts identified in the station area plans include a major focus on creating a range of housing options for people with different housing needs. The plan also discusses preserving existing housing in existing neighborhoods.

Salt Lake City Transportation Plan

The Salt Lake City Transportation Plan includes policies related to all forms of transportation, including automobile, mass transit, pedestrians, and bicycles. The plan correctly identifies the important link between transportation and land use and provides the following relevant direction for future land use:

- Salt Lake City will preserve and enhance residential communities within the City which allow residents to live, work and play in the same area;
- Salt Lake City will explore opportunities to increase residential and destination densities at major bus and rail transit nodes along transit corridors;
- Salt Lake City will promote development that is transit, pedestrian and bicycle friendly.

The Station Area Plans are consistent with this direction and aim at providing opportunities for land use to support mass transit and vice versa. The transportation plan provides direction for increasing the number of bicycle lanes within the City and maintaining those lanes to a high standard. The Station Area Plans indicate that finer grain network of bicycle paths and trails will be warranted as the station areas develop over time and bicycle use increases. While 400 South may not be able to provide all modes of transportation in a safe and convenient manner, it should be viewed as a portion of a network, with nearby parallel streets providing other opportunities, particularly for bicyclists.

Central Community Master Plan

The Central Community Master Plan was adopted in 2005. It identifies and discusses transit oriented development corridors along 400 South and notes transit oriented development districts create a walkable environment that encourages residents and employees to use modes of transit rather than the automobile. The Station Area Plans will implement that vision for the community that has been in effect for some time.

Wasatch Choices 2040 Plan

The Wasatch Choices 2040 Plan is a four county vision for land use and transportation in the future. Although not an officially adopted plan of the City, it includes many of the same goals discussed in the plans listed above and helps identify Salt Lake City's role in the region and the state. The plan states "over the coming years, the Wasatch Front is expected to annually add a population comparable to the city of Murray, or about 34,000. Growth in our region is largely inevitable; over two-thirds of this population will come from our children and grandchildren. Our challenge is to preserve or even enhance quality of life in the face of growth." With this statement in mind, the plan contains specific principles and objects for transportation planning, some of which are noted below.

- Optimize use and maintenance of existing infrastructure.
- Promote compact development consistent with market demand.
- Encourage contiguous growth to reduce infrastructure expenses.
- Develop a balanced, multi-modal transportation system.
- Coordinate transportation with regional employment, housing, educational and activity centers.
- Encourage future commercial and residential areas within close proximity of each other to reduce travel distances.
- Encourage a balance of jobs and housing in each part of the region to reduce travel distances.
- Support actions that reduce growth in per capita vehicle miles of travel.
- Make land-use and transportation decisions based on comprehensive understanding of their impact on each other.
- Encourage land use and housing policies to accommodate the need for a variety of housing types throughout the region.
- Encourage housing and other development near transit to maximize the efficiency of the public transportation system.

The 400 South Livable Communities Station Area Plans provide additional guidance for land use and transportation policies noted above. The station area plans recognize the growth will be occurring over the next several years and that compact development that utilizes existing investments in infrastructure is the best way to approach the increase.

Master Plan Summary

The proposed 400 South Livable Communities Station Area Plans are generally consistent with the policies and guidelines of the listed city and regional wide plan along with the adopted Community Master Plan. The Station Area Plans provide finer detail, are more specific to geographic areas and provide adequate guidance on future land use decisions. It is critical that future zoning be compatible with the Station Area Plans, reflect the communities' vision for each station area and can provide the necessary flexibility, processes and regulations to produce desired development. The plans provide for appropriate height, densities, and land use intensities in various geographic sections along the corridor.

These policies are important to achieve the City's goals of environmental, economical and socially sustainability as well as ensure the large public investment in infrastructure along 400 South is effective in revitalizing this area of the city and providing for the needs of the residents, business community and other stakeholders in the area.

Zoning Amendments Analysis and Findings

Background

Prior to 2005, the majority of the 400 South Corridor was zoned in a manner that promoted auto oriented development. The CC Commercial Corridor zoning district was mapped from about 250 East to 900 East. Some CS Community Shopping zoning was mapped on the 600 East blocks of 400 South. This zoning district was reflective of the development patterns that were created as cities, including Salt Lake City, planned around and for the private automobile. With the construction of the University Light Rail Line, the City acknowledged that the zoning in the corridor should be changed. In 2005, the City adopted Transit Corridor Zoning (TC-75) between 250 East and 600 East. For various reasons, no changes were made east of 600 East despite the Central Community Master Plan identifying the area as Transit Oriented Development on the Future Land Use Map. One of the goals of this project was to revisit the zoning along the corridor, particularly east of 600 East and work with stakeholders to identify the issues and formulate a plan and zoning modifications to implement the vision identified. Based on the public process and staff analysis, the proposal is to utilize the Transit Station Area Urban Neighborhood east of 400 East and Transit Station Area Urban Center west of 400 East.

A majority of the properties proposed to be changed are Transit Corridor District (TC-75), Moderate Density Multi-Family Residential (RMF-35) CS Community Shopping and CC Commercial Corridor. There are a few other zoning designations throughout the corridor. All of these existing zoning districts are considered Euclidian based zoning districts, which are based off of the theory of separating incompatible uses. However, due to new technology, building techniques and societal norms, certain segments of the population desire to live in mixed-use neighborhoods where only the most impactful types of uses are not allowed. From an environmental and public expenditure standpoint, the City encourages mixed use development. In order to implement the vision identified in the Station Area Plans by the Planning Division staff, the current zoning designations were reviewed and found that those existing zoning districts were not capable of implementing the station area plans. Even the more recently approved TC-75 for the corridor has not seen any reinvestment in the neighborhood. Therefore, Planning staff determined that the Transit Stations Area (TSA) zoning designations that were already part of the zoning ordinance were the best designations to implement the vision of transit oriented development and mixed use projects.

The TSA zoning regulations are intended to regulate the physical form of buildings and how they relate to transit, the street, public spaces, adjacent buildings, and adjacent neighborhoods. Less emphasis is placed on use. A table of prohibited uses lists those uses that are not allowed. All other uses would be permitted.

Design and Development Standards can be found within the existing TSA zoning district. These are regulations that are required of all projects. These include items such as building setbacks, height requirements, parking locations, and minimum design requirements such as first floor glass, the location of doors and entrances, etc.

Instead of using the traditional permitted and conditional uses to determine review and approval processes, the proposed ordinance intends to incentivize new development and redevelopment through a

series of Development Guidelines. These Guidelines include a value which is based on several factors, including the importance of that guideline towards implementing the policies of the station area plans or other adopted policies and goals of the City and the cost to include specific guidelines in a project. Each project is reviewed and assigned a Development Score. The Development Score is the sum of the values assigned to each Development Guideline.

Following the Guidelines is voluntary and is not required for any project. However, proposed projects will be reviewed based on the value of the Guidelines the applicant chooses to incorporate into the project. A series of thresholds have been identified to guide the process. Those projects that choose not to include any of the Guidelines or that choose to only include a minimum number (Tier 1) will be processed following the existing Conditional Building and Site Design Review process. Those projects that obtain a Development Score within Tier 2 will be processed using the Administrative Hearing process already authorized in the Zoning Ordinance. Projects that are assigned a Development Score in Tier 3 will be processed as over the counter projects after the Development Score is assigned. Tier 3 projects are deemed to be the most compliant with the vision for a specific station area. Because the vision, urban design framework, policies and strategies were developed in an extensive public process, public participation process is put at the beginning of the development process, instead of as a reaction to individual developments. The premise behind the tiered process is that desirable development, as indicated in the station area plans, should be easier to realize than less desirable development.

Additional regulations are required for any development that exceeds five acres. The primary purpose of this is to ensure that circulation and connectivity issues are addressed. The design of an internal circulation system and connectivity points to public rights of way are subject to approval from the Transportation Division. This would not be a departure from our current review process, but would provide guidance to developers in terms of what is expected.

The Development Guidelines were developed as part of the TSA rezoning process for the North Temple or Airport TRAX line corridor. They are a collection of design and development concepts that were either identified through the planning process or were already identified as a policy or goal of the City in a Policy document that applies to the area or the City as a whole. Each guideline includes a description of what is desirable, images that are to be used as examples, and a way to measure how the guideline has been implemented. For example, the Design Guideline for Density and Intensity of use includes specific density qualifications (number of dwelling units per acre), building height measures (reaching a certain percentage of the permitted building height) and floor to lot area ratios to determine if a project complies with the Design Guideline and whether it can count towards the Development Score.

The overall goals of the proposed zoning ordinance are:

- Put public participation up front in the planning process, instead of as a reaction to individual developments.
- Make it easier for desirable development to be processed.
- Create options for applicants in terms of how they design their project and how they want to be processed.
- To have the option of predefined outcomes.

Zoning Text Amendment

The proposed changes as part of this petition to the existing zoning text found in Chapter 21A.26.078 are

minimal. The TSA zoning district has been part of the zoning ordinance for a few years now. Several projects along the North Temple/Airport TRAX line have been reviewed with these standards and staff has found that few changes are needed.

One of the changes proposed to be made are some general clean up of some typographical errors that have been found in the chapter. Other changes include adding specific standards for the 400 South Livable Communities corridor as there are some different types of development adjacent to the street where the light rail is located. The biggest change is new language that ensures an additional setback is in place whenever a project in a TSA zoning district is adjacent to a low density residential or open space zoning district. Staff felt that this additional setback is necessary to protect the existing neighborhoods from new development and to allow adequate light and solar access.

The table of prohibited uses has been rearranged so that it is more consistent with other terms in the zoning ordinance. Some uses have been added to the table as it has been evaluated by staff that additional uses were necessary in order to maintain more transit and not automobile, oriented uses within all TSA zoning districts.

Zoning Map Amendment

The purpose of the zoning map amendment is to put the regulations in place that will guide new development and redevelopment towards implementing the 400 South Livable Communities Station Area Plans. The proposed zoning map amendment would follow the station area boundaries identified in the station area plans.

The primary concerns with any large scale zoning map amendment are the potential impacts it has on existing businesses and property values.

This ordinance does not impact existing businesses. All existing businesses that are listed as prohibited uses would be considered legal, nonconforming land uses. These uses are allowed to continue operating. These uses will become subject to zoning ordinance section 21A.38 Nonconforming Uses and Non-complying Structures. Under this section, non conforming uses are authorized to continue. There are specific regulations that govern the moving, enlarging or altering of nonconforming uses of land and structures. If destroyed by fire, earthquake or other natural disasters, a nonconforming use would be allowed to occupy a new building on the site. The most impacted land uses are those that include drive through windows, gas stations, and auto service types of uses. Those uses are all either permitted or conditional under the current zoning. They will be prohibited in the proposed ordinance.

In most cases, the development potential of land east of 600 East will increase. The development potential is based on what can be done with the property under existing zoning regulations. This will vary depending on the existing zoning and the proposed zoning. Typically, a property with more development potential will have a higher property value. The zoning of a property does impact the real estate value of the property. When other market influences are considered, it is difficult to determine the long term impact on property values. However, it is not anticipated that changing the zoning of these properties will, by themselves, decrease property values. Properties west of 600 East are mostly zoned TC-75, which permits buildings up to 75 feet in height and allows building up to 125 feet as a conditional use. The proposal reduces the conditional height in the Urban Neighborhood areas to 75 feet and 90 feet in the Urban Center neighborhood. Projects in the Urban Center areas that meet certain thresholds may add up to two additional stories of building height.

The impact of taller and more intense development has been raised as a concern, although it has not been as big of a concern as anticipated. Regardless, the boundaries of the Core and Transition Areas were drawn after considering many factors. The primary factor was the proximity to a transit platform. The Core Areas are all relatively close to a platform, while Transition Areas are further away. The Transition Area was created in order to step down development height and intensity as it gets closer to existing low density residential neighborhoods. Streets, which are typically 132 feet wide in most areas along 400 South, were used as boundaries where possible. As stated in the zoning text amendment sections, regulations requiring an increased setback when adjacent to residential zoning districts are included as an additional protection to the impacts of height and intensity. In addition, increased setbacks have been incorporated when a TSA designated property is located adjacent to a street which is less than 50 feet in width and includes certain residentially zoned property.

Findings

21A.50.050 Standards for general amendments.

A decision to amend the text of this title or the zoning map by general amendment is a matter committed to the legislative discretion of the city council and is not controlled by any one standard.

A. In making its decision concerning a proposed text amendment, the city council should consider the following factors:

- 1. Whether a proposed text amendment is consistent with the purposes, goals, objectives, and policies of the city as stated through its various adopted planning documents;**

Analysis: In reviewing the station area plans, several adopted master plans were considered, including the Urban Design Element, the Salt Lake Futures Commission Report, the Central Community Master Plan, and the Wasatch Choices 2040 Plan. The City's adopted Housing Plan and Transportation Plan also call for the type of development supported in the station area plans. The analysis of the station area plans indicated that they were generally consistent with these plans or explained a change in policy to those plans.

Finding: The proposed zoning text changes are consistent with the goals and policies identified in the companion station area plans and several other adopted master plans.

- 2. Whether a proposed text amendment furthers the specific purpose statements of the zoning ordinance;**

Analysis: The proposed changes enhance an existing chapter of the zoning ordinance, with a specific purpose statement. The general purpose statement of the zoning ordinance is to promote the health, safety, morals, convenience, order, prosperity and welfare of the present and future inhabitants of the City. In addition, the zoning ordinance is intended to lessen congestion in the streets, secure safety from fire and other dangers, provide adequate light and air, classify land uses and distribute land development and utilization, protect the tax base, secure economy in government expenditures, foster the City's industrial, business and residential development and protect the environment.

Finding: The proposed zoning ordinance furthers the specific purpose statements of the zoning ordinance.

3. Whether a proposed text amendment is consistent with the purposes and provisions of any applicable overlay zoning districts which may impose additional standards; and

Analysis: The existing zoning ordinance includes a statement that any applicable overlay zone supersedes the standards in the proposed ordinance. This section is not proposed to be modified as part of this petition. This is particularly relevant to the Central City Historic District, which includes 400 South between 500 and 700 East. Although the proposal includes changing the base zoning from TC-75 and CS in this area, the properties within the Historic District would still be subject to the H Historic Overlay regulations.

Finding: The existing zoning ordinance is consistent with the purposes of any applicable overlay zoning district.

4. The extent to which a proposed text amendment implements best current, professional practices of urban planning and design.

Analysis: The proposed changes continue to represent a new approach to zoning for Salt Lake City. This approach recognizes the value and importance of community input, the needs of developers and establishes an opportunity for the City, through private investment and development, to promote sustainable development practices, increase the housing stock, promote the business community, increase the use of alternative forms of transportation and improve public spaces.

Finding: The proposed changes continue to show how Salt Lake City is one of the few cities in the nation to implement this type of zoning, rather than the traditional Euclidean zoning that is widely used.

B. In making a decision to amend the zoning map, the city council should consider the following factors

1. Whether a proposed map amendment is consistent with the purposes, goals, objectives, and policies of the City as stated through its various adopted planning documents;

Analysis: In reviewing the proposed zoning map changes, several adopted master plans were considered, including the Urban Design Element, the Salt Lake Futures Commission Report, the Central Community Master Plan, and the Wasatch Choices 2040 Plan. The City's adopted Housing Plan and Transportation Plan also call for the type of development supported in the station area plans. The analysis of the station area plans indicated that they were generally consistent with these plans or explained a change in policy to those plans.

Finding: The proposed zoning map amendments are consistent with the goals and policies identified in the station area plans and several other adopted master plans.

2. Whether a proposed map amendment furthers the specific purpose statements of the zoning ordinance;

Analysis: The proposed zoning map amendment includes provisions for reducing the impact new development may have on existing areas. The boundaries of the proposed zoning district correspond to the boundaries in the station area plans. The station area plans identify the vision for what the areas around the transit stations should look like, how they work, what types of uses there are, etc. The existing character of the subject areas differs from what is identified in the long term vision for the area. Therefore, the important aspect to consider is the impact on those areas that are adjacent to the proposed zoning district boundaries.

Finding: The proposed zoning map amendments further the specific purpose statements of the zoning ordinance.

3. The extent to which a proposed map amendment will affect adjacent properties;

Analysis: The proposed amendment would affect those properties that are within the boundaries of the TC-75, CS, CC and RMF-35 zoning districts by rezoning some of these properties to TSA-UC and TSA-UN. The proposed zoning district would decrease some of the development potential of some properties and for others only increase it slightly. However, this should not be viewed as an adverse impact because the proposed regulations that allow similar or decreased scale development are consistent with what was identified through the public planning process as desirable development. As properties redevelop, there will be instances where a new project is considerably larger than what may be adjacent to it. The adverse impacts are more relevant where the proposed zoning district is adjacent to an area that will not be rezoned and has smaller mass and scale regulations than the proposed ordinance. The proposed ordinance contains provisions to reduce the impacts in these situations, such as increased setbacks than what currently exists, stepping of certain setbacks as the building height increases and more design standards than the current zoning requires. The intent of the proposal is to allow more building density and intensity along 400 South and step that density and intensity down as one moves closer to lower density residentially zoned areas.

Finding: The proposed zoning map amendment will have a minimal affect on adjacent properties due to the proposed zoning district containing provisions to reduce to impacts of the scale and mass of potential adjacent development.

4. Whether a proposed map amendment is consistent with the purposes and provisions of any applicable overlay zoning districts which may impose additional standards; and

Analysis: The proposed zoning map amendment is consistent with the purposes of any applicable overlay zoning district. The proposed zoning ordinance includes a statement that any applicable overlay zone supersedes the standards in the proposed ordinance. The Historic Preservation Overlay District is the only district within the boundaries of the proposed map amendments. Because the H Historic Preservation Overlay District

applies, the Historic Landmark Commission has the authority to approve the overall design of new construction and major additions when they occur between 500 East and 700 East and they have the authority to modify certain bulk and mass regulations to insure that the proposals do not negatively impact the historic character of the Historic District.

Finding: The proposed map amendments will be consistent with any existing or future overlay district within the boundaries of the changes.

5. The adequacy of public facilities and services intended to serve the subject property, including but not limited to roadways, parks and recreational facilities, police and fire protection, schools, stormwater drainage systems, water supplies, and wastewater and refuse collection.

Analysis: The project area is located within areas that are already served by public facilities and services. However, the proposed ordinance does increase the development potential of the area in some instances and decreases it others. Population, employment and household projections for the corridor indicate an increase in all three categories. These projections were done under the current zoning regulations. The capacity of the road is not anticipated to be greatly impacted, at least initially, due to the change in zoning. The desired type of development and the development promoted by the proposed ordinance is considered transit oriented development, which can reduce the need to use private automobiles. Other measures in the ordinance create incentives, primarily through a quicker review process, for other measures that reduce the energy use of new buildings, storm water runoff, and other sustainable measures. The proposed ordinance has been routed to other Departments and Divisions for comments. No comments were received that would indicate that the City would not be able to serve new development.

Finding: There appear to be adequate facilities in place to serve the boundaries of the proposed project.

Commission Options

The proposed 400 South Livable Communities Station Area Plans project is a reflection of the community's vision for 400 South. The creation of the plan was done with the intent of incorporating major themes identified through the public process. Once these items were identified, a series of best practices that were applicable to the community's vision were incorporated into the plan to guide future development in a manner that can help turn the community vision into reality. While there are many options in terms of how to address land use, the draft station area plans represent the preferred option of the community and Planning Division staff. Other options are:

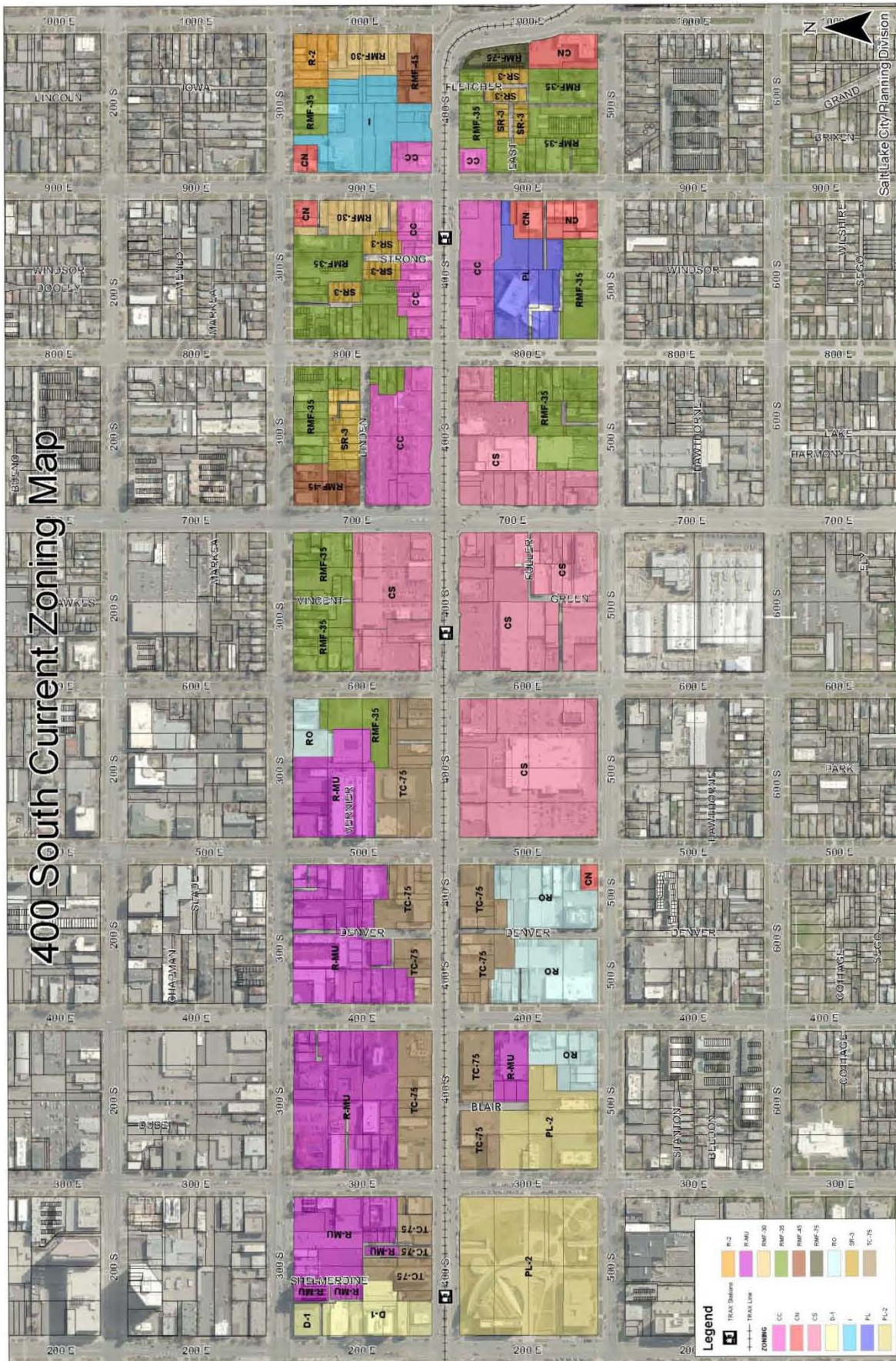
- Make no changes to the existing master plan and development regulations and allow development to continue in the manner that it currently is;
- Make consistent changes that would apply to the entire corridor; and
- Make limited changes to 400 South to the areas closest to the light rail stations.

After analyzing the comments from the community, the desire for a different type of development along 400 South eliminated the option to make no changes. If the proposed station area plans are not adopted,

the existing policies and regulations would remain in effect. Community input and existing conditions indicate that there are unique situation and characteristics area each station and that a one size fits all approach could not capitalize on the unique assets at each station. Making limited changes near the station areas would not provide enough land area to accommodate future projected growth.

Attachment A

Existing and Proposed Zoning Map



Attachment B

Existing Master Plan Map Designations

Attachment C

Proposed Zoning Text Amendment Changes



21A.26.078: TSA TRANSIT STATION AREA DISTRICT:

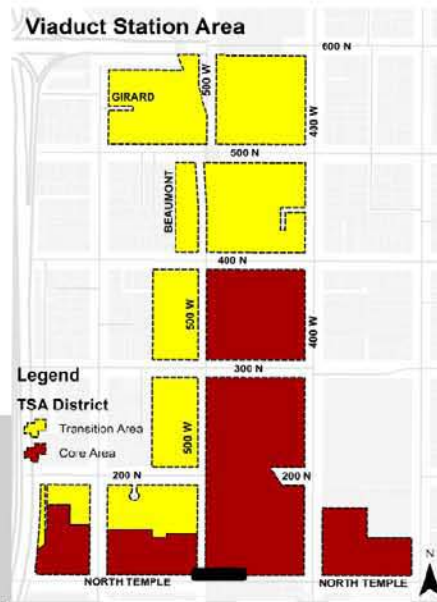
A. **Purpose Statement:** The purpose of the TSA transit station area district is to provide an environment for efficient and attractive transit and pedestrian oriented commercial, residential and mixed use development around transit stations. Redevelopment, infill development and increased development on underutilized parcels should include uses that allow them to function as part of a walkable, mixed use district. Existing uses that are complementary to the district, and economically and physically viable, should be integrated into the form and function of a compact, mixed use pedestrian oriented neighborhood. Each transit station is categorized into a station type. These typologies are used to establish appropriate zoning regulations for similar station areas. Each station area will typically have two (2) subsections: the core area and the transition area. Due to the nature of the area around specific stations, the restrictions of overlay zoning districts, and the neighborhood vision, not all station areas are required to have a core area and a transition area.

1. **Core Area:** The purpose of the core area is to provide areas for comparatively intense land development with a mix of land uses incorporating the principles of sustainable, transit oriented development and to enhance the area closest to a transit station as a lively, people oriented place. The core area is generally within a one-fourth ($\frac{1}{4}$) mile walk of a transit station platform. The core area may mix ground floor retail, office, commercial and residential space in order to activate the public realm. Buildings in this area should have minimal setbacks to encourage active outdoor use adjacent to the sidewalk, such as outdoor dining and patios that reflect the desired character of the area. Building facades should be varied and articulated, include storefronts adjacent to the street, windows on the street level, and have clearly defined entrances to provide visual interest to pedestrians. Buildings should be a minimum of two (2) or three (3) stories in height, depending on location, in order to define the street edge. Arcades, bays, and balconies are encouraged. The configuration of buildings must balance the needs of all modes of circulation with the safety and comfort of pedestrians and bicyclists. A vertical mix of uses, with office and residential above ground floor commercial uses is encouraged. A minimum of thirty (30) dwelling units per acre is encouraged within the core.
2. **Transition Area:** The purpose of the transition area is to provide areas for a moderate level of land development intensity that incorporates the principles of sustainable transit oriented development. The transition area is intended to provide an important support base to the core area and transit ridership as well as buffer surrounding neighborhoods from the intensity of the core area. These areas reinforce the viability of the core area and provide opportunities for a range of housing types at different densities. Transition areas are generally located within a one-half ($\frac{1}{2}$) mile from the station platform, but may vary based on the character of the area. Transition areas typically serve the surrounding neighborhood; include a broad range of building forms that house a mix of compatible land uses. The minimum desired density is ten (10) dwelling units per acre. Commercial uses may include office, retail, restaurant and other



commercial land uses that are necessary to create mixed use neighborhoods. Commercial uses can be clustered around intersections and along block faces to create neighborhood nodes.

- B. **Station Area Types:** A station area typology is the use of characteristics, such as building types, mix of land use, transit service and street network to create generalizations about an area that can be used to define a common vision for development of a transit station area. Each typology recognizes the important difference among places and destinations and takes into account the local context of a station and its surroundings. Each station area typically will include a core area, where the most intense development will occur, and a transition area, which is intended to create a buffer area between the core and those areas with generally lower intensities and densities. Prior to classifying a transit station into a specific type, a [station area specific area](#) plan must be adopted by the city council prior to applying this zoning district to a geographic area. Only those stations that have an adopted [station area](#) plan [that is supported by the regulations in this section](#) will be classified. [Refer to the official Salt Lake City Zoning Map to determine the zoning of the land within each station area.](#)
1. **Urban Center Station ([TSA-UC](#)):** An urban center station contains the highest relative intensity level and mix of uses. The type of station area is meant to support Downtown Salt Lake and not compete with it in terms of building scale and use. The intensity level of the area is characterized by a twenty four (24) hour population, active streetscapes, defined street walls and the presence of multiple types of public transit or as a node where several rail transit lines converge. Development generally occurs on vacant parcels or through redevelopment of underutilized parcels. The station area has a compact, dense, interconnected and walkable development pattern. Large scale development occurs closer to the station platforms; and is scaled back as it gets closer to less intense areas. Building forms vary, but are typically oriented to the pedestrian, are multiple stories in height, and contain a horizontal and vertical mix of land uses. Buildings up to ten (10) stories in height are allowed in the core, while buildings in the transition zone are approximately half that size. The station area contains a number of regional attractions, such as destination retail, employment, dining and entertainment and a high level of pedestrian activity. A variety of dense housing options exist. Development includes civic amenities, such as public gathering places. Uses that help implement the vision for the station and that area commonly found in an intense urban area are appropriate. The following stations are considered an urban center type of station: North Temple Viaduct Transfer Station, [and the Library Station.](#)



2. Urban Neighborhood Station ([TSA-UN](#)): An evolving and flexible development pattern defines an urban neighborhood station area. Development generally happens as infill on vacant parcels or redevelopment of underutilized parcels. These stations evolve in established residential areas where initial changes may add density and intensity in compact building forms that blend in with the residential character of the area. Urban neighborhoods consist of multilevel buildings that are generally lower scale than what is found in the urban center station area. The desired mix of uses would include ground floor commercial or office uses with the intent of creating a lively, active, and safe streetscape. A mix of building types are possible, ranging from single use structures to mixed use buildings. Residential uses are generally located above the first floor, although they can be located on the ground floor in certain situations. The highest residential densities and most intense land uses are generally located closest to the station platform. Urban neighborhoods are served by at least two (2) forms of transit, including light rail and bus service. The uses serve the surrounding neighborhood with nearby destinations and have the potential to attract people from other neighborhoods.

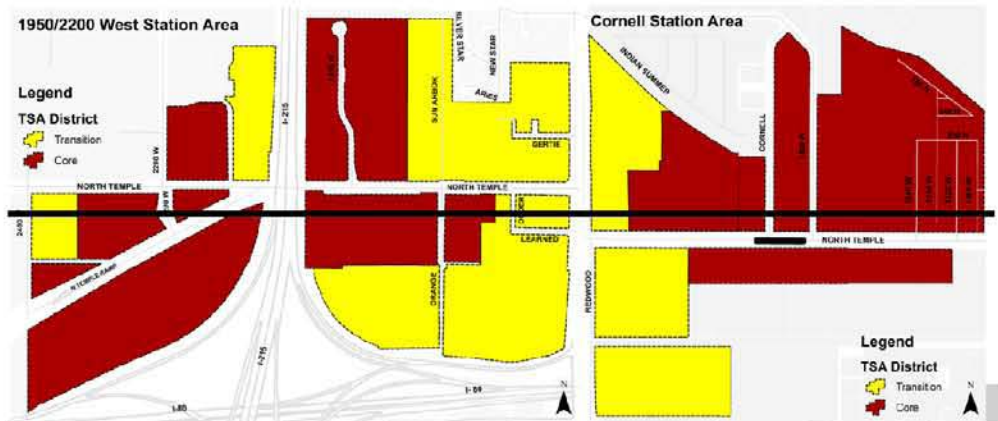
In some urban neighborhood station areas, a linear development pattern along commercial streets that intersect the transit corridor defines a neighborhood main street. Neighborhood main streets are approximately two (2) blocks long, with two (2) 4-story buildings located close to the sidewalk. The ground floors of buildings are typically occupied by active uses, such as retail or restaurants.



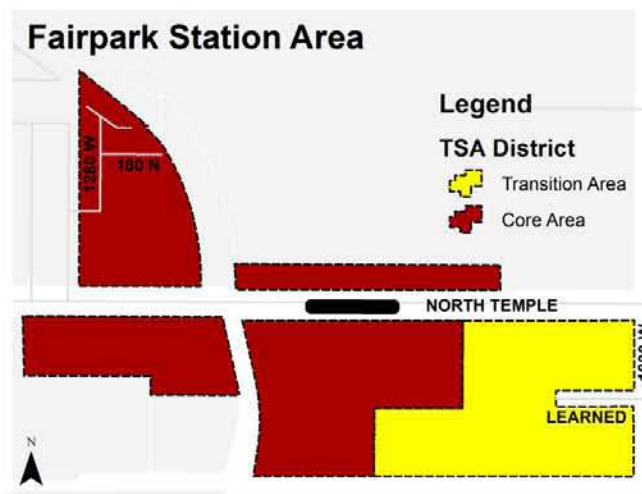
The following stations are considered to be urban neighborhood stations: 800 West, [Trolley \(600 East\)](#) and [900 East](#) light rail stations.



3. Mixed Use Employment Center Station ([TSA-MUEC](#)): A mixed use employment station is an area with a high concentration of jobs that attract people from the entire region. These areas generally start with a campus style development pattern and are dominated by a single type of use that generally employs a high number of people. Buildings are often large scale in nature and may have large footprints. New development occurs on vacant parcels. Redevelopment occurs on surface parking lots, underutilized land, or as additions to existing buildings as businesses expand. The primary mode of circulation is by automobile, but the area is served by at least two (2) types of mass transit which provides alternative modes of transportation for employees. Land uses that support the employment centers such as retail sales and service and restaurants are located throughout the station area and should occupy ground floor space in multi-story buildings oriented to the pedestrian and transit user. A mix of housing types and sizes are appropriate to provide employees with the choice to live close to where they work. Building types should trend toward more flexible building types over time. The area is likely to have large blocks and lacks a consistent street network. Connectivity for all modes of travel is important due to the limited street network. The following stations are considered to be mixed use employment center stations: 1950 West, 2200 West, and the Cornell light rail stations.



4. Special Purpose Station **(TSA-SP)**: The special purpose station is typically centered on a specific land use or large scale regional activity. These areas are generally served by a mix of transit options, usually light rail or bus. New development is limited due to the nature of the primary function of the area, but redevelopment of underutilized parcels is likely to occur. Land uses such as restaurants and retail support the dominant land use and attract people to the area. A mix of housing types and sizes are appropriate in certain situations. Future development should be aimed at increasing the overall intensity and frequency of use in the station area by adding a mix of uses that can be arranged and designed to be compatible with the primary use. The following stations are considered to be special purpose stations: Fairpark light rail station.





- C. Review Process: The intent of the review process is to make the process for desirable development easy to realize in a relatively quick time frame. The review process focuses on building forms and their relationship to adjacent buildings, the public street, transit and other public spaces. The review process for all new development and redevelopment within the transit station area zoning district is based on the development score which is generated by the "Transit Station Area Development Guidelines" hereby adopted by reference. The construction of new buildings require a development score. The following types of development are required to go through this review process:
- Any addition of 1,000 square feet or more that extend a street facing building façade or are located to the side of a building and are visible from a public space or
 - Additions that increase the height of an existing building or change the existing roof line;
 - Additions to the rear of buildings that are not adjacent to a public street, trail or other public space are not required to obtain a development score but must comply with all other applicable regulations. Signs, fences, accessory structures and any other structure or addition not listed in this section are not required to obtain a development score.
1. Presubmittal Conference: All applicants for development within the transit station area zoning district are required to attend a presubmittal conference with the planning division. The purpose of the presubmittal conference is to notify the applicant of the goals of the station area plans, the standards in this section, and the review and approval process.
 2. Development Review Application: After a presubmittal conference, the developer can submit a development review application. This application and all submittal requirements will be used to determine the development score. The application shall include a score sheet on which the development guidelines and their assigned values are indicated and two (2) checklists: one for the applicant's use and one for the planning division's use.
 3. Application Review: Table 21A.26.078C of this section summarizes the application review process. All applications shall be processed as follows:
 - a. Tier 1 Planning Commission Review: If a project is assigned a score less than 50 points, the project can only be approved by the planning commission through the conditional building and site design review process in chapter 21A.59 of this title. Once the applicant receives written notice of their score, they will be given thirty (30) days to notify the planning division of their intention to proceed with the project through the conditional building and site design review process or make necessary



plan adjustments to increase their development score to the minimum level in order to go through an administrative hearing process.

- b. Tier 2 Administrative Hearing: A project that has a development score between 50 and 99 points is eligible for an administrative hearing. Any project being reviewed at an administrative hearing shall be reviewed using the standards found in section 21A.59.060, "Standards For Design Review", of this title.
 - (1) Notice And Posting Requirements: Notice of the administrative hearing shall be done in accordance with subsection 21A.10.020C, "Conditional Building And Site Design Review", of this title.
 - (2) Administrative Hearing: After consideration of the information received from the applicant and any other interested party, the planning director, or designee may approve, approve with conditions, deny or refer the matter to the planning commission.
 - (3) Appeals Of Administrative Hearing Decision: Any person aggrieved by the decision made by the planning director or designee at an administrative hearing may appeal the decision to the Salt Lake City planning commission by filing notice of appeal within ten (10) days after the record of decision is published. The notice shall state the reason(s) for the appeal. Reason(s) for the appeal shall be based upon procedural error, the development score of the project or the criteria set forth in section 21A.59.060, "Standards For Design Review", of this title.
- c. Tier 3 Administrative Review: The planning director has the authority to approve a project scoring 100 points or more without holding a public hearing. The project shall be allowed to go through the standard building permit process. A public hearing is not required because the project incorporates adequate design guidelines or development incentives to be deemed compliant with the vision for the station area.

TABLE 21A.26.078C APPLICATION REVIEW

Development Score	Review Process
0 – 49 points	Planning commission conditional building and site design review process
50 – 99 points	Administrative hearing process
100 or more points	Administrative review

- D. Development Score: The purpose of the development score is to allow flexibility for designers while implementing the city's vision of the applicable station area plans and



the purpose of this zoning district. The development score measures the level of compatibility between a proposed project and the station area plan. A "station area plan" is a development, land use, urban design and placemaking policy document for the area around a specific transit station. The development score is based on the design guidelines and development incentives in the "Transit Station Area Development Guidelines" book, hereby adopted by reference. The "Transit Station Area Development Guidelines" shall be amended following the adopted procedures for zoning text amendments in chapter 21A.50, "Amendments", of this title. Each design guideline is assigned a value.

1. **Formulating The Score:** The development score is formulated by calculating all of the development guideline values for a particular project. Each design guideline and incentive is given a value based on its importance. Some guidelines are considered more important and carry a higher value than others. The planning director shall evaluate each project in the transit station area zone and assign a development score. The development standards in subsection G of this section and the design standards in subsection J of this section shall be complied with by all projects and are not calculated in the development score.
 2. **Using The Score:** Every development is required to meet a minimum development score. The minimum score represents a percentage of the total points possible.
 3. **Project Review:** A development score shall be assigned to all projects within the transit station area zoning district after a complete development review application is submitted. The planning director shall provide, in writing, a copy of the review checklist and explanation of the outcome of the score to the applicant within thirty (30) days of submitting a complete application.
 4. **Appeals:** An applicant may appeal the development score to the planning commission. In hearing the appeal, the planning commission shall hold a public hearing in accordance with section 21A.10.030 of this title. In deciding the appeal, the planning commission shall base its decision on its interpretation of the design guidelines, the development score and whether the project complies with the goals of the applicable station area plans and the purposes of the TSA zoning district.
- E. **Certificate Of Occupancy:** Prior to issuing a certificate of occupancy, a project shall be inspected by the city to determine if the project substantially complies with the development score and, if applicable, any conditions of approval required by the planning commission, administrative hearing officer or planning director. If the project does not receive final approval at the inspection, the project must be brought into compliance with the development score and, if applicable, any conditions of approval required by the planning commission, administrative hearing officer or planning director.
- F. **Prohibited Uses:** The intent of this section is to identify those land uses that are not compatible with transit oriented development due to the nature of the use, the land



requirements of the use, or the potential impacts of the use. Uses listed in table 21A.26.078F of this section and that have an "X" in a box next to the specific land use, indicates it is prohibited. Any use not listed, but is substantially similar to a use listed, shall be prohibited. All other uses are permitted. In the table headings, "C" stands for Core and "T" stands for Transition.

TABLE 21A.26.078F PROHIBITED USES

Use	Urban Center		Urban Neighborhood		Mixed Use Employment Center		Special Purpose	
	Core	Transition	Core	Transition	Core	Transition	Core	Transition
<u>Airport</u>	X	X	X	X	X	X	X	X
<u>Ambulance service (indoor) dispatching, staging and maintenance</u>	X	X	X	X	X	X	X	X
<u>Ambulance service (outdoor)</u>	X	X	X	X	X	X	X	X
<u>Amusement park</u>	X	X	X	X	X	X		
<u>Animal kennel</u>	X	X	X	X				
<u>Animal pound</u>	X	X	X	X	X	X	X	X
<u>Animal, raising of furbearing animals</u>	X	X	X	X	X	X	X	X
<u>Animal stable (private)</u>	X	X	X	X	X	X	X	X
<u>Animal stable (public)</u>	X	X	X	X	X	X		
<u>Animal stockyard</u>	X	X	X	X	X	X	X	X
<u>Auction (indoor)</u>	X	X	X	X	X	X		
<u>Auction (outdoor)</u>	X	X	X	X	X	X	X	X
<u>Auto salvage and recycling</u>	X	X	X	X	X	X	X	X
<u>Auto, truck, and RV repair, sales and/or service (minor or major)</u>	X	X	X	X	X	X	X	X
<u>Blacksmith shop</u>	X	X	X	X	X	X	X	X
<u>Bottling plant</u>	X	X	X	X	X	X	X	X
<u>Building materials distribution</u>	X	X	X	X	X	X	X	X
<u>Bus yards and repair facilities</u>	X	X	X	X	X	X	X	X
<u>Car washes</u>	X	X	X	X	X		X	X



Use	Urban Center		Urban Neighborhood		Mixed Use Employment Center		Special Purpose	
	Core	Transition	Core	Transition	Core	Transition	Core	Transition
Car wash as accessory use to gas station or convenience store that sells gas	X	X	X	X	X	X	X	X
Cemetery, any type	X	X	X	X	X	X	X	X
Check cashing/payday loan business	X	X	X	X	X	X	X	X
Chemical manufacturing and storage	X	X	X	X	X	X	X	X
Commercial parking lots not located in a parking structure	X	X	X	X	X		X	
Community correction facility, any size, large	X	X	X	X	X	X	X	X
Community correction facility, small	X	X	X	X	X	X	X	X
Concrete manufacturing	X	X	X	X	X	X	X	X
Contractor's yard/office	X	X	X	X	X	X	X	X
Detached single-family dwelling	X		X		X		X	
Drop forge industry	X	X	X	X	X	X	X	X
Drive-through windows and service	X	X	X	X	X	X	X	X
Dwelling, single family (detached)	X		X		X		X	
Electric generation facility; excluding solar, wind, or other similar renewable energy generation	X	X	X	X	X	X	X	X
Equipment rental (outdoor)	X	X	X	X	X	X	X	X



Use	Urban Center		Urban Neighborhood		Mixed Use Employment Center		Special Purpose	
	Core	Transition	Core	Transition	Core	Transition	Core	Transition
<u>Equipment, heavy (rental, sales, service)</u>	X	X	X	X	X	X	X	X
Explosives manufacturing and storage	X	X	X	X	X	X	X	X
Extractive industries	X	X	X	X	X	X	X	X
<u>Financial institution, with drive-through facility</u>	X	X	X	X	X	X	X	X
Flammable liquids or gases, heating fuel distribution and storage	X	X	X	X	X	X	X	X
<u>Food processing</u>	X	X	X	X	X	X	X	X
Gas stations	X	X	X	X	X	X	X	X
Heavy equipment rental, repair and sales	X	X	X	X	X	X	X	X
Heavy industrial	X	X	X	X	X	X	X	X
Heavy manufacturing	X	X	X	X	X	X	X	X
Homeless shelters	X	X	X	X	X	X	X	X
Impound lot	X	X	X	X	X	X	X	X
Incinerators, for any purpose <u>medical waste/hazardous waste</u>	X	X	X	X	X	X	X	X
<u>Industrial assembly</u>	X	X	X	X				
Jails	X	X	X	X	X	X	X	X
Kennel	X	X	X	X	X	X	X	X
Landfills, any type	X	X	X	X	X	X	X	X
<u>Limousine service (large)</u>	X	X	X	X	X	X	X	X
<u>Limousine service (small)</u>	X	X	X	X	X	X	X	X
Manufactured/mobile home sales and service	X	X	X	X	X	X	X	X
<u>Manufacturing and processing, food</u>	X	X	X					



Use	Urban Center		Urban Neighborhood		Mixed Use Employment Center		Special Purpose	
	Core	Transition	Core	Transition	Core	Transition	Core	Transition
Manufacturing, concrete or asphalt	X	X	X	X	X	X	X	X
Manufacturing, light	X	X	X	X				
Ministorage facilities	X	X	X	X	X		X	
Outdoor storage including storage or staging of fleet vehicles associated with a business, but not intended to include outdoor retail sales areas or outdoor storage as an accessory and incidental use to a principal use	X	X	X	X	X	X	X	X
Package delivery facility	X	X	X	X	X	X	X	X
Paint manufacturing	X	X	X	X	X	X	X	X
Poultry farm or processing	X	X	X	X	X	X	X	X
Printing plant	X		X	X				
Pawnshops	X	X	X	X	X	X	X	X
Railroad freight terminal facility	X	X	X	X	X	X	X	X
Railroad repair shop	X	X	X	X	X	X	X	X
Recreational vehicle park	X	X	X	X	X	X	X	X
Recycling processing center (indoor)	X	X	X	X	X	X	X	X
Recycling processing center (outdoor)	X	X	X	X	X	X	X	X
Refinery, petroleum products	X	X	X	X	X	X	X	X
Restaurant with drive-through facility	X	X	X	X	X	X	X	X



Use	Urban Center		Urban Neighborhood		Mixed Use Employment Center		Special Purpose	
	Core	Transition	Core	Transition	Core	Transition	Core	Transition
Retail goods establishment with drive-through facility	X	X	X	X	X	X	X	X
Retail services establishment with drive-through facility	X	X	X	X	X	X	X	X
Reverse vending machines	X	X	X	X	X	X	X	X
Rock, sand and gravel storage and distribution	X	X	X	X	X	X	X	X
Sewage treatment facility	X	X	X	X	X	X	X	X
Sexually oriented business	X	X	X	X	X	X	X	X
Sign painting/fabrication	X	X	X	X	X	X	X	X
Slaughterhouse	X	X	X	X	X	X	X	X
Stadium		X		X		X		
Storage (outdoor)	X	X	X	X	X	X	X	X
Storage, mini-warehouse	X	X	X	X	X		X	
Storage, public (outdoor)	X	X	X	X	X	X	X	X
Store, pawnshop	X	X	X	X	X	X	X	X
Store, superstore and hypermarket	X	X	X	X	X		X	
Store, warehouse club	X	X	X	X	X		X	
Taxicab facility	X	X	X	X	X	X	X	X
Theater, live performance		X		X		X		
Theater, movie		X		X		X		X
Tire distribution retail/wholesale	X	X	X	X	X	X	X	X
Transportation terminal, including bus, rail and trucking	X	X	X	X	X	X	X	X
Solid waste transfer station	X	X	X	X	X	X	X	X



Use	Urban Center		Urban Neighborhood		Mixed Use Employment Center		Special Purpose	
	Core	Transition	Core	Transition	Core	Transition	Core	Transition
Stable, public or private	X	X	X	X	X	X	X	X
Truck freight terminal	X	X	X	X	X	X	X	X
Truck stop	X	X	X	X	X	X	X	X
Trucking, repair, storage, etc. associated with extractive industries	X	X	X	X	X	X	X	X
Utility, electric generation facility	X	X	X	X	X	X	X	X
Utility, sewage treatment plant	X	X	X	X	X	X	X	X
Utility, solid waste transfer station	X	X	X	X	X	X	X	X
Vehicle, auction	X	X	X	X	X	X	X	X
Vehicle, automobile and truck repair	X	X	X	X	X	X	X	X
Vehicle, automobile and truck sales and rental (including large truck)	X	X	X	X	X	X	X	X
Vehicle, automobile rental agency	X	X	X	X	X	X	X	X
Vehicle, auto repair (major)	X	X	X	X	X	X	X	X
Vehicle auto repair (minor)	X	X	X	X	X	X	X	X
Vehicle automobile sales/rental and service (indoor)	X	X	X	X	X	X	X	X
Vehicle, automobile salvage and recycling (indoor)	X	X	X	X	X	X	X	X
Vehicle, automobile salvage and recycling (outdoor)	X	X	X	X	X	X	X	X
Vehicle, boat/recreational vehicle sales and service Vehicle, recreation	X	X	X	X	X	X	X	X



Use	Urban Center		Urban Neighborhood		Mixed Use Employment Center		Special Purpose	
	Core	Transition	Core	Transition	Core	Transition	Core	Transition
Vehicle, truck repair (large)	X	X	X	X	X	X	X	X
Vehicle, truck sales and rental	X	X	X	X	X	X	X	X
Warehouse as a primary use	X	X	X	X	X		X	
Welding shops	X	X	X	X	X	X	X	X
Wholesale distributors distribution	X	X	X	X	X		X	X
Woodworking mill	X	X	X		X		X	
Zoological park	X	X	X	X	X	X		

1. Existing Uses And Buildings: A use located within a station area legally existing at the time that this zoning district was adopted, but listed as a prohibited use in this subsection F, shall be considered a legal nonconforming use. A structure legally existing at the time this section was adopted, but not conforming to the standards in this chapter, shall be considered a legal noncomplying structure. Any legal nonconforming use or legal noncomplying structure is subject to chapter 21A.38 of this title.

G. Development Standards:

1. Intent: The purpose of the following development standards is to promote an intense and efficient use of land at increased densities in the station areas. The development standards are intended to create a safe and pleasant environment near transit stations by encouraging an intensive area of mixed use development and activities, pedestrian amenities and by limiting conflicts between vehicles and pedestrians. Development standards are intended to create a reasonably continuous building edge that defines the exterior spatial enclosure of the street or open space and protect adjacent low density residential zoning districts. With some exceptions, buildings line a street at or near the public right of way to the greatest extent possible.

2. Application: The dimensional requirements in ~~table 21A.26.078GD~~ of this section apply to all new buildings and developments as well as additions to existing buildings. The following development standards apply to the core and transition areas of all station types:

a. Building Height: The minimum and maximum building heights are found in table 21A.26.070Ga Building Height Regulations. Height limits are intended to control the overall scale of buildings, the compatibility with adjacent development, and the composition of the urban form of the



block. Minimum building heights in the core area relate to the width of the street, with a minimum ratio of one foot (1') of building height for every three feet (3') of street width. Building height is measured from the finished grade to the highest point of the building. The following exceptions apply:

- (1) The minimum building height applies to all structures that are adjacent to a public or private street. The building shall meet the minimum building height for at least 10% of the width of the street facing building wall.
- (2) Elevator shafts, parapet walls, and other projections are permitted subject to subsection 21A.36.020C, "Height Exceptions", of this title.
- (3) Projects that achieve a development score that qualifies for administrative review are eligible for an increase in height. The increase shall be limited to one story of ~~in~~habitable space. The height of the additional story shall be equal to or less than the average height of the other stories in the building.

TABLE 21A.28.078G2a BUILDING HEIGHT REGULATIONS

	<u>Minimum Height¹</u>	<u>Maximum Height</u>
<u>Urban center</u>		
<u>Core</u>	<u>30'</u>	<u>90'²</u>
<u>Transition</u>	<u>25'</u>	<u>60'</u>
<u>Urban neighborhood</u>		
<u>Core</u>	<u>25'</u>	<u>75'</u>
<u>Transition</u>	<u>0'</u>	<u>50'</u>
<u>Mixed use employment center</u>		
<u>Core</u>	<u>25'</u>	<u>75'</u>
<u>Transition</u>	<u>0'</u>	<u>60'</u>
<u>Special Purpose</u>		
<u>Core</u>	<u>25'</u>	<u>75'</u>
<u>Transition</u>	<u>0'</u>	<u>60'</u>

1. Minimum building heights apply to those properties with frontage on the street where fixed rail transit is located.
 2. Buildings with a roof that has at least 2 sloping planes may be allowed up to 105 feet. The additional height may include habitable space.

- b. Setbacks: Required building setbacks promote streetscapes that are consistent with the desired character of the street and various station typologies and its core and transition areas. Building setbacks create a safe environment that is inviting to pedestrians and transit users and maintain light, air and potential privacy for adjacent residential uses. In some instances, the setbacks limit the building envelope where the

existing development pattern would be negatively impacted by taller buildings.

(1) Front and Corner Yard Setback: Except as indicated below, there is no minimum setback. If a setback is provided, at least 50% of the street facing building façade shall be located within 5 feet of the front property line unless a larger setback is required below. All portions of a front yard not occupied by building, driveways, walkways or other similar feature must be completely landscaped or include an active outdoor use, such as outdoor dining, plazas or other similar outdoor use with the space not dedicated to active outdoor use completely landscaped. Parking, drive aisles or other paved areas for motor vehicles are prohibited. Walls up to three feet (3') in height, patios and other similar elements intended to activate the sidewalk can be located to the property line.

~~(2)~~ A) North Temple Boulevard: The front yard setback along North Temple Boulevard shall be fifteen feet (15') for a minimum of fifty percent (50%) of the width of the street facing facade. Up to fifty percent (50%) of a street facing facade may encroach up to the front property line. In this case, the area not occupied by the building footprint must be landscaped or include active outdoor use, such as outdoor dining, plazas, courtyards or other similar usable public space or use. Setbacks over fifteen feet (15') are not allowed. In locations where there is not a minimum sidewalk width of 10 (ten) feet, additional sidewalk width shall be installed by the developer so there is a minimum width of 10 feet when a new building is constructed or with additions that increase the gross building square footage by more than 50%.



~~(3)~~ B) 400 South/University Boulevard: The front yard setback along 400 South/University Boulevard shall be a minimum



of fifteen feet (15'). In locations where there is not a minimum sidewalk width of ten (10) feet, additional sidewalk width shall be installed by the developer so there is a minimum width of ten feet (10") when a new building is constructed or with additions that increase the gross building square footage by more than 50%.

(C) Streets with a Right of Way of 50 feet or less: When located on a street with a right of way 50 feet or less with an R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district on either side of the street, a minimum setback of 25% of the lot depth but no more than twenty five feet (25') is required. For buildings taller than twenty five feet (25'), the setback shall increase two feet (2') for every foot of height above twenty five feet (25') in height. Buildings may be stepped so taller portions of the building are farther away from the front property line.

(43) Side Yard Setback:

(A) Drive aisles are allowed in the side yard setback. In the transition subarea, parking is allowed in the side yard subject to subsection L of this section.

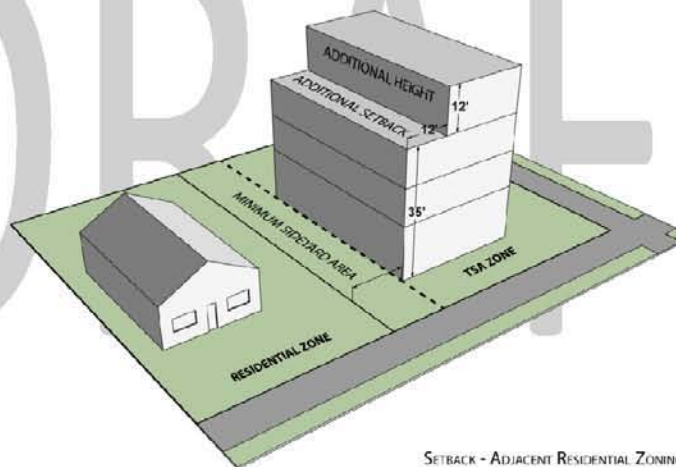
(B) Side yard setback when adjacent to a ~~single-family or two-family~~ certain zoning districts:

(i) Core Area: A minimum of twenty five feet (25') in the core area. When adjacent to an OS, R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district, the minimum side yard setback shall be increased one foot for every one foot increase in height above twenty five feet (25'). When a property in an R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district is separated from a TSA zoned property by an alley, the additional setback for height above twenty five feet (25') applies and shall be measured from the property line of the TSA zoned property. Buildings may be stepped so taller portions of a building are farther away from the side property line. The horizontal measurement of the step shall be equal to the vertical measurement of the taller portion of the building.

(ii) Transition Area: A minimum of fifteen feet (15') in the transition area. When adjacent to an OS, R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning

district, the minimum side yard setback shall be increased one foot for every one foot increase in height above fifteen five feet (15') . When a property in an OS, R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district is separated from a TSA zoned property by an alley, the additional setback for height above fifteen five feet (15') applies and shall be measured from the property line of the TSA zoned property. Buildings may be stepped so taller portions of a building are farther away from the rear property line. The horizontal measurement of the step shall be equal to the vertical measurement of the taller portion of the building.

~~(C)~~(iii) Side yard setback when adjacent to other uses or districts: No minimum side yard required.



~~(5)~~(4) Rear Yard Setback:

- (A) Core area: A minimum of fifteen feet (25'). When adjacent to an OS, R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district, the minimum rear yard setback shall be increased one foot for every one foot increase in height above twenty five feet (25'). When a property in an OS, R-1, R-2, SR, RMF-30 RMF-35 or RMF-45 zoning district is separated from a TSA zoned property by an alley, the additional setback for height above twenty five feet (25') applies and shall be measured from the property line of the TSA zoned property. ~~If a structure is located adjacent to a~~



~~single family or two family residential zoning district, the rear setback shall be equal to the height of the building on the subject property in the transit station area district.~~

Buildings may be stepped so taller portions of a building are farther away from the rear property line. The horizontal measurement of the step shall be equal to the vertical measurement of the taller portion of the building. ~~When a public alley is between the two (2) properties, the width of the alley may be counted towards the setback.~~

- (B) Transition area: A minimum of twenty five feet (25'). When adjacent to an OS, R-1, R-2, SR, RMF-30 or RMF-35 zoning district, the minimum rear yard setback shall be increased one foot for every one foot increase in height above twenty five feet (25') . When a property in an OS, R-1, R-2, SR, RMF-30 or RMF-35 zoning district is separated from a TSA zoned property by an alley, the additional setback for height above twenty five feet (25') applies and shall be measured from the property line of the TSA zoned property. Buildings may be stepped so taller portions of a building are farther away from the rear property line. The horizontal measurement of the step shall be equal to the vertical measurement of the taller portion of the building.

- ~~(65)~~ Special Setback Provisions For Properties Adjacent To Jordan River: For properties that are adjacent to the Jordan River, the building setback from the Jordan River shall be fifty feet (50'), measured from the annual high water line as defined in section 21A.34.130 of this title. For buildings over fifty feet (50') in height, the setback shall increase one foot (1') for every foot in height over fifty feet (50') up to a maximum of seventy five feet (75'). Portions of buildings over fifty feet (50') in height may be stepped back to comply with this standard.

c. Minimum Lot Area and street frontage requirements:

- (1) The minimum lot area applies to all new subdivisions of land and shall not be used to calculate residential density.
- (2) Any legally existing lot may be developed without having to comply with the minimum lot size requirements.
- (3) The minimum lot area for all areas of the TSA zoning district is 2,500 square feet.



(4) All subdivisions of land or combination of parcels must have a minimum of 40 feet of street frontage.

TABLE 21A.26.078CD DIMENSIONAL REQUIREMENTS

	Min. Height ²	Max. Height	Min. Front Yard	Max. Front Yard	Min. Side Yard ¹	Min. Rear Yard ¹	Minimum Lot Coverage For Structures	Minimum Lot Size (Sq. Ft.)
Urban center:	-	-	-	-	-	-	-	-
- Core	30'	90' ³	None	15'	None	None	70%	5,000
- Transition	25'	60'	None	15'	None	None	50%	1,500
Urban neighborhood:	-	-	-	-	-	-	-	-
- Core	25'	75'	None	10'	None	None	70%	5,000
- Transition	0'	50'	None	10'	None	None	50%	1,000
Mixed-use employment station:	-	-	-	-	-	-	-	-
- Core	25'	75'	None	15'	None	None	60%	5,000
- Transition	0'	60'	None	15'	None	None	50%	1,500
Special purpose:	-	-	-	-	-	-	-	-
- Core	25'	75'	None	25'	None	None	60%	10,000
- Transition	0'	60'	None	25'	None	None	50%	1,500

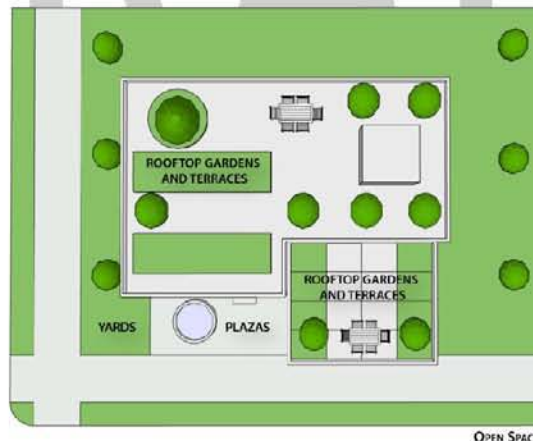
Notes:

- 1. Reference subsection G2 of this section for specific standards.
 - 2. Minimum building heights apply to those properties with frontage on the street where fixed rail transit is located.
 - 3. Buildings with a roof that has at least 2 sloping planes may be allowed up to 105 feet. The additional height may include inhabitable space.
- d. **Minimum Lot Coverage:** The intent of the minimum lot coverage is to ensure compact development.

de. Open Space: In order to provide space for passive and active recreation, public and private use, offset storm drainage due to nonpermeable surfaces and as an amenity to individual developments and their residents, employees and customers, open space is required for all new developments.

(1) Core Area:

- (A) Within the core area, open space may include landscaped yards, patios, public plazas, pocket parks, courtyards, rooftop and terrace gardens and other similar types of open space amenity.
- (B) A minimum of ten percent (10%) of the land area up to five thousand (5,000) square feet.



(2) Transition Area:

- (A) Within the transition area, open space may include landscaped yards, patios, public plazas, pocket parks, courtyards, rooftop gardens and terraces, community gardens and other similar types of amenities.



(B) The minimum open space requirement is ten percent (10%) of the land area up to two thousand five hundred (2,500) square feet.

(3) Access To Open Space: All required open space shall be accessible to the users of the building(s).

ef. Circulation And Connectivity: Development within the station area shall be easily accessible from public spaces and provide safe and efficient options for all modes of travel. Circulation networks, whether public or private, require adequate street, pedestrian and bicycle connections to provide access to development. The internal circulation network shall be easily recognizable, formalized and interconnected.

(1) All parking lots shall comply with the standards in section 21A.44.020, "General Off Street Parking Requirements", of this title.

(2) Parking is prohibited between the street facing building line and any front or corner side property line. This shall include any drive aisle that is not perpendicular to the front or corner side property line.

H. Residential Densities:

1. Core area: No maximum.
2. Transition area: No maximum.

I. Accessory Structures: No accessory structure shall be located in a required front yard or between the primary building and a property line adjacent to a public street.

J. Design Standards:

1. Purpose: Design standards create the fundamental characteristics of a transit oriented district and the basic design elements required for a successful transit station area. Design standards are intended to provide a safe and interesting walkable environment by connecting ground floor uses adjacent to the sidewalk areas, by encouraging the continuity of retail and service uses, providing surveillance opportunities on the street and public open spaces and framing the street by bringing portions of buildings up to the sidewalk. All buildings shall be designed for the context and character of the project and how they interact visually, functionally, and socially with the context of the public environment.
2. Application: The following design standards apply to all projects within the core and transition areas of all station area types:

- a. **Building Walls Adjacent To A Street:** Street-facing building facades shall provide architectural variety and scale. Changes in plane, color, texture, materials, scale of materials, patterns, art, or other architectural detailing are acceptable methods to create variety and scale. Building walls parallel to a public street and greater than thirty feet (30') in length shall be broken up by architectural features such as bay windows, recessed entrances or windows, balconies, cornices, columns, or other similar architectural features. The architectural feature may be either recessed or project a minimum of twelve inches (12").



- b. **Ground Floor Building Materials:** Other than ground windows and doors, eighty percent (80%) of the remaining ground floor wall area shall be clad in durable materials. Durable materials include brick, masonry, textured or patterned concrete and/or cut stone. Other materials may be used as accent or trim provided they cover twenty percent (20%) or less of the ground floor adjacent to a street.
- c. **Ground Floor Glass And Transparency:** All street-facing elevations of a development shall be designed so that the first floor street-facing facade has at least sixty percent (60%) clear glass between three (3) and eight feet (8') above grade to allow pedestrians to view activities inside the building or lighted display windows. There must be visual clearance behind the glass for a minimum of two feet (2'). Three-dimensional display windows at least two feet (2') deep are permitted and may be counted toward the sixty percent (60%) glass requirement. Ground floor windows of commercial uses shall be kept clear at night, free from any window covering, with internal illumination. When ground floor glass conflicts with the internal function of the building, other means shall be used to activate the sidewalk, such as display windows, public art,



architectural ornamentation or detailing or other similar treatment. The first floor elevation facing a street of all new buildings, or buildings in which the property owner is modifying the size of windows on the front facade, shall not have less than sixty percent (60%) glass surfaces. The reflectivity in glass shall be limited to eighteen percent (18%) as defined by ASTA standards. The planning director may approve a modification to this requirement if the planning director finds:

- (1) The requirement would negatively affect the historic character of the building, or
- (2) The requirement would negatively affect the structural stability of the building.
- (3) The ground level of the building is occupied by residential uses, in which case the sixty percent (60%) glass requirement may be reduced to forty percent (40%).

Any appeal of an administrative decision made pursuant to this subsection may be made to the planning commission.

- d. **Building Entrances:** The intent of regulating building entrances is to promote security on the street and public spaces by providing frequent points of access and sources of activity. Functional entrances to nonresidential uses should be located at an average of seventy five feet (75') or less from one another. At least one operable building entrance per elevation facing a public street shall be provided. Each ground floor leasable space is required to have an operable entrance facing the street and a walkway to the sidewalk. If a plaza or open space is provided as part of the development, a minimum of one entrance opening onto the plaza or open space shall be provided. This entrance shall be counted towards the spacing of functional entrances identified in this section and may count as the primary entrance to the building. All street facing building entrances shall be functional entrances and shall not be limited to emergency or employee entrances.
- e. **Ground Floor Residential Uses:** The interior floor elevation of ground floor residential units in the core area shall be a minimum of two feet (2') and a maximum of five feet (5') above grade. Dwelling units located on the ground floor and facing a public or private street shall have a minimum of one primary entrance facing the street in the core area. The facades of all buildings in the core and transition areas with ground floor residential uses shall feature elements that signal habitation such as windows, entrances, stairs, porches, bay windows, and balconies that are visible from the public street. Attached single-family dwellings, townhomes, row houses, ~~entrances that open onto a courtyard facing a public street,~~ and other similar housing types shall have a primary entrance facing the street for each unit adjacent to a street. Units may have their primary entrance



located on a courtyard, midblock walkway, or other similar area if the street facing facades have a primary entrance facing the street.

- f. **Parking Structures:** The intent of regulating parking structures is to minimize the visual impact of the structure and the cars parking within it, and to reduce their impact on the ground floors adjacent to public sidewalks and streets. Parking structures are permitted within the core and transition areas provided:
- (1) The ground floor of parking structures adjacent to a public street shall include an active use other than parking such as office, retail, residential leasing office, restaurant, etc. Parking is permitted behind the ground floor uses. If the ground floor does not include active use, then the structure must be set back behind a building or be a minimum of sixty feet (60') from a property line adjacent to a public street or sidewalk.
 - (2) The levels of parking above the first level facing the front or corner side lot line shall have horizontal floors and/or facades and not sloped.
 - (3) The levels of parking above the second level shall be designed to effectively screen the vehicles so they are not readily visible from an adjacent street.
 - (4) Below grade parking structures for structures with ground floor residential uses may extend a maximum of five feet (5') above the existing grade provided the above grade portion is screened with vegetation or architectural feature(s).
- g. **Mechanical Equipment:** All roof mounted mechanical and electrical equipment, communication antennas or dishes shall be enclosed, screened, organized, designed and located to be out of view from streets and public spaces. The parapet or enclosure shall be equal to or greater than the height of the equipment to be screened to reduce equipment noise and odors, and other impacts onto adjacent uses and maintain the integrity of overall architectural character and scale of the building. Mechanical equipment may be located on the ground provided it is behind the building, screened and not located in a required rear yard or side yard setback. Utility boxes are subject to section 21A.40.160, "Ground Mounted Utility Boxes", of this title.
- h. **Service Areas:** Service areas, loading docks, refuse containers and similar areas shall be fully screened from public view. All screening enclosures viewable from the street shall be either incorporated into the building architecture or shall incorporate building materials and detailing compatible with the building being served. All screening devices shall be

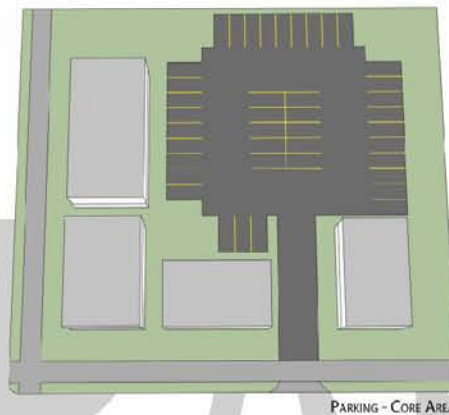


a minimum of one foot (1') higher than the object being screened. Dumpsters must be located a minimum of twenty five feet (25') from any building on an adjacent lot that contains a residential dwelling or be located inside of an enclosed building ~~single- or two-family residential zoning district.~~

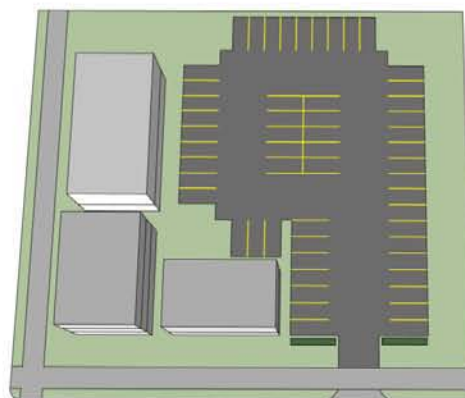
- K. Multiple Buildings On A Single Parcel: Multiple principal buildings on a single parcel are permitted provided each principal building meets the requirements of this chapter and each principal building obtained a separate development score. New principal buildings can be located towards the rear of a parcel provided there is an existing or additional new principal building that complies with the front yard building setbacks. If one principal building receives a development score lower than other principal buildings on the site, the project shall be processed based on the lowest development score obtained.
- L. Parking: The purpose of this subsection is to provide locations for off street parking. All off street surface parking lots should be located so that they are compatible with pedestrian oriented streets. New uses and development or redevelopment within this district shall comply with the requirements of this subsection.
1. Surface Parking Lots And Structures On Corner Properties: On corner properties, surface parking lots and structures shall be located behind principal buildings or at least sixty feet (60') from a front and corner side lot lines. Only one driveway and drive aisle is permitted per street frontage and the access point shall be located a minimum of 100 feet from the intersection of the front and corner side property lines. If the front or corner side property line is less than 100 feet in length, than the drive approach shall be located within 20 feet of the side or rear property line.
 2. Surface Parking In The Core Area: Surface parking lots in the core area are required to be located behind the principal building or to the side of a principal building. When located to the side of a building, the parking lot shall be:
 - a. Set back a minimum of thirty feet (30') from a property line adjacent to a public street. The space between the parking lot and the property line adjacent to a public street shall be landscaped or activated with outdoor dining, plazas or similar feature.
 - b. Screened with a landscaped hedge or wall that is at least thirty six inches (36") above grade and no taller than forty two inches (42") above grade. Landscaping berms are not permitted.
 - c. The parking lot shall be no wider than what is required for ~~one-two~~ rows of parking and one drive aisle as indicated in table 21A.44.020 of this title.
 - d. Only one driveway and drive aisle is permitted per street frontage and the access point shall be located a minimum of 100 feet from the intersection of the front and corner side property lines. If the front or corner side

property line is less than 100 feet in length, than the drive approach shall be located within 20 feet of the side or rear property line.

- e. Surface parking lots as a principal use: Surface parking lots as a principal use located on a lot that has frontage on a public street are prohibited.



3. Surface Parking In The Transition Area: Surface parking lots in the transition area are required to be located behind the principal building or to the side of a principal building. When located to the side of a principal building, the parking lot shall be:
- Set back so that no portion of the parking area other than the driveway is closer to the street than the front wall setback of the building. In cases where the front wall of the building is located within five feet (5') of a property line adjacent to a street, the parking lot shall be set back a minimum of eight feet (8'). The space between the parking lot and the property line adjacent to a street shall be landscaped or activated with outdoor dining, plazas or similar feature.
 - Screened with a landscaped hedge or wall that is at least thirty six inches (36") above grade and no taller than forty two inches (42") above grade. Landscaping berms are not permitted.
 - Surface parking lots as a principal use: Surface parking lots as a principal use located on a lot that has frontage on a public street are prohibited.



PARKING LOT - TRANSITION AREA

4. Walkways Through Parking Lots: Parking lots with more than fifteen (15) stalls shall provide a pedestrian walkway through the parking lot to the primary building entrance or a sidewalk providing access to a primary building entrance. One walkway must be provided for every three (3) drive aisles. Walkways shall be curb separated from the parking areas and a minimum of five feet (5') wide. Vehicles shall not overhang the walkway. Parking lot landscaping requirements in chapter 21A.48 of this title shall be included on the side of the walkway. Where the walkway crosses a drive aisle, a crosswalk that is clearly identified by a change in color, material, or similar technique shall be used.
 5. Surface Parking Lots as the Principal Use: Surface parking lots that are the principal use are permitted in the TSA zoning district provided the following standards are complied with:
 - a. The surface parking lot does not have street frontage on the street where the fixed rail transit is located.
 - b. The surface parking lot is setback a minimum of 15 feet from any property line adjacent to a public street.
 - c. The parking area is screened by a wall or landscaping that is a minimum of 36 inches and a maximum of 48 inches tall.
 65. Other Applicable Standards: All other standards in chapter 21A.44, "Off Street Parking And Loading", of this title shall apply.
- M. Conflicting Regulations: In cases where the regulations of this section conflict with another section of this zoning ordinance, this section shall take precedence except in situations where the conflict is related to the use of the property, in which case the more restrictive regulation takes precedence. In station areas within an overlay district, the overlay district shall take precedence.



N. Developments Over Five Acres:

1. Intent: Large scale developments have the potential to function as a self-contained mixed use neighborhood and could have both positive and negative impacts on nearby properties. All developments over five (5) acres in size shall be designed and planned to include a series of blocks and a network of public or private streets that connects to the existing public streets in the area and to adjacent development and neighborhoods. Buildings should be oriented to this street network. Regulating block size is necessary to provide development sites that are oriented to the pedestrian while accommodating other modes of transportation. A street network is required to ensure adequate circulation for pedestrians, bicycles, automobiles and service vehicles through the site, to adjacent sites and the public streets.

2. Application: These standards are in addition to all other applicable standards. In situations where the standards in this section conflict with a standard in another section, the standard in this section shall take precedence. A separate development score is required for each new principal building in a development over five (5) acres. A development over five (5) acres shall be subject to the applicable review process based on the lowest development score assigned to an individual building in the development.

a. Block Layout: The intent of regulating block size and dimension is to create a development pattern where all principal buildings have their primary facades facing a street, whether public or private. All developments over five (5) acres in size shall be designed to include a series of blocks based on the standards below.

- (1) The maximum perimeter dimension of any block shall be one thousand six hundred feet (1,600'). The maximum length of any individual block face shall be four hundred forty feet (440').
- (2) The maximum perimeter dimension of a block may be increased to two thousand four hundred (2,400) linear feet, and the maximum length of any block face increased to six hundred feet (600') provided a mid block pedestrian network is included. The mid block pedestrian network must be a minimum of twenty feet (20') wide and include pedestrian amenities such as lighting, benches, and other similar features. The mid block walkway shall connect to at least two (2) block faces or be extended to the property line to allow for future extension. The standards in subsection J2 of this section apply to building walls adjacent to a mid block walkway.



- b. **Housing Proximity To Transit:** Developments that include housing should cluster the housing so a minimum of fifty percent (50%) of the housing is located with one-fourth ($\frac{1}{4}$) mile walking distance of a transit platform.
- c. **Connectivity To Public Streets, Sidewalks, And Bicycle Lanes:** In order to ensure that the development will be fully integrated into the transit station area, that safe and efficient travelways are provided, and to limit the impact on the primary transit street and other adjacent streets, the internal circulation system, including private streets, drive aisles, sidewalks and bicycle lanes shall connect to the public street, sidewalks and bicycle lanes. All new streets shall be designed as a "complete street" defined as a street that provides dedicated space for pedestrians, bicyclists and automobiles.
- d. **Vehicle Access:** Regulating access to private property from public streets is necessary for integrating private development and public spaces. Limiting the number of access points and spacing between access points reduces areas of conflict between vehicles, pedestrians and bicycles. Maximum access widths promote a development pattern that is oriented to pedestrians and bicyclists while accommodating vehicles.
- (1) Access points located on public streets intended for vehicles shall be spaced a minimum of one hundred feet (100') apart.
 - (2) No property shall have more than one vehicle access point for every two hundred (200) linear feet of frontage on a public street.
 - (3) No access drive shall be greater than twenty four feet (24') wide.
 - (4) The location of all vehicle access points is subject to approval from the transportation division of the city. The standards of this section may be modified by the transportation division when, in the opinion of the director of the transportation division, a different design would improve the overall safety for all modes of transportation or improve the efficiency of the transportation network.
- e. **Internal Circulation:** Internal circulation systems allow for vehicles, pedestrians and bicyclists to move safely and efficiently throughout a development site. A logical, simple and well designed internal circulation system that connects with adjacent circulation networks provides room for vehicles, safe walking paths for pedestrians through the parking lot and the site to the public way, and well marked routes for bicycles traveling from public spaces to bicycle parking areas within a site. All new developments over five (5) acres are required to submit an internal circulation network plan.
- (1) **Travel Lanes That Connect Parking Areas With A Public Street:**
All internal vehicle travel lanes that connect internal parking areas



with a public street shall be designed to meet the minimum requirements in section 21A.44.020 of this title.

- (2) **Design Speed:** The internal circulation system shall be designed to move vehicles at speeds of twenty (20) miles per hour or less.
 - (3) **Future Access To Adjacent Properties And Rights Of Way:** All internal drive aisles, sidewalks, and paths shall be extended to property lines to allow for future cross access to adjacent properties when the adjacent property is undeveloped and to rights of way.
 - (4) **Centerlines:** The centerline of all internal streets shall be in line with the centerline of a street on the opposite side of an intersecting street unless the intersecting street is divided by a median. Offset streets shall be a minimum of two hundred feet (200') apart, measured from centerline to centerline.
 - (5) **Publicly Dedicated Streets:** Any street that is to be publicly dedicated shall meet the city's minimum construction and design standards (including street lighting, park strip, street trees, etc.).
 - (6) **Pedestrian Routes:** Pedestrian routes that provide safe, comfortable, clear and direct access throughout the development shall be provided. Pedestrian paths shall be bordered by residential fronts, green space, active open space, or commercial store fronts.
 - (7) **Bicycle Paths:** A coordinated system of bicycle paths should be provided.
 - (8) **Approval; Modification Of Standards:** The internal circulation network is subject to approval from the transportation division of the city. The standards of this section may be modified by the transportation division when, in the opinion of the director of the transportation division, a different design would improve the overall safety for all modes of transportation or improve the efficiency of the transportation network.
- f. **Parking:** Parking may be provided along any private street within a development over five (5) acres. The parking shall be counted towards the applicable off street parking standard when provided on private streets. All parking areas and stalls must comply with the parking lane widths identified in table 21A.44.020 of this title.
- g. **Open Space:** In order to provide space for passive and active recreation, public and private gatherings, offset storm drainage due to nonpermeable



surfaces and as an amenity to individual developments and their residents, employees and customers, usable open space is required for all new developments.

- (1) Required: In the core and transition areas of all station areas, a minimum of ten percent (10%) of the site, up to fifteen thousand (15,000) square feet, shall be devoted to open space. "Usable open space" is defined as landscaped areas, plazas, outdoor dining areas, terraces, rooftop gardens, stormwater retention areas, and any other similar type of area.
- (2) Connectivity To Adjacent Open Space: When adjacent to public open space, parks, trails and pathways, open space on developments over five (5) acres in size are encouraged to provide access to the public open space.

h. Landscaping: All areas not occupied by buildings, plazas, terraces, patios, parking areas, or other similar feature shall be landscaped. If a project is developed in phases, only those areas in a phase that is under construction shall be landscaped. Landscaping in future phases shall be installed as those phases develop. Areas in future phases may be used as community gardens or other active open space until such time as development of that phase begins.

Attachment D

Proposed Master Plan Changes



Please note that the following Transit Oriented Development (TOD) section will replace the text found on page 15 for the current Central Community Master Plan.

Transit-Oriented Development (TOD)

Introduction

Transit-Oriented-Development (TOD) is a land use and urban design concept that mixes land use near transit to maximize transportation options and provides people with choices about living near transit. A TOD neighborhood typically has a center with a light rail station or bus stop surrounded by relatively high-density development with progressively lower-density development spreading outward from the transit center. TOD's generally are located within a radius of one-quarter to one-half mile from a transit stop, as this is considered an acceptable walking distance.

TOD areas offer choices in housing, commerce and transportation. They are designed to be flexible so they can respond to changing economic and social conditions and endeavor to make development economically viable from a number of perspectives (city, transit agency, developer, resident, employer, etc.). TOD districts encourage the stabilization and revitalization of existing neighborhoods, as new uses are designed to support existing neighborhood characteristics.

TOD districts improve the quality of life in urban areas by:

- Including mixed-use development that will use transit at all times of day.
- Creation of excellent pedestrian facilities such as high quality pedestrian crossings, narrow streets, and tapering of buildings as they become more distant from the public transport node.
- Ensuring compatibility and connectivity with surrounding neighborhoods.
- Creating compact development within an easy walk of public transit and with sufficient density to support transit ridership.
- To establish a hierarchy of transportation which places the pedestrian first, bicycle second and auto third.
- Creating active places and livable communities that service daily needs and where people feel a sense of belonging and ownership.
- Including engaging, high quality public spaces (e.g. small parks or plazas) as organizing features and gathering places for the neighborhood.
- Encouraging a variety of housing types near transit facilities.
- Providing housing choices for a wide range of ages and incomes.
- Incorporating retail into a development if it is a viable use at the location. Ideally drawing customers both from both the TOD and a major street.



- Introducing creative parking strategies that integrate, rather than divide a site and reduce the presence of the auto.
- Creating TOD plans that are flexible so they can respond to changing conditions.
- Recognizing that all TOD's are not the same; each development is located within its own unique context and serves a specific purpose in the larger context.

TOD strengthens urban development but also helps manage future regional growth by encouraging growth to occur where the existing infrastructure is best suited to address it. The benefits of this type of development include:

- Opportunities for affordable and accessible living by incentivizing affordable and accessible housing goals, providing a range of housing options, and allows people to spend less of their income on the combined expenses related to housing and transportation.
- Reduction of the long term expenditures of tax dollars to build and maintain public utilities, roads and other auto-oriented infrastructure.
- More residents living close to commercial areas to support a strong, local oriented economy.

This reduces the overall cost of development and reduces negative environmental impacts on air and water quality while creating community oriented public places.

Transit-oriented development designations

Transit-oriented development districts within the Central Community have three designations: low-density, medium-density, and high-density. The Future Land Use map shows locations where these districts are supported by this master plan. Where conflicts between TOD and historic district overlay regulations occur, the historic overlay requirements govern.

Low-density transit-oriented development (light sage green on map): Low-density TOD supports residential uses with a density ranging from 1-20 dwellings per acre. The emphasis of low-density TOD design and land use relates to existing lower density residential and neighborhood commercial land uses. Low intensive development should be designed to assure compatibility in neighborhoods with established low-density characteristics by focusing on the massing and scale of the existing structures. Medium intensive land uses, such as a coffee shop, town house or daycare center, may be appropriate near the light rail station or busier intersections. Implementing low-density TOD areas may include development of accessory units in the rear yards of low-density residential land uses as well as small businesses that can be operated out of a residential structure. New structures should be in scale with the low-density neighborhood with modest increases in building height being appropriate if it provides opportunity for increasing the residential density. Zoning designations should require compatibility in these areas.



Medium-density transit-oriented development (medium sage green on map): Medium-density TOD supports residential land uses with a density range of 20-50 dwelling units per acre. The design emphasis for medium-density TOD is compatibility with existing medium and low-density residential and commercial development. Higher intensive uses may be located near light rail stations where applicable. Medium-density TOD areas include four to five story buildings with a mix of ground level retail or office space components with multi-story residential development above. These areas should have limits on the amount of space allocated for non-residential land uses by locating them closer to transit and at busier intersections. Individual residential land uses could remain within the TOD area. Zoning districts that allow four to five story building, including pedestrian oriented design standards and allow 20-50 dwelling units per acre are appropriate in areas with this designation.

High-density transit-oriented development (dark sage green on map): High-density TOD is similar to medium-density TOD except at a greater scale. These areas should be centers of high population with a concentration of pedestrians in close proximity to transit stations. These areas will be more successful in places that are well served by rail transit. Building heights are established for high density residential and higher intensity office or commercial uses. The ideal building type in these areas are vertical mixed use structures tall than five stories. High-density TOD supports the development of compact urban centers with 50 or more dwelling units per acre. These centers can provide accessibility and a multitude of benefits (efficiencies that result when many activities are physically close together). Design standards are critical in these areas in order to create people oriented spaces. Auto oriented development should be prohibited. Standards and processes should be put in place to allow for the easy and financially viable transition of auto oriented developments to people oriented development.

The transit-oriented development land use designations are shown on the Central Community TOD map and on the Future Land Use map.

Community input on Transit-Oriented Development

This section will be further modified after input from the next community open houses in February and March of 2012.

Future Transit-Oriented Development land use changes

TOD's are implemented by the public and private sectors. The public sector sets the stage by providing the transit, investing in public infrastructure and applying appropriate zoning regulations near the transit stations. The private sector generally finances, constructs and markets TOD's. When based on a sound and agreed upon vision, TOD implementation should be a simple and straightforward process. To ensure this is the case, the TOD station area plans intend to create the vision and set the stage for the private sector. The City has developed certain zoning districts to implement the various intensity levels of TOD that are based on input from a broad range of stakeholders. The areas listed below have gone through a community visioning process that had identified the desired development characteristics and therefore, support zoning changes based on the specific goals for each area.



400 South and University Boulevard

In June of 2011, the City received a grant from the Housing and Urban Development Department (HUD) to initiate the 400 South and University Boulevard Livable Communities project. The focus of the project was to enhance the Community's vision of the corridor and make it a place where people could live, work and shop. Inherent in this process was the inclusion of people who rely on the corridor for their daily needs, but for various reasons do not normally participate. The group included those who are traditionally under-represented in the planning process. The project determined that future development at all 400 South stations should be compatible with the existing neighborhood scale and that development should not supplant or compete with the central business district regional services (beyond the existing Trolley Square development). Development should enhance existing urban neighborhoods within walking distance of the transit line to create areas where pedestrian activity is the focus of daily transportation needs, without excluding the automobile. These concepts are critical to groups who rely on public transportation to access housing, employment, education, health care, and other daily needs.

The 400 South and University Boulevard Trax lines run from downtown to the University of Utah. The corridor incorporates a mix of transportation options, including light rail, bus and automobile. There are several bicycle lanes that parallel and intersect the street to create a complete transportation network through the area. When the light rail line opened in 2001, the pedestrian environment improved by adding street trees and widening the sidewalk. Unfortunately, the parking lane was removed, placing a travel lane immediately adjacent to the sidewalk. In order to make the corridor more pedestrian oriented, sidewalk improvements are necessary.

The Liveable Communities project identified specific land use goals and policies that can be found in the "400 South and University Boulevard Station Area Plans" in the addendum of this Central Community Master Plan. The station area plans include specific land use policies and a future land use map that should be used to help guide future land use, capital improvements and budget decisions.

West Temple Gateway

The West Temple Gateway area extends from 700 South to the 900 South Interstate off ramp and from 300 West to West Temple and includes the 200 West / 900 South future light rail stop. The West Temple Gateway area is part of a redevelopment project area created in 1987, which included two revitalization concept plans. These are not adopted policy plans but resource documents. The 1994 plan identified alternative concepts ranging from low-density residential infill to Big Box retail uses. A second analysis in 2001, after the light rail line was constructed, provided an Illustrative Plan that proposes a mixed use transit-oriented neighborhood containing residential, retail, office, and industrial land uses. Development of a West Temple Gateway small area master plan will provide detailed development guidelines for this area.



Transit Oriented Development goal

To create TOD development with a balanced mix of uses that generates 24-hour transit ridership. This development will have places to work, to live, to learn, to relax and to shop for daily needs. The goal will be achieved through land use designations, development guidelines, zoning, and both public and private funding.

Transit Oriented Development policies

Transit-Oriented Development policies fall into these general categories: location and variety of land use.

Location

Policy TOD-1.0 Based on the Future Land Use map and specific station area plans, establish Transit-Oriented Districts that will provide residents with housing, transportation and employment options at various densities near transit stations.

TOD-1.1 Develop station area plans for each transit station within the Central Community Planning Community.

TOD-1.2 Utilize a broad community involvement approach to identify appropriately located and scaled transit oriented development that put people first.

Variety of Land Use

Policy TOD-2.0 Encourage the development of mixed-use projects near light rail stations to create a livable, walkable urban environment.

TOD-2.1 Support a variety of low-, medium- and high-density residential uses around light rail stations in TOD districts, based on the Future Land Use map designations.

TOD-2.2 At light rail stations in TOD districts, establish a centralized core of land uses that support transit ridership. Anchor transit centers with land uses that act as destination points.

TOD-2.3 Encourage a variety of commercial uses that share the same clientele and patrons. For example, movie theaters provide a clientele to patronize restaurants, arcades, and retail businesses.



Central Community Master Plan Goals and Recommendations Addendum

Please note that the following station area plans will be added into the addendum located on pages 22-23 of the current Central Community Master Plan. These items will be added right after #15 of the 'Environment' section.

400 South and University Boulevard Station Area Plans

Each of the three station area plans include subareas as identified below:

Core Area: The purpose of the core area is to provide areas for comparatively intense land development with a mix of land uses incorporating the principles of sustainable, transit oriented development and to enhance the area closest to a transit station as a lively, people oriented place. The core area is generally within a one-fourth ($\frac{1}{4}$) mile walk of a transit station platform. The core area may mix ground floor retail, office, commercial and residential space in order to activate the public realm. Buildings in this area should have minimal setbacks to encourage active outdoor use adjacent to the sidewalk, such as outdoor dining and patios that reflect the desired character of the area. Building facades should be varied and articulated, include storefronts adjacent to the street, windows on the street level, and have clearly defined entrances to provide visual interest to pedestrians. Buildings should be a minimum of two (2) or three (3) stories in height, depending on location, in order to define the street edge. Arcades, bays, and balconies are encouraged. The configuration of buildings must balance the needs of all modes of circulation with the safety and comfort of pedestrians and bicyclists. A vertical mix of uses, with office and residential above ground floor commercial uses is encouraged. A minimum of thirty (30) dwelling units per acre is encouraged within the core.

Transition Area: The purpose of the transition area is to provide a moderate level of land development intensity that incorporates the principles of sustainable transit oriented development. The transition area is intended to provide an important support base to the core area and transit ridership as well as buffer surrounding neighborhoods from the intensity of the core area. These areas reinforce the viability of the core area and provide opportunities for a range of housing types at different densities. Transition areas are generally located within a one-half ($\frac{1}{2}$) mile from the station platform, but may vary based on the character of the area. Transition areas typically serve the surrounding neighborhood; include a broad range of building forms that house a mix of compatible land uses. The minimum desired density is ten (10) dwelling units per acre. Commercial uses may include office, retail, restaurant and other



commercial land uses that are necessary to create mixed use neighborhoods. Commercial uses can be clustered around intersections and along block faces to create neighborhood nodes.

Library Station Area

The Library Urban Center Station has the highest intensity level and mix of uses along 400 South. It supports Salt Lake City's central business district in terms of building scale and use. The area has a twenty-four-hour population, active streetscapes, defined street walls and multiple types of public transit. Development generally occurs on surface parking lots or through redevelopment of underutilized parcels.

The street frontages in the Library Station area have a compact, dense, interconnected and walkable development pattern, while the interior of blocks are open generally with parking lots. Large scale development is closer to the station; and is scaled back as it moves to the areas away from 400 South. Building forms vary, but are typically oriented to the pedestrian, are multiple stories in height, and contain a horizontal and vertical mix of land uses. Buildings up to thirty stories in height are allowed within the D-1 zoned sections, while buildings in the transition zone can be approximately three to four stories in height.

The area features a variety of dense, mixed use commercial and housing developments, providing residents with a number of housing options. The station area contains important regional attractions, such as the Library, City & County Building, Leonardo Museum, and the Public Safety Building. These buildings are a regional draw, bringing thousands of people to the area each day. The area also features office, dining and entertainment options with a high level of pedestrian activity. The Library and Washington Square feature a number of civic and cultural festivals during the summer.

The station area comprises of core and transition areas. The purpose of creating the different areas is to recognize the scale and nature of existing development patterns and identify the appropriate locations for growth. The general concept is that bigger buildings with the most dwelling units and a higher intensity level of commercial space should be located closest to the station in the core. The transition area reduces the scale, mass and intensity of new development as it moves away from the core.

In addition to the civic uses, and high density development surrounding the Library Station core, there is a significant amount of underutilized land. The Library Station Area Plan encourages the development of these areas to create medium to high density housing, mixed use development and appropriate support commercial along 400 South. By doing this, the area would further its role as a vibrant and contributory feature of Salt Lake City, while maintaining its function as a support to the downtown area.

The "Transit Station Area (TSA) Development Guidelines" will be used along 400 South to encourage a walkable urban neighborhood compatible with the adjacent historic district.



Library Station Area Goals:

1. Rezone properties fronting 400 South so that the zoning reflects the vision for the station area. Rezone the properties that are in the vicinity to a mix of zoning districts that promote high density residential development while allowing limited commercial types of uses.
2. Coordinate with Utah Department of Transportation (UDOT) to improve the functionality of 400 South for pedestrians. This could be accomplished by adding additional midblock crosswalks to the transit stations, allowing on-street parking during off peak travel hours, studying the impact of reducing the road width, whether in key spots or for longer stretches and other changes to the street that are aimed at creating a safe and functional transportation corridor for all users.
3. Work with the City Transportation Division to find appropriate bicycle routes that run on or are parallel to 400 South and that connect to appropriately located north/south bike routes.
4. Require midblock connections from 300 South and 500 South that connect to 400 South.
5. Incorporate way-finding features at the station area so people can easily locate station area assets such as Library, Library Plaza, City & County Building, Leonardo Museum and the Public Safety Building.
6. Maintain the original 15-foot landscaped setback along the 400 South street frontage.
7. Require properties to share driveway access as properties are redeveloped to reduce the number of drive approaches in the station area.
8. Minimize the number of driveways, garage entrances and dedicated turning lanes on all major pedestrian routes.
9. Encourage community services, including schools, childcare and museums with pedestrian connections to transit and other land uses.
10. Extend 450 South midblock walkway through to 650 East.
11. Develop and enhance existing public gathering spaces, including parks, plazas and courtyards to attract people and transform 300 South and 500 South into active pedestrian places.
12. Increase building intensity and residential densities closest to the transit station and gradually step down further away. Parking requirements should be lower closer to the station.



13. Discourage additional or expanded stand alone automobile oriented uses such as: large surface parking lots, fuel stations, auto repair shops, auto dealerships, large scale retail development, car washes and drive-thru facilities.
14. Mid-block crossings across 400 South should be considered adjacent to the Trax stop to help pedestrians cross safely and to discourage jaywalking.

Trolley Station Area

The Trolley Station is defined as an Urban Neighborhood Station Area. Urban Neighborhoods are places that have an established development pattern that contain a mix of uses and can support an increase in residential density and supporting commercial activities. New development generally occurs as infill, occurring on undeveloped or underutilized properties. Redevelopment of surface parking lots that front on 400 South is a priority. A compact development pattern is desired in order to focus new growth at the station and respect the existing scale and intensity of the surrounding neighborhood. The highest residential density and intensity of commercial land use occur closest to the transit station and are scaled down the further one moves from the station.

The station area comprises of core and transition areas. The purpose of creating the different areas is to recognize the scale and nature of existing development patterns and identify the appropriate locations for growth. The general concept is that bigger buildings with the most dwelling units and a higher intensity level of commercial space should be located closest to the station in the core. The transition area reduces the scale, mass and intensity of new development as it moves away from the core area.

Trolley Station is a unique Transit Station Area because it is located within the Central City Historic District. The Central City Historic District is centered on the 600 East landscaped medians, which are a character defining feature of the historic district. Over time, the 600 East medians have been degraded by the continual pressure from commercial development to allow vehicle access through the medians. The policy of the Trolley Station Area is to prohibit further dissection of the 600 East medians for vehicular traffic and to maintain the historical 15 foot landscaped setback of building along 600 East.

Despite being located within the Central City Historic District, the blocks that front on 400 South have lost most, if not all, of their historic character. Without the historic character being present, the boundaries of the Central City Historic District should be reviewed and amendments to the district boundaries should be proposed.

The "TSA Development Guidelines" will be used along 400 South to encourage a walkable urban neighborhood compatible with the adjacent historic district.



Trolley Station Area Goals:

1. The primary purpose of the Trolley Station Area is to provide housing and access to higher intensity employment, commercial centers, downtown, and to the University of Utah.
2. Rezone properties fronting 400 South so that the zoning reflects the vision for the station area. Rezone the properties that are in the vicinity to a mix of zoning districts that promote high density residential development, while allowing limited commercial types of uses in appropriate places. The properties fronting 400 South should be the focus of the station area and development should focus on creating an urban neighborhood in scale and purpose, and is not intended to supplant or compete with the much higher density central business district. Regional scale development beyond the existing Trolley Square commercial development is not encouraged.
3. Coordinate with the Utah Department of Transportation (UDOT) to improve the functionality of 400 South for pedestrians. This could be accomplished by adding additional midblock crosswalks to the transit stations, allowing on street parking during off peak travel hours, studying the impact of reducing the road width, whether in key spots or for longer stretches, and other changes to the street that are aimed at creating a safe and functional transportation corridor for all users.
4. Work with the Transportation Division to find appropriate bicycle routes that run on or parallel 400 South and that connect to appropriately located north/south bike routes.
5. Require midblock connections from 300 South and 500 South that connect to 400 South.
6. Incorporate way-finding features at the station area so people can easily locate station area assets such as Trolley Square, Gilgal Gardens and Liberty Park.
7. Maintain the original 15-foot landscaped setback along the 400 South street frontage.
8. Review the appropriateness of and consider amendments to the boundaries of the Central City Historic District to remove those blocks and portions of blocks that front on 400 South from the historic district due to the lack of remaining historic character. See the “.....” map to determine the areas that should be considered for removal from the Central City Historic District.
9. Preserve the 600 East medians and prohibit further bisections of the medians for the purpose of allowing vehicular access and left turns to private property or streets.
10. Extend 450 South midblock walkway through to 650 East.



11. Further multi-modal solutions to change the way 600 South is utilized between 500 East and 700 East. Possible solutions include adding middle of the street parking, midblock crosswalks, medians, or removing a lane of traffic in each direction to add bike lanes, larger park strips or angled parking.
12. Identify zoning solutions for the block faces across from Trolley Square on 600 East and 600 South. The focus should be to encourage development on vacant parcels, increase residential density and promote the preservation and adaptive reuse of contributing structures. The surface parking lot south of Trolley Square should be rezoned to allow Trolley Square to building a parking structure, retain the historic structures fronting on 600 South and build housing.
13. Adopt zoning that increases housing potential, but does not introduce extensive commercial development on the south side of 500 South between 500 and 600 East and both sides of the street between 400 and 500 East.
14. Encourage redevelopment of strip centers into higher density with structured parking with a potential park and ride. The block bounded by 400 and 500 South and 600 and 700 East should have the highest commercial densities of the area.
15. Encourage development that is compatible with the historic development pattern in the Central City Historic District where appropriate.
16. Reevaluate the densities on the northwest quadrant of the block bounded by 300 South and 400 South and 600 East and 700 East, to allow higher densities and design that is compatible with the adjacent historic character.
17. Mid-block crossings across 400 South should be considered adjacent to the Trax stop to help pedestrians to cross safely and to discourage jaywalking.

900 East Station Area

The 900 East Station is an Urban Neighborhood Station Area due to the established and predominating residential character and the potential for infill development along 400 South which adds variations of density and intensity of building forms that blend in and complement the existing residential character of the area. The development strategy of an urban neighborhood station may include the mixing of building types and uses, including the allocation of commercial or office uses to the ground floor, and residential uses above these floors. The highest residential densities and most intense land uses are generally located closest to the station platform along 400 South between 700 East and 900 East, particularly on the south side of 400 South.

The station area comprises of core and transition areas. The purpose of creating the different areas is to recognize the scale and nature of existing development patterns and identify the appropriate locations for growth. The general concept is that bigger buildings with the most



dwelling units and a higher intensity level of commercial space should be located closest to the station in the core. The transition area reduces the scale, mass and intensity of new development as it moves away from the core area. The north side of 400 South is part of the transition area due to the close proximity of the relatively low scale nature of the residential area to the north and the desire to maintain that character and the impacts that taller buildings on 400 South would have on privacy and solar access.

Contributing landmarks and existing neighborhood characteristics will influence future development in and around the 900 East Station. These existing traits include institutional uses such as the Intermountain Health Care facility, Bennion Elementary School, and religious structures. There are also various types of commercial and a strong mixture of single and multi-family residential building types.

900 East Station Area Goals:

1. Protect historic landmark sites that currently exist in the Station Area.
2. Coordinate with UDOT to improve the functionality of 400 South for pedestrians. This could be accomplished by adding additional midblock crosswalks to the transit stations, allowing on street parking during off peak travel hours, studying the impact of reducing the road width, whether in key spots or for longer stretches, and other changes to the street that are aimed at creating a safe and functional transportation corridor for all users.
3. Maintain original 15-foot landscaped setback along the 400 South street frontage.
4. Incorporate way-finding features at the station area so people can easily locate station area assets such as Gilgal Gardens and health care facilities.
5. Protect the 800 East medians by not allowing vehicular access cuts through the existing landscaped area.
6. Encourage infill uses that will eliminate surface parking lots with an emphasis on those that front 400 South or areas that can easily access the station platform.
7. Encourage shared parking facilities between uses where applicable.
8. Focus on mixed-use development with commercial and residential uses along 400 South and the east side of 700 East. In remaining areas the primary redevelopment focus should be on residential uses.
9. If the Bennion Elementary School discontinues to be utilized as a school, the property should be redeveloped with an emphasis on residential uses along the frontages but much of the open space behind should remain, and be enhanced and available to the public.



10. Create mid-block connections that draw pedestrians to 400 South and specifically to the 900 East TRAX station. This should be emphasized between 300 South and 400 South where connections could more easily be created by linking existing rights-of-way such as, Laker Court and Strongs Court.
11. Encourage nonconforming manufacturing or warehousing uses to relocate to appropriate locations in the city.
12. Adopt zoning that promotes high density housing with appropriate design standards adjacent to Gilgal Gardens in order to activate and provide security for the park.
13. Mid-block crossings across 400 South should be considered adjacent to the Trax stop to help pedestrians to cross safely and to discourage jaywalking.

DRAFT

Attachment E

Public Input

March 6, 2012

Attn: Maryann Pickering, Principal Planner
Salt Lake City Planning Division
PO Box 145480
Salt Lake City, Utah 84114-5480

Re: 400 South Livable Communities

Dear Ms. Pickering:

Let me begin by thanking you for your efforts on behalf of our neighborhood. I am deeply interested in its preservation and vitally concerned with any proposal that might negatively impact its well-being. Indeed, even the notion strikes me with trepidation and no small measure of alarm. Please allow me to give some historical perspective to my concerns.

More specifically, I wish to address the proposed 400 South plan with regard to two of the included blocks, having boundaries as follows: 700 to 900 East between 400 to 500 South. These two blocks include an elementary school (Bennion), two churches, one of them an historic landmark (L.D.S. Tenth Ward), a city park (Gilgal Gardens), a number of single family homes (my own included), and a community open space (corner lot playground of the school).

I have called the Bennion School neighborhood home for my entire life, as has my wife. We have chosen to stay in the byways of our youth and raise our children here. All of them have attended Bennion Elementary, as do several of our grandchildren at present. Over the years we have witnessed the vital role this school plays, not only in molding the lives of our children, but also in imbuing the neighborhood with character and stability. Our children are at once a reflection of our society and a portent of its future.

The historic Tenth Ward dates back to the original settlement of the Salt Lake valley, serving then as both school and religious meeting place. The L.D.S. Church has gone to great lengths to ensure its preservation and restoration. The Tenth Ward too has functioned as a community hub, engendering meaning into the lives of its parishioners and lending strength to common community ideals.

Gilgal Gardens is a unique sculptural treasure and an unequivocal community asset. Created by one of the Tenth Ward's past bishops, Thomas B. Child, it has drawn interest both locally and nationally. It remains a fascinating insight into folk art and shared religious heritage.

My children are fourth generation in the family home that I now live in. This victorian structure has served its occupants admirably (for more than a hundred years), as have a number of other homes in the referenced area. Combined, they bring a personality to the community and an integrity to its demeanor.

Now, more to the point of the matter. I am told that the proposed plan would better provide for the Trax ridership. I am perplexed. I thought Trax and the public transit system were

intended to serve the people, not visa-versa. To have it otherwise flies in the face of reason and would seem to me a clear case of the tail wagging the dog. The whole concept strikes me as absurd and would appear to constitute an instance of planning in reverse.

I have always understood good city planning to be balanced in its approach, conservative in its application, and protective of community interests in its overall implementation. Single family housing, community institutions, and neighborhood integrity are all vital aspects of the wholesome residential character that is its ideal, and together they provide the sought after stability of a well-balanced neighborhood. Any measure that would encroach upon, or otherwise threaten, the basic nature of such a community infrastructure would seem to me the direct antithesis of the kind of prudent foresight and conscientious planning conducive to the common weal and health of such a neighborhood.

I am an engineer by training and profession, and am accustomed to envisioning the likely outcomes of plans and designs. I was raised from two generations of architects and am wont to think myself attuned to considerations of functionality and usage. As well, and more importantly, I hope that I am sensitive to the people comprising the neighborhood and to their common concerns and aspirations. It is my perception that any attempt to up-zone the aforementioned area of which I speak would constitute a real and substantial threat to the community good and I am determined to fight against such an incursion.

I cannot but feel that turning our vibrant multi-faceted neighborhood into a high-rise feeder system for Trax would be a serious mistake and an unfortunate step backward for its residents. I wish to go on record as being adamantly opposed to such a measure. I will be more than glad to personally carry a petition throughout the neighborhood against its implementation, and will welcome the opportunity to address the planning commission regarding our concerns, should such responses be required. Please keep me informed as to the progress of this initiative so that I may meet it with commensurate counter-measures. Thank you.

Respectfully,
a concerned parent, neighbor, and homeowner,



Kent Jackson Fetzer
763 East 500 South
Salt Lake City, Utah 84102-2905

Phone (801) 359-4463 HOME
(801) 526-6222 WORK
E-mail kjf@eastontp.com

cc: James E. Yapias, Principal
Bennion Elementary School
429 South 800 East
Salt Lake City, Utah 84102

Bishop Randy Willardsen
L.D.S. Salt Lake Tenth Ward
821 East 500 South
Salt Lake City, Utah 84102

Mary Lee Peters, President
Friends of Gilgal Garden
474 G Street
Salt Lake City, Utah 84103

Gary Felt and Esther Hunter, Co-Chairs
East Central Community Council
606 Trolley Square
Salt Lake City, Utah 84102

March 15, 2012

Attn: Terry Cano, Section Manager
Architecture and Engineering Division
The Church of Jesus Christ of Latter-day Saints
50 East North Temple Street
Salt Lake City, Utah 84150-0012

Re: Historic Tenth Ward -- Zoning Change

Dear Brother Cano:

I am writing to apprise you of a pending zoning change, now being advanced by Salt Lake City, that could substantially impact an historically significant Church site. I refer to the Salt Lake Tenth Ward Chapel and Meeting House, located on the corner of 400 South and 800 East. This historic building dates back to the original settlement of the Salt Lake valley, serving then as both school and religious meeting place. The Church has gone to great lengths to ensure its preservation and restoration. It is, I believe, the only extant structure remaining of the originally constructed ward meeting halls in the Salt Lake valley.

The Tenth Ward building has admirably served its parishioners from its inception to the present. This noble edifice is at once inspiring in its design and uplifting in its bearing; and, as well, carries in its provenance a notable historic heritage. From pioneer times onward it has functioned as a community hub, engendering meaning into the lives of its members and lending strength to common community ideals. President Hinckley recounted that it was within this building that he first received his testimony of the gospel. It has in recent times been seismically retrofitted and remodelled at no small expense and is listed as a national historic site. (I believe President Kimball himself interceding on its behalf). I have always thought the Tenth Ward particularly beautiful and edifying -- full of character and ennobling in its spirit. One would be hard pressed to match its simple comeliness and far-reaching significance. I am deeply interested in its preservation and anxiously concerned with any proposal that might negatively impact its well-being.

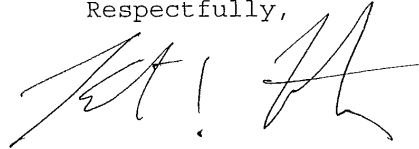
Now, to the heart of the matter. Although the proposed zoning change would not directly affect the Tenth Ward Building Site itself, it might as well. It would alter the zoning of the surrounding properties directly adjacent to it. This change would allow for the building of high-rise structures (up to 70 feet on the Fourth South side, and up to 50 feet on the Eighth East side) with very little frontage or parking required. It would thus serve to potentially hem in the Tenth Ward with discordant construction and ill-matched usages.

At the very least, albeit likely inadvertently, the City has shown insensitivity to the history and heritage of the area by

making statements like: "Review the appropriateness of and consider amendments to the boundaries of the Central City Historic District to remove those blocks and portions of blocks that front on 400 South from the historic district due to the lack of remaining historic Character." (400 South Livable Communities publication, February 2012.) At worst, they could set in motion developments that would significantly detract from a major historical site of both the City and the Church.

The City Planning and Zoning Department is still at a stage of soliciting and accepting public input (through the end of this month) regarding the proposed zoning changes. It would seem to me appropriate at this juncture for the Church to express its position regarding the proposed changes and their contingent impacts on its interests. I do not presume to instruct you on your course of action, but wish only to inform and alert you to matters potentially of bearing within your purview. I hope my efforts will prove useful in this regard and trust you will know best how to proceed.

Respectfully,



Kent Jackson Fetzer
763 East 500 South
Salt Lake City, Utah 84102-2905

Phone (801) 359-4463 HOME
(801) 526-6222 WORK
E-mail kjf@eastontp.com

cc: Bishop Randy Willardsen
L.D.S. Salt Lake Tenth Ward
821 East 500 South
Salt Lake City, Utah 84102

Grant J. Fetzer, Facility Specialist
L.D.S. Salt Lake Tenth Ward
452 South 800 East
Salt Lake City, Utah 84102

Maryann Pickering, Principal Planner
Salt Lake City Planning Division
P.O. Box 145480
Salt Lake City, Utah 84114-5480

March 23, 2012

Dear Editor,

Salt Lake City would seem to be acting with callous disregard in its proposed rezoning of the 400 South corridor. For some areas the change makes sense, for others it is wholly inappropriate.

For instance, within a two block area you have an elementary school (Bennion), two churches, one of them an historic landmark (L.D.S. Tenth Ward), a city park (Gilgal Gardens), a number of single family homes, and a community open space. The rezoning would incentivise developers to construct high-rise mixed commercial/residential structures there.

The City has also shown insensitivity to history and heritage in its pursuit of the plan. The Tenth Ward structure dates back to the original settlement of the Salt Lake valley, serving then as both school and religious meeting place. The L.D.S. Church has gone to great lengths to ensure its preservation and restoration. Yet the City seeks to "... remove those blocks and portions of blocks that front on 400 South from the historic district due to the lack of remaining historic Character." (400 South Livable Communities publication, February 2012.)

A major impetus behind the proposed development fast-tracking is to "... provide an important support base to the core area and transit ridership" and, more broadly, to foster "transit oriented development." (Ibid.) I am perplexed. I thought Trax and the public transit system were intended to serve the people, not visa-versa. This would appear to be a case of the tail wagging the dog, a sort of planning in reverse.

I have always understood good city planning to be balanced in its approach, conservative in its application, and protective of community interests in its overall implementation. Therefore, I believe the City should be held to a standard of thoughtful consideration, judicious deliberation, and measured action in its implementation of the City Master Plan. Anything less would be incompetence at best, and a violation of the public trust at worst.



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April 13, 2012

cc: Maryann Pickering, Principal Planner
Salt Lake City Planning Division
P.O. Box 145480
Salt Lake City, Utah 84114-5480

Joyce P. Valdez, Community Liaison
Office of the Mayor
451 South State Street, Room 306
P.O. Box 145474
Salt Lake City, Utah 84114-5474

Luke Garrott, Councilman
City Council Offices, Room 304
451 South State Street
Salt Lake City, Utah 84111

400 South Livable Communities

Do you support zoning regulations that would allow more transit oriented development along 400 South between 200 East and 1000 East?

Public comments as of April 17, 2012, 1:00 PM

All Participants around Salt Lake City



As with any public comment process, participation in Open City Hall is voluntary. The statements in this record are not necessarily representative of the whole population, nor do they reflect the opinions of any government agency or elected officials.

400 South Livable Communities

Do you support zoning regulations that would allow more transit oriented development along 400 South between 200 East and 1000 East?

Introduction

The Planning Division is considering making master plan, zoning and historic district boundary changes to properties near the 400 University TRAX line. The areas where the changes are to occur are located near the Library, Trolley and 900 East stations. The proposal includes new zoning designations for the area which are designed to encourage transit oriented or mixed use development around the TRAX stations.

400 South Livable Communities

Do you support zoning regulations that would allow more transit oriented development along 400 South between 200 East and 1000 East?

As of April 17, 2012, 1:00 PM, this forum had:

Attendees:	303
Participants around Salt Lake City:	30
Hours of Public Comment:	1.5

400 South Livable Communities

Do you support zoning regulations that would allow more transit oriented development along 400 South between 200 East and 1000 East?

All Participants around Salt Lake City

Scott Weaver in District 5

April 14, 2012, 11:35 AM

Living in close proximity to 400 S and TRAX I would like to state that I avoid 400 S whenever possible and never consider using TRAX. There is nothing convenient about either one. One could describe it as a transit disaster area.

It is amusing though, that current planners have to redo the work of previous planners so they can plan for the future where in the future other planners will redo their plan to make their own plan for the future and still never get it right. But I guess that is the role of government.

Let us hope that your rezone includes the Neff Floral property and forces the owner to do something with the blight they have provided the residents of SLC for over the past 20 years.

Anyone that is affected by a "Historic" designation should fight it to the death. You will never truly own your own property ever again.

Name not shown in District 6

April 11, 2012, 2:42 PM

I have already written off 400 south as a place to avoid because it is so congested, noisy, and unpleasant to navigate. I don't care what you do with it since I don't envision being compelled to go there again.

Name not shown outside Salt Lake City

March 31, 2012, 10:12 PM

Absolutely in support of this.

Sam Steele in District 3

March 31, 2012, 2:44 PM

Absolutely! This is long overdue and should have been done as soon as the TRAX line was planned for 400 South. Although better late than never, waiting so long really was a missed opportunity. I would encourage the planning division to do the same along the proposed streetcar line in the southern end of the city (and encourage South Salt Lake to do the same) if it hasn't already been done - don't wait for the line to be in for 10 years before making a change like this!

Name not shown in District 5

March 31, 2012, 10:16 AM

Yes, the area should be re-zoned to encourage multi-family and transit oriented development. The Trax line makes the area an extension of downtown and the University. Now all we have to do is convince UTA to reconnect downtown and the U without an annoying transfer.

Right now the 400 S. strip is a lost opportunity. I sometimes ride the 205 bus line to catch Trax (sometimes with kids) and walking the 1 block between stops is a bit of a nightmare for pedestrians because there are so many driveways coming out across the sidewalk. Since 400 S. is where lots of people connect from bus/Trax there ought to be more consideration of pedestrian accommodations -- especially at the Trolley station because the bus stop is a block away from the bus.

400 South Livable Communities

Do you support zoning regulations that would allow more transit oriented development along 400 South between 200 East and 1000 East?

All Participants around Salt Lake City

The mall between 700 and 600 S doesn't quite do the trick for pedestrians, again because of all the driveways. It's nice to have some buildings out by the sidewalk, but there are too many cars crossing the sidewalk.

The designated bike route goes through the area at 600 S. and it always feels especially hazardous going past the busy driveways at the Smith's Marketplace. the 300 S. Bike route is generally a nicer way to get downtown.

I was glad to see that some of the Office Max parking lot has become available for transit riders.

Calvin Tribby in District 4

March 30, 2012, 5:04 PM

Yes, please allow more transit oriented development along 400 South. Concomitantly, the pedestrian environment needs to be enhanced for safety and reoriented for accessibility; this should include a decrease in the number of driveways allowable and a restriction on parking lots from fronting sidewalks to meet these needs, respectively. Hopefully these considerations will be addressed in the zoning regulations, in addition to allowing mixed land uses, higher densities, and no minimum parking requirements for development.

Name not shown in District 6

March 30, 2012, 3:17 PM

I totally support this and hope to see more transit oriented development happen along 400 S

Peter Makowski in District 7

March 30, 2012, 11:13 AM

This absolutely should happen. The 400 S trax line has been a wasted opportunity from an urban design and TOD stand point. The city has an opportunity to take advantage of an existing transit corridor and create a more walkable and pedestrian friendly environment. Granted, 400 S is a state highway and can only be so pedestrian oriented, but changing design standards along the corridor will at very least steer the area in the right direction for the future.

Name not shown in District 4

March 29, 2012, 10:39 AM

Yes, why hasn't this been done already? No brainer!

Barbara Gardner in District 4

March 29, 2012, 9:57 AM

I live a block and a half away from the 400 South corridor, and I wholeheartedly support these changes. Many other commenters are right about the current use being a slice of suburbia in the city, oriented around cars (going very fast!) rather than pedestrians and transit. Anything that makes the area safer and more inviting for pedestrians has my vote. I would also like to see an emphasis on locally-owned, unique businesses rather than chains, franchises, and big-box stores. I would love to see 400 South lined with businesses like Cafe Niche and Coffe Noir rather than Cafe Rio and KFC.

Derek Hardman in District 4

March 29, 2012, 9:37 AM

400 South Livable Communities

Do you support zoning regulations that would allow more transit oriented development along 400 South between 200 East and 1000 East?

All Participants around Salt Lake City

The TRAX corridor along 400 South has long been a disappointment, offering all of the vices of car-oriented planning (strip malls, drive-thru restaurants, six lanes of traffic, etc) with none of its (hypothetical) virtues.

As many of the existing structure are approaching stages of retirement and renovation, it is *critical* that the City learn from its previous oversights and amend/clarify a plan/zoning strategy that facilitates high-density, transit-oriented development while providing an outline through which new "character" districts may emerge, or successfully merge with the pedestrian-friendly, transit corridor of Main Street in the CBD.

I fully support any action by the City that rebuffs efforts to install new car-oriented structures while a "master plan" for 400 South is drafted and approved.

Kyle Deans in District 5

March 29, 2012, 9:36 AM

I am completely in favor of these changes, and I am glad that they are finally coming to fruition. We haven't seen much urban growth along this corridor in the ten years since Trax has been opened, because there hasn't been appropriate zoning. This type of zoning won't do much to reduce our pollution in the valley, but it will create opportunities for growth in jobs and population while not increasing our pollution. 400 S and N. Temple both have huge potential to be very dense and vibrant urban corridors in Salt Lake City. With the ever expanding network or transit TOD will only increase in popularity. As was pointed out by in another comment, trends across the nation are pointing towards more city dwellers, young professionals and empty nesters are moving to urban nodes for many reasons. While I would prefer the zoning expand further than just the 400 S face of the blocks near a few of the stations, I understand the necessity to maintain the character of many of these neighborhoods and appreciate the planning departments efforts to balance both TOD and neighborhoods. I am anxious to see these master plans adopted and development to begin as SLC continues it's solid growth.

Keith Bartholomew in District 4

March 29, 2012, 9:23 AM

Yes I do support TOD regulations in the 400 South TRAX corridor. Creating an active, vibrant pedestrian-oriented environment along 400 South is critical to advancing Salt Lake City's quality of life. Right now, the street is a slice of suburban sprawl in the heart of our city. To turn that around, higher density, mixed use, pedestrian designed development needs to be encouraged to fill in vast parking lots and empty spaces. Small-scale open spaces/pocket parks are important, too.

Marshall Palm in District 4

March 29, 2012, 9:05 AM

I am glad that Luke Garrot is actively involved in the development along this corridor. As the 20 year owner of a single family home in the area we are keen to see that the neighborhoods are not further destroyed by landlords and ill- conceived apartment complexes with their associated social challenges.

Matthew Kirkegaard in District 6

March 29, 2012, 8:54 AM

I absolutely support these new zoning regulations. We need more mixed use, efficient living in our

400 South Livable Communities

Do you support zoning regulations that would allow more transit oriented development along 400 South between 200 East and 1000 East?

All Participants around Salt Lake City

city. This would reduce our dependence on car transport, increase livability, decrease pollution, yield economic benefits in the area, etc. The list goes on and on. In short, this transit oriented development would be a boon for our city and the area in question. This is precisely the action Salt Lake should be taking as it moves into the future, in clear contrast to the "conventional" development of sprawl our state has supported for so long.

William Littig in District 3

March 29, 2012, 8:33 AM

TRANSIT ORIENTED DEVELOPMENT (TOD) TREATS THE CITY LIKE IT IS A GAME "SIMMS CITY". NINTH EAST IS A LOGICAL LIMIT AS EAST OF THERE WITH FEW EXCEPTIONS ARE SMALL RESIDENTIAL STREETS "NEIGHBORHOODS". THE RAIL SYSTEM SHOULD NOT BE DESIGNING THIS OR ANY CITY. RAILS HAS BEEN REMOVED BEFORE DUE TO TECHNOLOGY AND POPULARITY. THE CURRENT SYSTEM WAS INITIALLY DESIGNED TO COMPLIMENT THE BUS SYSTEM AND VICE VERSA. THAT PLAN HAS FAILED AND WE NOW HAVE LARGE AND GROWING PARKING LOTS AT MANY STATIONS. PRICES HAVE CONTINUALLY RISEN ALTHOUGH WE ARE TOLD THE SYSTEM IS A SUCCESS. WITH LARGE PARKING LOTS AT ALL "UTA" FACILITIES MAYBE WE NEED NEW LEADERSHIP AND PLANNERS INVOLVED IN THE CITY AND PLANNING. ALLOWING SEVEN STORIES NEXT TO SMALL HOMES AND THINKING THOSE NEW TENANTS WON'T HAVE CARS IS NOT REALITY IT IS CRUELTY.

Vincent Oles in District 6

March 29, 2012, 8:18 AM

I support this proposal. By allowing mixed use close to Trax and bus routes it will provide a more vibrant urban character to develop. Mixing residential, commercial and office uses will promote more activity 24/7, support a more walkable/bikeable neighborhood and encourage more use of mass transit . Consideration of more landscaping and public art along this corridor could provide shade in the summer and showcase some of the great talent we have in the State.

Studies indicate that more people will be moving to city centers to avoid long commutes and to be closer to the variety of activities and functions found in the urban core.

This plan would promote an active link between the University and downtown.

Shannon Orr in District 6

March 29, 2012, 7:59 AM

Yes, mass transit is necessary for our community and our environmental goals.

From: [Kirk Huffaker](#)
To: [Pickering, Maryann](#)
Subject: 400 South
Date: Monday, April 16, 2012 11:16:04 AM

Hi Maryann

After our conversation last week, I'm formalizing my comments on behalf of UHF for the 400 South rezoning.

1) Regarding Gilgal Garden, we believe it is imperative to include the OS zoning category in those that require setbacks when adjacent to higher density zones such as the TSA 50 and 90 that are proposed and border the garden. Because sunlight is so important to the garden's well-being, all setbacks to the southwest and west will be important to maintain to allow as much sunlight to permeate the garden as possible.

2) Regarding the historic Rumel House on 500 East, we believe it would be best if the property be included in the TSA 90 zone that is proposed. It's our understanding that this will reduce the overall development potential directly on the property itself, which is better for preservation purposes. In addition, the proposed zoning will require a setback on future structures on the property adjacent from the south and west, therefore better respecting the scale of this adjacent historic property.

Thank you for considering our comments. Please contact me if there are any additional clarifications.

Kirk

Kirk Huffaker
Executive Director
Utah Heritage Foundation
POB 28
Salt Lake City, UT 84110-0028
p: 801.533.0858 x 105
www.utahheritagefoundation.org
www.slmodern.org

From: [cindy.cromer](#)
To: [Pickering, Maryann](#); [Norris, Nick](#)
Cc: [maha/mark.Barrani/Rex](#)
Subject: RE: meeting on 400 S TSAs
Date: Sunday, April 15, 2012 7:48:37 PM

all-I've just reread the East Downtown Master Plan (1990) in search of mistakes that the City has acknowledged making in the area and still have to go through the East Central Neighborhood Plan (1984). I realize that these plans are superseded by the Central Community Master Plan, but they are much more specific and they actually acknowledge where the City erred. Identifying those mistakes is critical to having any chance at getting it right this time.

There are numerous references to transfer of development rights in the East Downtown Plan, pp. 3, 4, 8, and 10. Transfer of development rights are also recommend as a tool in the Preservation Plan, which has now been sitting in the City Council office since January, 2010. c

(Just so I don't sound completely negative: The height restrictions to protect view corridors in the East Downtown have been remarkably well respected, well until the Public Safety Building. I am really amazed at how well the City has implemented them considering that the basis is a master plan and not a separate ordinance.)

Subject: meeting on 400 S TSAs

Nick and Maryann-Because my opposition is significant and the time line is now driven by the temporary zoning restriction, I am feeling an urgency to get together and talk about why what won't work on 400 S. I've organized my thoughts into--

1. the errors that the City has made in the past
2. the few, very few, good ideas it has implemented in the past
3. the concept of the Central City Historic District
4. the similarities and differences between 400 S and North Temple (The differences are why I have concluded that a points program similar to North Temple's will not work.)

I will fill in the bullet points on these topics somehow before we meet.

I am disappointed beyond speaking at the moment (the deficit won't last long) that the City invited Nadine Fogarty and Tim Van Meter here for the conference on North Temple and then failed to take advantage of their considerable expertise and ask them about the 400 S Corridor. Turns out that there is a transit corridor in Denver that hasn't seen any investment after 10 years. Getting their input was not your responsibility; I think the City was more interested in bragging about North Temple than in finding solutions on 400 S. I will be ranting about the failure to get Fogarty's and Van Meter's opinions on 400 S as soon as I have read their stuff. I don't want you to view my ranting as criticism of you in any way. Nick's presentation at the conference was very good and I hope he has an opportunity to recycle parts of it.

Maha Barrani who has lived for many years on the block north of the "residence inn," now apartments, would like to join us. She is a parent and self-employed. Her schedule is most open between 9 and 3. She has appointments this week Wednesday morning and Thursday from 12:00-1:00. Please let us know if you have any time open that would accommodate her schedule. Thursday morning won't work for me. Thanks so much, cindy

From: [Elizabeth Giraud](#)
To: [400 South](#)
Subject: RE: question about zoning specifics
Date: Thursday, April 12, 2012 4:37:21 PM

Thanks, Maryann. When I look at the Future Land Use Plan, it looks like the proposed changes have missed most of the significant buildings, but not all.

*Elizabeth Giraud, AICP
UDOT Architectural Historian
(801) 965-4917
UDOT Central Environmental
4501 S. 2700 W./Box 148450
Salt Lake City, Utah 84114-8450*

>>> 400 South <400South@slcgov.com> 4/12/2012 3:41 PM >>>

Hi Elizabeth.

Here is a draft of our changes. We are using the existing TSA zoning district, but making a few changes as you can see by this document.

Thank you. Maryann

From: Elizabeth Giraud [mailto:egiraud@utah.gov]
Sent: Tuesday, April 10, 2012 2:28 PM
To: 400 South
Subject: question about zoning specifics

Hi.

Where can I find information regarding the specifics of the proposed zones depicted in the future land-use map for the 400 S. rezoning project?

Elizabeth Giraud

*Elizabeth Giraud, AICP
UDOT Architectural Historian
(801) 965-4917
UDOT Central Environmental
4501 S. 2700 W./Box 148450
Salt Lake City, Utah 84114-8450*

From: [Kirk Huffaker](#)
To: [Pickering, Maryann](#)
Cc: [Elizabeth Bradley-Wilson](#)
Subject: 400 South plan
Date: Monday, April 09, 2012 5:04:11 PM

Hi Maryann

In viewing the recent 400 South TSA map, I noticed that the property at 365 S 500 E has been shaded the purple that equates to 90' height. The properties adjacent to the north have not been identified for rezoning as TSA. The property in question is the historic Eliza Gray Rumel House on which UHF has held a preservation easement since 2001. It was donated by a Rumel descendant before she passed away a few years later. Since an easement in placed on this property, condemnation, court, and acts of God are the only ways to remove or extinguish the easement. With this in mind, I'd like to request on behalf of UHF that SLC Planning reconsider rezoning this parcel and allow it to continue to be attached to the neighborhood zoning district to the north. This change would not only help preserve one of the oldest single family houses in east downtown, but could help UHF avoid a great amount of unnecessary legal and financial strain in defending its easement in the future.

Sincerely,
Kirk

Kirk Huffaker
Executive Director
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POB 28
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p: 801.533.0858 x 105
www.utahheritagefoundation.org
www.slmodern.org

From: [Lee Pettit](#)
To: [Pickering, Maryann](#)
Subject: 400 South Livable Communities
Date: Thursday, April 05, 2012 3:35:55 PM

April 6, 2012

Ms. Pickering:

My name is Lee Pettit I am the President and co-owner of Western Garden Center at 550 South 600 East. I was informed by our landlord (also a WGC co-owner) the city is redoing the zoning ordinance that affects this property.

I would request your consideration of several things that could affect our business, were we to make any improvements that would trigger a change to our "grandfathered" current uses. Please include "office", "retail", "reception/dining" and "Trolley Square parking" for properties located to the west of Trolley Square.

Most of the properties to the west of Trolley are already offices, including "wrap-arounds" on both 500 and 600 South. Our current uses include a reception area, offices, our retail, and parking that we share with VCBO Architecture and Trolley Square (Trolley is currently not a formal lessee due to their insurance carrier exclusion which will hopefully be resolved). Our parking area use has helped minimize Trolley Square patron parking in adjoining residential neighborhoods at night. Trolley Square's grand staircase entrance faces west is directly across from our parking lot.

We hope that rezoning does not affect the traditional use of this property, particularly as we keep trying to improve both our property and our future, to keep up with the times while reflecting the history of both 600 East and WGC.

Thanks for your consideration. Lee Pettit

From: [Wray Feathers tone](#)
To: [Pickering, Maryann](#)
Cc: [Hasenberg, Angela](#)
Subject: 900 East Station Zoning Proposals
Date: Monday, April 02, 2012 9:03:57 PM

I am writing to express my concern with the proposed zoning changes of 400 South. I live at 958 East on 400 South. I live at the entrance to Fletcher court. I purchased my house because I wanted to be near downtown and like the feel of a neighborhood in the city.

I am concerned with the zoning changes proposed east of the Village Inn. There are 5 houses between The Village Inn and Fletcher court. These houses have been purchased by landlords, including the trax house that rent them out with no attention to how they impact the neighborhood. Starting with the trax house next to the Village Inn cars are parking on the lawns and yards are not taken care of, sidewalks are not shoveled, and garbage left around. I have called to register my complaints and get passed around with no results.

Parking on Fletcher court is an issue, when I remodeled my house I was told that Fletcher Court was a public street and therefore my house setback had to be 10 feet as stated in the code. When I call about parking I am told it is a private street and there is nothing enforcement can do. I had to change my construction plans. Which way is it?

I am worried that increasing the density of housing and rental residence will further degrade the neighborhood and as the city has been absolutely worthless in assisting the residents of Fletcher court with any of these problems.

The idea that minimum desired density is 10 dwelling units per acre is outlandish. I don't want to live next to apartments. I have invested my life savings in my house and this proposal destroys my investment. Our neighborhood on Fletcher court is owned by the residents with the exception of two houses. We have a great community with young kids and a community garden. Having this high density rezoning imposed upon us will destroy what we have built. I have no desire to be boxed in by 40 foot apartment buildings full of drunk college students. I already have to deal with the Wilshire condos, with christmas trees, beer bottles and anything they throw off their balconies, and when I call the police I'm told they can't do anything unless I know where it came from. There is a corporation that oversees these types of building and they should be cited for violations made by their members, and their president should be liable. It is unfair to us homeowners that they are faceless because they are a condo association. I am sure this will continue with the apartments if they are built to the west of me.

My experience with the city will guarantee that whatever promises they or their representatives make, they will be broken.

The rezoning should stop on the west side of 900 East.

I supported trax but have perpetual problems with the loudness of the squeaking wheels and we have never experienced any mitigation of the issue. The city put in really bright lights along the street saying it was required, but never told me that when I was the only person to attend the trax meeting. My house is lit up inside every night by these lights. This kind of imposition is unacceptable.

I am concerned about what kind of impact this will have on the neighborhood, and how the city will not work with us to mitigate its impact. I though the IHC construction across fourth south was ok in the beginning, but the noise from the construction especially Saturday mornings is unbearable. The noise from poorly secured plastic sheeting when it is windy has kept me awake three nights, but there is no one to call to resolve the problem. Who approved early Saturday morning work times. I was never asked. I am across the street, I cannot imagine the crap the apartment residents next to the construction are putting up with.

Firstly to resolve these problems there would need to be a liaison that we could call to resolve

construction noise and other impacts. It is unacceptable that I can be kept awake or otherwise impacted and can not get it resolved. Whoever signs off on these kind of projects should have to pass their cell and home numbers to all of us impacted so we can call them at any time when we are being impacted.

If this seem unreasonable, then it it unreasonable to impact us.

Secondly the city and the construction company as well as the financiers should have to put up a large escrow account as in \$100,000 dollars which, if they impact us, say with two days of flapping noisy plastic sheeting they should have to forfeit to those impacted if they do not act promptly to secure them. This happened with the IHC construction, they sheeted it poorly before a storm and there was no one to call. The police did not even know who to contact. That is unacceptable. I was kept awake for two nights because it was a weekend.

My house is my investment for my retirement. If the city does what they propose they destroy my retirement. The city has failed to mitigate issues that are already ongoing with the rentals along 400 South, my calls are pushed around into someone's voice mail and never returned.

Until the city shows some interest and ability to mange things I am opposed to any changes in my neighborhood. It is not progress, it is profit driven. Enforce the rules about parking in front lawns, how many cars can be parked in a yard, how many units can be rented, how many people can live there, and I may be convinced that this could be a good move. Otherwise this plan is destructive to the fiber of my neighborhood and I absolutely oppose it.

Wray Featherstone
958 East 400 South
Salt Lake City, UT 84102
801.414.3064

Sent from my iPad

From: [Andrew Wilder](#)
To: [Pickering, Maryann](#); [400 South](#)
Cc: [Andrew Wilder](#); [Elizabeth Neilson](#)
Subject: 400 South Livability Project
Date: Friday, March 30, 2012 3:21:19 PM

To whom it may concern,

Seven years ago I moved to Salt Lake City to attend graduate school at the University of Utah. That same year, I purchased a single-family residence located at 438 Fletcher Court. After leaving the university I remained in Salt Lake, and for the past two years have worked as a software engineer for a small, biomedical device company located in Sugarhouse. I continue to live in the same house, and plan to remain here for the foreseeable future.

My decision to live in the 400S 900E area was based on many factors. The most prominent of which were: affordability of housing, proximity to TRAX and the University, and ease of access to the various shops and amenities necessary for modern urban life. I chose Fletcher court specifically, because it is a comfortable and quiet street with modest houses and welcoming neighbors.

As someone with an engineering background I fully support the goals of guided community development. More often than not our wealth of space and reliance on automobiles leads to chaotic sprawl poorly suited to pedestrian or centralized transit models. Urban planning is a means of ensuring that communities develop in an organized manner increasing livability for all, especially the most affected stakeholders. Overall, the 400 South Livability Project appears to be a well thought out plan that will greatly benefit Salt Lake and the communities bordering the transit corridor. There are several areas, however, where the plan could be improved.

1) The S/E corner of the 400S 900E intersection, currently designated as TSA Core, and the region directly to the East, currently designated as TSA Transition should be re-designated as residential to fit the existing character of this area. As a resident of this block, I strongly feel that development fitting the TSA Core and TSA Transition designation would adversely impact the neighborhood's stable, residential, family-centered character. The presence of the Wilshire apartment building - which at 70+ feet towers over the single story houses - presently exerts a tremendously negative impact on atmosphere of Fletcher Court. Addition of another 70 foot building at the S/E corner of 400S 900E and a row of 45 foot buildings on the North side of the block would effectively destroy the vitality and character of this area. Additionally, the increased demand for parking would exceed the already limited supply. While I recognize the need for more high-density housing to accommodate Salt Lake's growing population, I feel the apartment building currently located just south of this corner already meets the spirit of this goal in an unobtrusive manner.

2) The plan should explicitly identify areas to be used as public green space. It is not enough simply to require that developers devote 10% of lot area to open space. This requirement could be met by private, enclosed court yards - which would not benefit the community. Clearly it is difficult to convince owners of valuable private real estate to convert land to non-income producing open land. Thus, incentives for such actions need to be generous and clearly articulated. Additionally, areas currently owned by the public could be repurposed as parks. For example, 800 E from 300S to 500S could be converted from vehicular use into

park and pedestrian use.

3) The plan should explicitly encourage the development of pedestrian-only commercial zones where individuals are explicitly protected from cars. Pedestrian-only streets are a common part of nearly all European cities and are instrumental in engendering a sense of livability and encouraging widespread adoption of a pedestrian or public transit model. Many US cities have created such spaces - One example is Perl St. in Boulder, CO. Another example - in Salt Lake - is the portion of Main Street between North Temple and South Temple.

4) The plan should include stipulations requiring retail use in the majority (>80%) of ground floor space. Commercial retail spaces foster an inviting atmosphere, whereas ground floor residential spaces create an atmosphere of exclusion for passing pedestrians. Small, proprietor-owned shops that sell items affordable to intended residents should be explicitly incented. Proprietor-owned shops are a key ingredient to the formation of livable tight-knit communities. Shops covering the range of basic necessities should be present on each block; this will specifically facilitate the ability of residents to live without automobiles. Restaurants and other commercial operations that encourage people to gather and interact with one another should be incented. Chain businesses should be explicitly discouraged.

5) The plan should specifically address measures for encouraging a broad population profile. Specific attention should be given to encouraging typically alternative categories such as art and music production. Healthy communities are more often than not composed of individuals engaged in a broad spectrum of economic activities.

6) Plans for accommodating increased parking demand should be explicitly addressed. The increase in high-density housing will inevitably increase the demand for parking. Facilities for parking should be built by the city. Additionally, a parking accommodation plan should be required of every high-density housing unit developer.

As is, The 400 South Livability Project represents a tremendous opportunity to create a truly healthy urban environment geared towards pedestrian and centralized transit. Inclusion of the provision I have outlined, would, I feel, help the plan foster growth of an even more attractive and self-sustaining community.

Sincerely,
Andrew Wilder
438 Fletcher Court
Salt Lake City, UT 84102

From: [Elizabeth Neilson](#)
To: [Pickering, Maryann: 400 South](#)
Subject: Regarding "Livable Communities" - 900 E and 400 S
Date: Friday, March 30, 2012 11:20:27 AM

Dear Ms. Pickering,

As a resident of Fletcher Court (950 E at 400 S), an area affected by the proposed rezoning, I would like to add my perspective to the conversation.

I was present at the second community open house in mid-March and spoke for some time with several members of the planning division. Upon leaving the event, I had the impression that the division wishes sincerely to create the most agreeable possible environment for future residents, without failing to consider existing residents in affected areas.

At the core, I am fully in support of a more walkable, residential/commercial environment close to public transit. However, there are several particulars in the current plan that I believe will compromise current residents' quality of life to an excessive and unnecessary degree.

First, if there is "high density" housing on both west and east sides of 900 E, moving to "transition" housing bordering Fletcher Court's western side, how will parking be arranged? Even if the "target audience" for these units is the university population, I speculate that the vast majority of people will have at least one car. Being realistic, Salt Lake simply does not have the high-functioning network of public transit to warrant car-less-ness for all but the most deliberate individuals. The ensuing traffic jams around the 900/400 intersection would certainly influence negatively Fletcher's residents. A parking structure abutting our street would give it much less of a welcoming, commerce- and residence-based, urban feel than the desired plan intimates, and widening either 900 E or 400 S to accommodate greater circulation would make the area feel less like a neighborhood and more like a 700 E... nearly an interstate. One way to alleviate this density and unsightliness without curtailing the project is to limit the highest density housing to the west side of 900 E. There is already less residential conflict on this side of the street, and the total number of residents, and thus cars, would be somewhat reduced.

A less tangible but equally important consideration is the personality of the 900 E/400 S area. Fletcher Court has a distinct and attractive personality, which even the planning division has acknowledged in not adding the street to the area under consideration for rezoning and therefore, eventual modification. If part of the draw for would-be apartment dwellers is the proximity of a hip, hidden oasis like Fletcher Court and East Place, then choking these streets with tall buildings and traffic jams is certainly counterproductive, as it will dispirit this unique community considerably. Therefore, although retaining the view and the privacy of Fletcher Ct. and East Pl. is of personal interest to me, it is also in the interest of the planning division if it wishes to attract the sort of people who will add further urban vitality to our neighborhood in particular and to the light rail environs in general.

All that being said, I am in support of higher-density housing near the arteries of public transit. If the highest-density housing ended on the west side of 900 E, such changes would be much more realistic and create higher quality of life for the residents of Fletcher Court and East Place. On the east side of 900 E, housing

should be present at a maximum density of "transition" in the current location of the Village Inn, grading to 20-foot structures in the location of the current rental houses along 400 S. Parking structures should be created in the current vicinity of Office Max, which is already established as a non-residential area. Ground-floor businesses in the 900 E/400 S vicinity would be a great boon to all, even the residents of Fletcher. However, the greatest degree of cohesion will be attained if these spaces offer practical services to real residents, such as groceries and sundries. Restaurants and bars can absolutely cater to the real residents, as can specialty boutiques. These would all be welcome additions to create a more vibrant and livable atmosphere along Trax. However, if these are present to the exclusion of commerce that promotes a walk-able, practical, local life for residents, then the dense urban living experiment cannot fully be a success. In short, if there is housing added to the east side of 900 E, there should also be a small grocery store. There should be such a store on every rezoned block. This increases dramatically the "livable" nature of the communities under consideration.

In conclusion, I am writing to protect my own vested interests in the life I have created on Fletcher Court. Because this street is filled with other long term inhabitants like myself, we have an enviable and beautiful community. Others who see it appreciate its uniqueness and relaxing tone. Enclosing it on multiple sides by high-rise buildings will stymie our quality of life and privacy, and may even cause residents to search elsewhere for homes. However, the current proposed rezoning of the east side of 900 E is also not in the best interests of Salt Lake City. Another traffic jammed area does nothing for livability, and apartment blocks lacking in personality-charged surroundings do not attract the demographic that I expect the city would like to see along Trax. Therefore, I request that you consider limiting the 70-foot building zone to the west of 900 E, as well as parking. I also implore that you prioritize grocery stores and restaurants/bars above boutiques in our neighborhood, although the latter are welcome if the former are accounted for. It is my sincere belief that with these measures in place, Salt Lake can move forward in improving its urban personality and livable downtown, while maintaining the flair and character of older neighborhoods and their long-term residents.

Yours Sincerely,

Elizabeth Neilson
438 Fletcher Ct

From: [Esther Hunter](#)
To: [Pickering, Maryann](#)
Subject: Fw: 400 South
Date: Wednesday, March 28, 2012 7:41:36 AM

Here is the feedback that I have from Kathy. best, e

-----Original Message-----

From: Kathy Scott <kscott724@gmail.com>
Date: Wed, 28 Mar 2012 01:02:06
To: <estherehunter@live.com>
Subject: Re: 400 South

Indeed I am very concerned about the proposal to include the property SOUTH of Village Inn as TSA UN Core and the property EAST of Village Inn as TSA UN Transition.

Clearly, the block (900 to 1000 East and 400 to 500 South) includes property within 300 feet of a TRAX station. However, the block has significant history and integrity as a neighborhood with historic homes.

- * Both East Place and Fletcher Court are "court" streets lined with homes dating back to 1910 or before. To designate the portions of 900 East and 400 South that provide entrance to the courts as appropriate for dense and very tall structures would profoundly and negatively affect the character, charm, access, and property values of the narrow historic courts.
- * Also, the proposed changes would eliminate a smooth transition from core to residential. Keep in mind that Village Inn is the only commercial use on either block face and completely out of character with the residential nature of the rest of the block faces.
- * The best position expression for eliminating the proposed changes (or considering them to be an error) is the description of the plan/designation on the postcard sent to residents: a plan for "400 South between approximately 300 East and 900 East." Yes, 900 East is clearly a demarcation boundary between commercial, higher density property and an historic home neighborhood.

Please share my thoughts with the appropriate task groups.

Kathy Scott
(801) 322-5288

From: [Judi Short](#)
To: [Pickering, Maryann](#)
Subject: 4th south
Date: Monday, March 26, 2012 4:05:30 PM

I see this is on the agenda for the PC on 3/28. Is this just a briefing? No action? I have serious concerns and worries about Gilgal Garden, and would like to be able to talk about it, but cannot attend this meeting. Can you tell me the timeframe as this goes forward? Judi

--
Judi Short

From: [Lon Clayton](#)
To: [Pickering, Maryann](#)
Cc: [Lee Pettit](#)
Subject: 400 South Livable Communities
Date: Thursday, March 15, 2012 5:28:56 PM

Ms. Pickering,

I attended the recent Open House at Trolley Square regarding the Transit Station Area Zoning proposals. I am the principal of Clayton Properties I, LLC, which owns the Western Garden Center property at 550 South 600 East. (Western Garden Center is also my employer.) While we are not directly in a Transit Station Area Zoning, we are affected. John explained to me the potential Mixed Use Zoning for our property as envisioned.

I request that "office" (not just retail) be included for currently commercial properties on the west side of 600 East between 500 South and 600 South. I also request "Trolley Parking" be included for this same area, as it is on the south side of 600 South between 600 East and 700 East.

Include office in Mixed Use -

Currently "office" is either the only use or at least a significant component for the properties, including corners, along the west side of 600 East between 500 South and 600 South. The sole exception is an apartment building which - based solely on age - may be ripe for redevelopment. While Western Garden Center is a nursery, we have well over 10,000 sq ft of office on our property, including a portion of the VCBO Architecture building and well as our company offices which serves all 3 of our stores. I feel this traditional use should be respected and allowed to continue into the future, to survive property improvement, redevelopment and/or sale. Limiting the commercial component of Mixed Use Zoning to "retail only" in this particular block would force property owners and potential shop-owners to compete (probably poorly) with Trolley Square retail across the street. I feel the current (and future) office space users in this one block stretch of one side of the street (architecture, art gallery, interior design, retail store offices) would not otherwise be downtown high-rise office users.

Include Trolley Parking in Mixed Use -

Traditionally, VCBO Architecture and Western Garden Center have offered nighttime parking to Trolley Square and its patrons which parking significantly reduces the street parking overflow from Trolley Square into adjoining residential areas. Allowing the current and future property owners on the west side of 600 East between 500 South and 600 South the option of continuing to get "double use" of our parking lots for our daytime and Trolley's nighttime use is economically beneficial to us and beneficial to the community by reducing Trolley nighttime patron parking in nearby residential neighborhoods.

(We currently - temporarily - do not have a parking agreement with Trolley, because of insurance issues, time of use issues, potential change of Trolley ownership, and the fact that they are currently way under leasing capacity, and therefore don't have all the tenants - and parking customers - they want and need.)

Either restriction (limiting commercial to retail only instead of including office, and implicitly restricting Trolley parking) of this west side of one block of 600 East will

not benefit the nearby residential neighborhoods. Virtually all automobile traffic on this one block is and will be non-residential - because of Trolley Square - for the foreseeable future. Permitting future flexibility of present and potential property owners to include office use and Trolley parking use honors current use and makes sense for the future.

I, of course, understand redevelopment plans require City approval so it isn't like I think this request gives property owners carte blanche, but it does allow both the owners and the City more flexibility to agree on a "best use" mutually benefitting the city, the nearby neighborhoods, and the property owners.

Incidentally, our intent is to continue our Western Garden Center business, we have no re-development plans for the known future. I speak now because the last re-zoning of our property was to Neighborhood Commercial which is designed for and limited to properties perhaps only 1/10 of our property size. I hope now for zoning which will reflect our parcel size, our present continued use, compatibility with our adjoining 600 East commercial neighbors, and provide for future use all at the same time.

If you wish to post my comments publicly, please do me (and the public) the favor of editing the long-winded explanatory background so as to make this short enough to be readable!

Respectfully,

Lon Clayton
Western Garden Center
550 South 600 East
Salt Lake City, Utah 84102
801-364-7871
lon@westerngardens.com

From: [Norris, Nick](#)
To: "[cindy.cromer](#)"
Cc: [Pickering, Maryann](#)
Subject: RE: 400 South Transit Workshops
Date: Tuesday, February 21, 2012 7:55:26 AM
Attachments: 02-14-12 Central Community Master Plan 400 South amendment.docx
400 South Proposed Changes.pdf

In order for comments to get in the Planning Commission packets, we need them one week before the meeting. If they are received after that, they get emailed to them up to the day of the meeting, at which point they receive a copy just prior to the meeting. I am unsure what the deadline for the City Council is, but I believe they follow a similar protocol.

The Landmarks Commission will have a public hearing on the changes and we will ask them to make a recommendation. I expect both the PC and HLC to be briefed in March and hold public hearings after that, likely in April depending on feedback from stakeholders and info received at the briefings. Maryann can correct me if I have some of the dates wrong.

I have attached a copy of the modifications to the Central City master Plan and the proposed map. I had to reduce the file size of the map dramatically, so let me know if the version in this email is not legible. We are working on a simpler, easier to read version of the changes and the changes to the zoning ordinance identified in the map. We do expect changes to occur before we hold public hearings. Let us know what you think and we can talk about it at the open houses.

Nick Norris
Planning Manager
Salt Lake City Planning Division
801-535-6173
nick.norris@slcgov.com

From: cindy.cromer [mailto:3cinslc@live.com]
Sent: Monday, February 20, 2012 2:29 PM
To: Norris, Nick
Subject: FW: 400 South Transit Workshops

Nick-I got the o-o-o message. Can you respond in Maryann's absence? c

From: 3cinslc@live.com
To: maryann.pickering@slcgov.com
Subject: RE: 400 South Transit Workshops
Date: Mon, 20 Feb 2012 14:26:08 -0700

Maryann-Thanks for the announcement of the meetings. A couple of questions:
Could you remind me of the deadline for getting the proposal to the Planning Commission and then the City Council? (I think it should go to Landmarks too because of the Central City Historic District and the proximity to the Trolley Square Site.)
Is there a draft document available to read prior to the meetings, either in-house or public?

Thanks,
cindy cromer

From: Maryann.Pickering@slcgov.com
To: Maryann.Pickering@slcgov.com
Date: Fri, 17 Feb 2012 12:38:42 -0700
Subject: 400 South Transit Workshops

Hello.

Please see the attached notice for two upcoming meetings regarding the 400 South study that the Planning Department has been working on.

Thank you and we hope to see you there.

Maryann Pickering, AICP
Principal Planner
Salt Lake City Planning Division
451 South State Street, #406
PO Box 145480
Salt Lake City, UT 84111-5480
(801) 535-7660

From: [Judi Short](#)
To: [Pickering, Maryann](#)
Cc: [Dansie, Doug](#); [Paterson, Joel](#)
Subject: Re: 400 South Transit Workshops
Date: Friday, February 17, 2012 4:16:52 PM

I just skimmed through this report, let me tell you my concerns. I'm the Utah State University Master Gardener in charge of the Gilgal Garden. I am also on the Friends of Gilgal Gardeen (FOGG) Board. Right now, the environment around this park is pretty good, and we don't have too many undesirables in the park, that I am aware of. However, I have a big concern about the Wonder Bread factory. They have just declared Chapter 13, or some variation. I've been told, by someone that works there, that they have plans to move to the west side somewhere down the line.

That may have changed since their financial problems began. If that parcel redevelops, it will have a big impact on the Gilgal Garden. The factory is a huge buffer on the west side of the garden. If someone takes that building out, the whole ecosystem in that garden could change, and we should be cautious of that. I like the concept of 3-5 story apartments up against the street along 400 South, but would not be in favor of that at the rear of the parcel, because a tall building could produce too much shade. We could live with a paring terrace on the south part of that parcel that isn't more than two stories high, and if it had a wall on the garden side, or at least chain link up the east side of the parking terrace. The park is secured at night and we don't want people to be able to access the park through that parking lot or terrace. Thank you. Judi Short

On Fri, Feb 17, 2012 at 12:38 PM, Pickering, Maryann
<Maryann.Pickering@slcgov.com> wrote:

Hello.

Please see the attached notice for two upcoming meetings regarding the 400 South study that the Planning Department has been working on.

Thank you and we hope to see you there.

Maryann Pickering, AICP

Principal Planner

Salt Lake City Planning Division

451 South State Street, #406

PO Box 145480

Salt Lake City, UT 84111-5480

From: [cindy.cromer](#)
To: [Pickering, Maryann](#)
Subject: Powerpoint and District boundaries
Date: Wednesday, October 05, 2011 9:55:57 AM

Maryann-Thanks for all the effort you and the A Team put into the open house. The turnout was discouraging and reminded me of a parents' group I held years ago. NO ONE showed up, not even the supposed co-therapist who had a meltdown that night when he learned that his daughter had not been attending school!

Could you send the Powerpoint presentation that you prepared? I didn't want to ask for a "private showing" at the open house.

Also, the boundaries of the National Register District for Central City may be useful. Barbara Murphy or Nelson Knight at SHPO could help you because only the buildings within the National boundaries would be eligible for Federal tax credits. Janice and Carl may also have the info. c

From: [Darv Andersen](#)
To: [400 South](#)
Subject: Trash Trucks and expanding zoning variances
Date: Tuesday, October 04, 2011 3:07:05 PM

Unfortunately, I didn't get involved earlier with this and missed the meeting last night.

I have 2 suggestions to add to some of the other input from residents:

- 1) Noise reduction between the hours of 10am and 7am: There are constant garbage trucks in the area that come at odd hours to commercial buildings. We constantly have garbage picked-up next to our home between 1am and 6am.
- 2) Zoning restrictions on residential houses: There are height restrictions that prohibit 3 story homes or using more of the land than 60% that would help build larger homes in the area. Variances are difficult to acquire or take too long to get that would allow roof top decks (from what I know, a roof top deck is considered a story when this isn't the case) or more garage space (from a conversation with an employee of planning and zoning there can't be more than 50% front of house as garage). Creating more parking needs to be encouraged. We constantly hear complaints from residents that there isn't enough parking for visits. Off street parking should be encouraged and variances shouldn't be required to add a wider garage to a house when the setbacks aren't breached.

Thanks. It appears that other issues that I had were brought up by others. I appreciate the opportunity to add to others.

Darv Andersen
801-554-8526

From: james.cowley
To: ray.kingston; [400 South](#)
Cc: [Carolyn Abravanel](#); [Arla Funk](#); [Jo-Ann Wong](#); [Robert Austin](#); [Scott Wood](#); [Tobin Atkinson](#); [Bruce Spiegel](#); [Sue Heath](#); [Andrew Shaw](#); [Bob Springmeyer](#); [Bonnie Stevens](#); [Ken Sanders](#); [Dave Mash](#); [Randy Holladay](#); [Don Roylance](#); [Dave Jolley](#); [Derek P Jensen](#); [Greta DeJong](#); [John deJong](#); [Lynne Van Dam](#); [Palmer Depaulis](#); [Ed Firmage](#); [Freddie Nebeker](#); [Linda M Fontenot](#); [Mary Fresques](#); [allene fowler](#); [Bradley Gubler](#); [Bob Guymon](#); [Greg Thompson](#); [Larry Goldsmith](#); [Stephen Goldsmith](#); [The Luke Garrott Campaign](#); [Esther E. Hunter](#); [Creed Haymond](#); [LON JENKINS](#); [Joan Woodbury](#); [Jean Irwin](#); [Jean Welch Hill](#); [Jeff Bair](#); [Kait Kingston](#); [Erik Kingston](#); [Ken Nichols](#); [Kyle J. Bettit](#); [Elise Lazar](#); [Love, Jill](#); [Linda Caine](#); [Willie Littig](#); [Ray Wheeler](#); [jim williams](#)
Subject: Re: 400 South re-zoning
Date: Friday, June 03, 2011 6:21:25 PM

Mr. Kingston,

That is a terrific letter articulating a much needed rational and thoughtful approach. God knows that the public at large needs government to protect us against those marauding developers who will in their search for profit sacrifice our lifestyle. I do hope it is read and understood by those with political responsibility to our citizens.

James P. Cowley

From: ray kingston <enteleki.ray@gmail.com>
Date: Fri, 3 Jun 2011 17:17:01 -0600
To: <400South@slcgov.com>
Cc: Carolyn Abravanel <abravanel.cc@gmail.com>, Arla Funk <afun@comcast.net>, Jo-Ann Wong <cameragir184109@gmail.com>, Robert Austin <utahrobert@msn.com>, Scott Wood <abstractlabs@gmail.com>, Tobin Atkinson <tobinatkinson@hotmail.com>, Bruce Spiegel <brucepiegel00@msn.com>, Sue Heath <sue.heath@slc.k12.ut.us>, Andrew Shaw <ashaw@slcpl.org>, Bob Springmeyer <bobspring@bonnevilleresearch.com>, Bonnie Stevens <bhsartsmaven@gmail.com>, Ken Sanders <ken@dreamgarden.com>, Dave Mash <davemash1@comcast.net>, Randy Holladay <drholladay@msn.com>, Don Roylance <roylanced@gmail.com>, Dave Jolley <davejolley@msn.com>, Derek P Jensen <djensen@sltrib.com>, Greta DeJong <greta@catalystmagazine.net>, John deJong <john@catalystmagazine.net>, Lynne Van Dam <lynnevandam@mac.com>, Palmer Depaulis <palmer@utah.gov>, Ed Firmage <efirmage@xmission.com>, Freddie Nebeker <mfnebeker@xmission.com>, Linda M Fontenot <go2hebgen@aol.com>, Mary Fresques <mary@artistjohnbell.com>, allene fowler <allenefl@comcast.net>, Bradley

Gubler <bradleygubler@yahoo.com>, Bob Guymon
<parkerkipling@yahoo.com>, Greg Thompson <greg.c.thompson@utah.edu>,
Larry Goldsmith <lgoldsmi76@msn.com>, Stephen Goldsmith
<stgoldsmith@gmail.com>, The Luke Garrott Campaign
<lukegarrottcampaign@gmail.com>, "Esther E. Hunter"
<estherehunter@hotmail.com>, Creed Haymond <jchaymond@yahoo.com>,
LON JENKINS <lajenkins@joneswaldo.com>, Joan Woodbury
<joan@rriewoodbury.com>, Jean Irwin <jeantokudairwin@gmail.com>,
james cowley <pcow@comcast.net>, Jean Welch Hill
<jwhill4ag@gmail.com>, Jeff Bair <jeff_a_bair@hotmail.com>, Kait
Kingston <kait@theperformerstudio.com>, Erik Kingston
<erik.kingston@gmail.com>, Keri Nichols <kerinichols1@yahoo.com>, "Kyle
J. Bettit" <kylebetit@yahoo.com>, Elise Lazar <eliselazar@xmission.com>,
Jill Love <jill.love@slcgov.com>, Linda Caine <go2hebgen@mac.com>,
Willie Littig <williamlittig@hotmail.com>, Ray Wheeler
<Ray.wheeler@earthlink.net>, jim williams <jimwilms@yahoo.com>
Subject: 400 South re-zoning

Ms. Pickering,

Your offer for resident's input regarding the up-zoning of this corridor from the Center to the University is very much appreciated. I have lived within the the University neighborhood since 1952, residing now at 1070 East 400 South. I own two properties, my private residence and a triplex, located at 416-18-20 South 1100 East. During the mid-60's, I was recruited to donate my time to prepare a comprehensive study of the quality and quantity of residential units located

between South Temple and 900 South and from University Street to 900 East. At that time, there was a common practice (supported and encouraged by then, SLC Planning and Zoning Director, Vernon Jorgenson), to allow the demolition of private dwellings, willy-nilly, and replace them with a shotgun-type, 3-story high, long, narrow apartment buildings, usually without off-street parking. Most of that building types are still in this neighborhood. The zoning at that time was R-7. All neighborhoods in the study district were surveyed. The information collected was to identify the type of unit, the density, parking provided and the general upkeep of the

property. This study was a response to the on-going demolition of historic residences, to diminishing property values caused by shoddy, and ugly construction, and the over-crowding of the neighborhood streets with cars parked bumper to bumper along the curbs. I am certain that you have witnessed some of these structures. The survey documents including a detailed map of ALL properties within the study area and written material were presented to the SLC Planning and Zoning Director at a public meeting, followed by committee discussions, the result of which was to change the zoning designation from R-7 to R-3A. It was this change which, essentially, stimulated the preservation of hundreds of historic dwellings in the University neighborhoods.

My concern with the proposed 400 South plan, is that it will further erode the existence of private, single family dwellings adjacent to the corridor. It is my

belief that the designated change in zoning should be limited to only one city block in any direction from the Trax stations. As I am also sure that you are aware of the prime goal of any developer is to get the most possible income from the land, regardless of the detrimental impact and fallout on the neighbors. A classic example of this is the "Incline Terrace", and the City Apartment complexes which were allowed to be built at the curve of 400 South to 500 South, beginning at 1000 East, perhaps the sorriest project to be built near the University of Utah. The eastern-most of the Incline Terrace buildings caught fire 2 years ago, and nearly wiped out the others, and without incredible luck and the efforts of the SL Fire Department, the entire neighborhood of residence up to 1100 East and over to 500 South could have been destroyed.

I recognize that the idea of 'upping the density' around Trax Stations is deemed as being very important to the current City Administration. However, there ARE (OR NEED TO BE) limits, and I firmly believe that the efforts suggested in the 400 South plan would, by 'creeping, up-zoning' development interests, destroy the adjacent 'edges' of the up-zoned areas. That phenomenon is standard by-product of over-zealous developers, who are in ABUNDANCE in Utah. Whatever is decided, I hope that these sensitivities are respected. I also know that trying to change already 'adopted' policy in the minds of this administration is a non-starter. But I still have hope that the folks like you, in positions of leadership, will be true to your oath to protect and respect the citizens of the City.

Sincerely, M Ray Kingston FAIA.

From: [Ray Kingston](#)
To: [400 South](#)
Cc: [Carolyn Abravanel](#); [Arla Funk](#); [Jo-Ann Wong](#); [Robert Austin](#); [Scott Wood](#); [Tobin Atkinson](#); [Bruce Spiegel](#); [Sue Heath](#); [Andrew Shaw](#); [Bob Springmeyer](#); [Bonnie Stevens](#); [Ken Sanders](#); [Dave Mash](#); [Randy Holladay](#); [Don Roylance](#); [Dave Jolley](#); [David Tucker](#); [Derek P Jensen](#); [Greta DeJong](#); [John deJong](#); [Lynne Van Dam](#); [Palmer Depaulis](#); [Ed Firmage](#); [Freddie Nebeker](#); [Linda M Fontenot](#); [Mary Fresques](#); [Allene Fowler](#); [Bradley Gubler](#); [Bob Guymon](#); [Greg Thompson](#); [Larry Goldsmith](#); [Stephen Goldsmith](#); [The Luke Garrott Campaign](#); [Esther E. Hunter](#); [Creed Haymond](#); [LON JENKINS](#); [Joan Woodbury](#); [Jean Irwin](#); [James Cowley](#); [Jean Welch Hill](#); [Jeff Bair](#); [Kait Kingston](#); [Erik Kingston](#); [Keri Nichols](#); [Kyle J. Bettit](#); [Elise Lazar](#); [Love, Jill](#); [Linda Caine](#); [Willie Littig](#); [Ray Wheeler](#); [jim williams](#)
Subject: 400 South re-zoning
Date: Friday, June 03, 2011 5:17:09 PM

Ms. Pickering,

Your offer for resident's input regarding the up-zoning of this corridor from the Center to the University is very much appreciated.

I have lived within the the University neighborhood since 1952, residing now at 1070 East 400 South. I own two properties, my private residence and a triplex, located at 416-18-20 South 1100 East. During the mid-60's, I was recruited to donate my time to prepare a comprehensive study of the quality and quantity of residential units located between South Temple and 900 South and from University Street to 900 East. At that time, there was a common practice (supported and encouraged by then, SLC Planning and Zoning Director, Vernon Jorgenson), to allow the demolition of private dwellings, willy-nilly, and replace them with a shotgun-type, 3-story high, long, narrow apartment buildings, usually without off-street parking. Most of that building types are still in this neighborhood. The zoning at that time was R-7. All neighborhoods in the study district were surveyed. The information collected was to identify the type of unit, the density, parking provided and the general upkeep of the property. This study was a response to the on-going demolition of historic residences, to diminishing property values caused by shoddy, and ugly construction, and the over-crowding of the neighborhood streets with cars parked bumper to bumper along the curbs. I am certain that you have witnessed someof these structures. The survey documents including a detailed map of ALL properties within the study area and written material were presented to the SLC Planning and Zoning Director at a public meeting, followed by committee discussions, the result of which was to change the zoning designation from R-7 to R-3A. It was this change which, essentially, stimulated the preservation of hundreds of historic dwellings in the University neighborhoods.

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Sincerely, M Ray Kingston FAIA.

From: [Judi Short](#)
To: [Pickering, Maryann](#)
Subject: Re: 400 South/University Blvd Livable Community Workshop, Thursday, May 19, 2011
Date: Wednesday, May 11, 2011 8:48:40 PM

I am very sorry to miss this workshop, I have another conflict, Can you let me know of future meetings? I served on the city committee for the 4th south light rail line when it was originally built in 2001, and am interested in any possible changes. Thanks! Judi Short

On Fri, May 6, 2011 at 2:41 PM, <noreply.ced@slcgov.com> wrote:

This information was sent with automated software and is not monitored for replies. noreply.ced@slcgov.com is the group responsible for this information.

The Salt Lake City Planning Division recently began a project to study potential changes to the 4th South transit corridor. The goal of the project is to create station area plans and zoning standards for three stations stops along the University TRAX line. The three stations that will be evaluated include the Library, Trolley and 900 East stops.

The scope of the project is to address the following: identify redevelopment opportunities; promote the construction of new affordable housing; provide people with choices in the mode of transportation; choices in housing location and type; access to employment centers; and to assist in implementing the City's goals towards becoming a more livable city.

In conjunction with the project, the City has received a grant from the US Department of Housing and Urban Development (HUD) to assist with the public outreach process. Part of the requirement is for us to reach out to individuals from traditionally under-represented groups. Our hope is that we will get several participants to provide their input regarding the study area.

We would like to invite you to attend our first community workshop on May 19. The workshop will be held from 6pm to 8pm at the Main Library in the 4th Floor Conference Room. Attached is a flyer that can be printed out and posted within the community. In addition, if you maintain a distribution list as part of your business or service agency, we would greatly appreciate if you could send it out to those who may be interested.

Additional information or questions can be directed to Maryann Pickering, [801-535-7660](tel:801-535-7660) or maryann.pickering@slcgov.com

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Judi Short

From: [Norris, Nick](#)
To: ["cindy.cromer"](#); [Pickering, Maryann](#)
Subject: RE: TRAX stations on 400 S
Date: Thursday, April 07, 2011 1:16:41 PM

Cindy,

Thank you for your comments. We will be meeting with property owners, as we do with most zoning map amendments. We typically meet in smaller groups, so there are only about a dozen present at a time. That way we can focus on specific areas, block faces, etc. I anticipate this will be done on this project.

The ½ mile radius is a study area. As we gather input, we will start to piece together a map that shows properties that would be most likely to change in some manner, whether it is change of use, new development, addition, etc. We anticipate most of this will happen within ¼ mile of a transit stop, but may also include some thought on reviewing the zoning between ¼ and ½ mile to see if it appropriate to change it in order to promote more development happening within ¼ mile. That decision will be determined as we gather public input. SLC may not be able to support TOD up to a ½ mile now, but we may be able to 20-30 years down the road.

We would love to have any research you have done on historic properties along the corridor and within the study area. Unlike North Temple, which had limited historic resources, this study area has a high number of resources, spread throughout the study corridor.

As we stated with North Temple, our goal is to work with any interested stakeholder to identify a clear and agreed upon vision for the study area and to put regulations in place that make desirable development easy to realize and undesirable development difficult or impossible. It will be a fun project, and we look forward to it.

Nick Norris AICP
Planning Manager
Salt Lake City Corp.
451 South State Street #406
PO Box 145480
Salt Lake City, UT 84111-5480
(801)-535-6173
Nick.Norris@slcgov.com

From: cindy.cromer [mailto:3cinslc@live.com]
Sent: Thursday, April 07, 2011 12:29 PM
To: Pickering, Maryann; Norris, Nick
Subject: RE: TRAX stations on 400 S

Maryann and Nick-I have now been working on TOD on this corridor for over 12 years, and frankly I am interested in getting the job finished with this HUD grant. I will probably begin my public comments with the history of how the residents have been correct so far. So in the spirit of getting the job done this time:

I hope that you will start by convening the PROPERTY OWNERS along 400 S from 200 E to 700 E and find out why not a single one took advantage of the TC-75. I believe that I know, but you have to get the information from the horses' mouths. I do not see the meeting as a public one, but one by invitation limited to property owners. The results could direct the future efforts. For example, when the City throws out a bunch of carrots, perhaps it needs to put a time limit on how long property owners have to graze on them.

Re the 1/2 mile radius: All of my properties are located within a 1/2 mile radius of a TRAX station. What I find is that my properties located within 1-2 blocks of a station attract tenants who rely on TRAX and my properties 3+ blocks away do not. I suspect that is a reflection of the current market conditions in rental housing. There are enough vacancies within 2 blocks of a TRAX station to accommodate the people who rely on TRAX. This analysis would suggest that the City should look at a time frame in the redevelopment process and focus on the 1/4 mile radius first. Failure to do so will perpetuate the problem we have now in which developers purchase cheaper land further away from the stations and then petition for an upzone, demolishing the existing AFFORDABLE housing in the process.

Third suggestion: We need an inventory of the historic resources between 200 E and 550 E which would fall within the Station Areas. I did some hasty work for comments I made to the City Council about the lack of design guidelines in the TC-75. Yes, the zone addresses the pedestrian but it does nothing for the interface with neighboring properties. I still have the photos I took and will get them to you. The inventories for the National Register areas from 550 E to University Street will need to be updated to account for demolitions. There are some very well-informed residents and business owners who could help with this task.

Again, thanks Maryann for attending last night. I'm sorry that I didn't get to meet the person who must be your daughter. cindy

From: 3cinslc@live.com
To: maryann.pickering@slcgov.com
CC: nick.norris@slcgov.com; ccnc@rock.com; nickrupp@gmail.com
Subject: TRAX stations and CCNC
Date: Thu, 7 Apr 2011 11:39:03 -0600

Maryann-Thanks for covering the CCNC meeting last night. Tom maintains an e mail list that exceeds the number of people who normally attend our meetings. We will start collecting names and e mail addresses of people who are interested in the TRAX station areas.

Some of your business cards were left over. I will drop them off at the receptionist's desk the next time I'm in Planning. cindy

From: esther.e.hunter
To: Norris.Nick
Cc: Pickering.Maryann; Gary.Felt
Subject: Re: 400 South
Date: Tuesday, March 22, 2011 3:17:38 PM
Attachments: ADU+ECC+Position+3.17.11[1].pdf

Good to hear and very helpful Nick. This will be a very worthwhile process. The approach you are taking is a very thoughtful one. Attached is the letter the Executive Board recently send in on another policy related to ADU's. While unrelated, I am attaching it because the first two pages reflect the concerns and hopes of the Community Council area as a whole. As you know the ECC has long worried that this area needs this type of careful planning so that we change what needs to change, grow in good ways and keep what is so treasured all at the same time encouraging sustainable practices. Broad brush rarely works well for us and yet we want to be good neighbors and involved to find the best of all ideas. We look forward to working with you. best, e

Esther E.Hunter
Co-Chair
Eastside Community Council &
East Central Planning District
606 Trolley Square
Salt Lake City, Utah, 84102
east.central@live.com

From: Norris.Nick
Sent: Tuesday, March 22, 2011 3:02 PM
To: esther.e.hunter
Cc: Pickering.Maryann
Subject: RE: 400 South

I think the only thing we are considering at this point is that we want to focus future growth in appropriate places, and one of those places is along transit corridors and at transit stations. The questions are how do we do it and what is appropriate in each corridor and at each station. For 400 South we also want to analyze why the TC-75 zone is not working, the characteristics of the corridor, the limitations within the corridor, etc. It may be similar to N Temple, it may not. We are looking at the process as identifying an agreed upon vision and what needs to happen to implement that vision. The vision will be based on input from all stakeholders and incorporate existing city policies and best planning principles. The process, input, etc. will determine the final output. Our grant is really focused on improving the public process first and removing barriers to transit oriented development second.

From: esther e hunter [<mailto:estherehunter@hotmail.com>]
Sent: Tuesday, March 22, 2011 1:33 PM
To: Norris, Nick
Cc: Gary Felt
Subject: Re: 400 South

Sounds good Nick. We will help promote the workshop as well. It might even make sense to have a few smaller updates at the community meetings over the next many months to help keep everyone involved and informed.

Is there a revision of the plan being considered and or used on North Temple related to the new zoning plan? Not sure what it was called...block zoning? Need to dig your presentation out.

People are very interested in this approach. I have what you last presented but wondered if there is a modified version.

Does what I am asking make sense?

I would like to let the community development land use committee see the concepts for North Temple. I have the City Council staff report for North Temple. Anything that you might have electronically to let people get their heads around this will help set the right tone.

Appreciate your help. best, e

Esther E.Hunter
Co-Chair
Eastside Community Council &
East Central Planning District
606 Trolley Square
Salt Lake City, Utah, 84102
east.central@live.com

From: [Norris, Nick](#)
Sent: Tuesday, March 22, 2011 1:23 PM
To: '[esther e hunter](#)'
Subject: RE: 400 South

Okay. It will either be me or the project manager (Maryann Pickering). We don't have a date yet, but we will also be holding a community workshop in the next few months, hopefully in April, to help identify a vision for the areas around the Library, 700 E and 900 E stations.

From: esther e hunter [<mailto:estherehunter@hotmail.com>]
Sent: Tuesday, March 22, 2011 11:10 AM
To: Norris, Nick
Subject: Re: 400 South

That would be fantastic...May 12th.

From: [Norris, Nick](#)
Sent: Tuesday, March 22, 2011 7:47 AM
To: '[esther e hunter](#)'
Cc: [Gary Felt](#)
Subject: RE: 400 South

Probably. What date is the May meeting.

From: esther e hunter [<mailto:estherehunter@hotmail.com>]
Sent: Monday, March 21, 2011 11:57 PM
[east.central@live.com](mailto:esther.e.hunter@live.com)

From: [cindy.cromer](#)
To: [Pickering, Maryann](#)
Subject: RE: TRAX station areas
Date: Thursday, March 10, 2011 11:57:07 AM

Maryann-Thanks so much. I just whizzed through the document. It is the perfect size to send to folks in the community councils before we meet. I'm really pleased about the proposed outreach---very much a challenge in Central City and the Bryant neighborhood because of the transient residential population. This approach has so much more potential than what the City tried to force on the neighborhoods for 10 years. Yipee! cindy

From: Maryann.Pickering@slcgov.com
To: 3cinslc@live.com
Date: Wed, 9 Mar 2011 14:14:01 -0700
Subject: RE: TRAX station areas

Hi Cindy,

Attached is a copy of the HUD grant application. I would have to check on a concise document regarding North Temple with Nick when he returns. I'll get with him when he is back and then send you an email.

Again, as we move forward, we will keep you and other up to date so everyone can participate in the process.

Thanks, Maryann

From: cindy.cromer [mailto:3cinslc@live.com]
Sent: Tuesday, March 08, 2011 1:49 PM
To: Pickering, Maryann
Subject: RE: TRAX station areas

Maryann-Is a copy of the HUD grant application available? I can dig through the mountain of info I have on North Temple, but if you can recommend something concise from the North Temple effort, that would be helpful. I do plan to gather whatever materials I can and get together with other people soon. Only a couple of us followed the project on North Temple. So I can at least start there if I can't get a copy of the grant. Thanks for any help, cindy

(The long-term residents of these neighborhoods have already been working on Transit Oriented Development for over 10 years. So I hope that you'll consider us resources in all phases of the effort. We know the neighborhoods on a structure-by-structure basis.)

From: Maryann.Pickering@slcgov.com
To: 3cinslc@live.com
CC: joel.paterson@slcgov.com
Date: Mon, 7 Mar 2011 11:03:39 -0700
Subject: RE: TRAX station areas

Hi Cindy,

We are just beginning our initial research and assessment of the 400 South project. At this time, we do not have any materials or a proposal.

Once we complete our initial assessment of the area, we will begin having meetings to get

feedback from the members of the community. We had planned to include those community councils you noted and I will make sure you get materials once we have something available.

Please let me know if you have other questions, Maryann

From: cindy cromer [mailto:3cinslc@live.com]
Sent: Friday, March 04, 2011 4:05 PM
To: Pickering, Maryann
Cc: Paterson, Joel
Subject: FW: TRAX station areas

Maryann-Joel has referred me to you in my quest to locate a summary of the proposal to develop station areas along 400 S. Something in an electronic format would be ideal so that I could send it to the chairs of the Downtown, Central City, and East Central Community Councils. Please let me know if you have something that I could distribute. As outlined in my message to Joel below, we would like to get together and once more informed on the subject, meet with you. If you have materials from the North Temple project which are relevant, those would be helpful, too. I have lots of information on the North Temple station areas--too much in fact. Thanks, cindy cromer

From: joel.paterson@slcgov.com
To: 3cinslc@live.com
CC: Maryann.Pickering@slcgov.com
Date: Fri, 4 Mar 2011 14:11:01 -0700
Subject: RE: TRAX station areas

Cindy,

Maryann Pickering is the lead planner for the 400 South transit station area zoning project. Maryann's telephone number is: (801) 535-7660, e-mail: maryann.pickering@slcgov.com. It is my understanding that we are waiting for a notice to proceed on the grant but we will begin actual work on the project in the near future.

Thanks,

Joel Paterson, AICP
Planning Manager
Salt Lake City Planning Division
451 South State Street, Rm. 406
P.O. Box 14580
Salt Lake City, UT 84114-5480

Telephone: (801) 535-6141
E-mail: joel.paterson@slcgov.com

From: cindy cromer [mailto:3cinslc@live.com]
Sent: Thursday, March 03, 2011 2:44 PM
To: Paterson, Joel
Subject: TRAX station areas

Joel-Folks in Downtown, Central City, and East Central Community Councils have been talking about getting together to discuss the City's grant for the station areas on 400 S. I called to ask Nick if he had anything in writing that I could send around and to let him know that the community councils would be wanting to hear about the plans soon. Cecily answered the phone I think and said that Nick wouldn't be available. Who on the staff should I check with in his absence? My request is not terribly urgent. We haven't figured out a meeting time yet. Step 1 was to get some background info and circulate it. Step 2 was to get together to talk. Step 3 was to ask Planning to have someone talk to us. Let me

know when you have a chance. Thanks, cindy

From: lisa.c.thompson@comcast.net
To: Pickering, Maryann
Cc: judi.short@gmail.com; kirk@utahheritagefoundation.org
Subject: 400 South Corridor Zoning and Gilgal Garden
Date: Tuesday, April 24, 2012 10:39:16 PM

April 24, 2012

Dear Planning Commission-

I am writing in regard the to proposed rezoning of the 400 South Corridor. While I applaud the City's efforts to create walkable, mixed-use neighborhoods with easy access to public transit, I strongly believe that special consideration needs to be given to the impacts of the proposed zoning on Gilgal Garden.

Gilgal Garden is a Utah, and arguably, a national treasure. You may have seen if featured recently in CNN Travel as one of six spots for architecture lovers. <http://www.cnn.com/2012/04/10/travel/architecture-travel-story/index.html> Gilgal's sculptures, engraved stones, and plantings create a landscape filled with meaning. Scholars consider the garden a "visionary art environment." These works are usually created with found materials by people without formal artistic training to express deep-felt moral or religious convictions. A few visionary art environments, like Watts Towers in Los Angeles, have been preserved, but many more have been destroyed or are at risk.

I joined Friends of Gilgal Garden in 1997 in hopes of saving the garden from destruction. Since then numerous private individuals and community organizations have invested hundreds of thousands of dollars to purchase the garden, upgrade its facilities, and restores its sculptures and stones. Salt Lake City, of course, has been an invaluable partner in this work and committed its resources to maintaining the garden as a public space and an urban oasis.

The current zoning proposal threatens significant harm to the character of the garden. If surrounded by a 75-foot wall to the north and a 45-foot wall to the west, the garden will become a dark, enclosed box for much of the day. Buildings of this height would deprive the garden of the sunlight required to maintain the garden's plantings -- plantings that are a character-defining feature of the garden. The proposed zoning threatens the value of the garden as open space as well. The garden will feel hemmed-in and towered-over and lose its atmosphere of respite.

I urge you to lower the permitted building heights on the parcels abutting Gilgal Garden in order to preserve the community's investment in this unique artistic, historic, and open space resource. At the minimum, Salt Lake City should conduct modeling and light studies to evaluate the impacts of increased building heights and reduced sunlight on the garden before the Planning Commission takes action on the proposed zoning changes.

Thank you for taking my comments into consideration. I hope you'll stop by Gilgal

Garden soon.

Sincerely,

Lisa Thompson

SALT LAKE CITY PLANNING COMMISSION MEETING

In Room 326 of the City & County Building

451 South State Street, Salt Lake City, Utah

Wednesday, April 25, 2012

Present for the Planning Commission meeting were: Chairperson Angela Dean, Vice Chair Michael Gallegos and Commissioners, Emily Drown, Bernardo Flores-Sahagun, Michael Fife, Kathleen Hill, Clark Ruttinger, Marie Taylor, Matthew Wirthlin, and Mary Woodhead. Commissioner Lisa Adams was excused. A field trip was held prior to the meeting. Planning Commissioners present were: Commissioners Michael Gallegos, Clark Ruttinger, Marie Taylor, and Mary Woodhead. Staff members in attendance were Joel Paterson, Thomas Irvin, and Maryann Pickering.

A roll is being kept of all who attended the Planning Commission Meeting. The meeting was called to order at [5:35:38 PM](#). Audio recordings of the Planning Commission meetings are retained in the Planning Office for an indefinite period of time. Planning staff members present at the meeting were: Wilf Sommerkorn, Planning Director; Joel Paterson, Planning Manager; John Anderson, Principal Planner; Thomas Irvin, Principal Planner; Ray Milliner, Principal Planner; Maryann Pickering, Principal Planner; Paul Nielson, City Land Attorney; and Angela Hasenberg, Senior Secretary.

FIELD TRIP NOTES:

Planning Commissioners visited the following locations:

PLNBOA2011-00590- Alternative Parking Requirement Special Exception: located at approximately 740 South 700 East. Staff explained that the addition comes out the back, and will lose 2 stalls. Staff explained the new parking calculations. It was explained that nearby LDS church has offered to allow worshipers from the Islamic Society to park in its parking lot but this could not be allowed to count toward the required parking for the Islamic Society. Commissioners asked about parking at the Domino's site and drove by the LDS church lot.

PLNPCM2010-00647-400 South Livable Communities Station Area Plans: Commissioners toured Gilgal Gardens, staff explained zoning impacts. Commissioners toured 400 South and saw various areas to be rezoned.

[7:37:05 PM](#)

PLNPCM2010-00647 400 South Livable Communities Station Area Plans - A request by Mayor Ralph Becker for an amendment to the Central Community Master Plan, the Salt Lake City Zoning Ordinance and Zoning Map regarding transit station area plans along the 400 South transit corridor. The intent of the project is to implement livability goals for the corridor including land use policies and zoning that support mixed use development and transportation choices for current and future residents and workers. The project is located in City Council District 4 represented by

Luke Garrott. (Staff contact: Maryann Pickering at 801-535-7660 or maryann.pickering@slcgov.com)

Chairperson Dean recognized Maryann Pickering as staff representative.

Ms Pickering presented the petition as reflected in the staff report.

Ms Pickering stated that staff recommends approval of the plan.

**staff reports can be found on line at <http://www.slcclassic.com/boards/plancom/plancom.htm> **

Questions from the Commissioners:

Commissioner Hill asked about the concern about the 75 foot height limit. She asked if the buildings would be staggered as they relate to scale.

Ms. Pickering responded that it would relate to any TSA designation, height would be toward 400 south and shorted in the back.

Planning Manager Norris stated that in regard to height and scale, there were several things that were taken into consideration. Anything higher than 75 ft causes additional costs.

Commissioner Gallegos asked for the height issue to be addressed regarding height difference from the South side as opposed the North side of 400 South.

Planning Manager Norris stated that there were two rational thoughts regarding that, the biggest being solar access, the shadows on the south side would be cast further to the north, more of the building height is placed on the south side of the street.

Commissioner Fife asked to look at the 3-D model and look at Gilgal Gardens.

Planning Manager Norris discussed the use of the tool and could show shadow at various times of the day, and one can see solar access impact. He explained the details of how the model works, and showed that during the growing season, the gardens would have a relatively full sun condition, meaning more than six hours of sun light. Mr. Norris followed the seasons and showed the sun and shadow schedule and how it would impact Gilgal Gardens.

Commissioner Hill asked if it would be possible to see the impact of taller buildings on the same location.

Commissioner Flores asked about the point system regarding the TSA.

Planning Manager Norris explained that how new developments and major additions are reviewed, was based on a point system that is based on development guidelines. Those development guidelines relate to land use, building surface parking lots, eliminating non conforming uses, incorporating energy efficient design. Each guideline has a point value, the point value is based on the cost to the developer to incorporate that guideline into their project and the level of desirability of that guideline. The guidelines are set up in tiers with a set of standards. The uses are either permitted or prohibited. The second tier projects still utilize the conditional use and site design standards; however, they would be reviewed at an administrative hearing. The highest tier, the project is deemed to be a desirable development based on the stationary plans that apply to that particular area. Zoning review would be done at that time.

Commissioner Taylor asked about distinguishing elements, and wondered if there was a way to redesign TRAX stations.

Planning Manager Norris said there were no plans at this time. He explained that there are different characters of stations.

[8:16:21 PM](#)

Public Hearing:

Chairperson Dean opened the Public Hearing

Ester Hunter, Chairperson of East Central Community Council, 606 Trolley Square. Kathy Scott, 941 E. 500 S., Judy Short, 862 Harrison, Cindy Cromer 816 E 100 S, Andrew Wilder, 438 Fletcher Court, Elizabeth Nielson, 438 Fletcher Court, and Wray Featherstone 958 E 400 S all spoke in **opposition** to the petition.

Issues of concern:

- Understand Pressure will get worse, Trax dissects four neighborhoods.
- Concerns of development of inappropriate places
- Line of demarcation seems to be 9th E.
- Strongly supports density on the west side of 9th East,
- East Central exceeds the density of the future land use map currently in place
- Concerns about illegal uses and not regulated.
- Does not want tall buildings next to houses
- Would like the City to remove the transitional zone from areas with single family houses
- Concerns about the creep of density and and taller structures would be deleterious to the Strong and Fletcher Courts.
- Concerns about Gilgal Gardens, concerns about sunlight being blocked.
- Concerned about the houses on the south side of 400 South
- Concerns about compliance regarding point system

- Transfer of development rights should be extended to neighbors who respect the historic nature of neighboring homes.
- Concerns of 900 East and 400 South, is a vibrant community, and the proposed changes would change the character of the block.
- Review process should be changed.
- Neighbors should have a say in whom and what is developed in their neighborhood.

Planning Manager Nick Norris discussed the development potential under the existing zoning, and stated that voices had been heard, and the City would be open to re-evaluating the proposed zoning east of 900 E. Planning Manager Nick Norris stated that staff believes that the area west of 900 E should be a core area.

[8:48:24 PM](#)

Motion:

Commissioner Wirthlin made the motion with regard to PLNPCM2010-00647 400 South Livable Communities Stated Area Plan, I move that the Planning Commission continue the public hearing for a future date.

Commissioner Ruttinger seconded the motion.

Vote: Vote: Commissioners Wirthlin, Ruttinger, Drown, Taylor, Fife, Flores, Hill, Woodhead, and Gallegos all voted "aye" the motion passed unanimously.

**7.D. PLANNING COMMISSION
MAY 23, 2012 PUBLIC HEARING**

PLANNING COMMISSION STAFF REPORT
Legislative Item



Planning Division
Department of Community
and Economic Development

400 South Livable Communities
Master Plan and Zoning Map Amendments
PLNPCM2010-00647
May 23, 2012

Applicant: Mayor Ralph
Becker

Staff: Maryann Pickering
801-535-7660 or
maryann.pickering@slcgov.com

Tax ID: N/A

Current Zone: N/A

Master Plan Designation:
N/A

Council District: District 4
represented by Luke Garrott

Community Council: Central
City and East Central

Lot Size: N/A

Current Use: N/A

Applicable Land Use
Regulations:

- 21A.26 – TSA Transit
Station Area
- 21A.50 – Amendments

Attachments:

- A. Updated Proposed
Zoning Map
- B. Proposed Zoning Text
Changes

Request

A request by Salt Lake City Mayor Ralph Becker for an amendment to the Central Community Master Plan, Salt Lake City Zoning Ordinance and Zoning Map regarding transit station area plans along the 400 South transit corridor. The intent of the project is to implement livability goals for the corridor including land use policies and zoning that support mixed use development and transportation choices for current and future residents and workers. The Planning Commission is required to transmit a recommendation to the City Council for Master Plan and Zoning Map Amendment Requests.

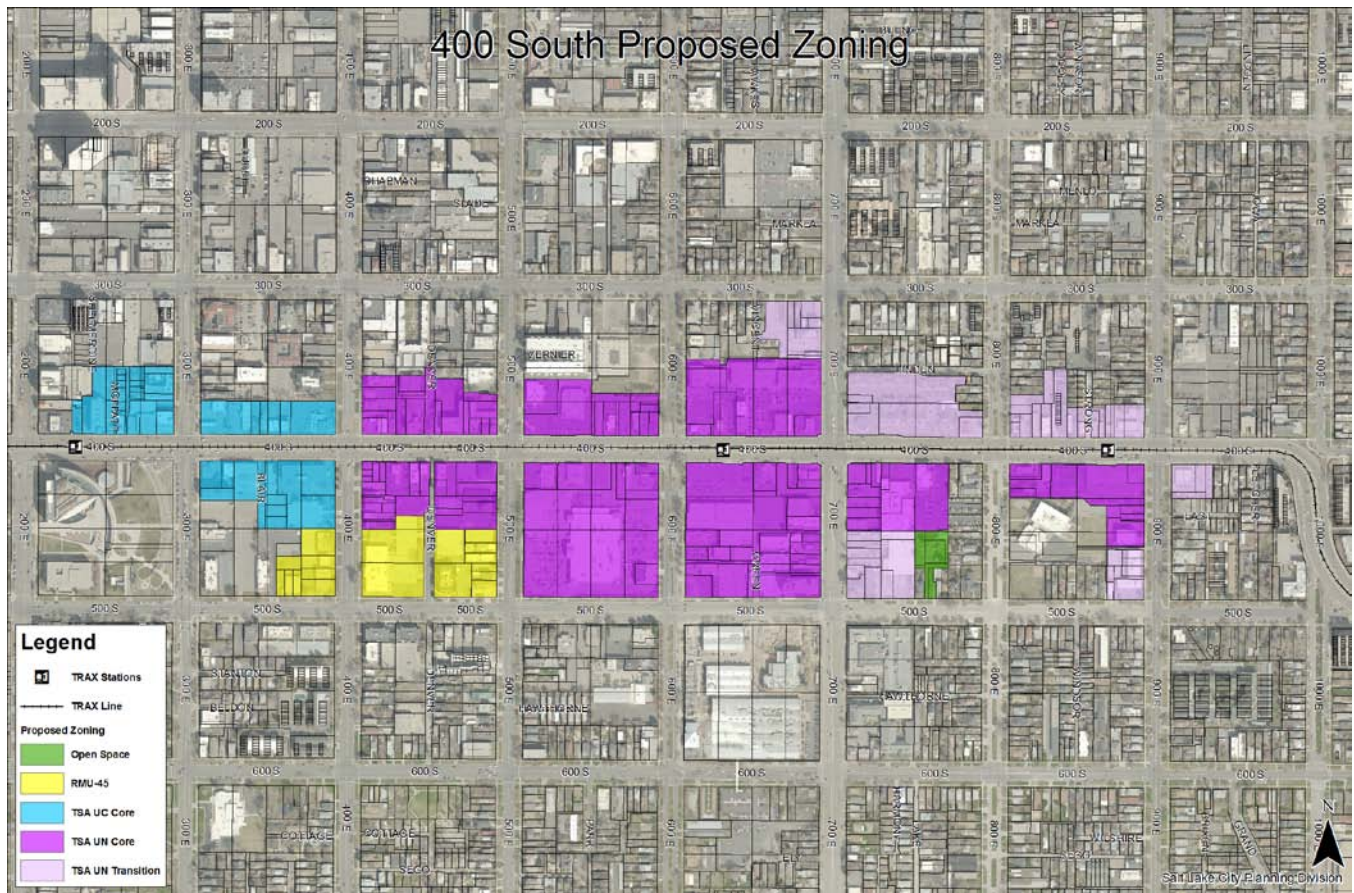
Recommendation

Based on the findings listed in the staff report, it is the Planning Staff's opinion that overall the project generally meets the applicable standards and therefore, recommends the Planning Commission transmit a favorable recommendation to the City Council relating to this request based on the following:

1. The proposed station area plans are compatible with city wide policies related to land use, including:
 - Salt Lake City Futures Commission Report (1998)
 - Salt Lake City Urban Design Element (1990)
 - Salt Lake City Community Housing Plan (2010 – currently under review by City Council)
 - Salt Lake City Transportation Plan (1996)
 - Central Community Master Plan (2005)
 - Wasatch Choices 2040 (2011)
2. The proposed station area plans update a portion of the Central Community (2005) Master Plans;
3. The proposed station area plans are generally consistent with the comments received during an extensive public participation process; and
4. The proposed plans include best practices to guide future development along and adjacent to 400 South.
5. The proposal furthers the purposes of the Title 21A;
6. The proposal is consistent with the factors of consideration identified in ordinance 21A.50 for zoning text and zoning map amendments.

Recommended Motion: Based on the findings listed in the staff report, testimony and plans presented, I move that the Planning Commission transmit a favorable recommendation to the City Council relating to this request to amend the Central Community Master Plan, Salt Lake City Zoning Ordinance and Zoning Map for station areas along the 400 South Corridor.

REVISED ZONING MAP



Map Updated May 9, 2012

Follow Up from April 25 Planning Commission Meeting

At the last Planning Commission meeting, there was concern expressed by some of the residents in attendance regarding the proposed rezoning on the block east of 900 East and south of 400 South. Based on the comments that were received, we have modified the zoning map to show that only the hard corner (where the Village Inn and their parking lot) will be changed to the transition area classification. No other changes were made to the zoning map since that last meeting.

The information provided in the April 25 Planning Commission staff report will not be duplicated in this report. Please click on the following link to view a copy of that staff report which includes the discussion of the various plans that this project conforms with as well as a discussion of the proposed zoning text and zoning map changes.

<http://www.slcclassic.com/boards/plancom/2012/April/maryann.pdf>

Meeting Notification for May 23, 2012 Planning Commission Meeting

Notice of the public hearing for the proposal includes:

- Public hearing notice mailed on May 11, 2012.
- Public hearing notice posted on City and State websites on May 11, 2012.
- Public hearing notice emailed to the Planning Division listserv on May 11, 2012.

Findings

21A.50.050 Standards for general amendments.

A decision to amend the text of this title or the zoning map by general amendment is a matter committed to the legislative discretion of the city council and is not controlled by any one standard.

A. In making its decision concerning a proposed text amendment, the city council should consider the following factors:

- 1. Whether a proposed text amendment is consistent with the purposes, goals, objectives, and policies of the city as stated through its various adopted planning documents;**

Analysis: In reviewing the station area plans, several adopted master plans were considered, including the Urban Design Element, the Salt Lake Futures Commission Report, the Central Community Master Plan, and the Wasatch Choices 2040 Plan. The City's adopted Housing Plan and Transportation Plan also call for the type of development supported in the station area plans. The analysis of the station area plans indicated that they were generally consistent with these plans or explained a change in policy to those plans.

Finding: The proposed zoning text changes are consistent with the goals and policies identified in the companion station area plans and several other adopted master plans.

- 2. Whether a proposed text amendment furthers the specific purpose statements of the zoning ordinance;**

Analysis: The proposed changes enhance and existing chapter of the zoning ordinance, with a specific purpose statement. The general purpose statement of the zoning ordinance is to promote the health, safety, morals, convenience, order, prosperity and welfare of the present and future inhabitants of the City. In addition, the zoning ordinance is intended to lessen congestion in the streets, secure safety from fire and other dangers, provide adequate light and air, classify land uses and distribute land development and utilization, protect the tax base, secure economy in government expenditures, foster the City's industrial, business and residential development and protect the environment.

Finding: The proposed zoning ordinance furthers the specific purpose statements of the zoning ordinance.

3. Whether a proposed text amendment is consistent with the purposes and provisions of any applicable overlay zoning districts which may impose additional standards; and

Analysis: The existing zoning ordinance includes a statement that any applicable overlay zone supersedes the standards in the proposed ordinance. This section is not proposed to be modified as part of this petition. In addition, the zoning with the existing H Historic Preservation Overlay District will change, but the overall district itself will not be changed.

No historic resources will be impacted within the existing Central City Historic District. There are however two Landmark sites within the boundaries of the 400 South Livable Communities Project. One site is the Tenth LDS Ward House located at the southwest corner of 400 South and 800 East. The current zoning of this site is Community Shopping (CS) and it is not proposed to be changed at this time.

The other landmark site is the Peck House located near the northwest corner of 500 South and 500 East. This property is currently zoned Residential Office (RO) and is proposed to be rezoned to Moderate/High Density Multi-Family Residential (RMF-45). It is not anticipated that any negative impact to either of these existing landmark sites as a result of the proposed TSA zoning designations.

Finding: The existing zoning ordinance is consistent with the purposes of any applicable overlay zoning district.

4. The extent to which a proposed text amendment implements best current, professional practices of urban planning and design.

Analysis: The proposed changes continue to represent a new approach to zoning for Salt Lake City. This approach recognizes the value and importance of community input, the needs of developers and establishes an opportunity for the City, through private investment and development, to promote sustainable development practices, increase the housing stock, promote the business community, increase the use of alternative forms of transportation and improve public spaces.

Finding: The proposed changes continue to show how Salt Lake City is one of the few cities in the nation to implement this type of zoning, rather than the traditional Euclidean zoning that is widely used.

B. In making a decision to amend the zoning map, the city council should consider the following factors

1. Whether a proposed map amendment is consistent with the purposes, goals, objectives, and policies of the City as stated through its various adopted planning documents;

Analysis: In reviewing the proposed zoning map changes, several adopted master plans were considered, including the Urban Design Element, the Salt Lake Futures Commission Report, the Central Community Master Plan, and the Wasatch Choices 2040

Plan. The City's adopted Housing Plan and Transportation Plan also call for the type of development supported in the station area plans. The analysis of the station area plans indicated that they were generally consistent with these plans or explained a change in policy to those plans.

Finding: The proposed zoning map amendments are consistent with the goals and policies identified in the station area plans and several other adopted master plans.

2. Whether a proposed map amendment furthers the specific purpose statements of the zoning ordinance;

Analysis: The proposed zoning map amendment includes provisions for reducing the impact new development may have on existing areas. The boundaries of the proposed zoning district correspond to the boundaries in the station area plans. The station area plans identify the vision for what the areas around the transit stations should look like, how they work, what types of uses there are, etc. The existing character of the subject areas differs from what is identified in the long term vision for the area. Therefore, the important aspect to consider is the impact on those areas that are adjacent to the proposed zoning district boundaries.

Finding: The proposed zoning map amendments further the specific purpose statements of the zoning ordinance.

3. The extent to which a proposed map amendment will affect adjacent properties;

Analysis: The proposed amendment would affect those properties that are within the boundaries of the TSA zoning district. The proposed zoning district would, in most cases, allow for development of a greater scale and mass than what exists currently. However, this should not be viewed as an adverse impact because the proposed regulations that allow greater mass and scale are consistent with what was identified through the public planning process as desirable development. As properties redevelop, there will be instances where a new project is considerable larger than what may be adjacent to it. The adverse impacts are more relevant where the proposed zoning district is adjacent to an area that will not be rezoned and has smaller mass and scale regulations than the proposed ordinance.

The proposed ordinance contains provisions to reduce the impacts in these situations such as imposing a mandatory step back of new buildings from the property line to lessen the impact of new development. The current zoning does not contain provisions for the stepping back and therefore, new development under the non TSA zoning designations would have more of an impact on adjacent development.

Finding: The proposed zoning map amendment will have a minimal affect on adjacent properties due to the proposed zoning district containing provisions to reduce to impacts of the scale and mass of potential adjacent development.

4. **Whether a proposed map amendment is consistent with the purposes and provisions of any applicable overlay zoning districts which may impose additional standards; and**

Analysis: The proposed zoning map amendment is consistent with the purposes of any applicable overlay zoning district. The proposed zoning ordinance includes a statement that any applicable overlay zone supersedes the standards in the proposed ordinance. The H Historic Preservation Overlay District is the only overlay district within the boundaries of the proposed map amendments. The zoning with the existing H Historic Preservation Overlay District will change, but the overall district itself will not be changed.

No historic resources will be impacted within the existing Central City Historic District. There are however two Landmark sites within the boundaries of the 400 South Livable Communities Project. One site is the Tenth LDS Ward House located at the southwest corner of 400 South and 800 East. The current zoning of this site is Community Shopping (CS) and it is not proposed to be changed at this time.

The other landmark site is the Peck House located near the northwest corner of 500 South and 500 East. This property is currently zoned Residential Office (RO) and is proposed to be rezoned to Moderate/High Density Multi-Family Residential (RMF-45). It is not anticipated that any negative impact to either of these existing landmark sites as a result of the proposed TSA zoning designations.

Finding: The proposed map amendments will be consistent with any existing or future overlay district within the boundaries of the changes.

5. The adequacy of public facilities and services intended to serve the subject property, including but not limited to roadways, parks and recreational facilities, police and fire protection, schools, stormwater drainage systems, water supplies, and wastewater and refuse collection.

Analysis: The project area is located within areas that are already served by public facilities and services. However, the proposed ordinance does increase the development potential of the area. Population, employment and household projections for the corridor indicate an increase in all three categories. These projections were done under the current zoning regulations. The capacity of the road is not anticipated to be greatly impacted, at least initially, due to the change in zoning. The desired type of development and the development promoted by the proposed ordinance is considered transit oriented development, which can reduce the need to use private automobiles. Other measures in the ordinance create incentives, primarily through a quicker review process, for other measures that reduce the energy use of new buildings, storm water runoff, and other sustainable measures. The proposed ordinance has been routed to other Departments and Divisions for comments. No comments were received that would indicate that the City would not be able to serve new development.

Finding: There appear to be adequate facilities in place to serve the boundaries of the proposed project.

Commission Options

The proposed 400 South Livable Communities Station Area Plans project is a reflection of the community's vision for 400 South. The creation of the plan was done with the intent of incorporating major themes identified through the public process. Once these items were identified, a series of best practices that were applicable to the community's vision were incorporated into the plan to guide future development in a manner that can help turn the community vision into reality. While there are many options in terms of how to address land use, the draft station area plans represent the preferred option of the community and Planning Division staff. Other options are:

- Make no changes to the existing master plan and development regulations and allow development to continue in the manner that it currently is;
- Make consistent changes that would apply to the entire corridor; and
- Make limited changes along 400 South to the areas closest to the light rail stations.

After analyzing the comments from the community, the desire for a different type of development along 400 South eliminated the option to make no changes. If the proposed station area plans are not adopted, the existing policies and regulations would remain in effect. Community input and existing conditions indicate that there are unique situation and characteristics area each station and that a one size fits all approach could not capitalize on the unique assets at each station. Making limited changes near the station areas would not provide enough land area to accommodate future projected growth.

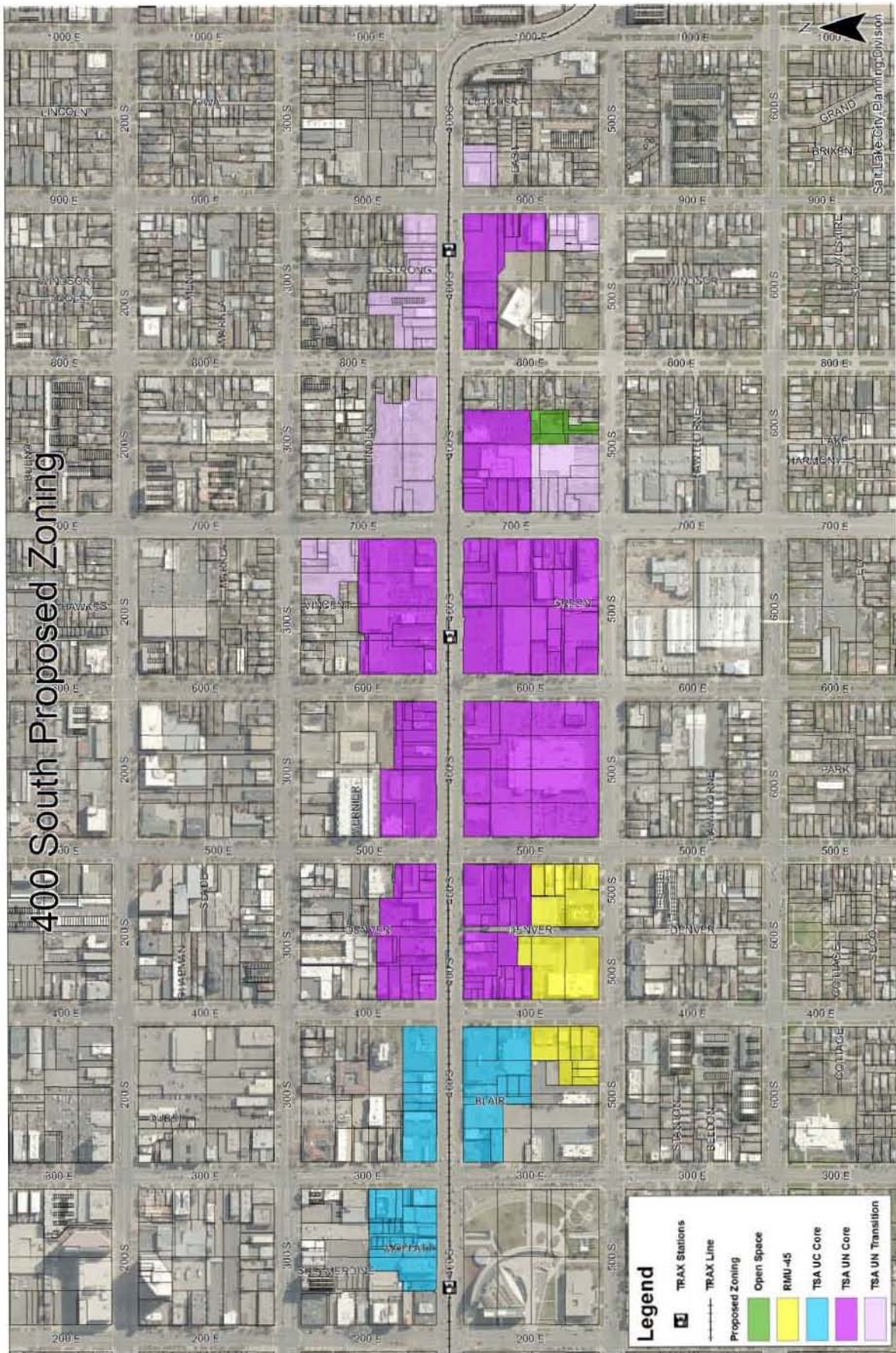
Potential Motions

Consistent with Staff Recommendation: Based on the findings listed in the staff report, testimony and plans presented, I move that the Planning Commission transmit a favorable recommendation to the City Council relating to this request to amend the Central Community Master Plan, Salt Lake City Zoning Ordinance and Zoning Map for station areas along the 400 South Corridor.

Not Consistent with Staff Recommendation: Based on the testimony, plans presented and the following findings, I move that the Planning Commission transmit a negative recommendation to the City Council relating to this request to amend the Central Community Master Plan, Salt Lake City Zoning Ordinance and Zoning Map for station areas along the 400 South Corridor.

Attachment A

Updated Proposed Zoning Map



Attachment B

Proposed Zoning Text Changes



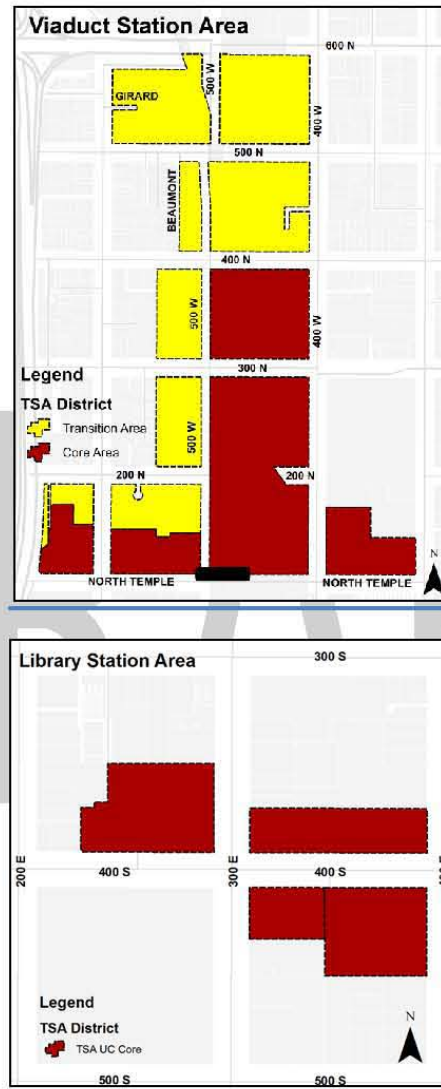
21A.26.078: TSA TRANSIT STATION AREA DISTRICT:

- A. Purpose Statement: The purpose of the TSA transit station area district is to provide an environment for efficient and attractive transit and pedestrian oriented commercial, residential and mixed use development around transit stations. Redevelopment, infill development and increased development on underutilized parcels should include uses that allow them to function as part of a walkable, mixed use district. Existing uses that are complementary to the district, and economically and physically viable, should be integrated into the form and function of a compact, mixed use pedestrian oriented neighborhood. Each transit station is categorized into a station type. These typologies are used to establish appropriate zoning regulations for similar station areas. Each station area will typically have two (2) subsections: the core area and the transition area. Due to the nature of the area around specific stations, the restrictions of overlay zoning districts, and the neighborhood vision, not all station areas are required to have a core area and a transition area.
1. Core Area: The purpose of the core area is to provide areas for comparatively intense land development with a mix of land uses incorporating the principles of sustainable, transit oriented development and to enhance the area closest to a transit station as a lively, people oriented place. The core area is generally within a one-fourth ($\frac{1}{4}$) mile walk of a transit station platform. The core area may mix ground floor retail, office, commercial and residential space in order to activate the public realm. Buildings in this area should have minimal setbacks to encourage active outdoor use adjacent to the sidewalk, such as outdoor dining and patios that reflect the desired character of the area. Building facades should be varied and articulated, include storefronts adjacent to the street, windows on the street level, and have clearly defined entrances to provide visual interest to pedestrians. Buildings should be a minimum of two (2) or three (3) stories in height, depending on location, in order to define the street edge. Arcades, bays, and balconies are encouraged. The configuration of buildings must balance the needs of all modes of circulation with the safety and comfort of pedestrians and bicyclists. A vertical mix of uses, with office and residential above ground floor commercial uses is encouraged. A minimum of thirty (30) dwelling units per acre is encouraged within the core.
 2. Transition Area: The purpose of the transition area is to provide areas for a moderate level of land development intensity that incorporates the principles of sustainable transit oriented development. The transition area is intended to provide an important support base to the core area and transit ridership as well as buffer surrounding neighborhoods from the intensity of the core area. These areas reinforce the viability of the core area and provide opportunities for a range of housing types at different densities. Transition areas are generally located within a one-half ($\frac{1}{2}$) mile from the station platform, but may vary based on the character of the area. Transition areas typically serve the surrounding neighborhood; include a broad range of building forms that house a mix of compatible land uses. The minimum desired density is ten (10) dwelling units per acre. Commercial uses may include office, retail, restaurant and other



commercial land uses that are necessary to create mixed use neighborhoods. Commercial uses can be clustered around intersections and along block faces to create neighborhood nodes.

- B. Station Area Types: A station area typology is the use of characteristics, such as building types, mix of land use, transit service and street network to create generalizations about an area that can be used to define a common vision for development of a transit station area. Each typology recognizes the important difference among places and destinations and takes into account the local context of a station and its surroundings. Each station area typically will include a core area, where the most intense development will occur, and a transition area, which is intended to create a buffer area between the core and those areas with generally lower intensities and densities. Prior to classifying a transit station into a specific type, a [station area specific area](#) plan must be adopted by the city council prior to applying this zoning district to a geographic area. Only those stations that have an adopted [station area](#) plan [that is supported by the regulations in this section](#) will be classified. [Refer to the official Salt Lake City Zoning Map to determine the zoning of the land within each station area.](#)
1. Urban Center Station ([TSA-UC](#)): An urban center station contains the highest relative intensity level and mix of uses. The type of station area is meant to support Downtown Salt Lake and not compete with it in terms of building scale and use. The intensity level of the area is characterized by a twenty four (24) hour population, active streetscapes, defined street walls and the presence of multiple types of public transit or as a node where several rail transit lines converge. Development generally occurs on vacant parcels or through redevelopment of underutilized parcels. The station area has a compact, dense, interconnected and walkable development pattern. Large scale development occurs closer to the station platforms; and is scaled back as it gets closer to less intense areas. Building forms vary, but are typically oriented to the pedestrian, are multiple stories in height, and contain a horizontal and vertical mix of land uses. Buildings up to ten (10) stories in height are allowed in the core, while buildings in the transition zone are approximately half that size. The station area contains a number of regional attractions, such as destination retail, employment, dining and entertainment and a high level of pedestrian activity. A variety of dense housing options exist. Development includes civic amenities, such as public gathering places. Uses that help implement the vision for the station and that area commonly found in an intense urban area are appropriate. The following stations are considered an urban center type of station: North Temple Viaduct Transfer Station [and the Library Station.](#)



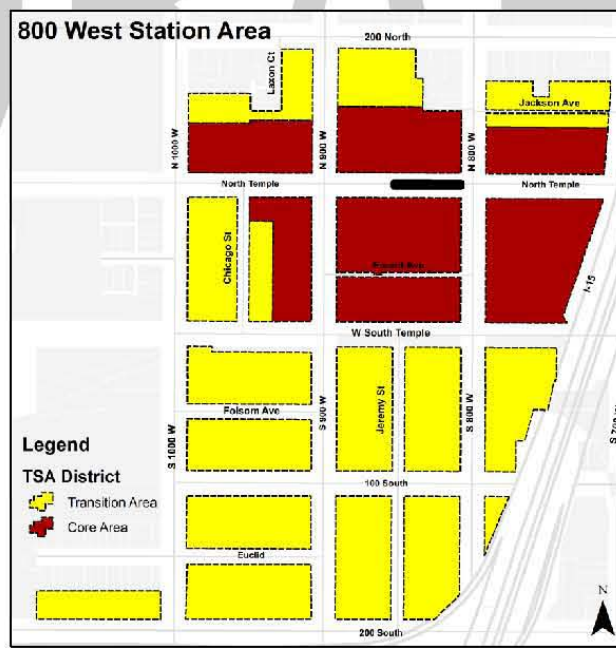
2. Urban Neighborhood Station ([TSA-UN](#)): An evolving and flexible development pattern defines an urban neighborhood station area. Development generally happens as infill on vacant parcels or redevelopment of underutilized parcels. These stations evolve in established residential areas where initial changes may add density and intensity in compact building forms that blend in with the residential character of the area. Urban neighborhoods consist of multilevel

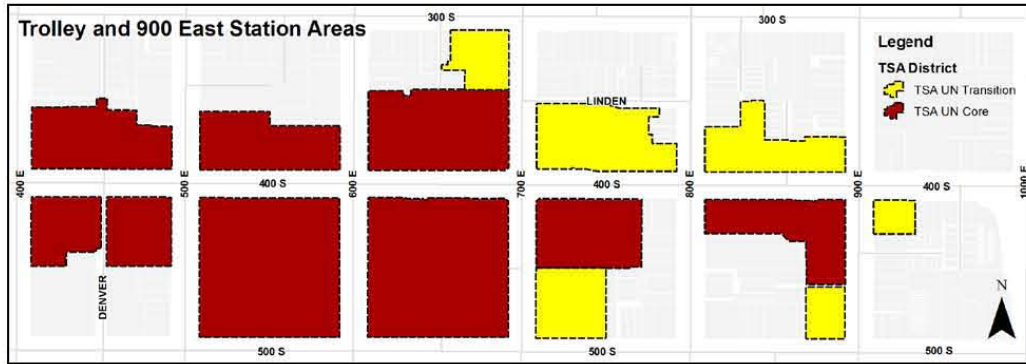


buildings that are generally lower scale than what is found in the urban center station area. The desired mix of uses would include ground floor commercial or office uses with the intent of creating a lively, active, and safe streetscape. A mix of building types are possible, ranging from single use structures to mixed use buildings. Residential uses are generally located above the first floor, although they can be located on the ground floor in certain situations. The highest residential densities and most intense land uses are generally located closest to the station platform. Urban neighborhoods are served by at least two (2) forms of transit, including light rail and bus service. The uses serve the surrounding neighborhood with nearby destinations and have the potential to attract people from other neighborhoods.

In some urban neighborhood station areas, a linear development pattern along commercial streets that intersect the transit corridor defines a neighborhood main street. Neighborhood main streets are approximately two (2) blocks long, with two (2) 4-story buildings located close to the sidewalk. The ground floors of buildings are typically occupied by active uses, such as retail or restaurants.

The following stations are considered to be urban neighborhood stations: 800 West, [Trolley \(600 East\)](#) and [900 East](#) light rail stations.

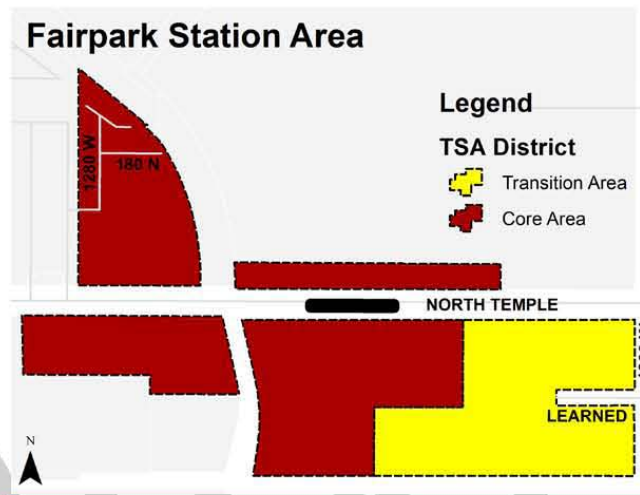




3. **Mixed Use Employment Center Station (TSA-MUEC):** A mixed use employment station is an area with a high concentration of jobs that attract people from the entire region. These areas generally start with a campus style development pattern and are dominated by a single type of use that generally employs a high number of people. Buildings are often large scale in nature and may have large footprints. New development occurs on vacant parcels. Redevelopment occurs on surface parking lots, underutilized land, or as additions to existing buildings as businesses expand. The primary mode of circulation is by automobile, but the area is served by at least two (2) types of mass transit which provides alternative modes of transportation for employees. Land uses that support the employment centers such as retail sales and service and restaurants are located throughout the station area and should occupy ground floor space in multi-story buildings oriented to the pedestrian and transit user. A mix of housing types and sizes are appropriate to provide employees with the choice to live close to where they work. Building types should trend toward more flexible building types over time. The area is likely to have large blocks and lacks a consistent street network. Connectivity for all modes of travel is important due to the limited street network. The following stations are considered to be mixed use employment center stations: 1950 West, 2200 West, and the Cornell light rail stations.



4. Special Purpose Station ([TSA-SP](#)): The special purpose station is typically centered on a specific land use or large scale regional activity. These areas are generally served by a mix of transit options, usually light rail or bus. New development is limited due to the nature of the primary function of the area, but redevelopment of underutilized parcels is likely to occur. Land uses such as restaurants and retail support the dominant land use and attract people to the area. A mix of housing types and sizes are appropriate in certain situations. Future development should be aimed at increasing the overall intensity and frequency of use in the station area by adding a mix of uses that can be arranged and designed to be compatible with the primary use. The following stations are considered to be special purpose stations: Fairpark light rail station.



- C. Review Process: The intent of the review process is to make the process for desirable development easy to realize in a relatively quick time frame. The review process focuses on building forms and their relationship to adjacent buildings, the public street, transit and other public spaces. The review process for all new development and redevelopment within the transit station area zoning district is based on the development score which is generated by the "Transit Station Area Development Guidelines" hereby adopted by reference. The construction of new buildings require a development score. The following types of development are required to go through this review process:
- Any addition of 1,000 square feet or more that extend a street facing building facade or are located to the side of a building and are visible from a public space or
 - Additions that increase the height of an existing building or change the existing roof line;
 - Additions to the rear of buildings that are not adjacent to a public street, trail or other public space are not required to obtain a development score but must comply with all other applicable regulations. Signs, fences, accessory structures and any other structure or addition not listed in this section are not required to obtain a development score.
1. Presubmittal Conference: All applicants for development within the transit station area zoning district are required to attend a presubmittal conference with the planning division. The purpose of the presubmittal conference is to notify the applicant of the goals of the station area plans, the standards in this section, and the review and approval process.



2. Development Review Application: After a presubmittal conference, the developer can submit a development review application. This application and all submittal requirements will be used to determine the development score. The application shall include a score sheet on which the development guidelines and their assigned values are indicated and two (2) checklists: one for the applicant's use and one for the planning division's use.
3. Application Review: Table 21A.26.078C of this section summarizes the application review process. All applications shall be processed as follows:
 - a. Tier 1 Planning Commission Review: If a project is assigned a score less than 50 points, the project can only be approved by the planning commission through the conditional building and site design review process in chapter 21A.59 of this title. Once the applicant receives written notice of their score, they will be given thirty (30) days to notify the planning division of their intention to proceed with the project through the conditional building and site design review process or make necessary plan adjustments to increase their development score to the minimum level in order to go through an administrative hearing process.
 - b. Tier 2 Administrative Hearing: A project that has a development score between 50 and 99 points is eligible for an administrative hearing. Any project being reviewed at an administrative hearing shall be reviewed using the standards found in section 21A.59.060, "Standards For Design Review", of this title.
 - (1) Notice And Posting Requirements: Notice of the administrative hearing shall be done in accordance with subsection 21A.10.020C, "Conditional Building And Site Design Review", of this title.
 - (2) Administrative Hearing: After consideration of the information received from the applicant and any other interested party, the planning director, or designee may approve, approve with conditions, deny or refer the matter to the planning commission.
 - (3) Appeals Of Administrative Hearing Decision: Any person aggrieved by the decision made by the planning director or designee at an administrative hearing may appeal the decision to the Salt Lake City planning commission by filing notice of appeal within ten (10) days after the record of decision is published. The notice shall state the reason(s) for the appeal. Reason(s) for the appeal shall be based upon procedural error, the development score of the project or the criteria set forth in section 21A.59.060, "Standards For Design Review", of this title.



- c. Tier 3 Administrative Review: The planning director has the authority to approve a project scoring 100 points or more without holding a public hearing. The project shall be allowed to go through the standard building permit process. A public hearing is not required because the project incorporates adequate design guidelines or development incentives to be deemed compliant with the vision for the station area.

TABLE 21A.26.078C APPLICATION REVIEW

Development Score	Review Process
0 – 49 points	Planning commission conditional building and site design review process
50 – 99 points	Administrative hearing process
100 or more points	Administrative review

- D. **Development Score:** The purpose of the development score is to allow flexibility for designers while implementing the city’s vision of the applicable station area plans and the purpose of this zoning district. The development score measures the level of compatibility between a proposed project and the station area plan. A “station area plan” is a development, land use, urban design and placemaking policy document for the area around a specific transit station. The development score is based on the design guidelines and development incentives in the “Transit Station Area Development Guidelines” book, hereby adopted by reference. The “Transit Station Area Development Guidelines” shall be amended following the adopted procedures for zoning text amendments in chapter 21A.50, “Amendments”, of this title. Each design guideline is assigned a value.
 1. **Formulating The Score:** The development score is formulated by calculating all of the development guideline values for a particular project. Each design guideline and incentive is given a value based on its importance. Some guidelines are considered more important and carry a higher value than others. The planning director shall evaluate each project in the transit station area zone and assign a development score. The development standards in subsection G of this section and the design standards in subsection J of this section shall be complied with by all projects and are not calculated in the development score.
 2. **Using The Score:** Every development is required to meet a minimum development score. The minimum score represents a percentage of the total points possible.
 3. **Project Review:** A development score shall be assigned to all projects within the transit station area zoning district after a complete development review application is submitted. The planning director shall provide, in writing, a copy of the review checklist and explanation of the outcome of the score to the applicant within thirty (30) days of submitting a complete application.



4. Appeals: An applicant may appeal the development score to the planning commission. In hearing the appeal, the planning commission shall hold a public hearing in accordance with section 21A.10.030 of this title. In deciding the appeal, the planning commission shall base its decision on its interpretation of the design guidelines, the development score and whether the project complies with the goals of the applicable station area plans and the purposes of the TSA zoning district.
- E. Certificate Of Occupancy: Prior to issuing a certificate of occupancy, a project shall be inspected by the city to determine if the project substantially complies with the development score and, if applicable, any conditions of approval required by the planning commission, administrative hearing officer or planning director. If the project does not receive final approval at the inspection, the project must be brought into compliance with the development score and, if applicable, any conditions of approval required by the planning commission, administrative hearing officer or planning director.
- F. Prohibited Uses: The intent of this section is to identify those land uses that are not compatible with transit oriented development due to the nature of the use, the land requirements of the use, or the potential impacts of the use. Uses listed in table 21A.26.078FE of this section and that have an "X" in a box next to the specific land use, indicates it is prohibited. Any use not listed, but is substantially similar to a use listed, shall be prohibited. All other uses are permitted. In the table headings, "C" stands for Core and "T" stands for Transition.

TABLE 21A.26.078FE PROHIBITED USES

Use	Urban Center		Urban Neighborhood		Mixed Use Employment Center		Special Purpose	
	Core	Transition	Core	Transition	Core	Transition	Core	Transition
<u>Airport</u>	X	X	X	X	X	X	X	X
<u>Ambulance service (indoor) dispatching, staging and maintenance</u>	X	X	X	X	X	X	X	X
<u>Ambulance service (outdoor)</u>	X	X	X	X	X	X	X	X
<u>Amusement park</u>	X	X	X	X	X	X		
<u>Animal kennel</u>	X	X	X	X				
<u>Animal pound</u>	X	X	X	X	X	X	X	X
<u>Animal, raising of furbearing animals</u>	X	X	X	X	X	X	X	X
<u>Animal stable (private)</u>	X	X	X	X	X	X	X	X
<u>Animal stable (public)</u>	X	X	X	X	X	X		
<u>Animal stockyard</u>	X	X	X	X	X	X	X	X
<u>Auction (indoor)</u>	X	X	X	X	X	X		
<u>Auction (outdoor)</u>	X	X	X	X	X	X	X	X



Use	Urban Center		Urban Neighborhood		Mixed Use Employment Center		Special Purpose	
	Core	Transition	Core	Transition	Core	Transition	Core	Transition
Auto, truck, and RV repair, sales and/or service (minor or major)	X	X	X	X	X	X	X	X
<u>Blacksmith shop</u>	X	X	X	X	X	X	X	X
<u>Bottling plant</u>	X	X	X	X	X	X	X	X
<u>Building materials distribution</u>	X	X	X	X	X	X	X	X
Bus yards and repair facilities	X	X	X	X	X	X	X	X
Car washes	X	X	X	X	X		X	X
<u>Car wash as accessory use to gas station or convenience store that sells gas</u>	X	X	X	X	X	X	X	X
Cemeteries, any type	X	X	X	X	X	X	X	X
<u>Check cashing/payday loan business</u>	X	X	X	X	X	X	X	X
Chemical manufacturing <u>and storage</u>	X	X	X	X	X	X	X	X
Commercial parking lots not located in a parking structure	X	X	X	X	X		X	
Community correction facilities, any size, large	X	X	X	X	X	X	X	X
<u>Community correction facility, small</u>	X	X	X	X	X	X	X	X
Concrete manufacturing	X	X	X	X	X	X	X	X
Contractor's yard/office	X	X	X	X	X	X	X	X
Detached single-family dwelling	X		X		X		X	
<u>Drop forge industry</u>	X	X	X	X	X	X	X	X
Drive-through windows and service	X	X	X	X	X	X	X	X
<u>Dwelling, single family (detached)</u>	X		X		X		X	



Use	Urban Center		Urban Neighborhood		Mixed Use Employment Center		Special Purpose	
	Core	Transition	Core	Transition	Core	Transition	Core	Transition
Electric generation facility; excluding solar, wind, or other similar renewable energy generation	X	X	X	X	X	X	X	X
<u>Equipment rental (outdoor)</u>	X	X	X	X	X	X	X	X
<u>Equipment, heavy (rental, sales, service)</u>	X	X	X	X	X	X	X	X
Explosives manufacturing <u>and storage</u>	X	X	X	X	X	X	X	X
Extractive industries	X	X	X	X	X	X	X	X
<u>Financial institution, with drive-through facility</u>	X	X	X	X	X	X	X	X
Flammable liquids or gases, heating fuel distribution <u>and storage</u>	X	X	X	X	X	X	X	X
<u>Food processing</u>	X	X	X	X	X	X	X	
Gas stations	X	X	X	X	X	X	X	
Heavy equipment rental, repair and sales	X	X	X	X	X	X	X	X
Heavy industrial	X	X	X	X	X	X	X	X
Heavy manufacturing	X	X	X	X	X	X	X	X
Homeless shelters	X	X	X	X	X	X	X	X
Impound lot	X	X	X	X	X	X	X	X
Incinerators, for any purpose <u>medical waste/hazardous waste</u>	X	X	X	X	X	X	X	X
<u>Industrial assembly</u>	X	X	X	X				
Jails	X	X	X	X	X	X	X	X
Kennel	X	X	X	X	X	X	X	X
Landfills, any type	X	X	X	X	X	X	X	X
<u>Limousine service (large)</u>	X	X	X	X	X	X	X	X
<u>Limousine service (small)</u>	X	X	X	X	X	X	X	X
Manufactured/mobile home sales and service	X	X	X	X	X	X	X	X



Use	Urban Center		Urban Neighborhood		Mixed Use Employment Center		Special Purpose	
	Core	Transition	Core	Transition	Core	Transition	Core	Transition
Manufacturing and processing, food	X	X	X					
Manufacturing, concrete or asphalt	X	X	X	X	X	X	X	X
Manufacturing, light	X	X	X	X				
Ministorage facilities	X	X	X	X	X		X	
Outdoor storage including storage or staging of fleet vehicles associated with a business, but not intended to include outdoor retail sales areas or outdoor storage as an accessory and incidental use to a principal use	X	X	X	X	X	X	X	X
Package delivery facility	X	X	X	X	X	X	X	X
Paint manufacturing	X	X	X	X	X	X	X	X
Poultry farm or processing	X	X	X	X	X	X	X	X
Printing plant	X		X	X				
Pawnshops	X	X	X	X	X	X	X	X
Railroad freight terminal facility	X	X	X	X	X	X	X	X
Railroad repair shop	X	X	X	X	X	X	X	X
Recreational vehicle park	X	X	X	X	X	X	X	X
Recycling processing center (indoor)	X	X	X	X	X	X	X	X
Recycling processing center (outdoor)	X	X	X	X	X	X	X	X
Refinery, petroleum products	X	X	X	X	X	X	X	X
Restaurant with drive-through facility	X	X	X	X	X	X	X	X
Retail goods establishment with drive-through facility	X	X	X	X	X	X	X	X
Retail services establishment with drive-through facility	X	X	X	X	X	X	X	X
Reverse vending machines	X	X	X	X	X	X	X	X



Use	Urban Center		Urban Neighborhood		Mixed Use Employment Center		Special Purpose	
	Core	Transition	Core	Transition	Core	Transition	Core	Transition
Rock, sand and gravel storage and distribution	X	X	X	X	X	X	X	X
Sewage treatment facility	X	X	X	X	X	X	X	X
Sexually oriented business	X	X	X	X	X	X	X	X
<u>Sign painting/fabrication</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>
<u>Slaughterhouse</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>
<u>Stadium</u>		<u>X</u>		<u>X</u>		<u>X</u>		
<u>Storage (outdoor)</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>
<u>Storage, mini-warehouse</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>		<u>X</u>	
<u>Storage, public (outdoor)</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>
<u>Store, pawnshop</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>
<u>Store, superstore and hypermarket</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>		<u>X</u>	
<u>Store, warehouse club</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>		<u>X</u>	
<u>Taxicab facility</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>
<u>Theater, live performance</u>		<u>X</u>		<u>X</u>		<u>X</u>		
<u>Theater, movie</u>		<u>X</u>		<u>X</u>		<u>X</u>		<u>X</u>
<u>Tire distribution retail/wholesale</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>
<u>Transportation terminal, including bus, rail and trucking</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>
Solid waste transfer station	X	X	X	X	X	X	X	X
Stable, public or private	X	X	X	X	X	X	X	X
Truck freight terminal	X	X	X	X	X	X	X	X
Truck stop	X	X	X	X	X	X	X	X
<u>Trucking, repair, storage, etc. associated with extractive industries</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>
<u>Utility, electric generation facility</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>
<u>Utility, sewage treatment plant</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>
<u>Utility, solid waste transfer station</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>	<u>X</u>



Use	Urban Center		Urban Neighborhood		Mixed Use Employment Center		Special Purpose	
	Core	Transition	Core	Transition	Core	Transition	Core	Transition
Vehicle, auction	X	X	X	X	X	X	X	X
Vehicle, automobile and truck repair	X	X	X	X	X	X	X	X
Vehicle, automobile and truck sales and rental (including large truck)	X	X	X	X	X	X	X	X
Vehicle, automobile rental agency	X	X	X	X	X	X	X	X
Vehicle, auto repair (major)	X	X	X	X	X	X	X	X
Vehicle auto repair (minor)	X	X	X	X	X	X	X	X
Vehicle automobile sales/rental and service (indoor)	X	X	X	X	X	X	X	X
Vehicle, automobile salvage and recycling (indoor)	X	X	X	X	X	X	X	X
Vehicle, automobile salvage and recycling (outdoor)	X	X	X	X	X	X	X	X
Vehicle, boat/recreational vehicle sales and service	X	X	X	X	X	X	X	X
Vehicle, recreation								
Vehicle, truck repair (large)	X	X	X	X	X	X	X	X
Vehicle, truck sales and rental	X	X	X	X	X	X	X	X
Warehouse as a primary use	X	X	X	X	X		X	
Welding shops	X	X	X	X	X	X	X	X
Wholesale distributors distribution	X	X	X	X	X		X	X
Woodworking mill	X	X	X		X		X	
Zoological park	X	X	X	X	X	X		

- Existing Uses And Buildings: A use located within a station area legally existing at the time that this zoning district was adopted, but listed as a prohibited use in this subsection F, shall be considered a legal nonconforming use. A structure legally existing at the time this section was adopted, but not conforming to the standards in this chapter, shall be considered a legal noncomplying structure.



Any legal nonconforming use or legal noncomplying structure is subject to chapter 21A.38 of this title.

G. Development Standards:

1. Intent: The purpose of the following development standards is to promote an intense and efficient use of land at increased densities in the station areas. The development standards are intended to create a safe and pleasant environment near transit stations by encouraging an intensive area of mixed use development and activities, pedestrian amenities and by limiting conflicts between vehicles and pedestrians. Development standards are intended to create a reasonably continuous building edge that defines the exterior spatial enclosure of the street or open space and protect adjacent low density residential zoning districts. With some exceptions, buildings line a street at or near the public right of way to the greatest extent possible.
2. Application: The dimensional requirements in ~~table 21A.26.078GD~~ of this section apply to all new buildings and developments as well as additions to existing buildings. The following development standards apply to the core and transition areas of all station types:
 - a. Building Height: The minimum and maximum building heights are found in table 21A.26.070Ga Building Height Regulations. Height limits are intended to control the overall scale of buildings, the compatibility with adjacent development, and the composition of the urban form of the block. Minimum building heights in the core area relate to the width of the street, with a minimum ratio of one foot (1') of building height for every three feet (3') of street width. Building height is measured from the finished grade to the highest point of the building. The following exceptions apply:
 - (1) The minimum building height applies to all structures that are adjacent to a public or private street. The building shall meet the minimum building height for at least 10% of the width of the street facing building wall.
 - (2) Elevator shafts, parapet walls, and other projections are permitted subject to subsection 21A.36.020C, "Height Exceptions", of this title.
 - (3) Projects that achieve a development score that qualifies for administrative review are eligible for an increase in height. The increase shall be limited to one story of ~~in~~habitable space. The height of the additional story shall be equal to or less than the average height of the other stories in the building.



TABLE 21A.28.078G2a BUILDING HEIGHT REGULATIONS

	Minimum Height ¹	Maximum Height
<u>Urban center</u>		
Core	30'	90' ²
Transition	25'	60'
<u>Urban neighborhood</u>		
Core	25'	75'
Transition	0'	50'
<u>Mixed use employment center</u>		
Core	25'	75'
Transition	0'	60'
<u>Special Purpose</u>		
Core	25'	75'
Transition	0'	60'

1. Minimum building heights apply to those properties with frontage on the street where fixed rail transit is located.
2. Buildings with a roof that has at least 2 sloping planes may be allowed up to 105 feet. The additional height may include habitable space.

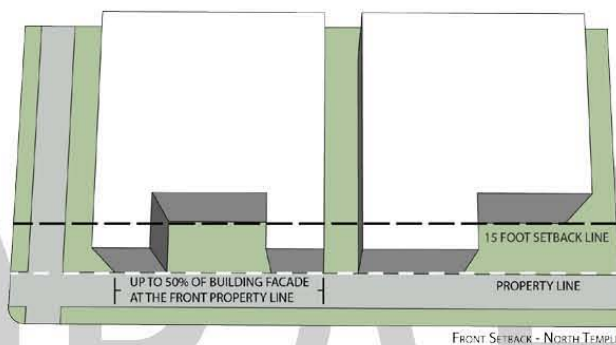
b. Setbacks: Required building setbacks promote streetscapes that are consistent with the desired character of the street and various station typologies and its core and transition areas. Building setbacks create a safe environment that is inviting to pedestrians and transit users and maintain light, air and potential privacy for adjacent residential uses. In some instances, the setbacks limit the building envelope where the existing development pattern would be negatively impacted by taller buildings.

(1) Front and Corner Yard Setback: Except as indicated below, there is no minimum setback. If a setback is provided, at least 50% of the street facing building facade shall be located within 5 feet of the front property line unless a larger setback is required below. All portions of a front yard not occupied by building, driveways, walkways or other similar feature must be completely landscaped or include an active outdoor use, such as outdoor dining, plazas or other similar outdoor use with the space not dedicated to active outdoor use completely landscaped. Parking, drive aisles or other paved areas for motor vehicles are prohibited. Walls up to three feet (3') in height, patios and other similar elements intended to activate the sidewalk can be located to the property line.

(~~2~~A) North Temple Boulevard: The front yard setback along North Temple Boulevard shall be fifteen feet (15') for a minimum of fifty percent (50%) of the width of the street facing facade. Up to fifty percent (50%) of a street facing facade may encroach up to the front property line. In this case, the area not occupied by the building footprint must be landscaped or

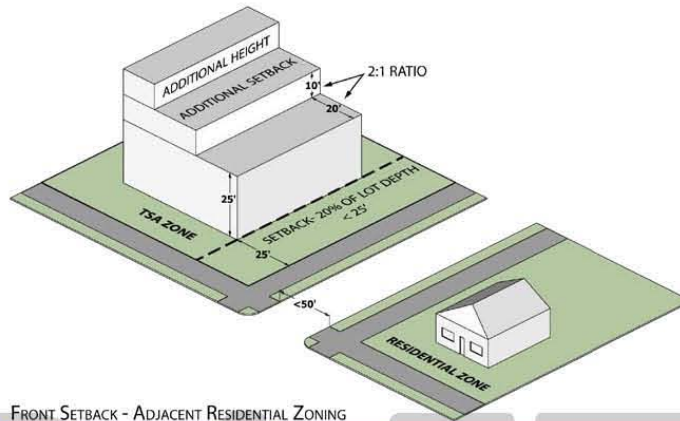


include active outdoor use, such as outdoor dining, plazas, courtyards or other similar usable public space or use. Setbacks over fifteen feet (15') are not allowed. In locations where there is not a minimum sidewalk width of 10 (ten) feet, additional sidewalk width shall be installed by the developer so there is a minimum width of 10 feet when a new building is constructed or with additions that increase the gross building square footage by more than 50%.



(B) 400 South/University Boulevard: The front yard setback along 400 South/University Boulevard shall be a minimum of fifteen feet (15'). In locations where there is not a minimum sidewalk width of ten (10) feet, additional sidewalk width shall be installed by the developer so there is a minimum width of ten feet (10') when a new building is constructed or with additions that increase the gross building square footage by more than 50%.

(C) Streets with a Right of Way of 50 feet or less: When located on a street with a right of way 50 feet or less with an R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district on either side of the street, a minimum setback of 25% of the lot depth but no more than twenty five feet (25') is required. For buildings taller than twenty five feet (25'), the setback shall increase two feet (2') for every foot of height above twenty five feet (25') in height. Buildings may be stepped so taller portions of the building are farther away from the front property line.



FRONT SETBACK - ADJACENT RESIDENTIAL ZONING

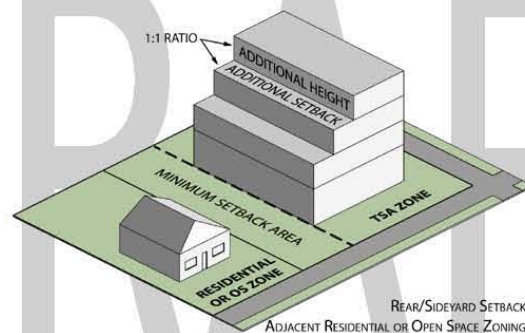
~~(43)~~ Side Yard Setback:

- (A) Drive aisles are allowed in the side yard setback. In the transition subarea, parking is allowed in the side yard subject to subsection L of this section.
- (B) Side yard setback when adjacent to a ~~single family or two-family certain~~ zoning districts:
- (i) Core Area: A minimum of twenty five feet (25') in the core area. When adjacent to an OS, R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district, the minimum side yard setback shall be increased one foot for every one foot increase in height above twenty five feet (25'). When a property in an R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district is separated from a TSA zoned property by an alley, the additional setback for height above twenty five feet (25') applies and shall be measured from the property line of the TSA zoned property. Buildings may be stepped so taller portions of a building are farther away from the side property line. The horizontal measurement of the step shall be equal to the vertical measurement of the taller portion of the building.
 - (ii) Transition Area: A minimum of fifteen feet (15') in the transition area. When adjacent to an OS, R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district, the minimum side yard setback shall be



increased one foot for every one foot increase in height above fifteen five feet (15'). When a property in an OS,R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district is separated from a TSA zoned property by an alley, the additional setback for height above fifteen five feet (15') applies and shall be measured from the property line of the TSA zoned property. Buildings may be stepped so taller portions of a building are farther away from the rear property line. The horizontal measurement of the step shall be equal to the vertical measurement of the taller portion of the building.

~~(C)~~(iii) Side yard setback when adjacent to other uses or districts: No minimum side yard required.



(54) Rear Yard Setback:

- (A) Core area: A minimum of fifteen feet (25'). When adjacent to an OS, R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 zoning district, the minimum rear yard setback shall be increased one foot for every one foot increase in height above twenty five feet (25'). When a property in an OS, R-1, R-2, SR, RMF-30 RMF-35 or RMF-45 zoning district is separated from a TSA zoned property by an alley, the additional setback for height above twenty five feet (25') applies and shall be measured from the property line of the TSA zoned property. ~~If a structure is located adjacent to a single family or two family residential zoning district, the rear setback shall be equal to the height of the building on the subject property in the transit station area district.~~
 Buildings may be stepped so taller portions of a building are farther away from the rear property line. The horizontal measurement of the step shall be equal to the vertical



measurement of the taller portion of the building. ~~When a public alley is between the two (2) properties, the width of the alley may be counted towards the setback.~~

- (B) Transition area: A minimum of twenty five feet (25'). When adjacent to an OS, R-1, R-2, SR, RMF-30 or RMF-35 zoning district, the minimum rear yard setback shall be increased one foot for every one foot increase in height above twenty five feet (25'). When a property in an OS, R-1, R-2, SR, RMF-30 or RMF-35 zoning district is separated from a TSA zoned property by an alley, the additional setback for height above twenty five feet (25') applies and shall be measured from the property line of the TSA zoned property. Buildings may be stepped so taller portions of a building are farther away from the rear property line. The horizontal measurement of the step shall be equal to the vertical measurement of the taller portion of the building.

- ~~(65)~~ Special Setback Provisions For Properties Adjacent To Jordan River: For properties that are adjacent to the Jordan River, the building setback from the Jordan River shall be fifty feet (50'), measured from the annual high water line as defined in section 21A.34.130 of this title. For buildings over fifty feet (50') in height, the setback shall increase one foot (1') for every foot in height over fifty feet (50') up to a maximum of seventy five feet (75'). Portions of buildings over fifty feet (50') in height may be stepped back to comply with this standard.

c. Minimum Lot Area and street frontage requirements:

- (1) The minimum lot area applies to all new subdivisions of land and shall not be used to calculate residential density.
- (2) Any legally existing lot may be developed without having to comply with the minimum lot size requirements.
- (3) The minimum lot area for all areas of the TSA zoning district is 2,500 square feet.
- (4) All subdivisions of land or combination of parcels must have a minimum of 40 feet of street frontage.

TABLE 21A.26.078GD DIMENSIONAL REQUIREMENTS

-	Min. Height ^a	Max. Height	Min. Front	Max. Front	Min. Side	Min. Rear	Minimum Lot	Minimum Lot Size



	-	-	Yard	Yard	Yard ⁺	Yard ⁺	Coverage For Structures	(Sq. Ft.)
Urban center:	-	-	-	-	-	-	-	-
- Core	30'	90' ²	None	15'	None	None	70%	5,000
- Transition	25'	60'	None	15'	None	None	50%	1,500
Urban neighborhood:	-	-	-	-	-	-	-	-
- Core	25'	75'	None	10'	None	None	70%	5,000
- Transition	0'	50'	None	10'	None	None	50%	1,000
Mixed use employment station:	-	-	-	-	-	-	-	-
- Core	25'	75'	None	15'	None	None	60%	5,000
- Transition	0'	60'	None	15'	None	None	50%	1,500
Special purpose:	-	-	-	-	-	-	-	-
- Core	25'	75'	None	25'	None	None	60%	10,000
- Transition	0'	60'	None	25'	None	None	50%	1,500

Notes:

- ~~1. Reference subsection G2 of this section for specific standards.~~
- ~~2. Minimum building heights apply to those properties with frontage on the street where fixed rail transit is located.~~
- ~~3. Buildings with a roof that has at least 2 sloping planes may be allowed up to 105 feet. The additional height may include inhabitable space.~~

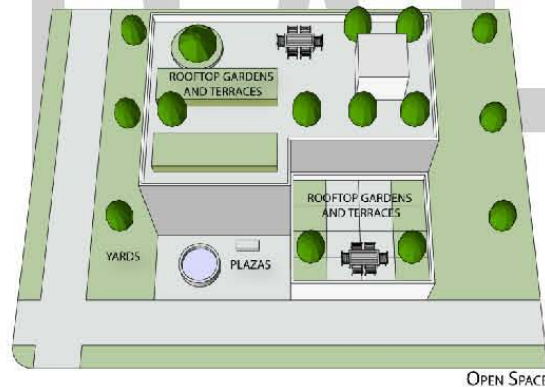
~~d. Minimum Lot Coverage: The intent of the minimum lot coverage is to ensure compact development.~~

d.e. Open Space: In order to provide space for passive and active recreation, public and private use, offset storm drainage due to nonpermeable surfaces and as an amenity to individual developments and their residents, employees and customers, open space is required for all new developments.

(1) Core Area:

(A) Within the core area, open space may include landscaped yards, patios, public plazas, pocket parks, courtyards, rooftop and terrace gardens and other similar types of open space amenity.

(B) A minimum of ten percent (10%) of the land area up to five thousand (5,000) square feet.



(2) Transition Area:

(A) Within the transition area, open space may include landscaped yards, patios, public plazas, pocket parks, courtyards, rooftop gardens and terraces, community gardens and other similar types of amenities.

(B) The minimum open space requirement is ten percent (10%) of the land area up to two thousand five hundred (2,500) square feet.

(3) Access To Open Space: All required open space shall be accessible to the users of the building(s).



ef. Circulation And Connectivity: Development within the station area shall be easily accessible from public spaces and provide safe and efficient options for all modes of travel. Circulation networks, whether public or private, require adequate street, pedestrian and bicycle connections to provide access to development. The internal circulation network shall be easily recognizable, formalized and interconnected.

(1) All parking lots shall comply with the standards in section 21A.44.020, "General Off Street Parking Requirements", of this title.

(2) Parking is prohibited between the street facing building line and any front or corner side property line. This shall include any drive aisle that is not perpendicular to the front or corner side property line.

H. Residential Densities:

1. Core area: No maximum.
2. Transition area: No maximum.

I. Accessory Structures: No accessory structure shall be located in a required front yard or between the primary building and a property line adjacent to a public street.

J. Design Standards:

1. Purpose: Design standards create the fundamental characteristics of a transit oriented district and the basic design elements required for a successful transit station area. Design standards are intended to provide a safe and interesting walkable environment by connecting ground floor uses adjacent to the sidewalk areas, by encouraging the continuity of retail and service uses, providing surveillance opportunities on the street and public open spaces and framing the street by bringing portions of buildings up to the sidewalk. All buildings shall be designed for the context and character of the project and how they interact visually, functionally, and socially with the context of the public environment.
2. Application: The following design standards apply to all projects within the core and transition areas of all station area types:
 - a. Building Walls Adjacent To A Street: Street-facing building facades shall provide architectural variety and scale. Changes in plane, color, texture, materials, scale of materials, patterns, art, or other architectural detailing are acceptable methods to create variety and scale. Building walls parallel to a public street and greater than thirty feet (30') in length shall be broken up by architectural features such as bay windows, recessed entrances or windows, balconies, cornices, columns, or other similar

architectural features. The architectural feature may be either recessed or project a minimum of twelve inches (12").



ARCHITECTURAL VARIETY AND SCALE

- b. Ground Floor Building Materials: Other than ground windows and doors, eighty percent (80%) of the remaining ground floor wall area shall be clad in durable materials. Durable materials include brick, masonry, textured or patterned concrete and/or cut stone. Other materials may be used as accent or trim provided they cover twenty percent (20%) or less of the ground floor adjacent to a street.
- c. Ground Floor Glass And Transparency: All street-facing elevations of a development shall be designed so that the first floor street-facing facade has at least sixty percent (60%) clear glass between three (3) and eight feet (8') above grade to allow pedestrians to view activities inside the building or lighted display windows. There must be visual clearance behind the glass for a minimum of two feet (2'). Three-dimensional display windows at least two feet (2') deep are permitted and may be counted toward the sixty percent (60%) glass requirement. Ground floor windows of commercial uses shall be kept clear at night, free from any window covering, with internal illumination. When ground floor glass conflicts with the internal function of the building, other means shall be used to activate the sidewalk, such as display windows, public art, architectural ornamentation or detailing or other similar treatment. The first floor elevation facing a street of all new buildings, or buildings in which the property owner is modifying the size of windows on the front facade, shall not have less than sixty percent (60%) glass surfaces. The reflectivity in glass shall be limited to eighteen percent (18%) as defined by ASTA standards. The planning director may approve a modification to this requirement if the planning director finds:



- (1) The requirement would negatively affect the historic character of the building, or
- (2) The requirement would negatively affect the structural stability of the building.
- (3) The ground level of the building is occupied by residential uses, in which case the sixty percent (60%) glass requirement may be reduced to forty percent (40%).

Any appeal of an administrative decision made pursuant to this subsection may be made to the planning commission.

- d. **Building Entrances:** The intent of regulating building entrances is to promote security on the street and public spaces by providing frequent points of access and sources of activity. Functional entrances to nonresidential uses should be located at an average of seventy five feet (75') or less from one another. At least one operable building entrance per elevation facing a public street shall be provided. Each ground floor leasable space is required to have an operable entrance facing the street and a walkway to the sidewalk. If a plaza or open space is provided as part of the development, a minimum of one entrance opening onto the plaza or open space shall be provided. This entrance shall be counted towards the spacing of functional entrances identified in this section and may count as the primary entrance to the building. All street facing building entrances shall be functional entrances and shall not be limited to emergency or employee entrances.
- e. **Ground Floor Residential Uses:** The interior floor elevation of ground floor residential units in the core area shall be a minimum of two feet (2') and a maximum of five feet (5') above grade. Dwelling units located on the ground floor and facing a public or private street shall have a minimum of one primary entrance facing the street in the core area. The facades of all buildings in the core and transition areas with ground floor residential uses shall feature elements that signal habitation such as windows, entrances, stairs, porches, bay windows, and balconies that are visible from the public street. Attached single-family dwellings, townhomes, row houses, ~~entrances that open onto a courtyard facing a public street,~~ and other similar housing types shall have a primary entrance facing the street for each unit adjacent to a street. Units may have their primary entrance located on a courtyard, midblock walkway, or other similar area if the street facing facades have a primary entrance facing the street.
- f. **Parking Structures:** The intent of regulating parking structures is to minimize the visual impact of the structure and the cars parking within it, and to reduce their impact on the ground floors adjacent to public sidewalks and streets. Parking structures are permitted within the core and transition areas provided:



- (1) The ground floor of parking structures adjacent to a public street shall include an active use other than parking such as office, retail, residential leasing office, restaurant, etc. Parking is permitted behind the ground floor uses. If the ground floor does not include active use, then the structure must be set back behind a building or be a minimum of sixty feet (60') from a property line adjacent to a public street or sidewalk.
- (2) The levels of parking above the first level facing the front or corner side lot line shall have horizontal floors and/or facades and not sloped.
- (3) The levels of parking above the second level shall be designed to effectively screen the vehicles so they are not readily visible from an adjacent street.
- (4) Below grade parking structures for structures with ground floor residential uses may extend a maximum of five feet (5') above the existing grade provided the above grade portion is screened with vegetation or architectural feature(s).

g. Mechanical Equipment: All roof mounted mechanical and electrical equipment, communication antennas or dishes shall be enclosed, screened, organized, designed and located to be out of view from streets and public spaces. The parapet or enclosure shall be equal to or greater than the height of the equipment to be screened to reduce equipment noise and odors, and other impacts onto adjacent uses and maintain the integrity of overall architectural character and scale of the building. Mechanical equipment may be located on the ground provided it is behind the building, screened and not located in a required rear yard or side yard setback. Utility boxes are subject to section 21A.40.160, "Ground Mounted Utility Boxes", of this title.

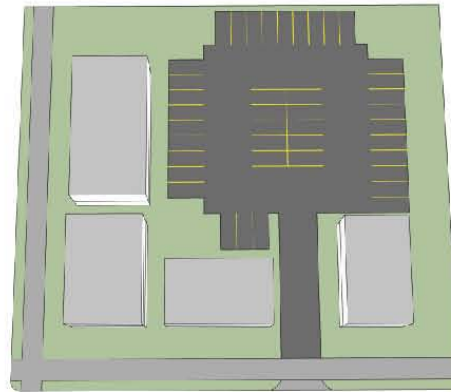
h. Service Areas: Service areas, loading docks, refuse containers and similar areas shall be fully screened from public view. All screening enclosures viewable from the street shall be either incorporated into the building architecture or shall incorporate building materials and detailing compatible with the building being served. All screening devices shall be a minimum of one foot (1') higher than the object being screened. Dumpsters must be located a minimum of twenty five feet (25') from any building on an adjacent lot that contains a residential dwelling or be located inside of an enclosed building ~~single or two family residential zoning district.~~

K. Multiple Buildings On A Single Parcel: Multiple principal buildings on a single parcel are permitted provided each principal building meets the requirements of this chapter and

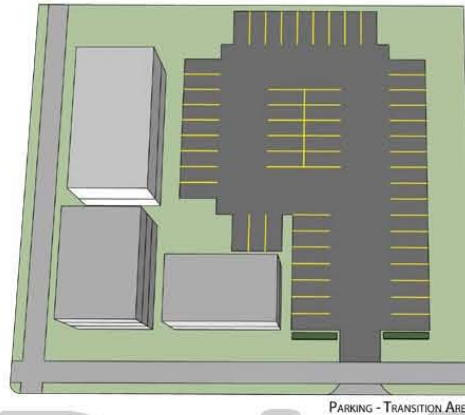


each principal building obtained a separate development score. New principal buildings can be located towards the rear of a parcel provided there is an existing or additional new principal building that complies with the front yard building setbacks. If one principal building receives a development score lower than other principal buildings on the site, the project shall be processed based on the lowest development score obtained.

- L. Parking: The purpose of this subsection is to provide locations for off street parking. All off street surface parking lots should be located so that they are compatible with pedestrian oriented streets. New uses and development or redevelopment within this district shall comply with the requirements of this subsection.
1. Surface Parking Lots And Structures On Corner Properties: On corner properties, surface parking lots and structures shall be located behind principal buildings or at least sixty feet (60') from a front and corner side lot lines. Only one driveway and drive aisle is permitted per street frontage and the access point shall be located a minimum of 100 feet from the intersection of the front and corner side property lines. If the front or corner side property line is less than 100 feet in length, than the drive approach shall be located within 20 feet of the side or rear property line.
 2. Surface Parking In The Core Area: Surface parking lots in the core area are required to be located behind the principal building or to the side of a principal building. When located to the side of a building, the parking lot shall be:
 - a. Set back a minimum of thirty feet (30') from a property line adjacent to a public street. The space between the parking lot and the property line adjacent to a public street shall be landscaped or activated with outdoor dining, plazas or similar feature.
 - b. Screened with a landscaped hedge or wall that is at least thirty six inches (36") above grade and no taller than forty two inches (42") above grade. Landscaping berms are not permitted.
 - c. The parking lot shall be no wider than what is required for ~~one~~ two rows of parking and one drive aisle as indicated in table 21A.44.020 of this title.
 - d. Only one driveway and drive aisle is permitted per street frontage and the access point shall be located a minimum of 100 feet from the intersection of the front and corner side property lines. If the front or corner side property line is less than 100 feet in length, than the drive approach shall be located within 20 feet of the side or rear property line.
 - e. Surface parking lots as a principal use: Surface parking lots as a principal use located on a lot that has frontage on a public street are prohibited.



3. Surface Parking In The Transition Area: Surface parking lots in the transition area are required to be located behind the principal building or to the side of a principal building. When located to the side of a principal building, the parking lot shall be:
- Set back so that no portion of the parking area other than the driveway is closer to the street than the front wall setback of the building. In cases where the front wall of the building is located within five feet (5') of a property line adjacent to a street, the parking lot shall be set back a minimum of eight feet (8'). The space between the parking lot and the property line adjacent to a street shall be landscaped or activated with outdoor dining, plazas or similar feature.
 - Screened with a landscaped hedge or wall that is at least thirty six inches (36") above grade and no taller than forty two inches (42") above grade. Landscaping berms are not permitted.
 - Surface parking lots as a principal use: Surface parking lots as a principal use located on a lot that has frontage on a public street are prohibited.



4. Walkways Through Parking Lots: Parking lots with more than fifteen (15) stalls shall provide a pedestrian walkway through the parking lot to the primary building entrance or a sidewalk providing access to a primary building entrance. One walkway must be provided for every three (3) drive aisles. Walkways shall be curb separated from the parking areas and a minimum of five feet (5') wide. Vehicles shall not overhang the walkway. Parking lot landscaping requirements in chapter 21A.48 of this title shall be included on the side of the walkway. Where the walkway crosses a drive aisle, a crosswalk that is clearly identified by a change in color, material, or similar technique shall be used.
 5. Surface Parking Lots as the Principal Use: Surface parking lots that are the principal use are permitted in the TSA zoning district provided the following standards are complied with:
 - a. The surface parking lot does not have street frontage on the street where the fixed rail transit is located.
 - b. The surface parking lot is setback a minimum of 15 feet from any property line adjacent to a public street.
 - c. The parking area is screened by a wall or landscaping that is a minimum of 36 inches and a maximum of 48 inches tall.
 6. Other Applicable Standards: All other standards in chapter 21A.44, "Off Street Parking And Loading", of this title shall apply.
- M. **Conflicting Regulations:** In cases where the regulations of this section conflict with another section of this zoning ordinance, this section shall take precedence except in situations where the conflict is related to the use of the property, in which case the more restrictive regulation takes precedence. In station areas within an overlay district, the overlay district shall take precedence.



N. Developments Over Five Acres:

1. Intent: Large scale developments have the potential to function as a self-contained mixed use neighborhood and could have both positive and negative impacts on nearby properties. All developments over five (5) acres in size shall be designed and planned to include a series of blocks and a network of public or private streets that connects to the existing public streets in the area and to adjacent development and neighborhoods. Buildings should be oriented to this street network. Regulating block size is necessary to provide development sites that are oriented to the pedestrian while accommodating other modes of transportation. A street network is required to ensure adequate circulation for pedestrians, bicycles, automobiles and service vehicles through the site, to adjacent sites and the public streets.
2. Application: These standards are in addition to all other applicable standards. In situations where the standards in this section conflict with a standard in another section, the standard in this section shall take precedence. A separate development score is required for each new principal building in a development over five (5) acres. A development over five (5) acres shall be subject to the applicable review process based on the lowest development score assigned to an individual building in the development.
 - a. Block Layout: The intent of regulating block size and dimension is to create a development pattern where all principal buildings have their primary facades facing a street, whether public or private. All developments over five (5) acres in size shall be designed to include a series of blocks based on the standards below:
 - (1) The maximum perimeter dimension of any block shall be one thousand six hundred feet (1,600'). The maximum length of any individual block face shall be four hundred forty feet (440').
 - (2) The maximum perimeter dimension of a block may be increased to two thousand four hundred (2,400) linear feet, and the maximum length of any block face increased to six hundred feet (600') provided a mid block pedestrian network is included. The mid block pedestrian network must be a minimum of twenty feet (20') wide and include pedestrian amenities such as lighting, benches, and other similar features. The mid block walkway shall connect to at least two (2) block faces or be extended to the property line to allow for future extension. The standards in subsection J2 of this section apply to building walls adjacent to a mid block walkway.



- b. **Housing Proximity To Transit:** Developments that include housing should cluster the housing so a minimum of fifty percent (50%) of the housing is located with one-fourth ($\frac{1}{4}$) mile walking distance of a transit platform.
- c. **Connectivity To Public Streets, Sidewalks, And Bicycle Lanes:** In order to ensure that the development will be fully integrated into the transit station area, that safe and efficient travelways are provided, and to limit the impact on the primary transit street and other adjacent streets, the internal circulation system, including private streets, drive aisles, sidewalks and bicycle lanes shall connect to the public street, sidewalks and bicycle lanes. All new streets shall be designed as a “complete street” defined as a street that provides dedicated space for pedestrians, bicyclists and automobiles.
- d. **Vehicle Access:** Regulating access to private property from public streets is necessary for integrating private development and public spaces. Limiting the number of access points and spacing between access points reduces areas of conflict between vehicles, pedestrians and bicycles. Maximum access widths promote a development pattern that is oriented to pedestrians and bicyclists while accommodating vehicles.
- (1) Access points located on public streets intended for vehicles shall be spaced a minimum of one hundred feet (100') apart.
 - (2) No property shall have more than one vehicle access point for every two hundred (200) linear feet of frontage on a public street.
 - (3) No access drive shall be greater than twenty four feet (24') wide.
 - (4) The location of all vehicle access points is subject to approval from the transportation division of the city. The standards of this section may be modified by the transportation division when, in the opinion of the director of the transportation division, a different design would improve the overall safety for all modes of transportation or improve the efficiency of the transportation network.
- e. **Internal Circulation:** Internal circulation systems allow for vehicles, pedestrians and bicyclists to move safely and efficiently throughout a development site. A logical, simple and well designed internal circulation system that connects with adjacent circulation networks provides room for vehicles, safe walking paths for pedestrians through the parking lot and the site to the public way, and well marked routes for bicycles traveling from public spaces to bicycle parking areas within a site. All new developments over five (5) acres are required to submit an internal circulation network plan.
- (1) **Travel Lanes That Connect Parking Areas With A Public Street:**
All internal vehicle travel lanes that connect internal parking areas



with a public street shall be designed to meet the minimum requirements in section 21A.44.020 of this title.

- (2) Design Speed: The internal circulation system shall be designed to move vehicles at speeds of twenty (20) miles per hour or less.
 - (3) Future Access To Adjacent Properties And Rights Of Way: All internal drive aisles, sidewalks, and paths shall be extended to property lines to allow for future cross access to adjacent properties when the adjacent property is undeveloped and to rights of way.
 - (4) Centerlines: The centerline of all internal streets shall be in line with the centerline of a street on the opposite side of an intersecting street unless the intersecting street is divided by a median. Offset streets shall be a minimum of two hundred feet (200') apart, measured from centerline to centerline.
 - (5) Publicly Dedicated Streets: Any street that is to be publicly dedicated shall meet the city's minimum construction and design standards (including street lighting, park strip, street trees, etc.).
 - (6) Pedestrian Routes: Pedestrian routes that provide safe, comfortable, clear and direct access throughout the development shall be provided. Pedestrian paths shall be bordered by residential fronts, green space, active open space, or commercial store fronts.
 - (7) Bicycle Paths: A coordinated system of bicycle paths should be provided.
 - (8) Approval; Modification Of Standards: The internal circulation network is subject to approval from the transportation division of the city. The standards of this section may be modified by the transportation division when, in the opinion of the director of the transportation division, a different design would improve the overall safety for all modes of transportation or improve the efficiency of the transportation network.
- f. Parking: Parking may be provided along any private street within a development over five (5) acres. The parking shall be counted towards the applicable off street parking standard when provided on private streets. All parking areas and stalls must comply with the parking lane widths identified in table 21A.44.020 of this title.
- g. Open Space: In order to provide space for passive and active recreation, public and private gatherings, offset storm drainage due to nonpermeable



surfaces and as an amenity to individual developments and their residents, employees and customers, usable open space is required for all new developments.

- (1) Required: In the core and transition areas of all station areas, a minimum of ten percent (10%) of the site, up to fifteen thousand (15,000) square feet, shall be devoted to open space. "Usable open space" is defined as landscaped areas, plazas, outdoor dining areas, terraces, rooftop gardens, stormwater retention areas, and any other similar type of area.
- (2) Connectivity To Adjacent Open Space: When adjacent to public open space, parks, trails and pathways, open space on developments over five (5) acres in size are encouraged to provide access to the public open space.

h. Landscaping: All areas not occupied by buildings, plazas, terraces, patios, parking areas, or other similar feature shall be landscaped. If a project is developed in phases, only those areas in a phase that is under construction shall be landscaped. Landscaping in future phases shall be installed as those phases develop. Areas in future phases may be used as community gardens or other active open space until such time as development of that phase begins.

From: [Marylee Peters](#)
To: [Pickering, Maryann](#)
Subject: Planning Commission Letter
Date: Wednesday, May 23, 2012 10:41:17 AM

May 22, 1012

Mary Ann Pickering, Principal Planner
Planning Department - Salt Lake City Government
451 State Street
Salt Lake City, Utah 84111

Dear Planning Commission,

The Board of Directors of the Friends of Gilgal Garden respectfully request that the Planning Commission recommend that the property that is adjacent to the west side of Gilgal Garden be zoned RMF 35. We have grave concerns that without such a zoning recommendation the sunlight necessary to maintain the plantings in this unique open space will be blocked by building heights.

The garden is used by an estimated 15,000 visitors annually. It was recently photographed along with the LDS Temple and the City County building for a national cable broadcast urging tourists to visit Salt Lake City. It is frequently visited by school, community and church groups. Visitors refer to it as an oasis in the middle of the city—a place for quiet reflection. It is a local treasure and a cultural heritage.

Twelve years ago, when this property was to be sold for development, a group of local open space advocates, preservationists, artists and art historians joined together to raise the \$600,000 to purchase the property. Because they considered Gilgal Garden valuable and a unique open space, Salt Lake County gave \$400,000 toward the purchase of the property, followed by the Church of Jesus Christ of Latter-day Saints Foundation, and the George S. and Dolores Dore Eccles Foundation who donated \$100,000 each. The property was then deeded to Salt Lake City to protect and maintain.

Since that time thousands of volunteer hours have been donated by the Salt Lake County Master Gardeners who have reclaimed and restored the garden landscape with the help of community volunteers. The board members of the Friends of Gilgal Garden have also donated thousands of hours in design and coordination of restoration of garden infrastructure and conservation of the sculptures. To date over \$200,000 has been spent in these efforts to restore and maintain the Garden.

We urge you to personally visit Gilgal Garden so that you can understand the importance of maintaining this very special open space in what will be a densely populated area when the new zoning goes into effect.

Thank you for your consideration of this matter.

Sincerely,

David Duke Mary Lee Peters

Co Presidents - Friends of Gilgal Garden

Mary Lee Peters
maryleeslc@gmail.com

From: lisa.c.thompson@comcast.net
To: [Pickering, Maryann](#)
Cc: [Mary Lee Peters](#)
Subject: 400 South Corridor Rezone and Gilgal Garden
Date: Tuesday, May 22, 2012 10:01:36 PM

May 22, 2012

Dear Planning Commission-

As discussion of the proposed rezoning of the 400 South Corridor was postponed in April, I wanted to resend my comments on this issue prior to the May 23 Planning Commission Meeting. I applaud the City's efforts to create walkable, mixed-use neighborhoods with easy access to public transit, but I strongly believe that special consideration needs to be given to the impacts of the proposed zoning on Gilgal Garden.

Gilgal Garden is a Utah, and arguably, a national treasure. You may have seen it featured recently in CNN Travel as one of six spots for architecture lovers. <http://www.cnn.com/2012/04/10/travel/architecture-travel-story/index.html> Gilgal's sculptures, engraved stones, and plants create a landscape of meaning. Scholars consider the garden a "visionary art environment." These works are usually created with found materials by people without formal artistic training to express deep-felt moral or religious convictions. A few visionary art environments, like Watts Towers in Los Angeles, have been preserved, but many more have been destroyed or are at risk.

I joined Friends of Gilgal Garden in 1997 in hopes of saving the garden from destruction. Since then numerous private individuals and community organizations have invested hundreds of thousands of dollars to purchase the garden, upgrade its facilities, and restore its sculptures and stones. Salt Lake City, of course, has been an invaluable partner in this work and committed its resources to maintaining the garden as a public space and an urban oasis.

The current zoning proposal threatens significant harm to the character of the garden. If surrounded by a 75-foot wall to the north and a 45-foot wall to the west, the garden will become a dark, enclosed box for much of the day. Buildings of this height would deprive the garden of the sunlight required to maintain the garden's plantings -- plantings that are a character-defining feature of the garden. The proposed zoning threatens the value of the garden as open space as well. The garden will feel hemmed-in and towered-over and lose its atmosphere of respite.

I urge you to lower the permitted building heights on the parcels abutting Gilgal Garden in order to preserve the community's investment in this unique artistic, historic, and open space resource.

Thank you for taking my comments into consideration. I hope you'll stop by the garden soon.

Sincerely,

Lisa Thompson

From: [Tim Roberts](#)
To: [Pickering, Maryann](#)
Subject: Gilgal Garden
Date: Wednesday, May 23, 2012 11:33:34 AM
Attachments: SkiRide_Utah_signature.png

Please think of how rezoning of the neighborhood might affect the garden. This garden is to be sort of an "oasis", once the neighborhood gets built up to 75-90 feet tall on 400 South, there won't be much green space left, and if the garden is mostly shaded, the landscape will suffer from "failure to thrive". You need to ask them to protect the garden, put a lower intensity zoning in place west of the garden, make sure we have enough light to keep this garden growing. It has an international reputation and we need to keep it that way.

--

Tim Roberts

Ski Utah

801.878.0832 - office

801.910.1116 - cell

www.skiutah.com

www.rideutah.com



From: [Diane Cotsonas](#)
To: [Pickering, Maryann](#)
Subject: Concern about Gilgal Garden
Date: Wednesday, May 23, 2012 11:39:56 AM

Commissioners, please protect Gilgal Garden by putting a lower intensity zoning in place west of the garden. It needs enough light to keep the garden growing. So many people have invested countless hours in making this a unique city park, and we should protect it for future generations. Sincerely, Diane Cotsonas, Salt Lake City resident

From: [Wray Featherstone](#)
To: [Pickering, Maryann](#)
Subject: Re: 400 South Project
Date: Wednesday, May 23, 2012 3:35:19 PM

I would attend tonight but I finally have work and its night work.
Thanks for your email.

I wanted to express my concern about the Office max Corner and the negative impact it might have if it was redeveloped into an even bigger big box store. There was some discussion about the points system and that if a project had enough points it would be approved with no neighborhood hearing process. I would like to see the point system to take into account local ownership and development giving more points to say a locally owned specialty grocery eatery rather than a kmart or walmart. When is this type of development discussed?

Thanks for hearing our input on the 400 south project, our neighborhood has become pretty close and we dont want that endangered by a development project or concept master plan driven by someone who is not in touch with its impact upon us.

-Wray Featherstone

On 5/23/12, Pickering, Maryann <Maryann.Pickering@slcgov.com> wrote:

> My apologies for this late email.

>

> The Planning Commission will be holding its second public hearing on the
> project tonight. However, I did want all three of you to know that we have
> removed the proposed rezoning of the properties along your block with the
> exception of the Village Inn corner site and their associated parking.
> Staff has left this corner in the event it is ever redeveloped, it will be
> done so in a way that provides protection to the neighborhood.

>

> You are welcome to attend the meeting tonight if you wish. I don't believe
> anyone of you would have gotten the postcard notice since you probably more
> than 300 feet from a property proposed to be rezoned.

>

> A revised zoning map is included for your reference.

>

> Please let me know if you have additional questions.

>

> Maryann Pickering, AICP
> Principal Planner
> Salt Lake City Planning Division
> 451 South State Street, #406
> PO Box 145480
> Salt Lake City, UT 84111-5480
> (801) 535-7660

>

>

From: [Amber Banks](#)
To: [Pickering, Maryann](#)
Subject: [QUAR] Gilgal Sculpture Garden
Date: Thursday, May 24, 2012 6:15:50 AM
Importance: Low

Will you please forward this to the planning Commissioners?

Please protect the Gilgal Sculpture Garden, by putting a lower intensity zoning in place west of the garden, make sure there is enough light to keep this garden growing.

Thank you very much!

Amber Banks
801-413-8416

To Salt Lake City Planning Commission
Re Comments on 400 S Livable Communities
From Cindy Cromer
May 23, 2012

Most important comment I can make is the following: As long as the City Council continues to upzone properties blocks away from TRAX...at times contrary to this Commission's recommendation...400 S will NOT redevelop in the direction we say that we want it to. I will of course make this statement to members of the City Council. I'm just practicing with you.

The "Devil is in the Details." (Dave Richards)
"Amen." (Cindy Cromer)

1. Circles around three areas on the map (handout): These areas are problematic under the proposal because half or more of the existing block face consists of open space and single family residences. The proposal to reduce the front setback will create a split personality on these sections of 300 S and 500 S. I don't know the answer but the staff's computer simulation shows you the problem.

2. More points for

-transferring height away from open space, historic structures, or existing low density residences. The transfer would occur within the same development, for example transferring mass away from the resources mentioned in #1 as worth respecting.

-placing a building on the Cultural Register (currently a measley 5 points) Doing so is a permanent reduction in the development potential of the property and should be worth lots of points. (Being worthy of the Cultural Register is a big deal. The City would be gaining protection for character-defining buildings.)

-accessible units because of the additional cost of construction

3. Fewer points for

-structured parking. The current system awards enough points for structured parking to get the applicant to tier 2 on that one item alone.

Excerpt of
SALT LAKE CITY PLANNING COMMISSION MEETING
Room 326 of the City & County Building
451 South State Street, Salt Lake City, Utah
Wednesday, May 23, 2012

A roll is being kept of all who attended the Planning Commission Meeting. The meeting was called to order at [5:36:26 PM](#). Audio recordings of the Planning Commission meetings are retained in the Planning Office for an indefinite period of time.

Present for the Planning Commission meeting were: Chairperson Angela Dean, Vice Chair Michael Gallegos and Commissioners, Lisa Adams, Emily Drown, Michael Fife, Bernardo Flores-Sahagun, Clark Ruttinger, Marie Taylor, Matthew Wirthlin, and Mary Woodhead. Commissioner Kathleen Hill was excused.

Planning Staff members present at the meeting were: Wilf Sommerkorn, Planning Director; Joel Paterson, Planning Manager; Nick Britton, Senior Planner; Janice Lew, Senior Planner; Carl Leith, Senior Planner; Ray Milliner, Principal Planner; Maryann Pickering, Principal Planner; Paul Nielson, City Land Attorney; and Michelle Moeller, Senior Secretary.

FIELD TRIP NOTES:

A field trip was held prior to the meeting. Planning Commissioners present were: Commissioners Lisa Adams, Michael Fife, Bernardo Flores-Sahagun, Clark Ruttinger and Marie Taylor. Staff members in attendance were Joel Paterson, Ray Milliner and Maryann Pickering. The following locations were visited:

Java Jo's, 877 South 1300 East

Staff presented the proposal and discussed issues raised by members' of the public including traffic and circulation, public notice and safety.

400 South Corridor

The Commission visited Gilgal Garden and discussed the proposed zoning, proposed allowable building height and impact on the park. The Commission visited the area of 900 East and 400 South and review the proposed zoning.

[5:47:21 PM](#)

PLNPCM2010-00647 - 400 South Livable Communities Station Area Plans - A request by Salt Lake City Mayor Ralph Becker for an amendment to the Central Community Master Plan, Salt Lake City Zoning Ordinance and Zoning Map regarding transit station area plans along the 400 South transit corridor. The intent of the project is to implement livability goals for the corridor including land use policies and zoning that support mixed use development and transportation choices for current and future residents and workers. The project is located in City Council District 4 represented by Luke Garrott. (Staff contact: Maryann Pickering at 801-535-7660 or maryann.pickering@slcgov.com)

Ms. Maryann Pickering, Principal Planner, reviewed the petition as presented in the Staff Report (located in the case file). She stated Staff recommended approval of the petition as presented.

The Commissioners and Staff reviewed the Transit Station Area Development Guidelines, which standards were necessary for approval or if people could pick and chose which standards to apply to their project in order to get the most points. They discussed Gilgal Garden and the effects the proposal could have on the property. Staff explained the designation of the property as Open Space would further protect it from development and if the proposed zoning designations were not approved, new buildings could potentially be constructed in a manner that shaded the garden and hindered growth of the plants.

The Commissioners and Staff discussed the permitted and prohibited uses in the area. They discussed the possibility of big box stores developing in the area and the regulations that would need to be followed to meet the TSA zoning requirements and development guidelines. They discussed the options of giving local businesses more points than national stores. Staff explained other Cities had similar provisions and had been challenged legally.

PUBLIC HEARING [5:59:49 PM](#)

Chairperson Dean opened the Public Hearing.

The following individuals spoke in favor of the proposal:

Cindy Cromer

David Duke

Kathy Scott

They made the following comments:

- As long as the City Council continued to up zone properties blocks away from TRAX, 400 South would not develop in the desired direction.
- It was important to protect Gilgal Garden from surrounding development.
- Issues regarding the Block between 900 and 1000 East and 400 and 500 South, were treated correctly and the result was well received.

Commissioner Woodhead congratulated staff on a job well done. She stated there was a lot of input from the Community and Staff responded effectively.

MOTION [6:10:41 PM](#)

Commissioner Woodhead moved regarding PLNPCM2010-00647, that the Planning Commission forward a favorable recommendation to the City Council based on the evidence and findings in the Staff Report, the public hearings, the written recommendation and the other materials. She recommended the addition that there be fewer points rewarded for structured parking than what was set forth in the proposal. Commissioner Wirthlin seconded the motion.

Commissioner Taylor asked what would happen with the current Historic Districts in the area. Ms. Pickering outlined the historic district and explained Staff was not proposing to change any boundaries at this time. She noted that changing the boundaries of the Central City Historic District may be addressed at a later date.

Commissioners Gallegos, Woodhead, Wirthlin, Ruttinger, Adams, Drown, Flores-Sahagun, and Ruttinger voted “aye”. Commissioner Fife and Taylor abstained. The motion passed 8-0-2.

8. ORIGINAL PETITION



Petition Initiation Request

Planning Division
Community & Economic Development Department



To: Mayor Becker

From: Wilf Sommerkorn, Planning Director *WS*

Date: September 7, 2010

CC: Frank Gray, Community and Economic Development Director; Mary De La Mare-Schaefer, Community & Economic Development Department Deputy Director;
Cheri Coffey, Assistant Planning Director; Nick Norris, Planning Manager;
file

SCANNED TO: *Mayor*
SCANNED BY: *Rachel*
DATE: *9.8.10*

Re: Initiate Petition relating to Transit Station Area Planning for the 400 South Corridor between 200 East and approximately 950 East.

The Planning Division is requesting that you initiate a petition to review the current zoning around the light rail stations on 400 South. The current zoning is a mix of D-1, TC-75, CS, and various residential zones. The purpose of the review is to identify existing zoning regulations that discourage or prohibit transit oriented development compatible with the surrounding development patterns. The proposed process will include several community meetings to identify issues and long term visions for the area, compare those visions with the current zoning along the corridor and determine if more appropriate zoning regulations are required to implement the community vision. It is anticipated that the process will be similar to the process used as part of the North Temple Station Area Plans project.

The Central Community Master Plan already identifies the corridor as one that should be developed following a transit oriented development pattern. The initial proposal would be to utilize the newly adopted TSA zoning district as a base zoning district. The TSA zoning district was created with a consideration towards applying it in other parts of the City and was crafted in a manner that allows for station area specific zoning regulations.

It is anticipated that a zoning map and text amendment would be required. Specific details of these amendments would be determined after the initial public visioning process is complete.

If you have any questions, please contact me.

Thank you.

Concurrence to initiate the zoning text amendment petition as noted above.



Ralph Becker, Mayor



Date

Remarks:

Petition No: **PLNPCM2010-00647**

By: **Salt Lake City Planning Division**

Review Current Zoning for Light Rail Stations on 400 South

Date Filed: September 7, 2010

Address: **400 South between 200 East and approx 950 East**