



MEMORANDUM

PLANNING DIVISION
DEPARTMENT *of* COMMUNITY *and* NEIGHBORHOODS

To: Salt Lake City Planning Commission

From: Amanda Roman, Urban Designer
801-535-7660 or amanda.roman@slc.gov

Date: April 8, 2026

Re: Planned Development and Design Review Modification & Time Extension Request
for District North at approximately 523 W 200 N

ACTION REQUIRED:

Vote on granting Design Review modifications to originally approved building materials and a one-year time extension for the Planned Development and Design Review approval for District North at approximately 523 W 200 N.

RECOMMENDATION:

Planning staff recommend approving the building material changes and granting a one-year time extension for the Planned Development and Design Review approvals which would change the projects expiration date to April 9, 2027.

BACKGROUND/DISCUSSION:

The District North project received Planned Development and Design Review approval on March 26, 2025 (Planning File # PLNPCM2024-01232 & PLNPCM2025-00021). The decision gave approval for a residential development with 350 units, ranging from studios to two-bedrooms.

The project received Planned Development approval for 5 feet of additional building height and a reduced front yard setback. It was granted Design Review approval for additional building length, reduced ground floor use, reduced ground floor glass, reduction of durable materials, and a modification of the building entrance requirement.

The development was designed under the TSA-UC-T zoning district standards, which as of October 2025, are no longer in effect. The approvals remain vested under the TSA-UC-T, the prior design standards and design review standards at the time of approval. The property has since been rezoned to [MU-6](#) as part of a city-wide zoning district consolidation. A time extension is necessary to ensure the project remains vested because the development could not be constructed as originally approved because it no longer meets the updated regulations.

Request for Design Review Modifications

The applicant is seeking approval to modify the design standard in section 21A.37 that regulates durable building materials. The proposed updates would bring the project closer to compliance with current design standards compared to the original approval.

The applicant is proposing to change the building materials on the upper two levels of the northeast corner from plaster/EIFS (a non-durable material) and brick to paneling consisting of fiber cement board and to remove the private balconies at the same location. The reduction in plaster/EIFS resulted in slight changes to the durable material percentages that do not require Planning Commission approval (see building elevations in Attachment B). The percentage of plaster/EIFS on the 200 North façade would decrease from 24.3% to 18.2% and from 19.7% to 16.2% on the 500 West façade. The proposed material changes comply with the current MU-6 zoning requirements, which require that at least 70% of materials on both upper and lower levels be durable.

If the modification is approved, the applicant intends to relocate amenity space from the ground floor interior courtyard to the northeast corner of the building on floors 5 and 6. Additionally, dwelling units currently planned for floors 5 and 6 would be moved to the building's interior. The removal of the private balconies is a direct result of this residential unit relocation but are not tied to a specific design standard.

Under the former TSA zoning, plaster/EIFS was limited to 10% of upper-level facades. While the proposed design would still exceed that limit, it aligns with the updated and simplified MU-6 zoning standards by meeting the 70% durable material requirement on both the upper and lower levels.

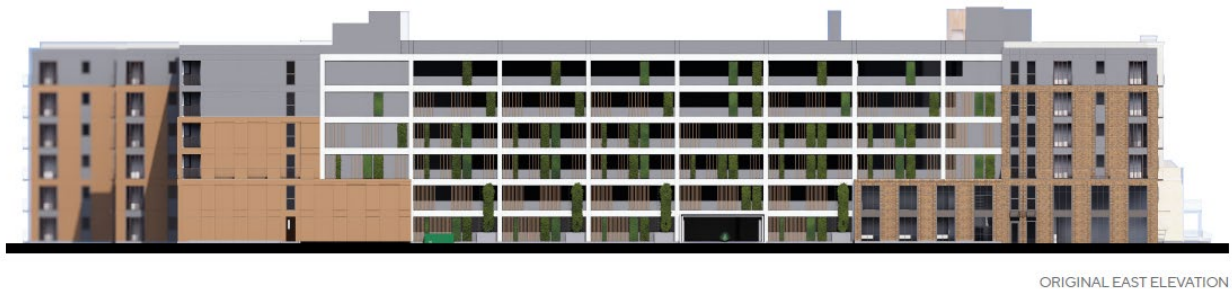
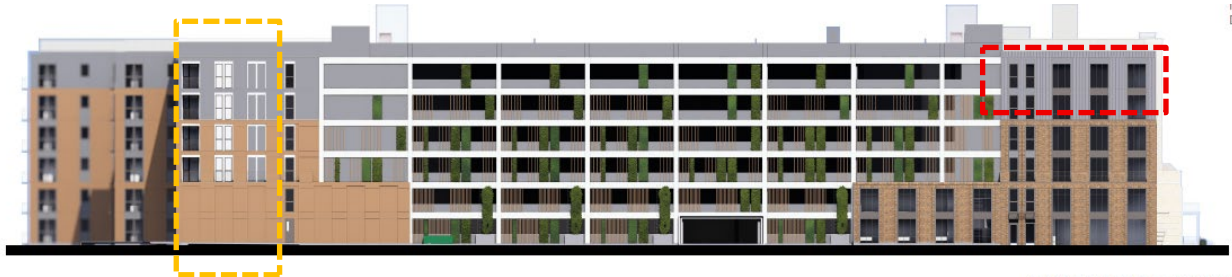
200 North Elevation

- Material changed from plaster (non-durable) and brick to paneling (tiles/fiber cement board) on the upper two levels of the north facing façade.
- Private balconies on levels 3-6 the northeast corner were removed due to changes in the floorplan and increase in residential amenities.
- Townhomes (outlined in orange) were widened to allow for larger 3-bedroom units. They remain in the same location but were reduced from 9 to 6 units. While this is a noted change, it is not part of the requested Design Review modifications.



500 West Elevation

- Material changed from plaster (non-durable) and brick to paneling (tiles/fiber cement board) on the upper two levels of the east facing façade.
- Private balconies on floor 3-6 of the northeast corner were removed due to changes in the floorplan and increase in residential amenities.
- Windows and balconies were added to the residential units on the southern portion of the façade (outlined in orange), which is the result of changing the interior floorplan.



[Section 21A.59.080.B](#) allows modifications to approved design review plans when the proposed modification does not require a change to a condition of approval or a finding that was identified in the staff report or record of decision. The proposal meets these criteria and there are no other design review modifications being requested. The updated plans are provided in Attachment B of this report.

Time Extension Request

Planned Development and Design Review approvals expire in one year from the date of approval, unless a building permit is submitted. The applicant is requesting a time extension of their approvals and submitted the request prior to the expiration date of March 26, 2026. Their letter requesting the extension and justification for requesting the extension is included in Attachment A of this report. Both [Sections 21A.55.080](#) Time Limit on Approved Planned Development and [Section 21A.59.060](#) Time Limit on Approved Applications for Design Review, allow for a one-year extension if the applicant is able to demonstrate there is no unmitigated impact and if the applicant requests a time extension prior to the project's expiration date.

The applicant requested the extension on March 2, 2026. Due to unanticipated site constraints and the current economic situation, they are unable to start construction within the initial timeline and have not yet submitted complete building plans to Building Services. Other than the request to change building materials and the removal of balconies, the plans have not been modified. The applicant has worked with Planning staff to resolve the site constraints (which do not affect the site or building design) and is continuing to work towards obtaining building permits. They anticipate submitting for building permits by the end of summer 2026.

This is the applicant's first extension request. If the extension is approved, they will need to submit complete building plans to Building Services before April 9, 2027.

Staff finds that the project meets the conditions of approval, under the former TSA-UC-T (Transit Station Area – Urban Center – Transit) zoning district and is recommending the Planning Commission approve the minor modification to building materials and the one-year time extension request.

ATTACHMENTS:

- A. Time Extension Request Letter
- B. Updated Building Elevations
- C. Original Planning Commission Record of Decision
- D. Original Planning Commission Staff Report Packet

ATTACHMENT A: TIME EXTENSION REQUEST LETTER

From: [Nicholai Lazarev](#)
To: [Roman, Amanda](#)
Subject: (EXTERNAL) 523 W 200 N (District North) - Project Extension Request
Date: Monday, March 2, 2026 9:52:25 AM
Attachments: [image413016.png](#)

Caution: This is an external email. Please be cautious when clicking links or opening attachments.

Hi Amanda,

Hope you're doing well! We would like to respectfully request a one-year extension to our entitlements for 523 W 200 N. We were hoping to complete drawings and submit for building permit before the entitlement deadline on March 26th but unfortunately we had a few design issues we needed to resolve and it delayed our process. We are currently putting together full construction drawings and hope to see material improvements in construction pricing; although from what we hear it hasn't been too positive. That said, the only reason we are requesting the extension is because we simply will miss the deadline before submitting for building permit. We are hopeful for a project start for 523 W 200 N by this year, or Q1 2027.

Thank you as always for all your help with this project!

Best,
Nicholai



Nicholai Lazarev
Partner | The Boyer Company
t. [240-338-5347](tel:240-338-5347)
101 South 200 East, Suite 200
Salt Lake City, Utah 84111
www.boyercompany.com

Notice: Starting 3/01/26 - 3/31/26 Emails will come from @boyer.com as we transition away from @boyercompany.com

ATTACHMENT B: UPDATED BUILDING ELEVATIONS

NOTE:

THE PROPOSED ELEVATION EXCEEDS THE PERCENTAGE OF GLAZING AND DURABLE MATERIALS OF THE ORIGINAL DESIGN - SEE A6

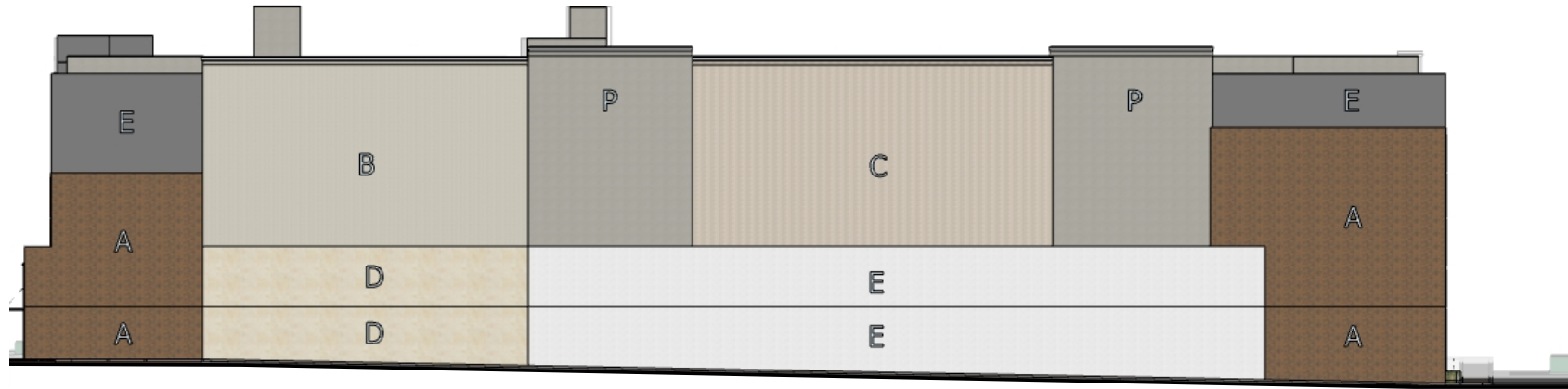


PROPOSED ELEVATION



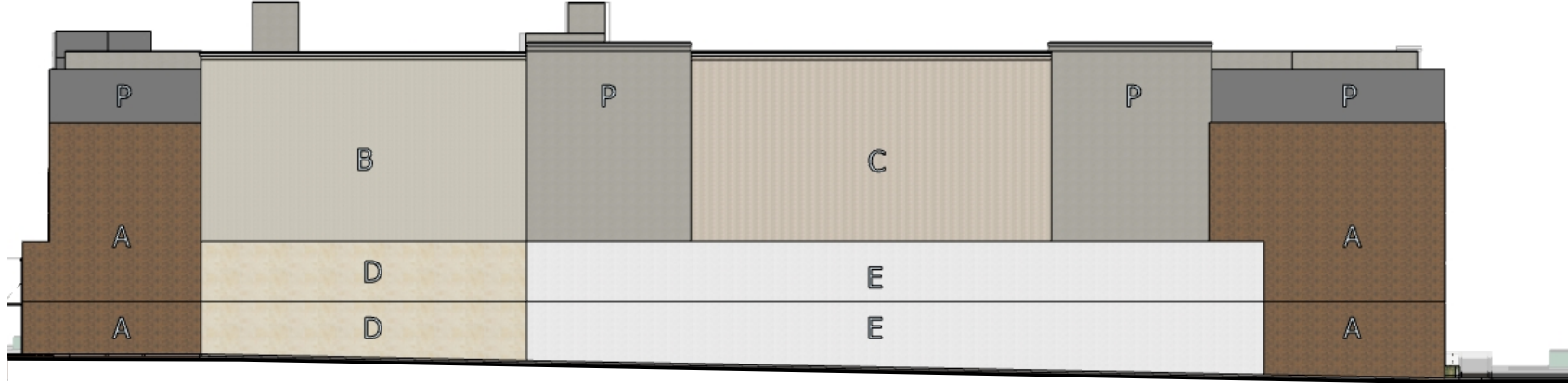
ORIGINAL ELEVATION

DURABLE MATERIAL CALCULATIONS - NORTH - PROPOSED			
NORTH FAÇADE - UPPER FLOORS			
SYMBOL	DESCRIPTION	AREA	PERCENTAGE OF TOTAL
A	BRICK (THINK BRICK OR FIBER CEMENT BOARDS)	3,052 SF	17.9%
B	BRICK (THINK BRICK OR FIBER CEMENT BOARDS)	2,967 SF	17.4%
C	BRICK (THINK BRICK OR FIBER CEMENT BOARDS)	3,279 SF	19.3%
D	TILES (TILES OR FIBER CEMENT BOARDS)	992 SF	5.8%
E	PANELS (TILES OR FIBER CEMENT BOARDS)	3,628 SF	21.3%
P	ARCHITECTURAL PLASTER (NON DURABLE)	3,090 SF	18.2%
UPPER FLOOR FAÇADE AREA		17,008 SF	100.0%
NORTH FAÇADE - GROUND FLOOR			
A	BRICK (THINK BRICK OR FIBER CEMENT BOARDS)	1,142 SF	25.5%
D	TILES (TILES OR FIBER CEMENT BOARDS)	908 SF	20.3%
E	PANELS (TILES OR FIBER CEMENT BOARDS)	2,432 SF	54.3%
P	ARCHITECTURAL PLASTER (NON DURABLE)	0 SF	0.0%
GROUND FLOOR FAÇADE AREA		4,482 SF	100.0%



PROPOSED ELEVATION - NORTH

DURABLE MATERIAL CALCULATIONS - NORTH - ORIGINAL			
NORTH FAÇADE - UPPER FLOORS			
SYMBOL	DESCRIPTION	AREA	PERCENTAGE OF TOTAL
A	BRICK (THINK BRICK OR FIBER CEMENT BOARDS)	3,394 SF	20.0%
B	BRICK (THINK BRICK OR FIBER CEMENT BOARDS)	2,967 SF	17.4%
C	BRICK (THINK BRICK OR FIBER CEMENT BOARDS)	3,279 SF	19.3%
D	TILES (TILES OR FIBER CEMENT BOARDS)	992 SF	5.8%
E	PANELS (TILES OR FIBER CEMENT BOARDS)	2,246 SF	13.2%
P	ARCHITECTURAL PLASTER (NON DURABLE)	4,130 SF	24.3%
UPPER FLOOR FAÇADE AREA		17,008 SF	100.0%
NORTH FAÇADE - GROUND FLOOR			
A	BRICK (THINK BRICK OR FIBER CEMENT BOARDS)	1,142 SF	25.5%
D	TILES (TILES OR FIBER CEMENT BOARDS)	908 SF	20.3%
E	PANELS (TILES OR FIBER CEMENT BOARDS)	2,432 SF	54.3%
P	ARCHITECTURAL PLASTER (NON DURABLE)	0 SF	0.0%
GROUND FLOOR FAÇADE AREA		4,482 SF	100.0%



ORIGINAL ELEVATION - NORTH



DISTRICT NORTH

SALT LAKE CITY, UT

NORTH BUILDING ELEVATION
DURABLE MATERIAL PERCENTAGES COMPARISON



A6

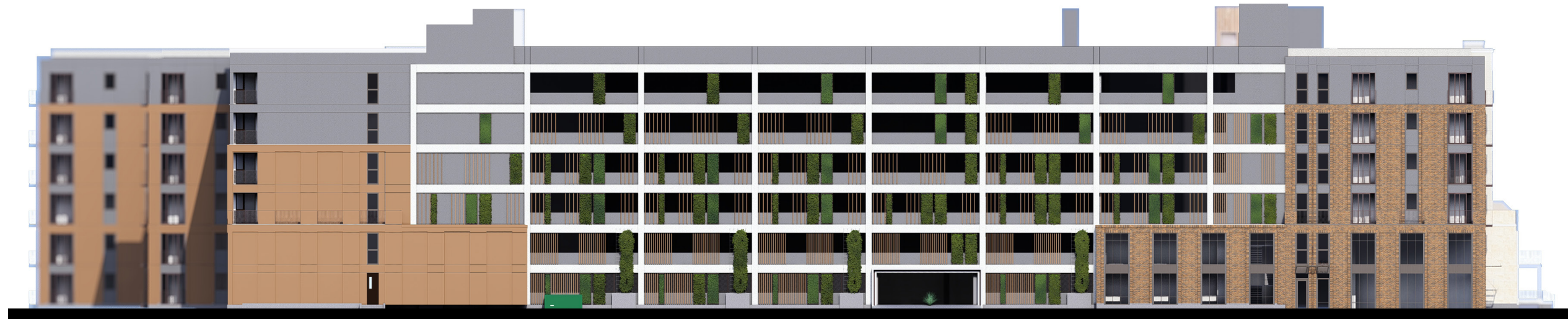
JOB NO. 2023-0651
DATE 03-17-2026

NOTE:

THE PROPOSED ELEVATION EXCEEDS THE PERCENTAGE OF GLAZING AND DURABLE MATERIALS OF THE ORIGINAL DESIGN - SEE A6

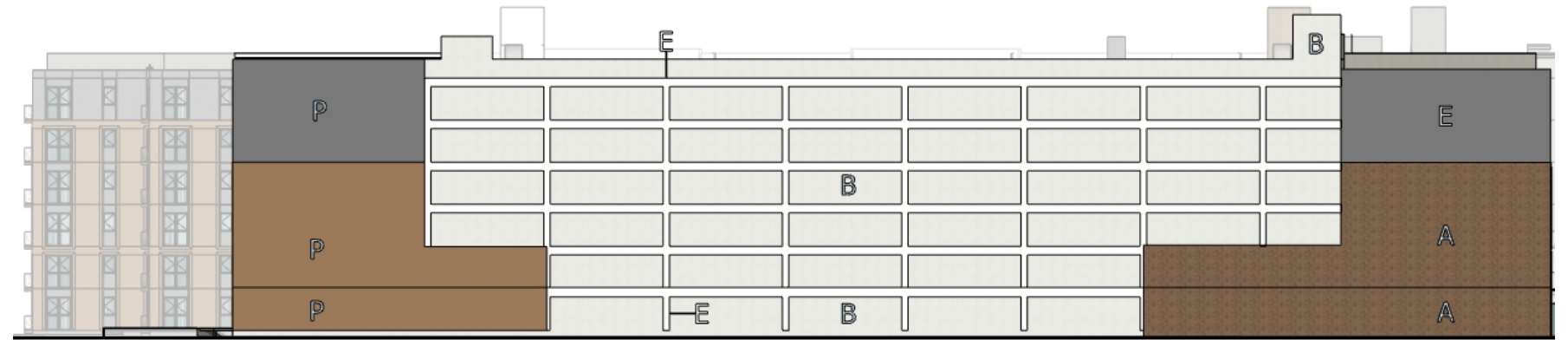


PROPOSED EAST ELEVATION



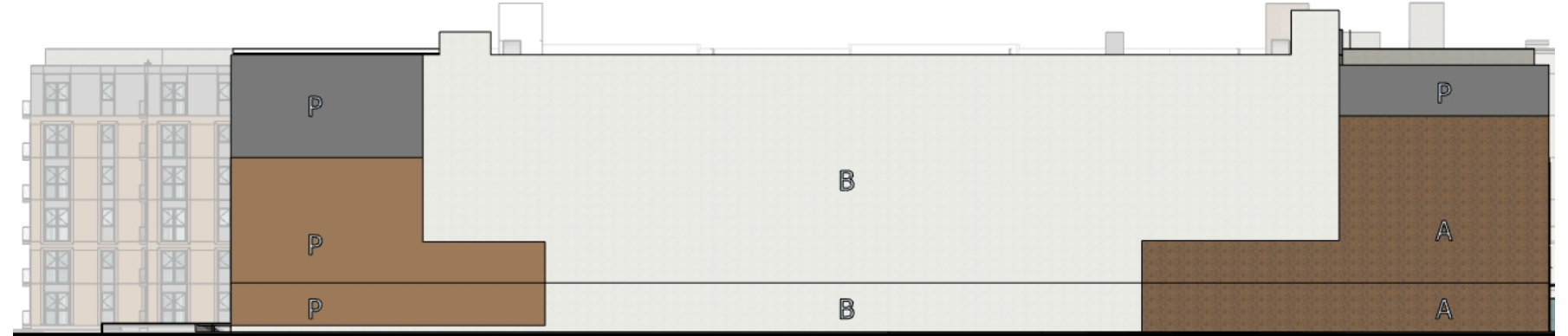
ORIGINAL EAST ELEVATION

DURABLE MATERIAL CALCULATIONS - EAST - PROPOSED			
WEST FAÇADE - UPPER FLOORS			
SYMBOL	DESCRIPTION	AREA	PERCENTAGE OF TOTAL
A	BRICK (THINK BRICK OR FIBER CEMENT BOARDS)	1,995 SF	11.5%
B	PAINTED CONCRETE	8,898 SF	51.1%
E	PANELS (TILES OR FIBER CEMENT BOARDS)	3,700 SF	21.3%
P	ARCHITECTURAL PLASTER (NON DURABLE)	2,814 SF	16.2%
UPPER FLOOR FAÇADE AREA		17,407 SF	100.0%
WEST FAÇADE - GROUND FLOOR			
A	BRICK (THINK BRICK OR FIBER CEMENT BOARDS)	1,150 SF	30.9%
B	PAINTED CONCRETE	1,415 SF	38.0%
E	PANELS (TILES OR FIBER CEMENT BOARDS)	382 SF	10.3%
P	ARCHITECTURAL PLASTER (NON DURABLE)	774 SF	20.8%
GROUND FLOOR FAÇADE AREA		3,721 SF	100.0%



PROPOSED ELEVATION- EAST

DURABLE MATERIAL CALCULATIONS - EAST - ORIGINAL			
WEST FAÇADE - UPPER FLOORS			
SYMBOL	DESCRIPTION	AREA	PERCENTAGE OF TOTAL
A	BRICK (THINK BRICK OR FIBER CEMENT BOARDS)	2,503 SF	14.4%
B	PAINTED CONCRETE	11,473 SF	65.9%
E	PANELS (TILES OR FIBER CEMENT BOARDS)	0 SF	0.0%
P	ARCHITECTURAL PLASTER (NON DURABLE)	3,431 SF	19.7%
UPPER FLOOR FAÇADE AREA		17,407 SF	100.0%
WEST FAÇADE - GROUND FLOOR			
A	BRICK (THINK BRICK OR FIBER CEMENT BOARDS)	1,150 SF	30.9%
B	PAINTED CONCRETE	1,797 SF	48.3%
E	PANELS (TILES OR FIBER CEMENT BOARDS)	0 SF	0.0%
P	ARCHITECTURAL PLASTER (NON DURABLE)	774 SF	20.8%
GROUND FLOOR FAÇADE AREA		3,721 SF	100.0%



ORIGINAL ELEVATION- EAST

**ATTACHMENT C: PLANNING COMMISSION RECORDS
OF DECISION - Issued March 27, 2025**



March 27, 2025

Nicholai Lazarev
Boyer Company
101 S 200 E Suite 200
Salt Lake City, UT, 84108

**RE: Record of Decision for Petition PLNPCM2024-01232 & PLNPCM2025-00021
Boyer District North Planned Development and Design Review**

Dear Jason:

On March 26, 2025, the Salt Lake City Planning Commission granted Planned Development approval for the property located at approximately 523 W 200 N.

This Record of Decision provides you with information regarding the date action was taken, the decision of the Planning Commission, including any approval conditions, the one-year time limit on the approval, the limitations on modifications to the plans, and the 10-day appeal period.

Project Description

The Planning Commission reviewed and approved the following project:

Planned Development: The applicant is requesting Planned Development approval for the following modifications to the TSA zoning district standards:

- *Request for up to 5' of additional building height from a maximum of 60' to approximately 63'9"*
- *Reduced front yard setback from 25' to approximately 8'4"*

Design Standards Modifications: Design review is required for the following modifications to the Design Standards in chapter 21A.37:

- *Increase in street facing building façade length from 200' to 319' along 200 N and 360' along 500 W*
- *Reduction in durable materials:*
 - *200 N: Request to use 24% on the upper levels of the façade, when 10% is permitted*
 - *500 W: Reduction in lower-level durable materials from 90% to 85%, the non-durable materials is plaster (similar to EIFS) which is not permitted on the ground floor*
 - *500 W: Request to use 17% plaster on the upper levels, when 10% is permitted*
- *Reduction of the ground floor use requirement from 90% active use to zero along 500 W. The applicant is proposing to build the parking structure facing the rail lines. 500 West is a dead-end public street.*
- *Reduction in ground floor glass from 60% to 15% along 500 W*
- *Modification to the spacing of building entrances along both 200 N and 500 W*

Conditions of Approval

The following conditions were applied to the approval of the proposal:

1. *Final approval of the details for lighting and site details to be delegated to Planning Staff to verify compliance with the associated Planned Development and Design Review Standards.*

Review Process Standards and Findings of Fact

The Planning Commission made specific findings related to the standards of review for Planned Developments as stated in Chapter 21A.55 of the Zoning Ordinance. The decision was also based on the purpose of the zoning ordinance, the purpose of the zoning district where the project is located, the information contained in the staff report, the project details provided by you, testimony from the public, and the discussion of the Planning Commission. Copies of this information will be made available online here: <https://www.slc.gov/planning/planning-commission-agendas-minutes/>.

Modifications to the Approved Plans

To obtain a building permit, all plans must be consistent with the plans reviewed and approved by the Planning Commission. The plan approved by the Planning Commission constitutes the site design in relation to building placement and design, landscaping, mobility and circulation elements, and any elements that were approved as zoning modifications through the Planned Development process. Modifications to the plan require an application to the Planning Division, and the Planning Director can only approve narrowly defined minor modifications as listed in 21A.55.100B of the Zoning Ordinance. Any modification not listed as a minor modification requires approval by the Planning Commission.

Modifications to the design review aspects required an application to the Planning Division, and the Planning Director can only approve narrowly defined minor modifications as listed in 21A.59.080A of the Zoning Ordinance. Any modification not listed as a minor modification requires approval by the Planning Commission.

One Year Time Limit on Approval

No planned development approval shall be valid for a period longer than **one year** from the date of approval unless a building permit is issued or complete building plans and building permit applications have been submitted to the Division of Building Services and Licensing. An extension of one year may be granted by the Planning Commission. Extension requests must be submitted in writing prior to the expiration of the planned development approval.

10-Day Appeal Process

There is a 10-day appeal period in which any affected party can appeal the Planning Commission's decision. This appeal period is required in the City's Zoning Ordinance and allows time for any affected party to protest the decision if they so choose. The appeal would be heard by the Appeals Hearing Officer. Any appeal, including the filing fee, must be submitted by the close of business on April 7, 2025.

The summary of action for the Planning Commission meeting is located on the Planning Division's website at: <https://www.slc.gov/planning/public-meetings/planning-commission-agendas-minutes/>.

If you have any questions, please contact me at 801-535-7930 or Kelsey.lindquist@slcgov.com.

Sincerely,



Kelsey Lindquist
Planning Manager

cc: File

ATTACHMENT D: ORIGINAL PC STAFF REPORT PACKET



Staff Report

PLANNING DIVISION

DEPARTMENT of COMMUNITY and NEIGHBORHOODS

To: Salt Lake City Planning Commission
From: Amanda Roman, Urban Designer
amanda.roman@slc.gov, 801.535.7660
Date: March 26, 2025
Re: PLNPCM2024-01232 – Boyer District North Design Review
PLNPCM2025-00021 – Boyer District North Planned Development

Design Review & Planned Development

PROPERTY ADDRESS: 523 W 200 N

PARCEL ID: 08-36-352-024-0000

MASTER PLAN: [North Temple Boulevard Plan](#)

ZONING DISTRICT: TSA-UC-T Transit Station Area – Urban Core - Transition

REQUEST:

Salt Lake City has received a request from Nicholai Lazarev, representative of the Boyer Company, seeking Design Review and Planned Development approval for a multi-family development at approximately 523 W 200 N. The site consists of one parcel totaling approximately 2.91 acres (126,760 SF). The subject property is in the TSA-UC-T District within the Viaduct Transfer Station Area of the North Temple Boulevard Plan.

The proposed development is approximately 64 feet tall and will include 350 units, ranging from studios to two-bedrooms. 500 N is a dead-end street that runs parallel to the FrontRunner rail line and terminates at the North Temple viaduct, which is a half block south of the subject property. The street is public, which has made it difficult for the applicant to meet the street facing building façade design standards.

Planned Development: The applicant is requesting Planned Development approval for the following modifications to the TSA zoning district standards:

- Request for up to 5' of additional building height from a maximum of 60' to approximately 63'9"
- Reduced front yard setback from 25' to approximately 8'4"

Design Standards Modifications: Design Review approval is required for the following modifications to the Design Standards in [chapter 21A.37](#):

- Increase in street facing building façade length from 200' to 319' along 200 N and 360' along 500 W
- Reduction in durable materials:
 - 200 N: Request to use 24% on the upper levels of the façade, when 10% is permitted

- 500 W: Reduction in lower level durable materials from 90% to 85%, the non-durable materials is plaster (similar to EIFS) which is not permitted on the ground floor
- 500 W: Request to use 17% plaster on the upper levels, when 10% is permitted
- Reduction of the ground floor use requirement from 90% active use to zero along 500 W. The applicant is proposing to build the parking structure facing the rail lines. 500 West is a dead-end public street.
- Reduction in ground floor glass from 60% to 15% along 500 W
- Modification to the spacing of building entrances along both 200 N and 500 W

RECOMMENDATION:

Based on the information and findings listed in the staff report, it is the Planning Staff's opinion that the request generally meets the applicable standards of approval and therefore recommends the Planning Commission approve the request with the following condition:

1. Final approval of the details for lighting and site details to be delegated to Planning staff to verify compliance with the associated Planned Development and Design Review Standards.

ATTACHMENTS:

ATTACHMENT A: Vicinity & Zoning Map

ATTACHMENT B: Applicant Materials

ATTACHMENT C: Site & Vicinity Photos

ATTACHMENT D: TSA-UC-T Zoning Standards

ATTACHMENT E: Planned Development Standards

ATTACHMENT F: Design Review Standards

ATTACHMENT G: Public Process & Comments

ATTACHMENT H: Department Review Comments

PROJECT DESCRIPTION

Proposed Development

The proposed development, located at 523 W 200 N, will provide 350 units, ranging from studios to two bedrooms. The site has approximately 363 feet of frontage on 200 North and 358 feet of frontage along 500 West. The multi-family building is wrapped with a combination of two-story townhomes, which face 200 North and ground floor units that face the western and southern interior property lines. The eastern façade, which faces 500 West, consists of a six-story parking structure with a single ingress and egress for vehicles and approximately 439 stalls. The rooftop of the parking structure contains the building's amenities. Due to the confluence of Front Runner, Trax, and the termination of 500 West, the proposed project is finding it difficult to comply with all of the design and zoning requirements.

Quick Facts

Height: 63'9" (6 stories)

Number of Residential Units: 350 units

Ground Floor Uses:

200 North – Nine two-story townhomes, lounge

500 West – Parking structure, lobby, ski/bike repair space

Exterior Materials: architectural plaster (EIFS), thin brick, stone veneer, with aluminum awnings and storefront details

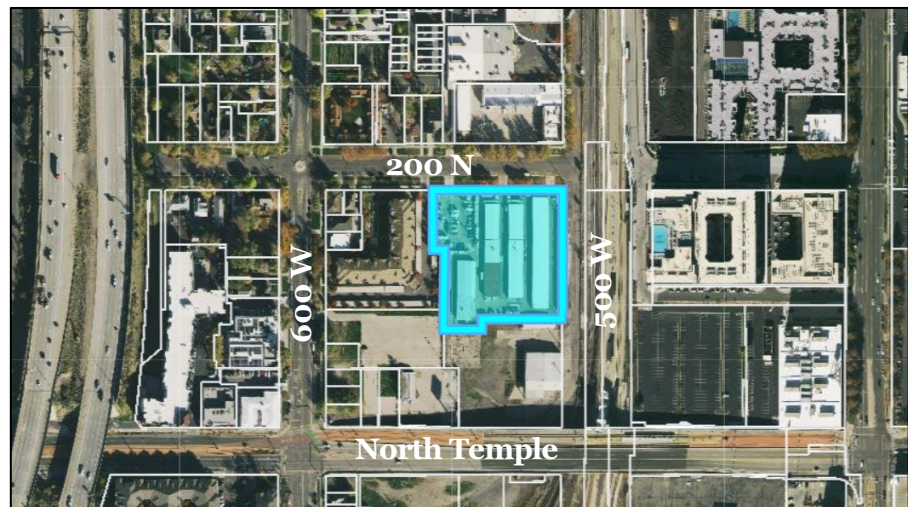
Parking: 439 stalls

Review Process & Standards: Planned Development, Design Review, TSA-UC-T zoning standards, and general zoning standards

Project Location & Neighborhood Context

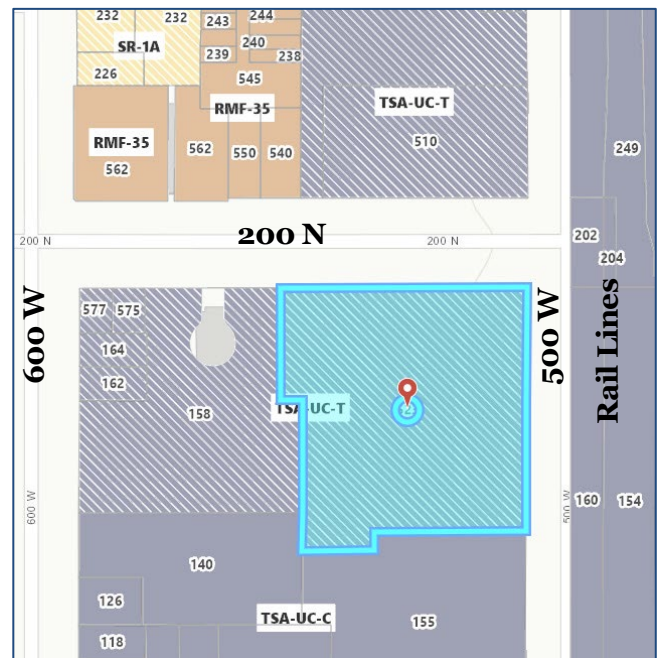
The subject property, located at the northeastern corner of 200 N 500 W, consists of a single parcel totaling 2.91 acres. It lies on the border of the Fairpark and Capitol Hill Community Council boundaries.

The property is zoned TSA-UC-T and currently contains a single-story office building, a self-storage facility, and an RV parking lot. These existing structures will be demolished to make way for the property's redevelopment.



Surrounding Zoning and Properties

- North: The properties to the north are zoned TSA-UC-T, RMF-35 (Multi-Family Residential), and SR-1 (Special Development Pattern Residential).
 - The TSA-UC-T properties feature the Missio Dei Community Church facing 200 N, with the Union Event Center directly behind it.
 - The RMF-35 properties along 200 N include two existing single-family homes and what appears to be a single-story apartment complex.
 - Two-story homes are located behind these properties, accessible from 600 West and 300 North.
- West: Abutting the property to the west is a three-story apartment complex.
- South: To the south, there is a paved vacant lot and a lumber yard, both of which are upzoned to TSA-UC-C.



Zoning Map

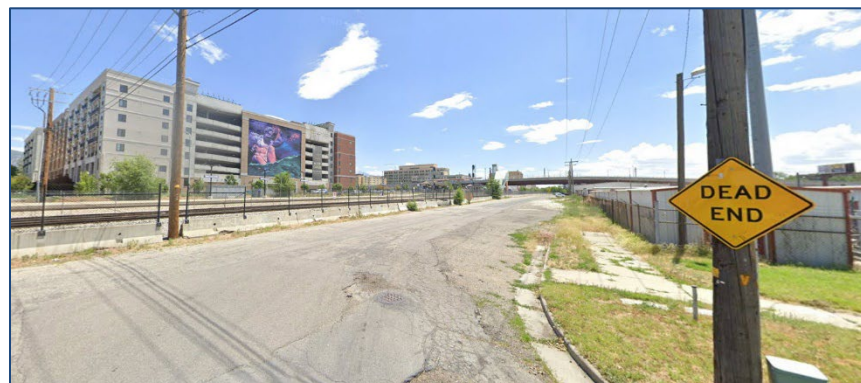
Existing Conditions and Pedestrian Access

The surrounding area currently experiences limited pedestrian activity due to 500 West being a dead-end street. Although the project site is near the heavily used North Temple FrontRunner Station, and only 100 feet from the station, there is no direct pedestrian connection to it. Pedestrians must walk around the block to 600 West and use the ramp to access the station because there are no at-grade connections. Sidewalks and bicycle lanes are also lacking in the area.



North Temple FrontRunner and TRAX Station

In terms of urban context, the development pattern on 500 West is similar to that found on the eastern side of the rail lines, in the Hardware District. The proposed design reflects this pattern, with open-air parking structures facing the rail line and all housing units oriented away from the tracks.



500 West Existing Conditions

APPROVAL PROCESS AND COMMISSION AUTHORITY

Per section 21A.55.030 of the Zoning Ordinance, the Planning Commission may approve a Planned Development as proposed or may impose conditions necessary or appropriate for the Planned Development to comply with the standards. The Planning Commission may deny an application for a Planned Development if it finds that the proposal does not meet the intent of the base zoning district (TSA-UC-T), does not meet the purpose of a Planned Development, or is not consistent with the standards and factors as set forth in section 21A.55.

Design Reviews may be approved administratively or when required, by the Planning Commission. Per section 21A.59.030 of the Zoning Ordinance, the Planning Commission shall approve a project if it finds that the proposal complies with the purpose of the zoning district and applicable Overlay District(s), the purpose of the individual design standards that are applicable to the project, and the project is compliant with the design review objectives. The Commission may also add conditions or modifications.

KEY CONSIDERATIONS

The key considerations listed below were identified through the analysis of the project:

1. Requested Zoning Modifications
2. Compliance with City Goals & Policies Identified in Adopted Plans

Consideration 1: Requested Zoning Modifications

Planned Development: The applicant is requesting Planned Development approval for the following modifications to the TSA zoning district standards:

- Request for up to 5' of additional building height from a maximum of 60' to approximately 63'9"
- Reduced front yard setback from 25' to approximately 8'4"

Design Standards Modifications: Design Review approval is required for the following modifications to the Design Standards in [chapter 21A.37](#):

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 - 500 W: Reduction in lower level durable materials from 90% to 85%, the non-durable materials is plaster (similar to EIFS) which is not permitted on the ground floor
 - 500 W: Request to use 17% plaster on the upper levels, when 10% is permitted
- Reduction of the ground floor use requirement from 90% active use to zero along 500 W. The applicant is proposing to build the parking structure facing the rail lines. 500 West is a dead-end public street.
- Reduction in ground floor glass from 60% to 15% along 500 W
- Modification to the spacing of building entrances along both 200 N and 500 W

Planned Development Requests

Additional Building Height

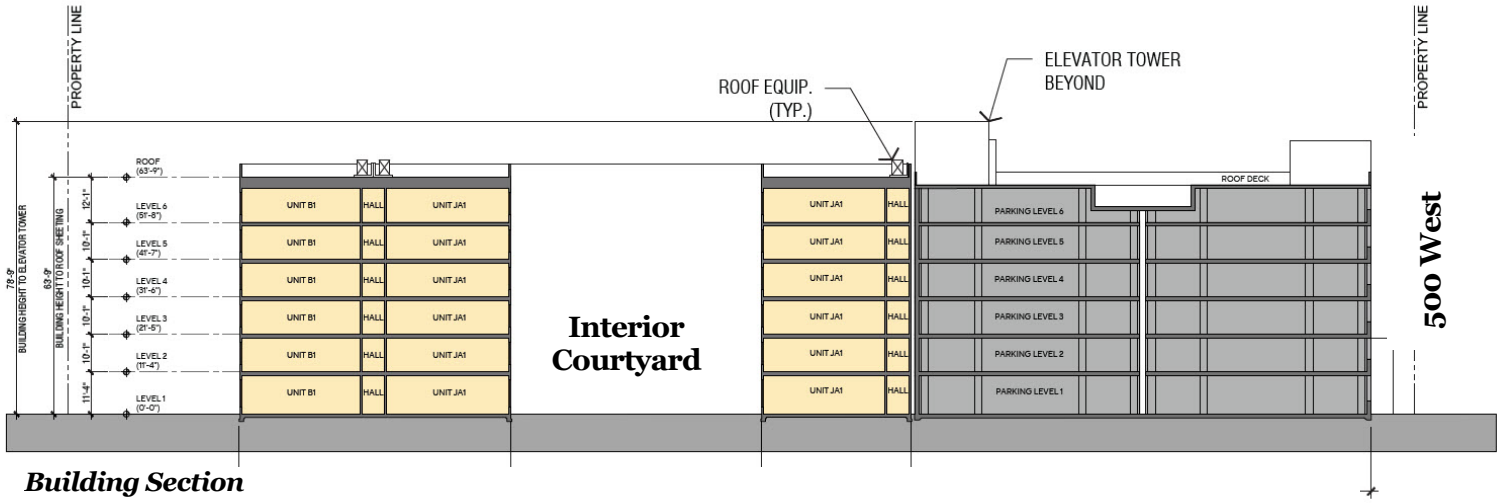
The building has a height of approximately 63'9", and up to 78'9" when including the parapet and elevator bulkheads. The TSA-UC-T zoning district allows for a maximum building height of 60 feet. Because the project is located in the TSA zoning district, the applicant was required to submit a Development Score Review. Projects that receive 125 or more points are eligible for administrative approval. The proposal received 129 number of points but still needs modifications to the TSA-UC-T zoning standards; therefore, must receive Planning Commission approval. Until recently, projects that receive 125 or more points could build an additional story above the maximum building height, but recent code amendments removed the provision. Because this provision is no longer in code, the applicant must receive Planned Development approval for the additional 3'9" of building height.

The building includes six levels of residential units. The 500 West façade does not include units due to the location of the structured parking. Levels two through six of the building are approximately 10 feet tall from floor to ceiling, with the ground floor being slightly taller at 11'4" in height. The requested additional building height allows for the floor plate for the townhomes to be slightly raised from the sidewalk, while not having to sacrifice ceiling height on the ground level of the building. The additional height, in addition to the reduced front yard setback discussed below, will enhance the pedestrian realm by creating space for each townhome to have an elevated stoop along 200 North. The stoops are representative of the single-family homes in the area and allow residents to use the space in front of each unit. Raising the floorplate also allows for the windows of the units to not be directly in front of the sidewalk, allowing for privacy and security for the residents.

Considering the development potential of surrounding properties and the overall building design, the additional 3'9" of height is not expected to have adverse impacts. The building will be of similar height to the newer development on the east side of the rail tracks.



200 N Elevation – PD request is for 3'9" of additional building height, which allows the townhomes on 200 N to be elevated from the public sidewalk, creating a sense of privacy and security



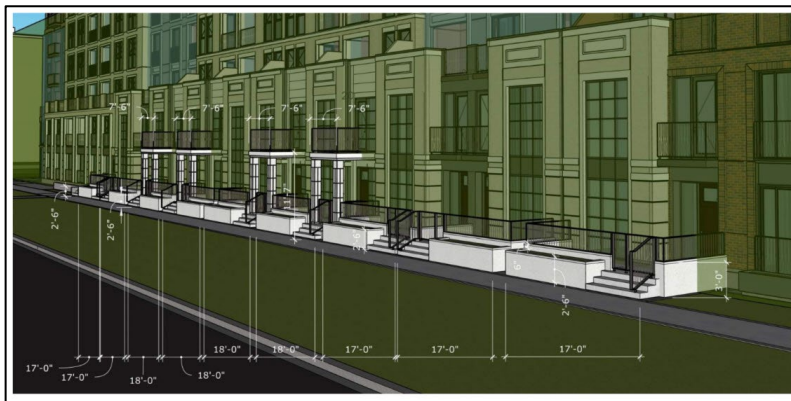
Building Section

Reduced Front Yard Setback on 200 North

A front yard setback of 25% of the lot depth up to 25' is required when a property abuts or is across the street from an RMF-35 zoning district. The application is seeking a reduction from 25' to approximately 8'4".

The townhomes are proposed to have a setback of 8'4". The ground level patios are built to the setback, which allow the ground floor of the building to be adjacent to the sidewalk, creating a pedestrian scaled, cohesive street wall where none exists at this time. To reduce the sense of height and bulk along 200 North, the main building massing is stepped back from the property line approximately 14' at the third level.

The building is not directly across the street from the RMF-35 zoned properties and the reduced setback is not anticipated to cause additional impacts to the two existing single-family homes located on those lots (identified by stars on the zoning map).



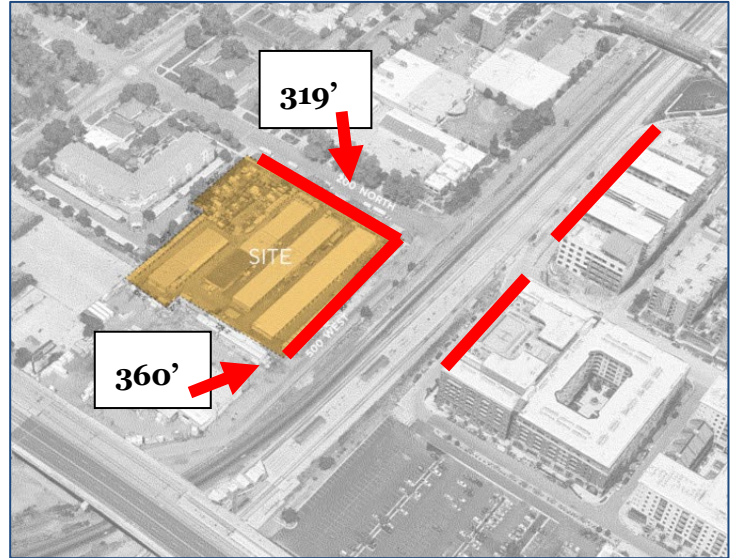
Townhomes along 200 N are placed close to the street with ground level patios



RMF-35 zoning is across the street, requiring a front yard setback

Increase of Allowable Building Length

The maximum street facing façade length in the TSA-UN-T District is 200 feet. The applicant is requesting approval for approximately 319-foot building façade along 200 North, which spans the east-west length of the property and 360 feet along 500 North, which runs parallel to the railroad tracks. The purpose of implementing a maximum building façade length is to break up large expanses of building and to create spaces which are more human scale and comfortable to the pedestrian.



200 North

The applicant is proposing a 319-foot building façade along 200 North. To provide for a fire access lane, the western end of the building has a 40-foot setback from the three-story apartment building.

The 200 North façade includes nine two-story townhomes and a residential lobby and lounge situated at the western edge of the property. To reduce the horizontal scale and avoid a monolithic appearance, the building incorporates stepbacks, varying massing, and different materials, breaking up the façade and providing some pedestrian interest and engagement.

Each townhome stands approximately 25 feet in height and is designed with a ground floor patio and an upper floor deck. The inclusion of these patios is a deliberate design choice that reflects the character of the single-family homes across the street, fostering a sense of connection between the building and the surrounding neighborhood. By introducing private outdoor spaces that open onto the street, the building contributes to the streetscape and increases "eyes on the street." This is a critical aspect of urban design, as it promotes safety and encourages a more active, engaging atmosphere.



200 North Facade

The third level of the building steps back roughly 14 feet from the top of the townhomes, creating a distinct base that adds visual interest. The townhomes are elevated from the sidewalk, with 3-foot-tall ground level planters framing each patio and delineating public and private spaces.



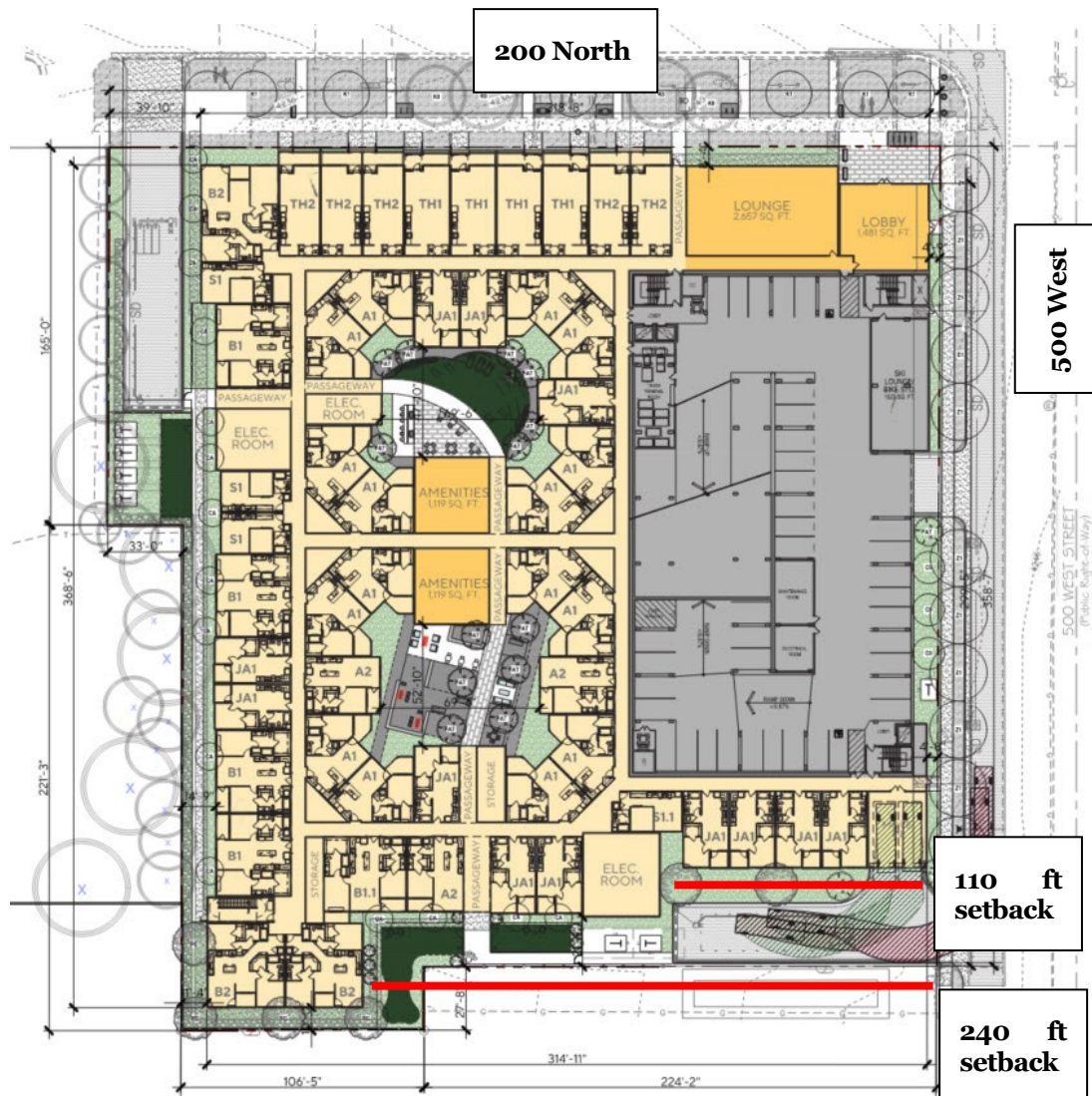
Ground floor along 200 North

These patios extend closer to the sidewalk than the rest of the building, providing a change in massing between 7-15 feet. The building also has a change in building materials with each end of the building clad in brick, while the middle incorporates either the townhomes or the lounge, which has storefront window system. This combination of massing, stepbacks, and material transitions helps reduce the visual bulk of the building and creates a more engaging design that enhances the pedestrian experience.

500 West

The applicant is requesting an increase in the allowable building length from 200 feet to approximately 360 feet along the 500 West façade. Of the 360 feet, approximately 60 feet is setback between 110 and 240 feet from the front line of the building. The developments loading/move in space and a small dog park are within the setback area.

500 West is a dead-end public street with minimal pedestrian traffic. Pedestrians heading south must walk to 600 West, as no continuous sidewalk or bicycle network exists due to the rail lines and the North Temple



bridge blocking further pedestrian routes. Residential amenity spaces are clustered on the north side of the property. Because the street is public, all new development must adhere to the design standards in Chapter 21A.37 that address street facing facades. The proposed building length is similar to developments across the rail to the east. Due to its location and lack of walkability, the additional building length is not anticipated to have a negative impact on the neighborhood.

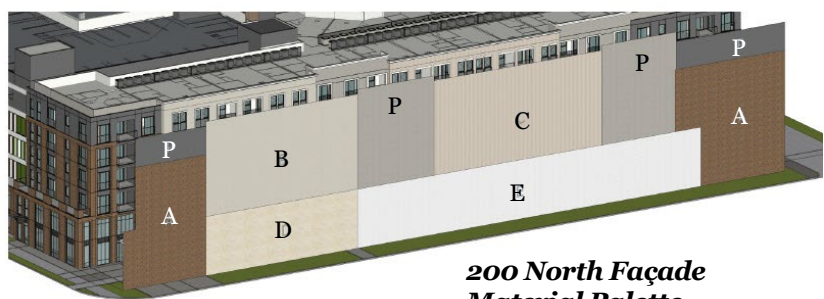
Reduction of Durable Building Materials

The TSA zoning district requires 90% durable materials on the ground floor and 60% on the upper levels. The TSA district also does not permit the use of EIFS on the ground floor and only allows for 10% on the upper floors.

200 North – Upper Level Modifications

The applicant is requesting to use 24% architectural plaster, comparable to EIFS, on the upper levels on the 200 North facade. The overall building façade is clad in 80% durable materials, while the upper levels are clad in approximately 76% durable materials. The ground level provides 100% durable materials. Both corners of the building and the two interior building facades include brick, recessed windows, and architectural detailing intended to offset the reduction in the upper level’s percentage of durable materials.

Building Materials – Upper Levels			
	Material	Area	% of Total
A	Brick/Fiber Cement	3,394 SF	20%
B	Brick/Fiber Cement	2,969 SF	17.5%
C	Brick/Fiber Cement	3,279 SF	19.3%
D	Tile/Fiber Cement	1,849 SF	5.8%
E	Tile/Fiber Cement	2,247 SF	13.2%
P	Plaster (EIFS)	4,130 SF	24.3%
Total Façade Area		20,746 SF	100%



200 North Façade Material Palette

500 West – Ground Floor & Upper Level Modifications

The applicant proposes to use concrete/masonry block as the primary material for the 500 West façade. The southern portion of the façade, where the loading area is located, will consist of mostly architectural plaster, which is similar to stucco/EIFS. The TSA zoning district does not permit the use of stucco/EIFS on the ground floor of buildings and limits its usage to 10% for the upper levels.

The ground floor of the building is clad in just under 15% plaster, with the upper levels clad in 17%. The portion on the south, where the move-in and fire lane are, is plaster. There is also a small portion of the northern upper floor that consists of the plaster material.

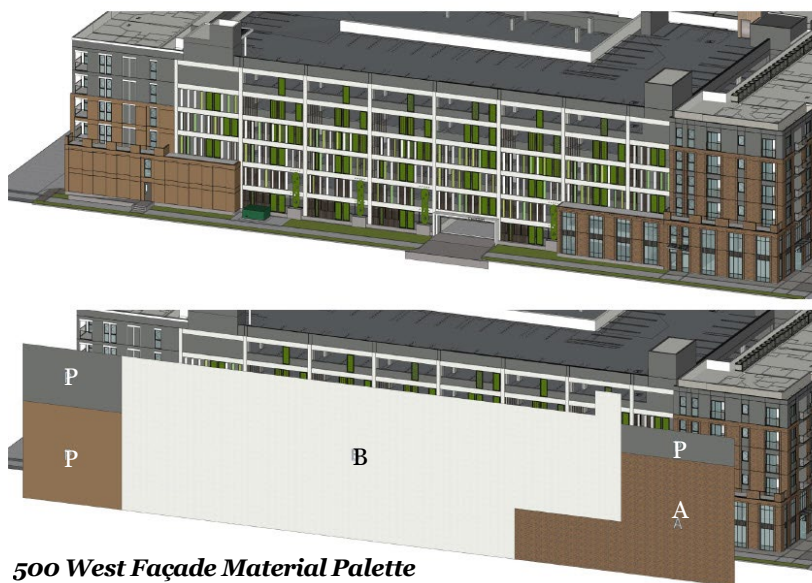
While architectural plaster is not considered a durable material, its use on the 500 West façade is offset by the placement of brick on the northern end, providing a higher-quality finish along the more visible sections of the building when residents will be entering the lobby.

To help soften the appearance of the parking structure, the applicant is also proposing to plant additional trees within the building setback area and add live plant material to the garage screening.

500 West Building Materials					
		Ground Floor		Upper Levels	
	Material	Area	% of Total	Area	% of Total
A	Brick/Fiber Cement	1,150 SF	30.91%	2,503 SF	14.4%
B	Concrete/Masonry Block (partially screened)	2,055 SF	55.23%	11,813 SF	68.1%
P	Plaster (EIFS)	516 SF	14.87%	3,023 SF	17.4%
Total Façade Area		17,339 SF	100%	3,721 SF	100%

Reduction in the Ground Floor Use Requirement along 500 West

The applicant has requested to reduce the ground floor use requirement along 500 West from 90% active uses to effectively zero. The majority of the building's ground floor is dedicated to parking, but there are some features that still encourage pedestrian activity. The ground floor bike/ski repair area doesn't count as an "active use" because it isn't open to the general public, but its location at the northeastern corner of the building next to the double-height lobby does help bring some activity to that corner of the building.



500 West Façade Material Palette

The decision to place the parking garage along 500 West, rather than an interior lot line, is a deliberate design choice aimed at ensuring the residential areas remain quieter and more comfortable. By orienting the garage towards the rail lines, the design effectively buffers the residential units from the noise, and physical vibrations caused by the nearby rail and FrontRunner tracks. This approach helps maintain a quieter living environment for residents, free from the disruptive effects of rail activity.

Furthermore, this design strategy is not unique to this project. It reflects a broader development pattern observed in the Warehouse District on the eastern side of the track where many newer developments also place parking structures along the rail lines, with residential units situated away from the tracks. In this context, the decision to orient the parking structure in this way aligns with the overall design approach of the area, enhancing both the functionality of the building and livability for residents of both the project and abutting properties

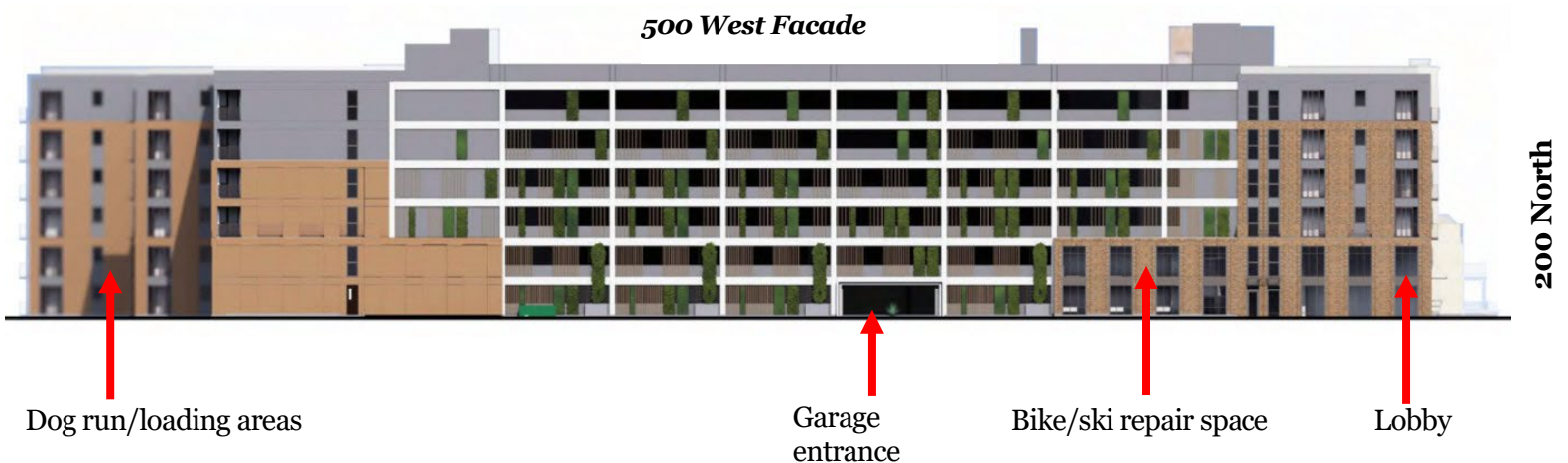


Bike/ski repair space Lobby Lounge

Reduction in the Ground Floor Glass Requirement along 500 West

The applicant is seeking a reduction in ground-floor glass coverage along the 500 West facade, requesting 15% glass instead of the required 60% in the TSA-UC-T zoning district. This reduction is due to the primary use being a parking structure, which occupies approximately half of the ground floor length along 500 West.

Given that 500 West is unlikely to become a through-street due to the rail lines, and the area is not expected to support retail uses, the requirement for ground-floor transparency is not as relevant. While the standard for glass coverage is not met, the design incorporates several mitigating factors. The parking structure is set back from the eastern lot line, allowing for landscaping to soften the façade. Additionally, the upper levels feature screening panels and a living plant wall, enhancing the aesthetic quality of the structure. The bike/ski repair area on the northeastern corner includes floor-to-ceiling windows, which wrap around to the corner lobby, creating visual interest and contributing to pedestrian activity.



Modification to Building Entrances

200 North

The TSA zone requires one operable building entrance for every 40 feet of building length. The 200 North building façade is 319 feet long, requiring eight entrances. Each of the nine townhomes has a distinctive entry feature leading to 200 North. The ground floor lounge and lobby span approximately 105 feet of the ground floor façade and provide one entrance into the lobby from 200 North. The lounge area between the lobby and townhomes does not have an exterior entrance. Of the 10 entrances, the one leading to the lobby exceeds the 40-foot spacing requirement.

500 West

The applicant is requesting to reduce the required amount of ground floor entrances from eight to effectively one. There are two stairwell exits on the 500 West façade and the only functional entrance is to the lobby on the north end of the building. The remainder of the ground floor is dedicated to the parking structure, which can be accessed via one vehicular entry point or from the interior of the building.

Consideration 2: Compliance with City Goals & Policies Identified in Adopted Plans

[Plan Salt Lake \(2015\)](#)

Plan Salt Lake is a comprehensive, citywide vision designed to create a framework for managing the City's growth. The plan establishes a series of citywide policies that address crucial aspects like placemaking, density management, the integration of various land uses, and enhanced connectivity. Each Guiding Principle is linked to a range of initiatives that work to carry out the shared vision: a city that is inclusive, resilient, and economically prosperous. Applicable initiatives from the plan are below.

Neighborhoods: Vibrant neighborhoods are fundamental to the health and vitality of Salt Lake residents and visitors. Ones neighborhoods should enhance their quality of life by ensuring access to basic goods, services, and amenities, and providing opportunities for social interaction. Community gathering spaces may vary in size and use, but should be designed to be safe, distinctive, and engaging.

- *Maintain neighborhood stability and character.*
- *Support neighborhoods and districts in carrying out the City's collective Vision.*
- *Create a safe and convenient place for people to carry out their daily lives.*
- *Support neighborhood identity and diversity.*
- *Support policies that providess people a choice to stay in their home and neighborhood as they grow older and household demographics change.*
- *Promote accessible neighborhood services and amenities, including parks, natural lands, and schools.*
- *Provide opportunities for and promotion of social interaction.*

Growth: Responsible growth should minimize developments impact on the natural environment by focusing on transit-oriented development, concentrating density in areas already served by existing infrastructure, and promoting best practices in building and urban design.

- *Locate new development in areas with existing infrastructure and amenities, such as transit and transportation corridors.*
- *Encourage a mix of land uses.*

- *Promote infill and redevelopment of underutilized land.*
- *Accommodate and promote an increase in the City's population.*
- *Provide access to opportunities for a healthy lifestyle (including parks, trails, recreation, and healthy food).*

Housing: As the City's population continues to grow, providing residents with access to a wide variety of housing types that are not only attainable, but affordable, is a top priority. This involves preserving the existing housing stock, increasing the number of new family-sized housing units, and strategic placement of new development transit and neighborhood nodes. Ongoing efforts should focus on reducing barriers to obtaining housing, enabling residents to age in place, and taking proactive measures to address displacement.

- *Increase the number of medium density housing types and options.*
- *Encourage housing options that accommodate aging in place.*
- *Direct new growth toward areas with existing infrastructure and services that have the potential to be people-oriented.*
- *Enable moderate density increases within existing neighborhoods where appropriate.*

Transportation & Mobility: The establishment and enhancement of the City's transportation networks is pivotal to advancing a vibrant and connected community, where all residents live within ¼ mile to a transit option. The continued investment in a variety of transportation modes, from public transit and cycling infrastructure to widened sidewalks and the creation of midblock walkways, will create a more accessible and sustainable urban environment.

- *Create a complete circulation network and ensure convenient equitable access to a variety of transportation options by:*
 - *Having a public transit stop within ¼ mile of all residents.*
 - *Expanding pedestrian and bicycle networks and facilities in all areas of the City.*
- *Prioritize connecting residents to neighborhood, community, regional, and recreation nodes by improved routes for walking, biking, and transit.*
- *Incorporate pedestrian oriented elements, including street trees, pedestrian scale lighting, signage, and embedded art, into our rights-of-way and transportation networks.*

Air Quality: Air quality has been identified as one of the most important issues facing residents of Salt Lake City. Initiatives should be directed to reduce greenhouse gas emissions and create opportunities for reduced individual and citywide energy consumption.

- *Protect and enhance the existing urban forest, including street trees.*

Beautiful City: Salt Lake City's unique urban form, with its large blocks and natural surroundings, create aesthetic and recreational opportunities. New development should enhance the quality of life and reflect the City's commitment to creating people-focused development.

- *Support and encourage architecture, development, and infrastructure that:*
 - *Is people-focused;*
 - *Responds to its surrounding context and enhances the public realm;*
 - *Reflects our diverse cultural, ethnic, and religious heritage; and*
 - *Is sustainable, using high quality materials and building standards.*
- *Promote and expand the City's street lighting program throughout the City.*
- *Reinforce and preserve neighborhood and district character and a strong sense of place.*
- *Protect, maintain, and expand the City's urban forest, including the provision of adequate space and infrastructure for street trees to thrive.*

- *Create opportunities to connect with nature in urban areas.*

Staff Discussion: The proposed development helps fulfill numerous objectives of Plan Salt Lake within 8/13 areas of focus. The proposal seeks to establish neighborhood character and identity with dedicated studio spaces for artists along the ground floor and facades which include art panels along the street frontage and midblock walkway. The focus on art will not only beautify the space but will create a sense of place within the neighborhood.

The development will replace underutilized land within the TSA-UC-T zone and provide medium density housing in support of the TSA zoning district. The building will be in scale with nearby existing multifamily developments and will include a diverse mix of unit types ranging from studio to 2-bedroom. The wide array of unit types provided will allow for aging in place and accommodate people in different stages of life.

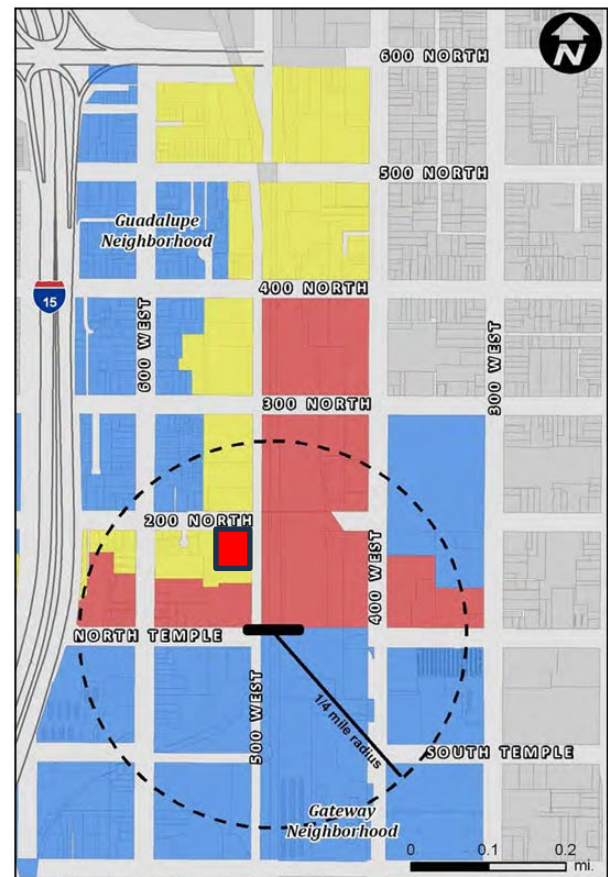
The proposal includes upgrades to utilities and will provide landscaped park strips with street trees, streetlights, sidewalks, and curb and gutters. The improvements will make the area more inviting and livable. Landscaping and provided trees within the park strips will have a positive impact to the air quality.

The location of the proposed development is well serviced by transit and is less than 2 blocks to the nearest TRAX and FrontRunner station and is in the vicinity of multiple bus routes. The development will provide a convenient place for residents to live, access jobs, and pursue entertainment opportunities.

[North Temple Boulevard Plan \(2010\)](#)

The purpose of the TSA zoning district is to, “*provide an environment for efficient and attractive transit and pedestrian oriented commercial, residential and mixed use development around transit stations. Redevelopment, infill development and increased development on underutilized parcels should include uses that allow them to function as part of a walkable, mixed use district. Existing uses that are complementary to the district, and economically and physically viable, should be integrated into the form and function of a compact, mixed use pedestrian oriented neighborhood.*”

The Viaduct Transfer Station Area provides a mix of high-density housing and commercial uses, with regional draws that attract people for entertainment, shopping, or work. The Transition area, where the subject property is located, contains a slightly less intense mix of uses. Housing is typically 3-4 story multifamily with a lot of existing single-family homes. The subject property (identified with a red square on the map) is within a Transitional Area (yellow on the map) which are areas deemed appropriate for mixed use and less intensive transit-oriented zoning.



The North Temple station is an asset to this neighborhood, but it creates connectivity issues as there is no direct access to the station west of the railroad tracks. NTBP recommends creating better connections and installing infrastructure along 500 West. While the project does not create a new connection, it does meet the policy statements in the plan. Locating a mid-rise building with 350 new units near the station supports the use of public transit and the Core areas where commercial and retail uses are heavy.

DISCUSSION

The subject property is zoned TSA-UC-T. An Urban Center Station contains the highest relative intensity level and mix of uses. The type of station area supports downtown Salt Lake City, but not compete with it in terms of building scale and use. The purpose of the Transition area is to *“provide areas for a moderate level of land development intensity that incorporates the principles of sustainable transit oriented development. The transition area is intended to provide an important support base to the core area and transit ridership as well as buffer surrounding neighborhoods from the intensity of the core area. These areas reinforce the viability of the core area and provide opportunities for a range of housing types at different densities.”*

The project upholds the purpose of the TSA-UC-T district by providing new residential units near a transit station and by activating the 200 North sidewalk by placing ground floor units along the sidewalk facing other residential uses and positioning the parking structure so it faces the rail lines. The placement of the parking structure provides a buffer between the noise of the rail and the living units.

Approving the Design Review and Planned Development requests will result in a better product than what could be built under the base TSA-UC-T zoning regulations alone. The increased height allows for the nine townhomes to have stoops and raised patios, providing the residents with privacy and delineating private from public space. The reduced front yard setback is a reasonable accommodation considering the context. The requirement to have a 25-foot setback when across the street from the RMF-35 zone is to allow for developments to step down in intensity. The zoning directly across the street is TSA-UC-T, with a slight overlap with the RMF-35 zone. The reduction will not have additional impacts on the two single-family homes. Overall, the additional residential density near a transit station is broadly supported by both Plan Salt Lake and the North Temple Boulevard Plan.

While the applicant is asking for modifications to the TSA-UC-T standards and the design standards in Chapter 21A.37, the overall development will replace a storage facility with housing and enhance the pedestrian experience along 200 North through architectural detailing, landscaping, and new right-of-way improvements. The proposal meets the purpose of the Planned Development, which is to enable more efficient use of the land and results in a more enhanced product. Due to the nature of the site and the existing development across the rail lines, Planning staff believe the requested modifications are reasonable and won't negatively impact the neighborhood nor the overall goals of the North Temple Boulevard Plan or Plan Salt Lake.

STAFF RECOMMENDATION

Planning staff is recommending approval of the Design Review (PLNPCM2024-01232) and Planned Development (PLNPCM2025-00021) petitions that will facilitate the redevelopment of 523 W 200 N. The District North proposal meets the standards of review for both processes. The standards below are conditions of approval and will be met during the building permit process.

- Planned Development Standard C.5: Complies with Conditions. Staff is recommending that final details of the building lighting be delegated to staff for approval.
- Design Standard J. Complies with Conditions. Staff is recommending that final details of the building signage be delegated to staff for approval.
- Design Standard K: Complies with Conditions. Staff is recommending that final details of the building lighting be delegated to staff for approval.

NEXT STEPS

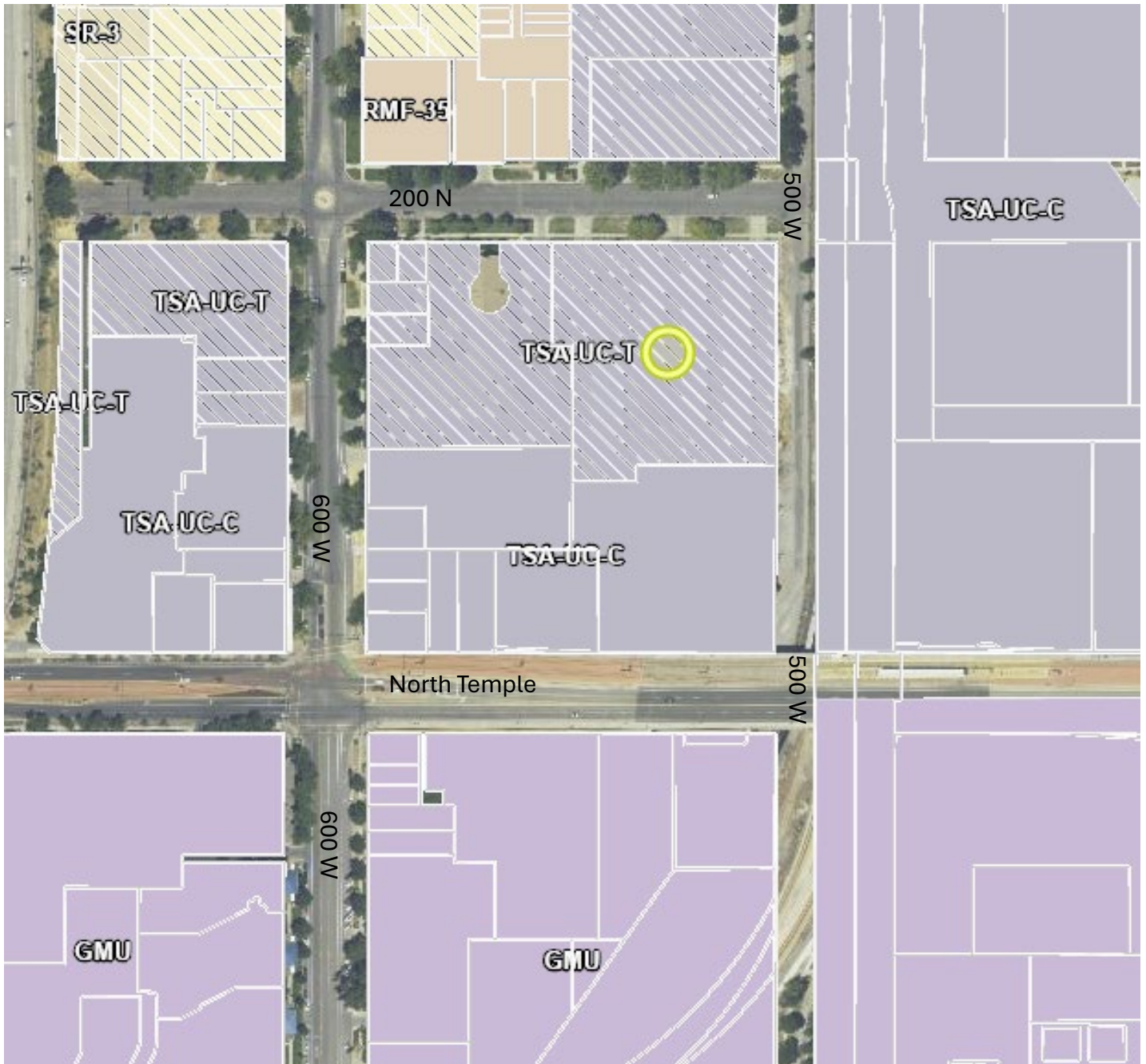
Approval of the Requests

If the petitions are approved by the Planning Commission, the applicant will need to comply with the conditions of approval, including any of the conditions required by City departments and the Planning Commission. Unless specified in the zoning ordinance as a minor modification, any modification to the development plan must be reviewed and approved by the Planning Commission.

Denial of the Requests

If the petitions are denied, the applicant would be required to redesign the building to meet the standards of the TSA-UC-T zoning district and the applicable Design Standards in Chapter 21A.37. The applicant would not need to comply with the more stringent design standards required through the Design Review process and would not need to meet the objectives or standards for a Planned Development.

ATTACHMENT A: Vicinity & Zoning Maps



ATTACHMENT B: Applicant Materials

DISTRICT NORTH

DESIGN REVIEW

SALT LAKE CITY, UTAH





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 RESIDENTIAL LIFESTYLE A5
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 CONCEPTUAL 3D RENDERING A7
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LANDSCAPE

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 PRELIMINARY PLANTING L4
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 ROOF DECK ENLARGEMENT L6
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CIVIL

SITE PLAN C1
 GRADING AND DRAINAGE PLAN C2
 UTILITY PLAN C3

PROJECT TEAM:

APPLICANT/OWNER:

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RESIDENTIAL ARCHITECT:

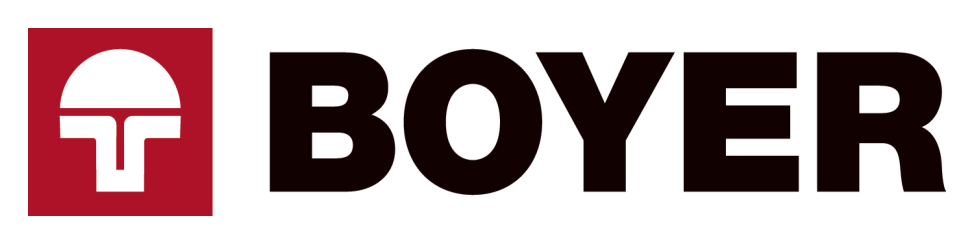
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DISTRICT NORTH

SALT LAKE CITY, UT

SHEET INDEX



A1



Situated at 523 W and 200 N, just north of downtown Salt Lake City, **DISTRICT NORTH** offers a great opportunity for a highly connected urban development.

With its proximity to I-15 and within walking distance of North Temple Station, **DISTRICT NORTH** is perfectly positioned for transit-oriented development, offering excellent connectivity for commuters, residents, and young professionals. As Salt Lake City continues to expand, the northern areas near downtown are poised for significant development, with city growth strategies focusing on higher-density development that enhance walkability and public transit access. The site's close proximity to downtown ensures convenient access to the city's business district, cultural attractions, and entertainment options, making it appealing for both commuters and residents who seek a quieter yet well-connected urban environment with industrial influences.

The surrounding neighborhood is a dynamic mix of industrial, commercial, and residential uses. Industrial and warehouse spaces to the west highlight the area's historical role in Salt Lake City's industrial development, while to the east and southeast, ongoing urban revitalization is fostering new residential growth. This location, at the intersection of various land uses, provides a unique opportunity for a development that unites the area's industrial past with the modern urban lifestyle and commercial vibrancy, making it an ideal space for forward-thinking development.

Our objective for **DISTRICT NORTH** is to stimulate the transformation of this neighborhood from its current underdeveloped state, into a dynamic and upscale urban environment aligned with the City's vision for the future of North Temple. The proposed 6-story, 350

home development draws inspiration from classical architecture, seamlessly integrating traditional principles with modern aesthetics. This fusion results in a structure that embodies timeless elegance while showcasing innovative design. By marrying classical elements, which emphasize order, proportion, and permanence, with modern characteristics that prioritize simplicity, transparency, and adaptability, we pay homage to historical traditions while addressing contemporary needs for living and working spaces.

The design draws from classical principles of symmetry and proportion, blending tradition with modern functionality. Along 200 N, expansive floor-to-ceiling windows connect interior and exterior spaces, contrasting with the solid classical materials to balance lightness and structure.

Inside, open floor plans provide flexible, adaptable spaces for varied uses. The parking structure is embraced as a design feature, with its grid breaking down the facade's scale. Shading baguettes and vegetation panels are integrated into the grid, extending greenery from the landscape up the first two levels, creating a seamless transition with the natural surroundings.

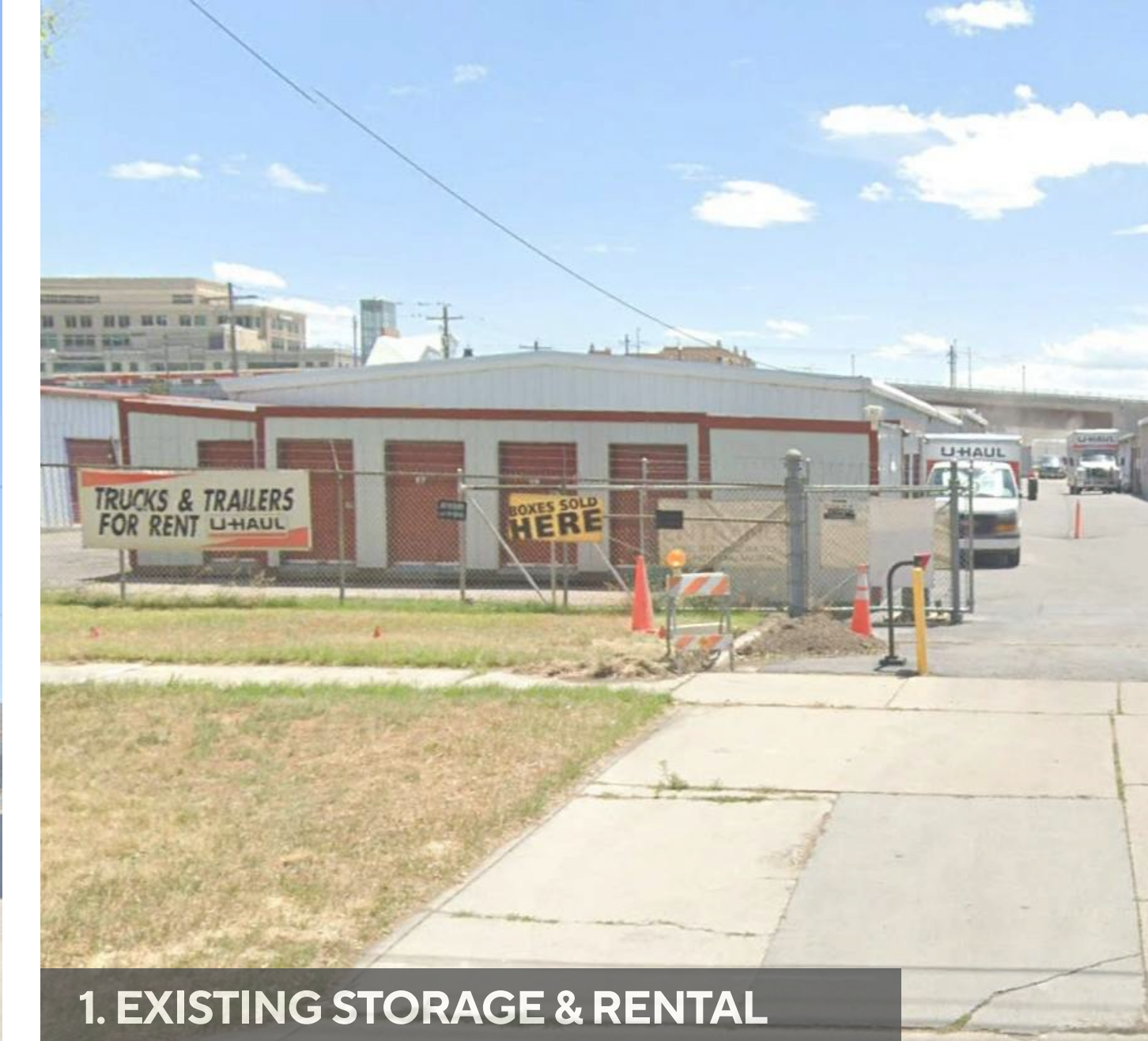
DISTRICT NORTH is a classically inspired modern architecture development that aims to deliver a visually captivating, and enduring aesthetic that enhances the overall character of the neighborhood while contributing positively to the urban fabric of Salt Lake City.

SALT LAKE CITY DEVELOPMENT STANDARDS			
Subject	Code	Current Zoning	Provided
General Plan/ Zoning	Section 21A.26.078	TSA-UC-T	TSA-UC-T
Minimum lot area		2,500 sf	2.91 AC
Minimum lot width		40'	360'
Setback Standards		Minimum : None At least 50% of the street facing building facade shall be within 5' of the front or corner side property line.	At least 50 % building façade along 200 N and 500 W is set within 5' of the property line.
Max Building Height		Minimum 25' and Maximum 60'	The Building Height is 63'-9" to the roof sheeting and is 78'-9" to the elevator tower.
Open Space		1 sf for every 10 sf of land area included in the development, up to 2,500 sf for transition areas.	The project provides over 19,000 sf of open space between courtyards and roof deck that are accessible to all residents.
Circulation and Connectivity		Any new development shall provide a midblock walkway if a midblock walkway on the subject property has been identified in a master plan that has been adopted by the city	NA
Design Standards		EIFS and Stucco Limitation: Not allowed on the ground floor of street facing building facades. And allowed for up to 10% of the upper level street facing facades.	Along 200 North, Only 5 % of Stucco is used on the ground floor and 19% on the upper level. Along 500 West, we are using 69 % of Stucco on the ground floor and 87% on the upper level. Requesting modification for this requirement.
		Front and Corner Side Yard Design Requirements: (1) In yards greater than 10' in depth, one shade tree shall be planted for every 30' of street frontage. (2) At least 30% of the front or corner side yards shall be covered in live plant material if yards includes patios or private yards for ground floor residential uses that cover at least 50% of the provided front or corner side yard. (3) At least 30% of the front or corner side yard shall be occupied by outdoor dining areas, patios, outdoor public space, or private yards for ground floor residential uses. (4) Driveways necessary for vehicle access to the site are allowed regardless of compliance with the minimum percentages required by this subsection.	1- NA.Front and corner side yard are below 10'. 2- 30 % of the front or corner yard will be covered in live plant material. 3- oFront Yard = 3,267 sf oNon Applicable Vehicular Drive = 203 sf Total Front yard Open space : 3,064 -Applicable hardscape = 1,948 (63.58%) oPrivate Patios/Walks – 1,230 oEnhanced Paving – 718 -Live Vegetation= 1,116 (36.42%) oGround Landscape – 936 oRaised Planters – 180 4- Driveway to the parking structure is proposed along 500 W.
		Entry Feature Requirements: All required building entries shall include at least one of the following features: Entry Feature Requirements: All required building entries shall include at least one of the following features: (1) An awning or canopy over the entrance that extends a minimum 5' from the street facing building facade; (2) A recessed entrance that is recessed at least 5' from the street facing facade; (3) A covered porch that is at least 5' in depth and at least 40 sf; (4) A stoop that is at least 2' above sidewalk level and that includes an awning or canopy that extends at least 3' from the street facing building facade.	All required building entries will comply and include at least one of the entry feature requirements.

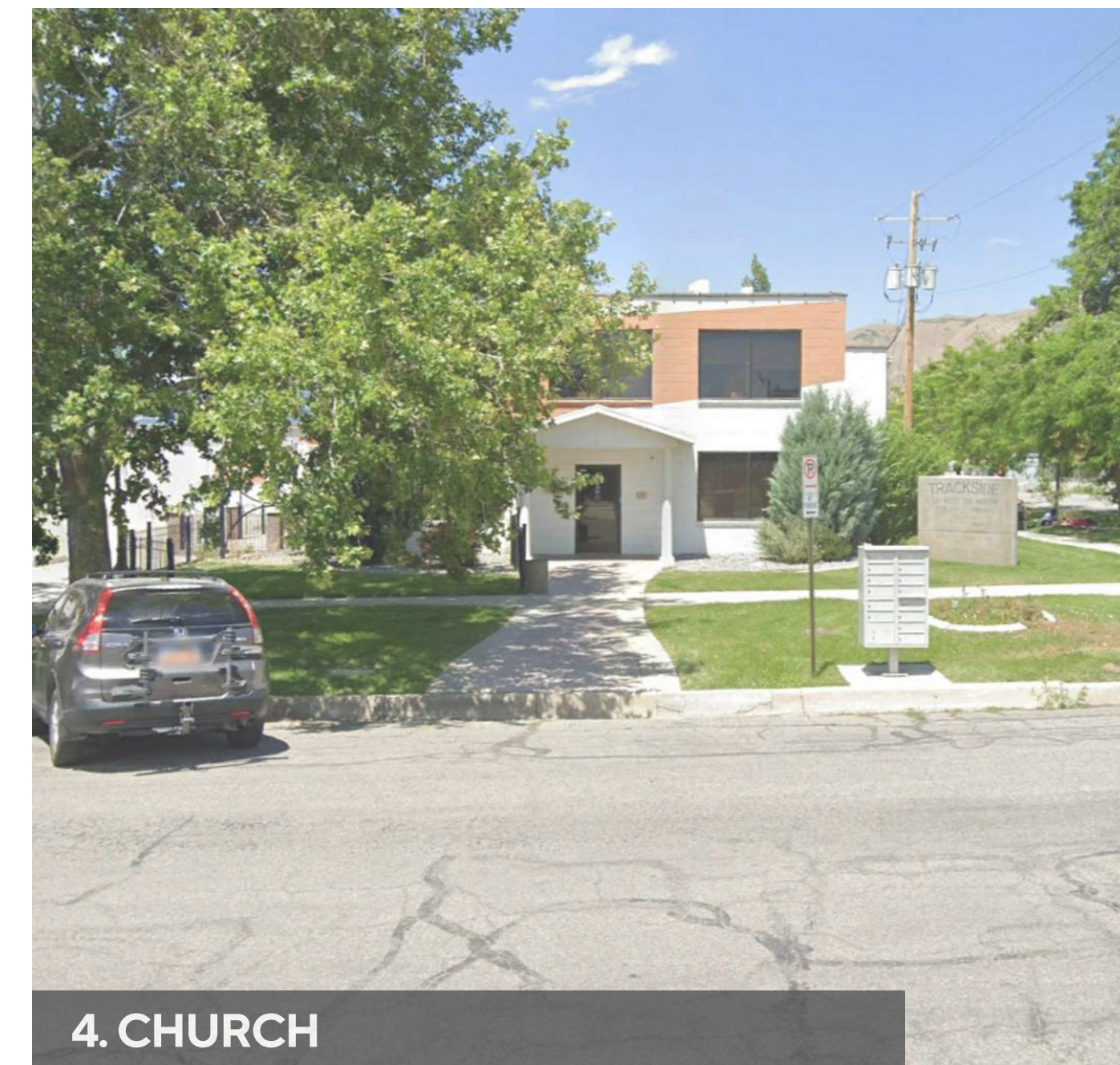
SALT LAKE CITY DEVELOPMENT STANDARDS				
Subject	Code	Current Zoning		Provided
Parking	Section 21A.44 & Salt Lake City Off-Street Parking Standards Manual	Studio	no minimum - 2 maximum	1
		1 Bedroom	0.5 minimum - 2 maximum	1
		2+ Bedrooms	1 minimum - 3 maximum	2
		Dimensions	8'-9"x17'-6" with 23'-4" drive aisle/ 9'-0"x17'-6" with 22'-7" drive aisle	8'-6"x17'-6" with 24'-1" drive aisle
		Parking adjacent to wall/column, where door opening is impacted	Stalls that are located adjacent to a wall/column need to be 1' wider	9'-6"x17'-6"
		Minimum Accessible Spaces Required	Provide 1 per 50 parking space if the provided parking spaces are 101 to 500.	Complies. Required: 11 stalls Provided: 11 Stalls
		Driveway Width	Minimum 12' single lane and 24' for two-lane. Maximum 30'	Complies. 24'
		Bicycle	Residential Uses: 1 per 3 units	Complies. Required: 117 spaces if open and 58 spaces if enclosed. Provided: 58 spaces The Bicycle parking will be enclosed.
			Each 1 bicycle parking space that is within a secure/enclosed bicycle parking facility may be used to satisfy the requirement of two (2) required bicycle parking spaces.	
			Minimum 6' x 2' with 6" gap between the stalls	
Electric Vehicle Parking	At least 1 parking space shall be provided for every 25 parking spaces provided. In addition, 20 % of the stalls provided shall be EV READY	Complies. Required: 104 stalls Provided: 104 stalls		



2. MULTI-FAMILY APARTMENTS



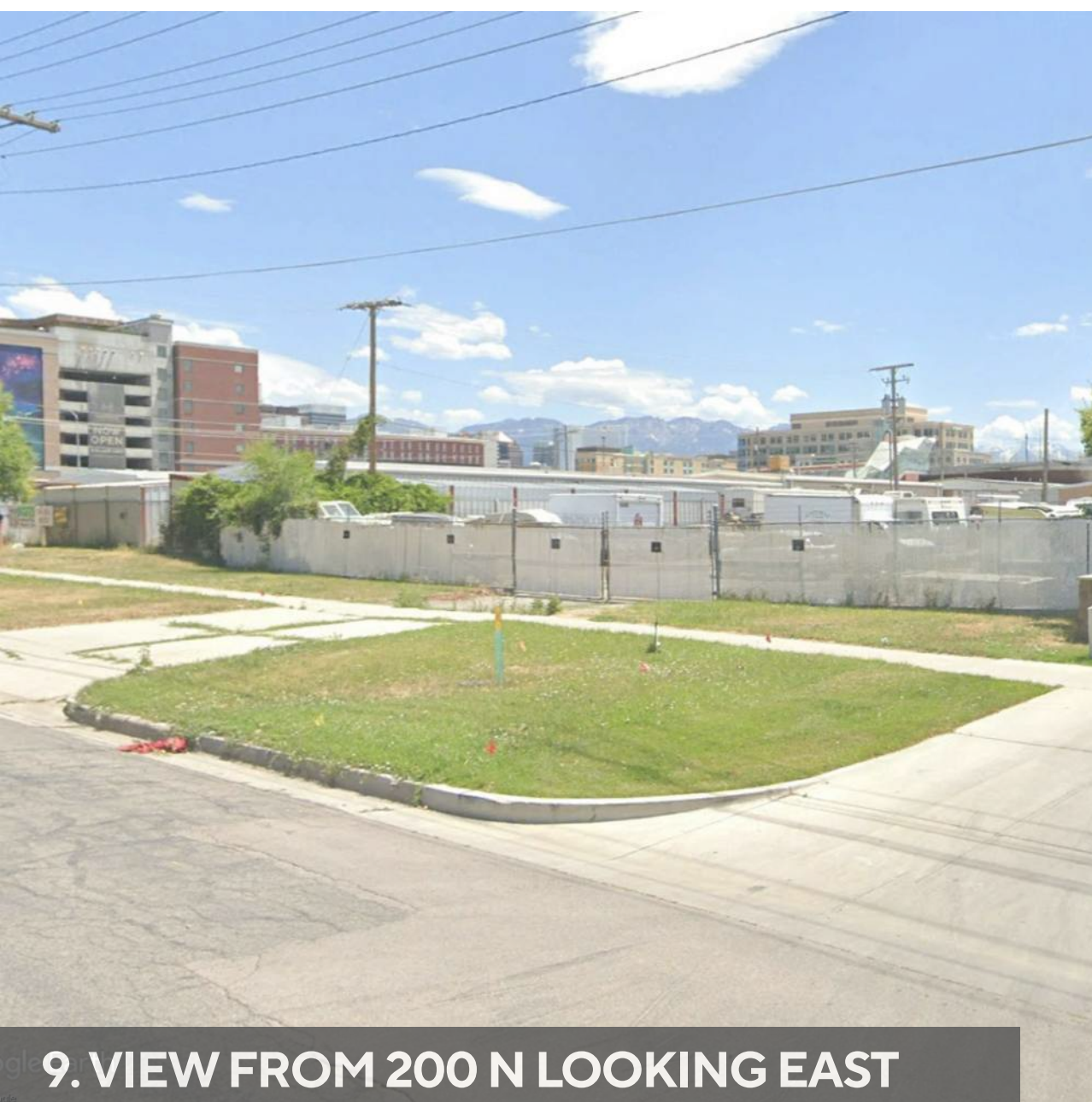
1. EXISTING STORAGE & RENTAL



4. CHURCH



3. MULTI-FAMILY APARTMENTS



9. VIEW FROM 200 N LOOKING EAST



8. VIEW FROM 500 N LOOKING WEST



7. VIEW FROM 500 N LOOKING SOUTH



6. MULTI-FAMILY APARTMENTS



5. MULTI-FAMILY APARTMENTS



DISTRICT NORTH

SALT LAKE CITY, UT

PLNPMC2024-01232
PLNPMC2025-00021

RESIDENTIAL LIFESTYLE

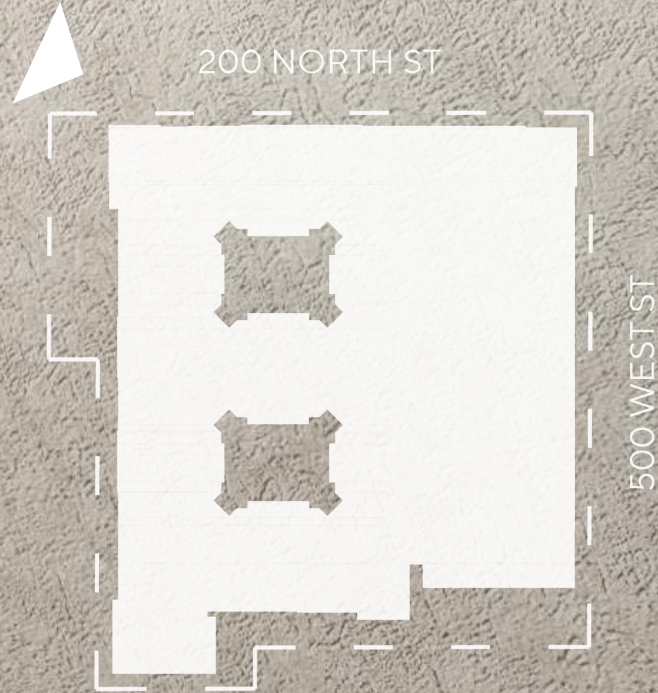


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JOB NO. 2024-0651
DATE 03-06-2025
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BUILDING SUMMARY								
UNIT TYPE	LEVEL 1	LEVEL 2	LEVEL 3	LEVEL 4	LEVEL 5	LEVEL 6	%	TOTAL
S1	4	4	5	5	5	5	8.0%	28
STUDIOS	4	4	5	5	5	5	8.0%	28
JA1	12	15	23	23	23	23	34.0%	119
A1	15	14	14	14	14	14	24.3%	85
A2	4	4	4	4	4	4	6.9%	24
TH1	4						1.1%	4
TH2	5						1.4%	5
1 BEDS	40	33	41	41	41	41	67.7%	237
B1	5	9	11	11	11	11	16.6%	58
B2	3	4	5	5	5	5	7.7%	27
2 BEDS	8	13	16	16	16	16	24.3%	85
SUBTOTAL	52	50	62	62	62	62	100%	350
TOTAL	350							

UNIT SUMMARY				
UNIT TYPE	UNIT NET AREA (SQ. FT.)	NUMBER OF UNITS	TOTAL NET AREA (SQ. FT.)	TOTAL UNIT %
S1	541	28	15,148	8.0%
JA1	615	119	73,185	67.7%
A1	750	85	63,750	
A2	759	24	18,216	
TH1		4	0	
TH2		5	0	
B1	1,133	58	65,714	24.3%
B2	1,167	27	31,509	
TOTAL	764 AVERAGE	350	267,522	100%

PRIVATE OPEN SPACE				
UNIT TYPE	BALCONY NET AREA (SQ. FT.)	NUMBER OF UNITS	BALCONY DIMENSIONS	TOTAL NET AREA (SQ. FT.)
S1	0	28	6'-0" X 10'-6"	0
JA1	54	119	6'-0" X 10'-6"	6,426
A1	70	85	6'-0" X 10'-6"	5,950
A2	59	24	6'-0" X 10'-6"	1,416
B1	59	58	6'-0" X 10'-6"	3,422
TH2	52	5	6'-0" X 10'-6"	260
TOTAL	55	319		17,474

VEHICLE PARKING SUMMARY							
RESIDENTIAL STALLS REQUIRED							
UNIT TYPE	NUMBER OF UNITS	RATIO	TOTAL STALLS				
STUDIOS	28	1.00	28				
1 BEDROOM	237	1.00	237				
2 BEDROOM	85	2.00	170				
TOTAL UNITS	350		435				
TOTAL RESIDENTIAL STALLS PROVIDED		1.24 RATIO	435				
USPS STALL PROVIDED			1				
LEASING STALLS PROVIDED			3				
TOTAL STALLS PROVIDED			439				
ACCESSIBLE STALLS REQUIRED							
USE TYPE	QUANTITY	RATIO	REQUIRED	NUMBER OF REQUIRED			
				STD.	VAN		
RESIDENTIAL (ASSIGNED)	435	1 PER 50	10	8	2		
LEASING (ASSIGNED)	3	1 PER 50	1	0	1		
TOTAL ACCESSIBLE STALLS REQUIRED:			11	8	3		
NOTE: THE REQUIRED ADA STALLS ARE INCLUDED IN PARKING COUNT. ONE (1) VAN ADA STALL FOR EVERY SIX (6) ADA STALLS REQUIRED.							
EVCS STALLS REQUIRED							
EV CATEGORY		NUMBERS OF STALLS	RATIO	REQUIRED			
RESIDENTIAL							
EVSE		435	1 PER 25	17			
EV READY			20%	87			
RESIDENTIAL EVCS STALLS REQUIRED				104			
NOTE: THE REQUIRED EV STALLS ARE INCLUDED IN PARKING COUNT.							
RESIDENTIAL STALLS PROVIDED							
PARKING LEVEL	STALL COUNT PER USE					TOTAL STALLS PROVIDED	
	STANDARD	COMPACT	USPS	ADA	EVSE		EV READY
1	20	7	1	2	2	8	39
2	41	8	1	2	3	14	68
3	46	8		2	3	15	74
4	46	8		2	3	15	74
5	46	8		2	3	15	74
6	43	8		1	3	14	69
7	31	0			2	8	41
TOTALS	274	47	2	11	18	88	439
TOTAL STALLS PROVIDED							
TOTAL STALLS PROVIDED			439				

SHORT TERM BICYCLE PARKING SUMMARY			
PARKING TYPE	RATIO	SPACES REQUIRED	SPACES PROVIDED
1 - 25 DWELLING UNITS	1 SPACE PER 10 UNIT	3	3
26 - 100 DWELLING UNITS	1 SPACE PER 15 UNITS	5	5
101 - 200 DWELLING UNITS	1 SPACE PER 20 UNITS	5	5
201+ DWELLING UNITS	1 SPACE PER 40 UNITS	4	4
TOTAL SHORT TERM BICYCLE PARKING		16	16

BICYCLE PARKING			
PARKING TYPE		REQUIRED RATIO	SPACES PROVIDED
RESIDENTIAL	350 UNITS	1 PER 3 UNITS	117
TOTAL PARKING REQUIRED			117
BICYCLE PARKING PROVIDED			58
NOTE: Secure/Enclosed bicycle parking - Each one (1) bicycle parking space that is within a secure/enclosed bicycle parking facility may be used to satisfy the requirement of two (2) required bicycle parking spaces.			

DISTRICT NORTH	
THE JACOBSON IS A 350-UNIT RESIDENTIAL DEVELOPMENT FEATURING A 5-STORY TYPE III-A OVER 1-STORY TYPE I-A PODIUM. THIS CONFIGURATION PARTIALLY SURROUNDS A 6-STORY PARKING STRUCTURE. ATOP THE PARKING STRUCTURE, A POOL AND AMENITY DECK PROVIDE RECREATIONAL SPACE AND LEISURE FACILITIES FOR THE RESIDENTS.	
PROJECT ADDRESS: 523 W. 200 N., SALT LAKE CITY, UT 84116	
ZONING:	
GROSS LAND AREA:	2.91 ACRES
BUILDING AREA (GFA):	562,228 SQ. FT.
BUILDING AREA (EXCLUDING PARKING & ROOF DECK):	380,800 SQ. FT.
PARKING & ROOF DECK AREA:	181,428 SQ. FT.
TOTAL UNITS:	350 UNITS
DENSITY:	120 DU/AC
FAR (PARKING STRUCTURE/ROOF DECK NOT INCLUDED):	3.0



DISTRICT NORTH

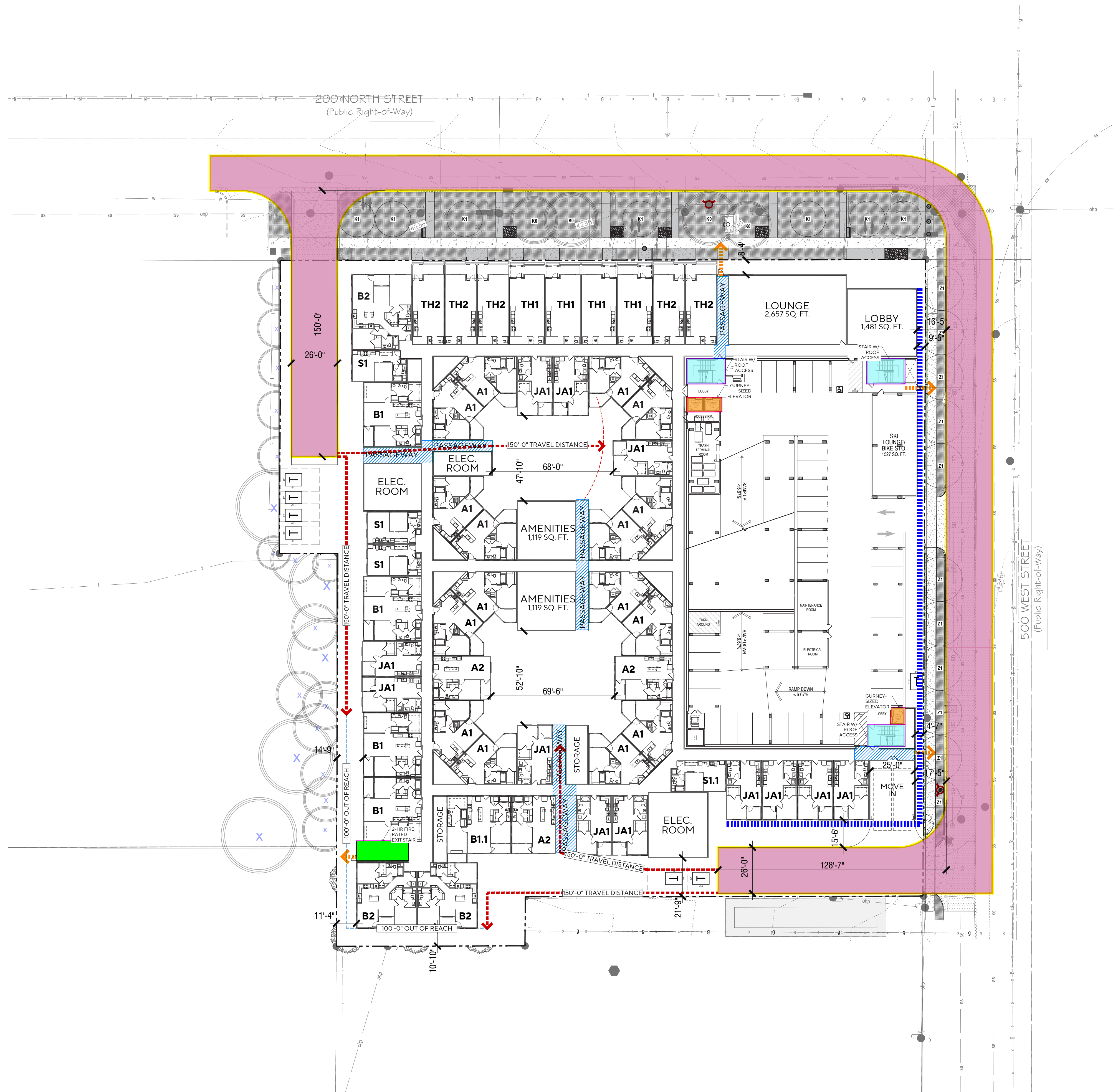
SALT LAKE CITY, UT












PROJECT SUMMARY

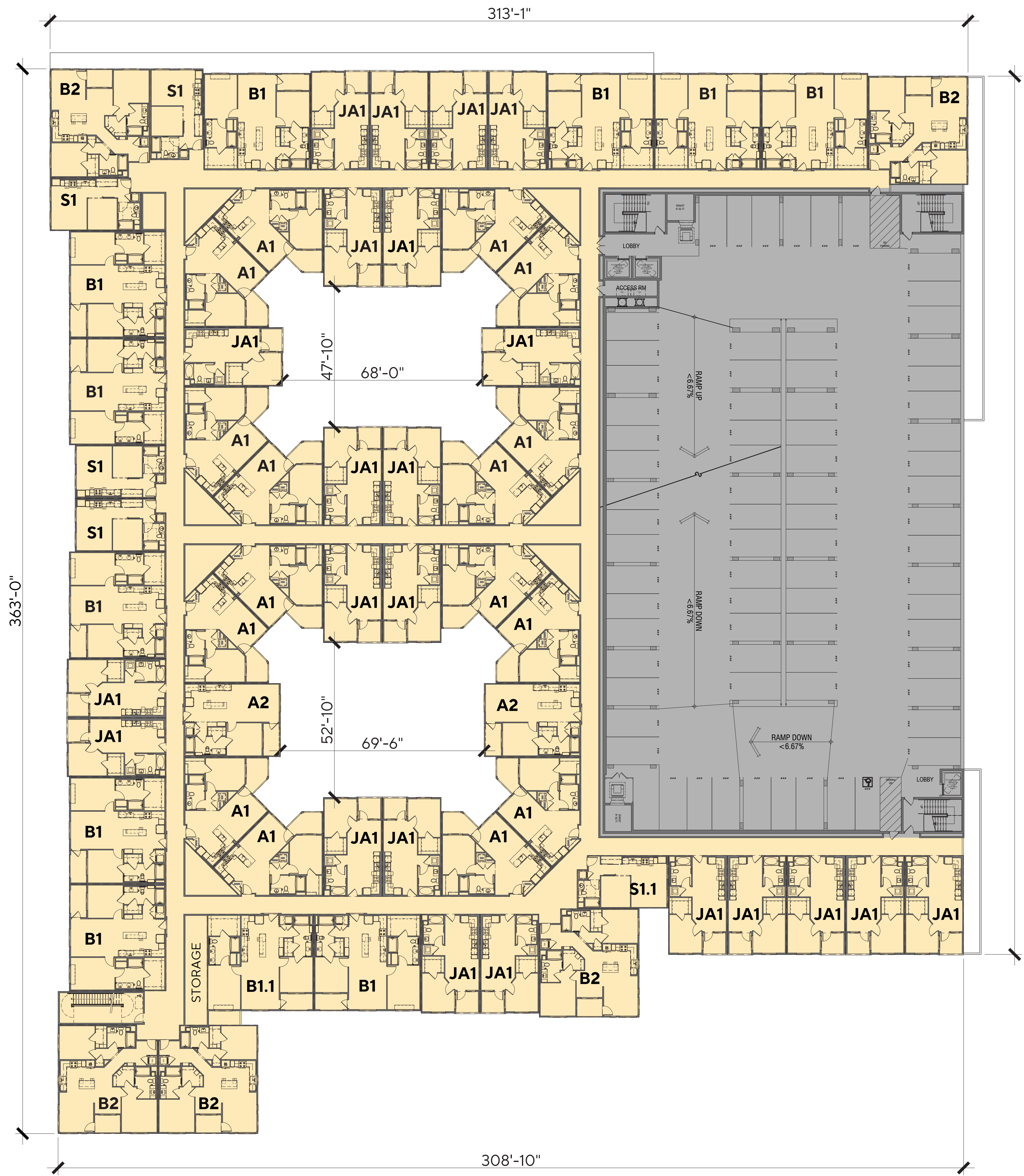


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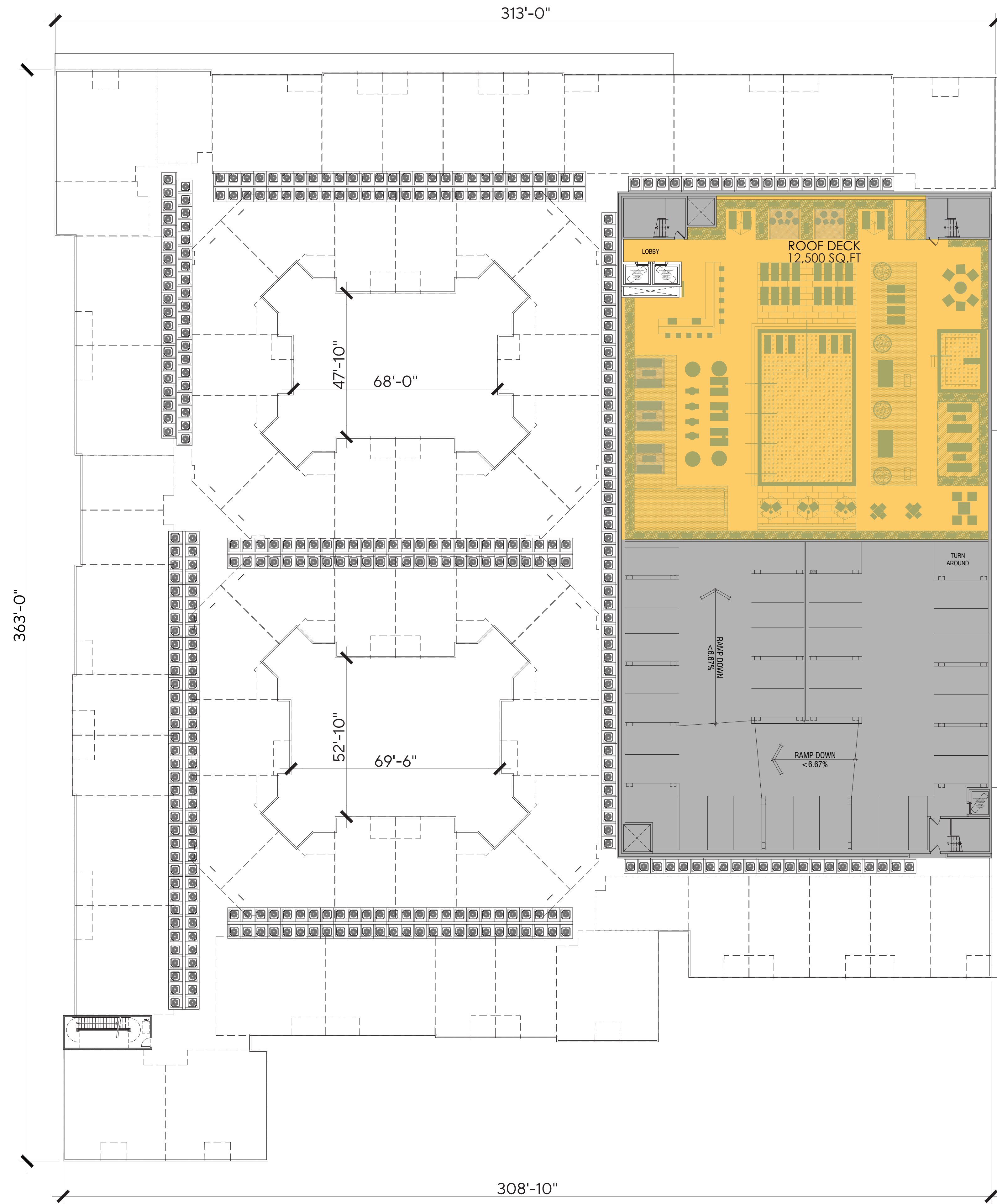


- LEGEND**
-  26-FT WIDE FIRE LANE
 -  EXISTING FIRE HYDRANT
 -  PROPOSED FIRE HYDRANT
 -  STANDPIPE
 -  2-HR EXIT STAIR NOT ROOF ACCESSIBLE
 -  2-HR EXIT STAIR ROOF ACCESSIBLE
 -  GURNEY-SIZED ELEVATOR
 -  2-HR FIRE RATED PASSAGEWAY
 -  150-FT MAX. FIRE HOSE PULL
 -  PATH OF EXIT
 -  AERIAL TRUCK LADDER ACCESS TO BUILDING



LEGEND

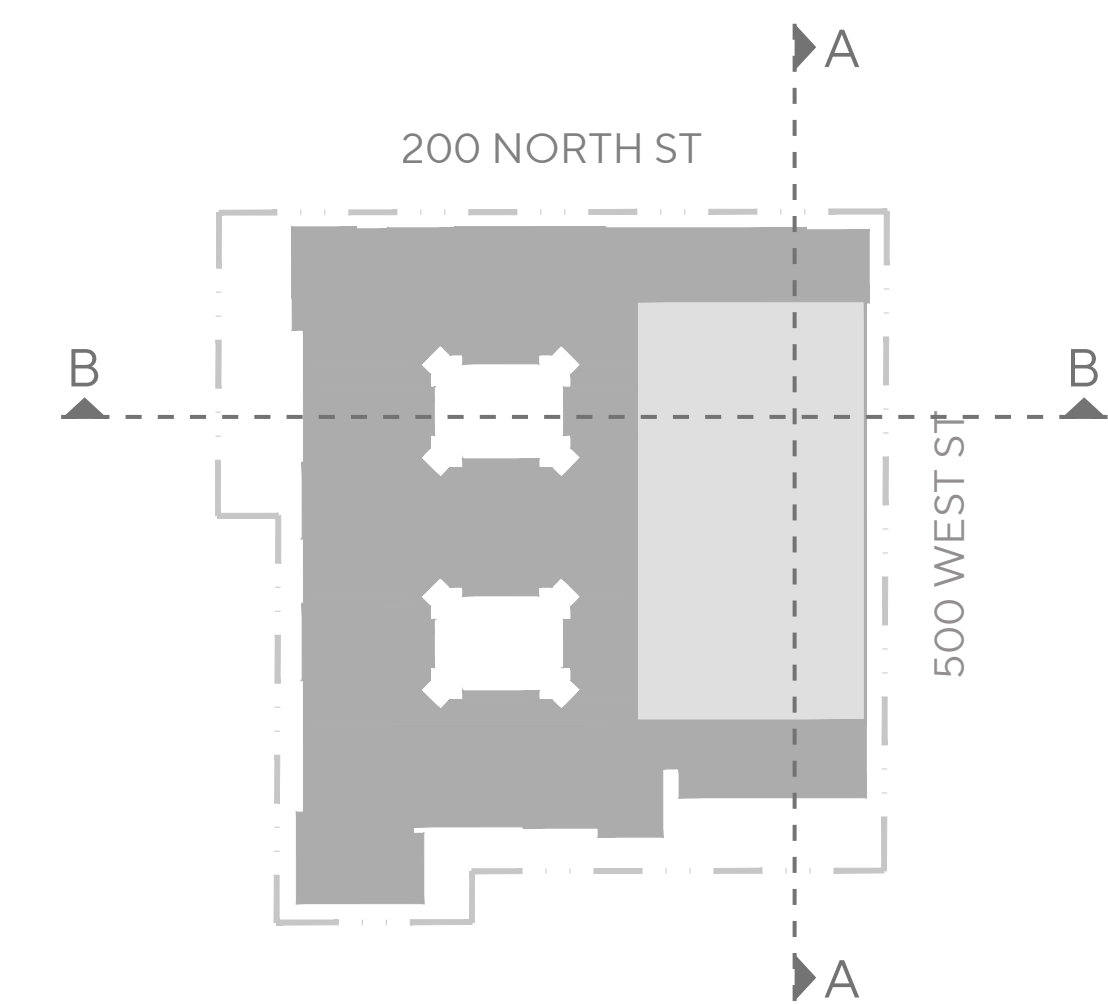
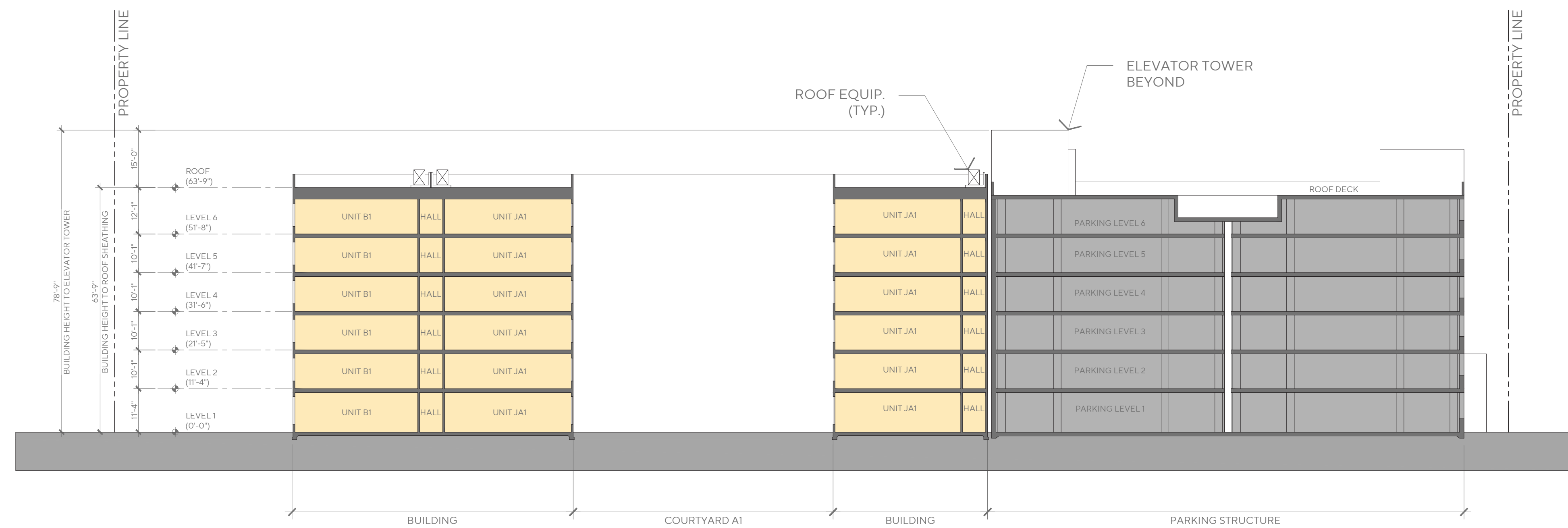
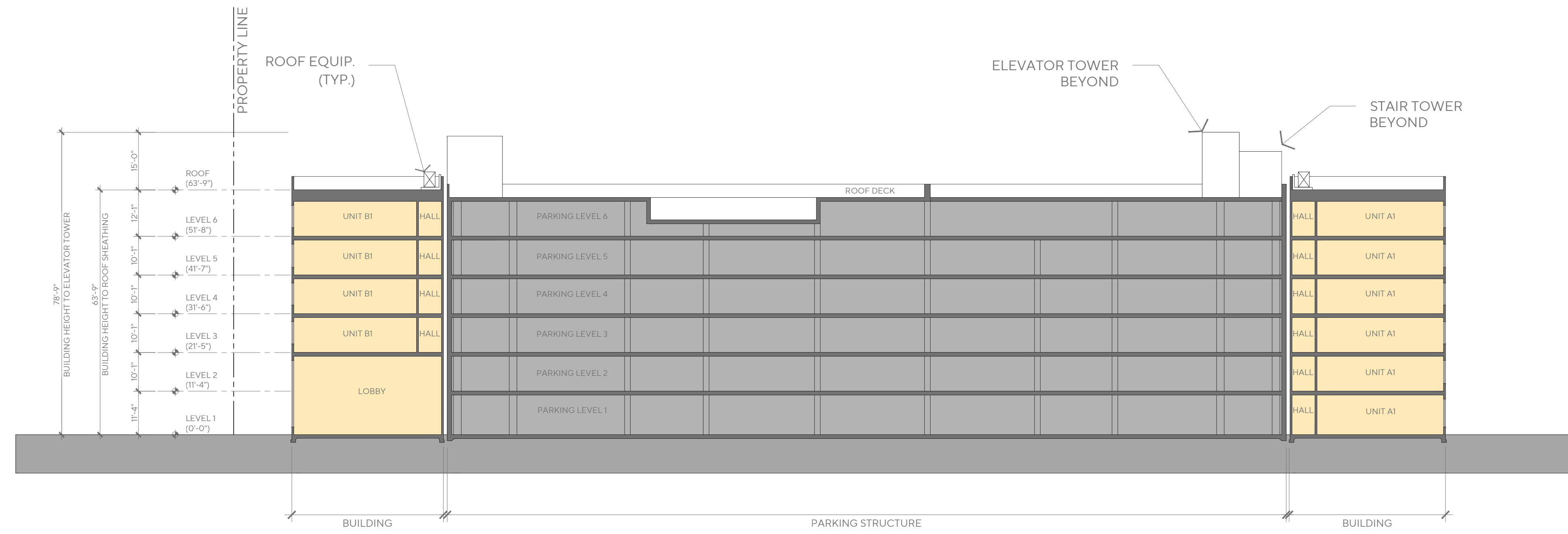
- LEASING & AMENITIES
- RESIDENTIAL
- PARKING



LEGEND

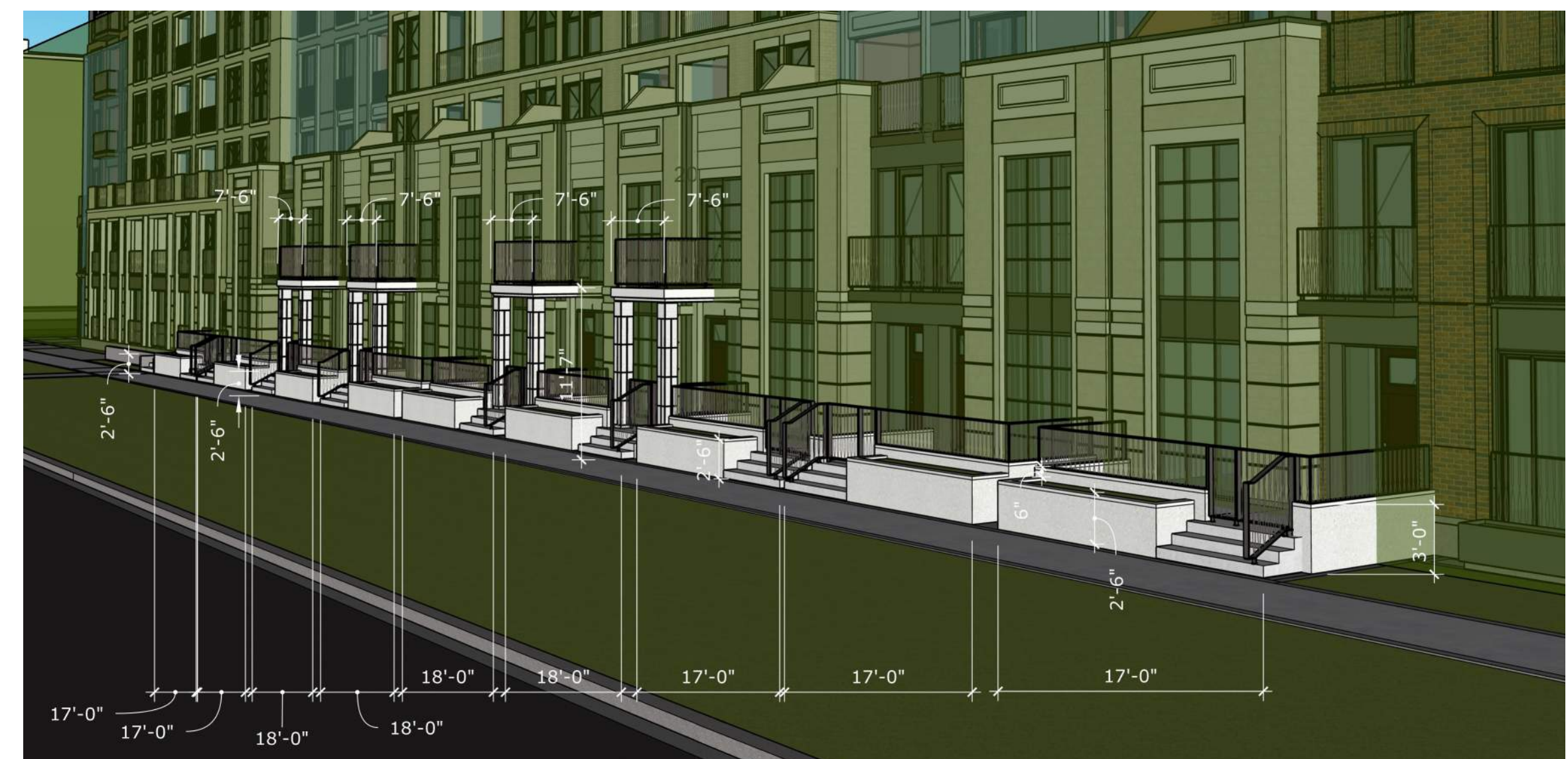
- LEASING & AMENITIES
- RESIDENTIAL
- PARKING

- LEGEND**
- LEASING & AMENITIES
 - RESIDENTIAL
 - PARKING





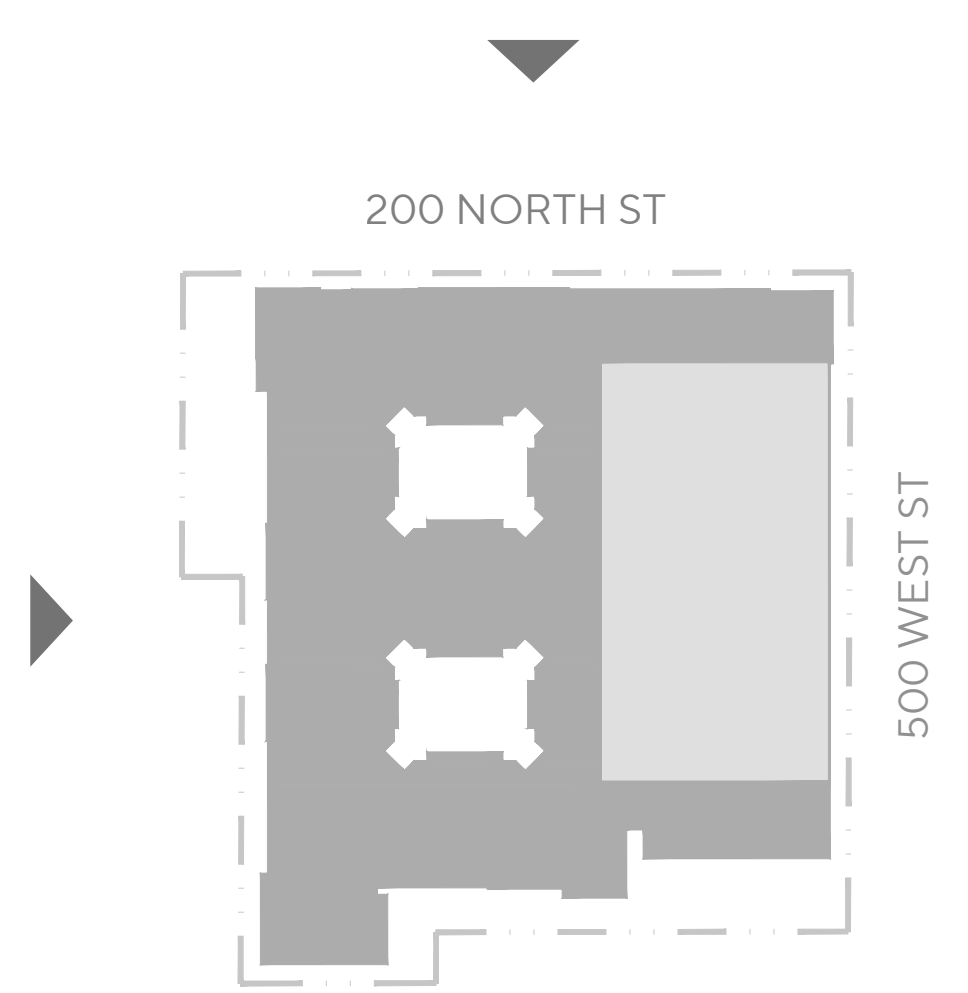
NORTH ELEVATION

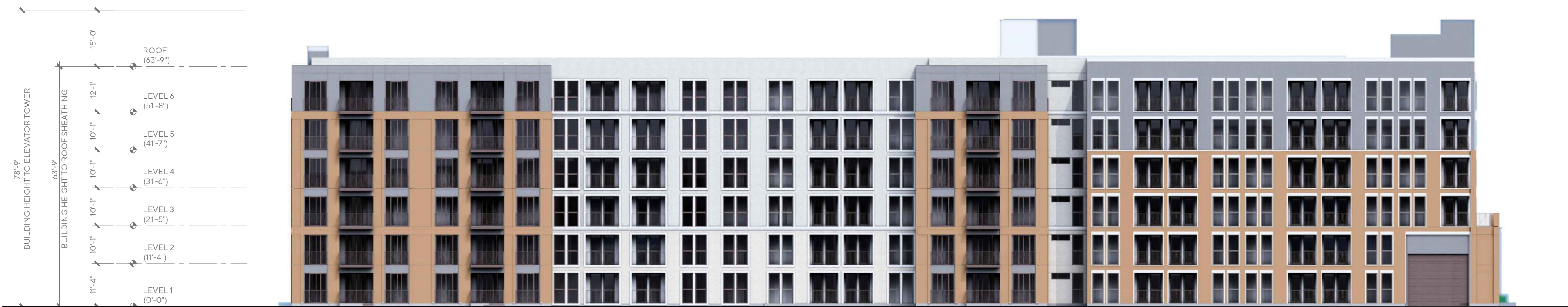


NORTH ELEVATION WALLS DIMENSIONS



WEST ELEVATION

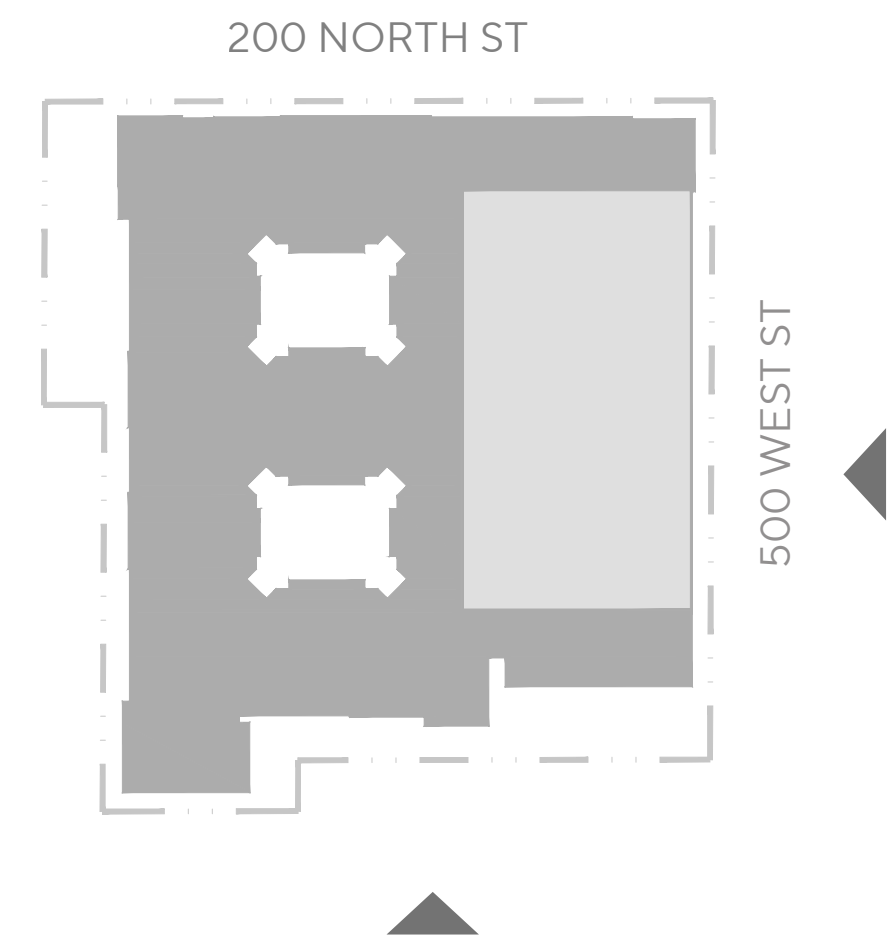


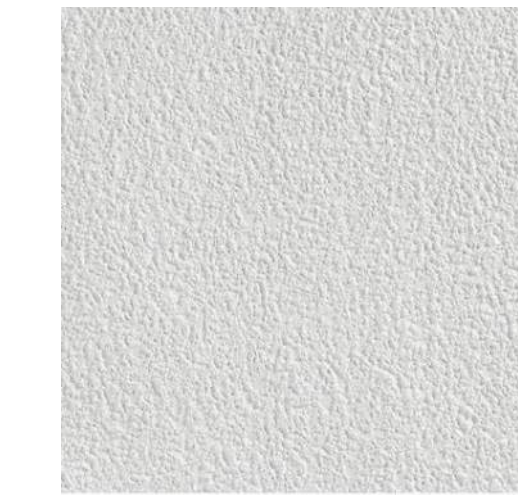


SOUTH ELEVATION

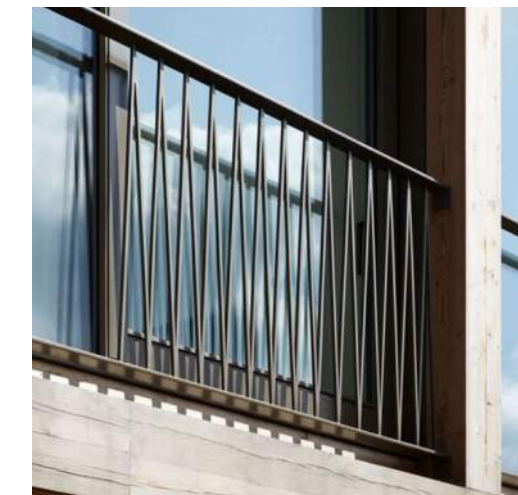


EAST ELEVATION





1. ARCHITECTURAL PLASTER LIGHT SAND FINISH



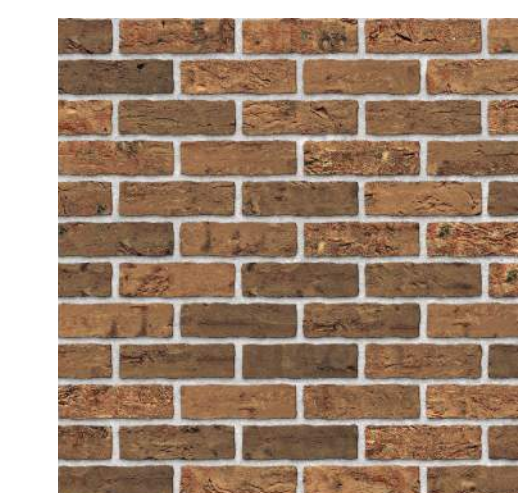
5. METAL RAILING - CUSTOM DARK BRONZE



2. ARCHITECTURAL PLASTER SMOOTH FINISH



6. ALUMINUM AWNING - CUTSOM DARK BRONZE



3. THICK BRICK



7. ALUMINUM STOREFRONT - CUTSOM DARK BRONZE



4. VERTICAL LIVING WALL AND SHADING SCREEN



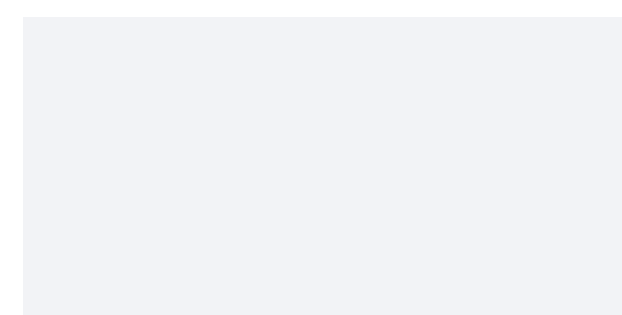
8. HIGH PERFORMANCE VINYL WINDOW DARK BRONZE



9. THIN BRICK



10. TRAVERTINE TILE/ FIBER CEMENT



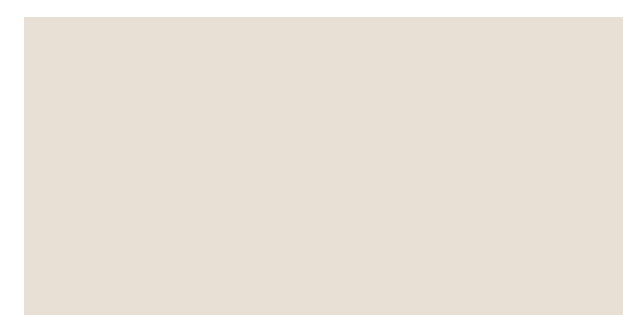
A. SW 7006 EXTRA WHITE



B. SW 9565 FORGED STEEL



C. SW 6101 SANDS OF TIME



D. SW 0053 PORCELAIN



E. SW 7017 DORIAN GRAY



F. SW 7048 URBAN BRONZE

DESIGN STANDARDS 21.A.37		
STANDARDS (CODE SECTION)	TSA-UC	NOTES
Ground floor use (%) (21A.37.050.A.1)	80	The Project follows with Option 2 (Ground Floor use + Visual interest)
Ground floor use + visual interest (%) (21A.37.050.A.2)	60/25	Assuming that residential uses can fulfill the ground floor requirement, the entire ground floor along 200 N meets this criterion. Double-height common areas (lobby, lounge) and double height townhome residences cover more than 64% of the ground-level facade facing 200 N (312 ft total length with 200 ft of double height spaces), featuring spaces with soaring ceilings that enhance the sense of spaciousness and luxury. All street-facing facades ensure complete visual engagement through a diverse selection of materials, distinct architectural elements, variations in the facade, and the use of color. We are requesting modification for the ground floor use standard along 500 W, since the parking structure is positioned there.
Building materials: ground floor (%) (21A.37.050.B.1)	90	The ground floor façades wall area facing 200 N includes 92 % of durable material and 31 % facing 500 W. This includes thin brick veneer. Requesting modification to this standard along 500 W.
Building materials: upper floors (%) (21A.37.050.B.2)	60	The upper floor façades wall area facing 200 N includes 70 % of durable material and 13 % of durable material facing 500 W. This includes thin brick veneer. Requesting modification to this Standard along 500 W.
Glass: ground floor (%) (21A.37.050.C.1)	60	The ground floor glazing is proposed to vary, based on the type of use and corresponding program. The North Façade facing 200N is calculated to an average of 53% . The ground level primarily consists of residential units that face the street, which allows us to reduce the glazing requirement by 15%. And the Glass percentage is 15 % on the East Façade's facing 500 W since the building is mostly occupied by a parking structure.
Building entrances (feet) (21A.37.050.D)	40	The 200 North facade features a total of 11 entry points: 2 for the Lobby and Lounge area and 9 individual entrances for the townhomes. The spacing between townhome entrances is less than 40 feet, while the distance for the lobby and lounge area exceeds 90 feet. On the 500 West facade, primarily occupied by a parking structure, we are proposing 3 entrances: 2 exits for the building at either end and 1 entrance for the Ski Lounge. We are requesting a modification to this standard for both facades.
Blank wall: max length (feet) (21A.37.050.E)	15	The maximum length of any blank wall uninterrupted by windows, doors, or architectural detailing at the ground floor level along 200N is well under the required 15'. Along N 500 W Street, there is a 33' Length of wall with expressed / furred out architectural pilasters but lacking any glazing at the south East Edge of the building and the side of the move-in loading bay.

DESIGN STANDARDS 21.A.37		
STANDARDS (CODE SECTION)	D-2	NOTES
Street facing facade: max length (feet)(21A.37.050.F)	200	We request a modification to allow a facade length of approximately 315 feet along 200 North and 300 feet along 500 West. To mitigate this along 200 N, the building's massing is stepped back above the first two floors more than 6 feet, with the base articulated with double height townhomes facing the street and private patios on stoops that extend to the edge of the property line. This sections of building are enhanced with a distinct architectural style and enhanced materiality to further breakdown the scale of the building and create visual interest and an enhanced pedestrian experience.
Lighting: exterior (21A.37.050.H)	X	Complies. All proposed lighting will be shielded and directed down per code.
Lighting: parking lot (21A.37.050.I)	X	Not applicable. Project is not located adjacent to a residential zone or land use. However, the parking structure lighting proposed is internal ceiling mounted and will comply with code.
Screening of mechanical equipment (21A.37.050.J)	X	Complies. All mechanical equipment to be screened from public view and sited to minimize their visibility and impact.
Screening of service areas (21A.37.050.K)	X	Complies. All service areas, loading docks, refuse containers and similar areas are fully screened from public view.
Ground floor residential entrances for dwellings with individual unit entries (21A.37.050L)	X	All the Townhomes on the ground floor along 200 North will have a primary entrance facing the street.

DESIGN REVIEW 21.A.59.050	
STANDARDS	RESPONSE
A. Any new development shall comply with the intent of the purpose statement of the zoning district and specific design regulations found within the zoning district in which the project is located as well as the City's adopted "urban design element" and adopted master plan policies and design guidelines governing the specific area of the proposed development.	<p>The proposed project satisfied the intent for TSA Transit Station Area District, in providing a pedestrian friendly facade that interacts and responds to the developing Urban Environment in its area, enriching the visual experience of transient users with a multifamily project that engages the street, with its rich materiality and familiar, urban design feel that strongly complements the experience of future residents, but also enhances that of existing neighbors by replacing the under-utilized commercial uses currently on site with high quality housing.</p> <p>The proposed project is located in a parcel that despite being across the rail tracks for the North Temple Train station, pedestrian traffic is forced to traverse the neighborhood along the fenced off train-tracks or around two city blocks and over the W North Temple St. Bridge. The N500W street to the east of our Site is effectively an abandoned service road for use by the construction materials yard and personal storage facilities we propose to replace. The lack of any sidewalk infrastructure reinforces this notion, and thus, we argue there is very little expectation for pedestrian traffic from and towards the Train station outside of the residents of our proposed project, who will need to traverse more than half a mile (over 3,000 ft) each way to reach a retail component. The specific conditions on this parcel, are arguably excluding from being a Transition, nor a Core Area in relationship with pedestrian traffic flows around the Train Station.</p> <p>As such, we propose the elimination of the retail component requirement for our site, as we believe such a space would remain vacant and is not financially viable as it will not be able to attract transient users of the train station.</p>
B. Development shall be primarily oriented to the sidewalk, not an interior courtyard or parking lot. 1. Primary entrances shall face the public sidewalk (secondary entrances can face a parking lot) 2. Building(s) shall be sited close to the public sidewalk, following and responding to the desired development patterns of the neighborhood. 3. Parking shall be located within, behind, or to the side of buildings.	<p>The main entrance of the building is positioned prominently along the corner of 200 N that we consider the main street frontage for the project, and N 500 W that in its current condition, we consider functioning as dead-end, internal an access road.</p> <p>The building's proposed typology engages the existing sidewalks with its main entrance being located at the corner and highlighted by its destiched architecture, but also with the two story townhomes/rowhouses that each enjoys its direct private access from 200 N, along with a covered corridor access that connects them to the parking structure. This creates a district base for the building that enhances and promotes the engagement with the urban environment that the District Plan promotes.</p> <p>The Parking itself is located along the side of the building, providing the needed sound isolation from the train-tracks and is itself covered by compatible architectural elements.</p>
C. Building façades shall include detailing and glass in sufficient quantities to facilitate pedestrian interest and interaction. 1. Locate active ground floor uses at or near the public sidewalk. 2. Maximize transparency of ground floor facades. 3. Use or reinterpret traditional storefront elements like sign bands, clerestory glazing, articulation, and architectural detail at window transitions. 4. Locate outdoor dining patios, courtyards, plazas, habitable landscaped yards, and open spaces so that they have a direct visual connection to the street and outdoor	<p>The building facades utilize large fenestration openings, approaching an effective floor-to-ceiling glazing experience that people have connected with upscale urban residences. The Townhomes and common spaces along 200 N, utilized double height storefront type glazing, that illuminates their double height interiors with soft northern light and showcases their unique feel and desirability.</p> <p>Each of the district massing elements of the proposed project, utilizes its own architectural detailing around the openings, enhancing interest and providing a rich visual experience for transient users.</p> <p>The physical separation from the sidewalk, facilitated though the use of private outdoor courts on stoops, allows for this transparency without compromising the sense of safety and the ability to isolate their living space from the street, if this is desired.</p> <p>The Common open spaces, and Pool and Amenities Deck located close the corner of 200 N and 500 W, allowing for views down the street and into the mountain ranges.</p>
D. Large building masses shall be divided into heights and sizes that relate to human scale. 1. Relate building scale and massing to the size and scale of existing and anticipated buildings, such as alignments with established cornice heights, building massing, step-backs and vertical emphasis. 2. Modulate the design of a larger building using a series of vertical or horizontal emphases to equate with the scale (heights and widths) of the buildings in the context and reduce the visual width or height. 3. Include secondary elements such as balconies, porches, vertical bays, belt courses, fenestration and window reveals. 4. Reflect the scale and solid-to-void ratio of windows and doors of the established character of the neighborhood or that which is desired in the master plan.	<p>1. The building's massing and height is directly relatable to the stock of recently developed multifamily and mixed-use developments in immediate proximity but also within a 1 mile radius.</p> <p>2. The building's massing is broken up both in horizontal and vertical planes, with a distinct, two-story high ground floor that relates to pedestrian urban experience, but also an architectural language with clean, deliberate moves that break up the mass in interesting portions or distinct building sections, further articulated visually by changes in materiality and color.</p> <p>3. Numerous architectural details enhance the richness of the facade, including both inset and projected balconies with distinct materiality, enhanced materials applied on an actual articulated form vs. a simple surface application and deep reveals with a variation of architectural styles around most fenestration.</p> <p>4. The ground floor strives to maximize the available fenestration for the proposed spaces, be it of common or private occupancy, offering large, operable fenestration and a floor-to-ceiling height that is superior to most existing developments in the area for this product type. The facade treatment and articulation along of all public facing building sides, is aimed to reduce the massing of the building down to familiar, human scaled elements.</p> <p>The porosity and transparency of the ground floor along 200 N, provides a rich experience of materiality and architectural elements, catering towards stimulating the interest of pedestrians along the property edges.</p>

DESIGN REVIEW 21.A.59.050	
STANDARDS	RESPONSE
E. Building facades that exceed a combined contiguous building length of two hundred feet (200') shall include: 1. Changes in vertical plane (breaks in facade); 2. Material changes; and 3. Massing changes.	<p>The North façade along 200 N is proposed to exceed 200' of contiguous length, but to mitigate this we are proposing the recommended step-back of the north facade, changing both the architectural styles and introducing material changes for the first two levels of the building.</p> <p>The Eastern façade, facing N 500 W and along the train tracks will be stylistically articulated both vertically and horizontally through the use of architectural projections, enhanced materials and changes in color and texture, but is not proposed to have the deep massing changes as the north edge of the building is.</p> <p>The first two floors along all street-front edges of the building are a gesture down to human scale, with materiality and articulation reminiscent of 19th and early 20th Century urban multifamily blocks with contemporary interpretations of Georgian and compatible neo-classical undertones. The alternation of projecting and inset balconies, along with furring out projections along all four façades which are in-turn emphasized by material changes, enhance the sense of depth and richness of the facade for both residents and transient viewers.</p>
F. If provided, privately-owned public spaces shall include at least three (3) of the six (6) following elements: 1. Sitting space of at least one sitting space for each two hundred fifty (250) square feet shall be included in the plaza. Seating shall be a minimum of sixteen inches (16") in height and thirty inches (30") in width. Ledge benches shall have a minimum depth of thirty inches (30"); 2. A mixture of areas that provide seasonal shade; 3. Trees in proportion to the space at a minimum of one tree per eight hundred (800) square feet, at least two inch (2") caliper when planted; 4. Water features or public art; 5. Outdoor dining areas; and 6. Other amenities not listed above that provide a public benefit.	<p>The project does not provide privately-owned public spaces. The common open spaces on the courtyards do provide for more than one seating areas that will meet or exceed the specified requirements for shading and landscaping.</p> <p>The common courtyards and roof deck will provide for outdoor dining and assembly areas, but these spaces are not meant to be shared with the public.</p>



DISTRICT NORTH

SALT LAKE CITY, UT

DESIGN REVIEW
21.A.59.050



A21

DESIGN REVIEW 21.A.59.050	
STANDARDS	RESPONSE
<p>G. Building height shall be modified to relate to human scale and minimize negative impacts. In downtown and in the CSHBD Sugar House Business District, building height shall contribute to a distinctive City skyline.</p> <p>1. Human scale</p> <ul style="list-style-type: none"> Utilize step backs to design a building that relate to the height and scale of adjacent and nearby buildings, or where identified, goals for future scale defined in adopted master plans. For buildings more than three (3) stories or buildings with vertical mixed use, compose the design of a building with distinct base, middle and top sections to reduce the sense of apparent height. <p>2. Negative impacts:</p> <ul style="list-style-type: none"> Modulate taller buildings vertically and horizontally so that it steps up or down to its neighbors. Minimize shadow impacts of building height on the public realm and semi-public spaces by varying building massing. <p>Demonstrate impact from shadows due to building height for the portions of the building that are subject to the request for additional height.</p> <ul style="list-style-type: none"> Modify tall buildings to minimize wind impacts on public and private spaces, such as the inclusion of a wind break above the first level of the building. <p>3. Cornices and rooflines:</p> <ul style="list-style-type: none"> Cohesiveness: Shape and define rooflines to be cohesive with the building's overall form and composition. Complement Surrounding Buildings: Include roof forms that complement the rooflines of surrounding buildings. Green Roof And Roof Deck: Include a green roof and/or accessible roof deck to support a more visually compelling roof landscape and reduce solar gain, air pollution, and the amount of water entering the stormwater system. 	<p>1. Human Scale The building is designed with a distinct base, middle and top, emphasizing the base of the building along its frontage and the effective interface with pedestrian traffic.</p> <p>The first two floors along all street-front edges of the building are a gesture down to human scale, with materiality and articulation reminiscent of 19th Century Neo-Classical urban blocks and row housing developments. Beyond the street appealing and elegant styling, the upper floors are being further separated and set-back forming usable balconies along the 3rd level for the north facing units, softening the most visible edges of the proposed building. All of these moves are emphasized through material changes that increase the depth perception of each change on both vertical and horizontal planes.</p> <p>The proposed roof deck is proposed along the eastern edge of the building, away from the more residential minded existing development to the West of the project and facing the Downtown area and the mountain ranges to the East.</p> <p>2. Negative Impacts There are no small scale residential neighboring developments that would benefit from stepping the building down to meet them at their level: the majority of neighboring buildings are industrial / automotive dealerships and the single multifamily residential development to our West is will enjoy sufficient separation of 100' or more from the proposed development.</p> <p>As such there are no established outdoor commercial, educational or residential uses to be impacted by shading due to the new development for a window of time that would be of concern.</p> <p>3. Roofline The Mass of the building was broken down with angled furred-out wall treatments, with each segment getting a slightly modified parapet height to increase visual interest. Strategic architectural style and material alteration assists the way viewers perceive both the massing and the roofline of the project.</p>
<p>H. Parking and on site circulation shall be provided with an emphasis on making safe pedestrian connections to the sidewalk, transit facilities, or mid-block walkway.</p>	<p>Parking entrance is located along N 500 W , about 180' south of the nearest intersection and should not impact the safety of pedestrian connections to the sidewalks, by providing sufficient line of sight and room for incoming and outgoing traffic to queue in and out of the structure.</p>
<p>I. Waste and recycling containers, mechanical equipment, storage areas, and loading docks shall be fully screened from public view and shall incorporate building materials and detailing compatible with the building being served. Service uses shall be set back from the front line of building or located within the structure. (See subsection 21A.37.050K of this title.)</p>	<p>Waste and recycling containers, mechanical equipment, storage areas are located inside the parking structure. The loading docks are located in the alley and hidden from public view.</p>

DESIGN REVIEW 21.A.59.050	
STANDARDS	RESPONSE
<p>J. Signage shall emphasize the pedestrian/mass transit orientation.</p> <p>1. Define specific spaces for signage that are integral to building design, such as commercial sign bands framed by a material change, columns for blade signs, or other clearly articulated band on the face of the building.</p> <p>2. Coordinate signage locations with appropriate lighting, awnings, and other projections.</p> <p>3. Coordinate sign location with landscaping to avoid conflicts.</p>	<p>Signage will be provided to clearly identify the main residential entrance and to identify wayfinding for the parking garage. All signage location will be designed with the existing and new landscaping in mind so as not to obstruct wayfinding and views.</p>
<p>K. Lighting shall support pedestrian comfort and safety, neighborhood image, and dark sky goals.</p> <p>1. Provide street lights as indicated in the Salt Lake City Lighting Master Plan.</p> <p>2. Outdoor lighting should be designed for low-level illumination and to minimize glare and light trespass onto adjacent properties and uplighting directly to the sky.</p> <p>3. Coordinate lighting with architecture, signage, and pedestrian circulation to accentuate significant building features, improve sign legibility, and support pedestrian comfort and safety.</p>	<p>Lighting Design Review Standard is noted by the design team. Street Lighting will be designed in accordance to the Salt Lake City lighting Master Plan. Outdoor lighting will be designed for low-level illumination and to minimize glare and light trespass onto adjacent properties and uplighting directly to the sky. Lighting will coordinated with potential signage locations to maximize the attention of the tenant's as well as providing safe pedestrian walkways along 200 N and 500 W.</p>
<p>L. Streetscape improvements shall be provided as follows:</p> <p>1. One street tree chosen from the street tree list consistent with the City's urban forestry guidelines and with the approval of the City's Urban Forester shall be placed for each thirty feet (30') of property frontage on a street. Existing street trees removed as the result of a development project shall be replaced by the developer with trees approved by the City's Urban Forester.</p> <p>2. Hardscape (paving material) shall be utilized to differentiate privately-owned public spaces from public spaces. Hardscape for public sidewalks shall follow applicable design standards. Permitted materials for privately-owned public spaces shall meet the following standards:</p> <ul style="list-style-type: none"> Use materials that are durable (withstand wear, pressure, damage), require a minimum of maintenance, and are easily repairable or replaceable should damage or defacement occur. Where practical, as in lower-traffic areas, use materials that allow rainwater to infiltrate into the ground and recharge the water table. Limit contribution to urban heat island effect by limiting use of dark materials and incorporating materials with a high Solar- Reflective Index (SRI). Utilize materials and designs that have an identifiable relationship to the character of the site, the neighborhood, or Salt Lake City. Use materials (like textured ground surfaces) and features (like ramps and seating at key resting points) to support access and comfort for people of all abilities. Asphalt shall be limited to vehicle drive aisles. (Ord. 14-19, 2019) 	<p>1. Street Trees: -300 North – We have ~313' ft of frontage / 30 = 10.4 We are providing (7) new trees and preserving (4) koeleruteria paniculata trees for a total of (11) street trees per Urban Forestry standards within an existing parkstrip. The intent is to match and extend the existing tree layout and species in lieu of removing established trees in the large parkstrip. Koeleruteria paniculata is an approved Urban Forestry, medium parkstrip tree that will work well with constraints of existing utilities in lieu of using a large park strip tree. The intent is to provide vegetation beyond the minimum requirements for total landscape coverage. -500 West – We have ~307' ft of frontage / 30 = 10.2 We are providing (11) new Zelkova Serrata street trees from the Urban Forestry 6' parkstrip – Medium tree species list.</p> <p>2. Although we have only a small amount of publicly accessible privately owned spaces, the hardscape has been defined by changes in materials for both public and private uses. Durable materials have been chosen to complement the architectural design, finishes, and color scheme. Additionally, asphalt has been restricted to drive aisles only.</p>



SITE ITEMS

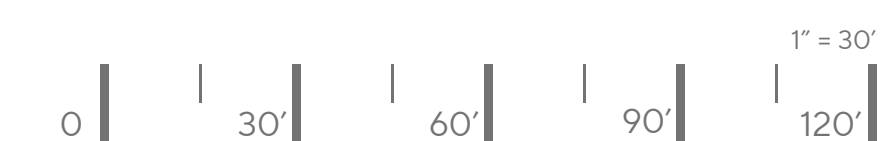
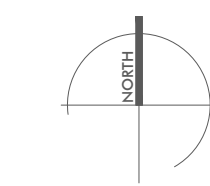
- 1 PROPERTY LINE
- 2 NEIGHBORING WALL TO REMAIN
- 3 TRANSFORMER - SCREENED WITH LANDSCAPING HEDGE WHERE VISIBLE
- 4 6' HEIGHT SECURED FENCING
- 5 4' HEIGHT FENCED DOG RUN AREA
- 6 LANDSCAPE SCREENING AT PARKING GARAGE
- 7 PRIVATELY OWNED PUBLIC ENTRANCE WITH ENHANCED PAVING
- 8 BIKE RACK LOCATION
- 9 PRIVATE INSTALL BENCH
- 10 PRIVATE TOWNHOME STOOPS
- 11 PUBLIC CARRIAGEWAY

SURFACE MATERIALS

- ROW PLANTING AREA
33% MINIMUM LANDSCAPE COVERAGE
- ROW SIDEWALK
PER SLC ENGINEERING STANDARDS
- CONCRETE WALKWAYS
- ENHANCED PAVING SUCH AS PAVERS OR COLORED CONCRETE
- PRIVATE SITE LANDSCAPE AREA
33% MINIMUM LANDSCAPE COVERAGE
- PRIVATE SITE LANDSCAPE AREA
DECORATIVE GRAVEL
- PRIVATE SITE LANDSCAPE AREA
PERMEABLE WALKING SURFACE
- ARTIFICIAL TURF
- WATER AMENITY

PRIVATE AMENITIES

- A ARTIFICIAL TURF GATHERING LAWN
- B ARTIFICIAL WORKOUT TURF
- C SOFT SURFACE SEATING AREA
- D ENHANCED PAVING AMENITY SPACE
- E CONCRETE AMENITY PATIO
- F OUTDOOR DINING / GRILL
- G FIRE FEATURE
- H POOL
- I SPA
- J COVERED SUN DECK



LANDSCAPE CALCS

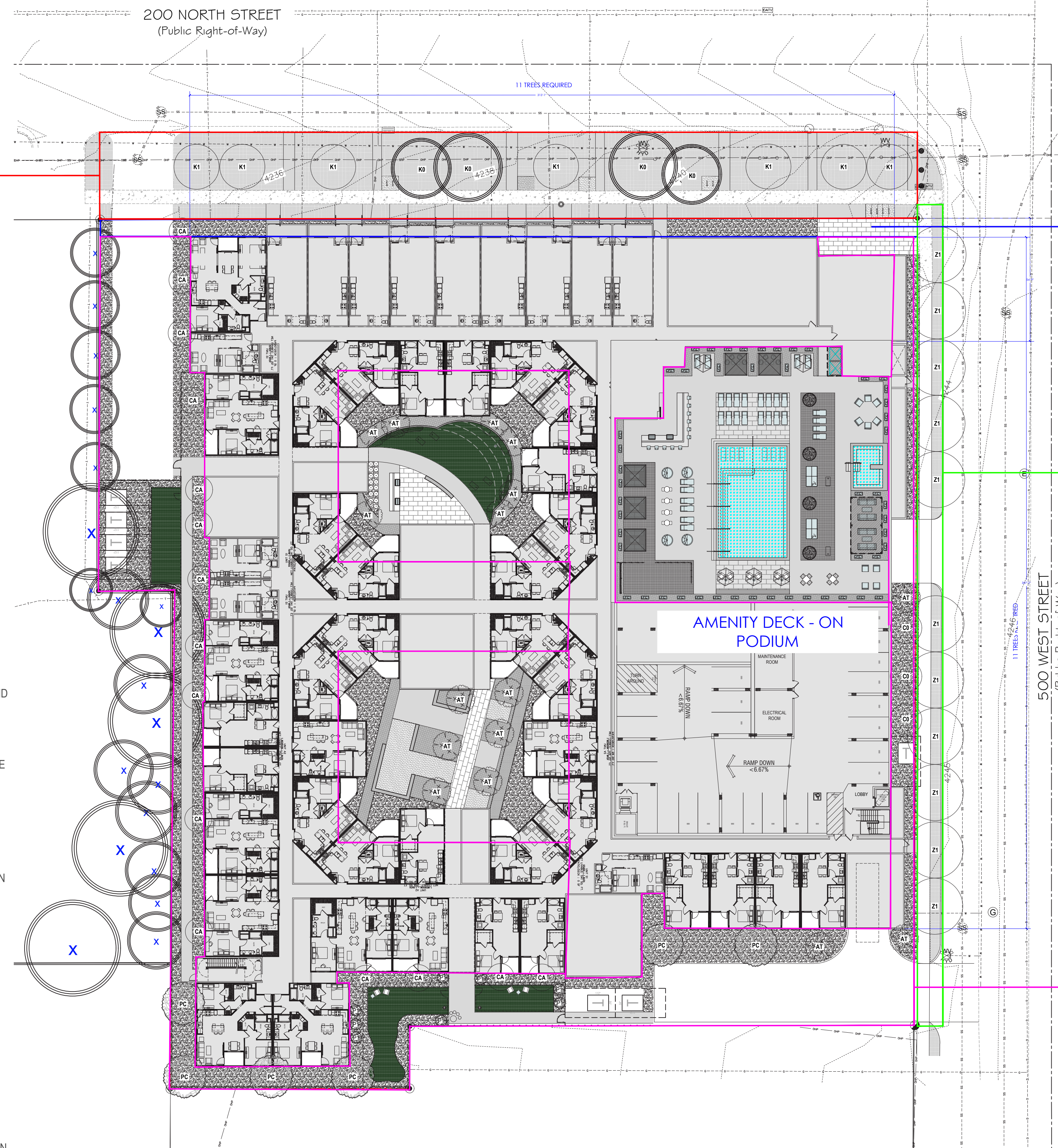
LANDSCAPE 200 NORTH ROW CALCULATIONS

200 NORTH:	S.F.:	PERCENTAGE:
TOTAL AREA:	13,580	100%
VEHICULAR AREA:	700	5.15%
EXISTING SIDEWALK	2,130	15.68%
HARDSCAPE	1,385	10.20%
<i>(20% MAXIMUM)</i>		
CARRIAGEWAYS	780	
ACCESS WALKS	605	
OTHER (UTILITIES ETC.)		
LANDSCAPE AREA:	9,365	68.96%
TURF	0	0%
PLANTED MULCH AREA:	9,365	68.96%

LIVE VEGETATION:
(33% OF PLANTING AREA REQUIRED OF 9365)
 SHRUBS: 490*7 AVG. 3,430 S.F. 36.63%
 TREE CANOPY: 3,600 S.F. 36.84%
 *ESTIMATED TREE SOIL AVAILABLE PER TREE: 1,400 S.F.

PLANT SELECTIONS (100% REQUIRED):
 WATERWISE SHRUBS: 490/490 100%
 WATERWISE TREES: 11/11 100%
 4/4 EXISTING
 7/7 PROPOSED

*SHADE TOLERANT SHRUBS ONLY ON NORTH FACING AREAS
 *314 L.F. / 30 = 10.46 TREES REQUIRED
 *ALL SELECTIONS PER URBAN FORESTRY SUGGESTED TREE LIST
 *ALL FUTURE SELECTIONS PER SLC BMP WATERWISE MANUAL



LANDSCAPE CALCS

FRONT YARD CALCULATIONS

TOTAL AREA:	3,267 S.F.	
VEHICULAR DRIVE	203 S.F.	
FRONT YARD OPEN SPACE:	3,064 S.F.	100%
APPLICABLE HARDSCAPE:	1,948	63.58%
PRIVATE PATIO/ WALKS	1,230	
ENHANCED PAVING	718	
LIVE VEGETATION:	1,116	36.42%
GROUND LANDSCAPE	936	
RAISED PLANTERS	180	
TURF	0	0%

*ALL FRONT YARD LANDSCAPING TO BE 100% LANDSCAPED WITH SHRUBS AND GROUNDCOVERS IN NARROW STRIP BETWEEN ROW AND BUILDING FACADE

LANDSCAPE 500 WEST ROW CALCULATIONS

200 NORTH:	S.F.:	PERCENTAGE:	LIVE VEGETATION:
TOTAL AREA:	4,385	100%	<i>(33% OF PLANTING AREA REQUIRED OF 1,210)</i>
VEHICULAR AREA:	775	17.67%	SHRUBS: 51*8.5 AVG. 433 S.F. 35.83%
SIDEWALK:	2,340	53.36%	TREE CANOPY: 2,750 S.F. 227%*
HARDSCAPE	60	1.36%	*CANOPY COVERS SIDEWALK ETC.
<i>(20% MAXIMUM)</i>			
CARRIAGEWAYS	0		*ESTIMATED TREE SOIL AVAILABLE PER TREE: 750 S.F.
ACCESS WALKS	60		10X15X5
OTHER (UTILITIES ETC.)	0		PLANT SELECTIONS (80% REQUIRED):
LANDSCAPE AREA:	1,210	27.59%	WATERWISE SHRUBS: 51/51 100%
TURF	0	0%	WATERWISE TREES: 11/11 100%
PLANTED MULCH AREA:	1,210	27.59%	4/4 EXISTING 7/7 PROPOSED

*308 L.F. / 30 = 10.26 TREES REQUIRED
 *ALL SELECTIONS PER URBAN FORESTRY SUGGESTED TREE LIST
 *ALL FUTURE SELECTIONS PER SLC BMP WATERWISE MANUAL

LANDSCAPE PRIVATE SITE CALCULATIONS

	S.F.:	PERCENTAGE:	LIVE VEGETATION:
TOTAL AREA:	126,817	100%	<i>(33% OF LANDSCAPE AREA REQUIRED OF 14,185)</i>
BUILDING FOOTPRINT:	77,581	61.18%	SHRUBS: 730*8 AVG. 5,840 S.F. 41.17%
<i>(EXCLUDING AMENITY SPACES)</i>			
VEHICULAR AREAS:	6,660	5.25%	TREE CANOPY: 3,640 S.F. 25.66%
OTHER (UTILITIES ETC.)	0		52x75
LANDSCAPE AREAS & OPEN SPACE:			PLANT SELECTIONS (100% REQUIRED):
IMPERVIOUS HARDSCAPE	19,801	15.61%	WATERWISE SHRUBS: 730/730 100%
ACCESS WALKS	9,750		*SHADE ONLY PLANTS HAVE BEE SELECTED PER SITE TOLERANT LOCATIONS
ARTIFICIAL TURF	5,186		WATERWISE TREES: 52/52 100%
(DOG PARKS / RECREATION ZONES)			*ALL FUTURE SELECTIONS PER SLC BMP WATERWISE MANUAL
ACCENT PAVING	2,815		
*POOL AMENITY:	2,050		
PERMEABLE LANDSCAPE:	18,070	14.25%	
TURF	0		
PLANTED MULCH AREA:	14,185		
DECORATIVE GRAVEL	3,120		
(SPECIMEN PLANTING ONLY)			
PERMEABLE SEATING:	765		



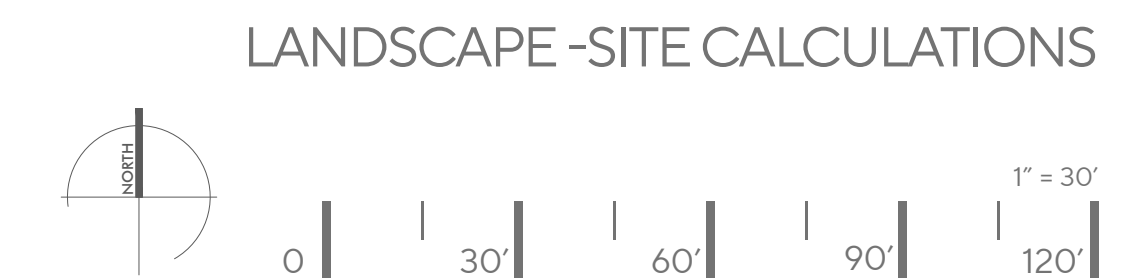
LANDSCAPE NOTES

- LANDSCAPE GENERAL NOTES**
REGULATIONS:
 1. ALL DOCUMENTS SUBMITTED FOR PERMITTING SHALL BE FINAL CONSTRUCTION DOCUMENTS
 2. ALL IMPROVEMENTS SHALL CONFORM TO THE GOVERNING (SALT LAKE CITY, UT) STANDARDS AND SPECIFICATIONS.
 3. CONTRACTOR SHALL CALL BLUE STAKES OF UTAH TO VERIFY AND NOTE EXISTING UTILITIES. CONTRACTOR SHALL BE RESPONSIBLE FOR FINAL LOCATION OF ALL UTILITIES. CONTRACTOR IS LIABLE FOR DAMAGES TO EXISTING INFRASTRUCTURE AND NEW IMPROVEMENTS
 4. CONTRACTOR TO OBTAIN PERMITS FROM REQUIRED AGENCY WITH JURISDICTION OVER THE SITE AND/OR CONSTRUCTION PROCESS
- SITE PREPARATION:**
 1. ALL LANDSCAPE AREAS TO HAVE WEEDS REMOVE AND GRUBBED
 2. APPLY, AS NEEDED, CERTIFIED APPLICATIONS OF HERBICIDE
 3. POSITIVE DRAINAGE IS TO BE MAINTAINED AWAY FROM ALL STRUCTURES
 4. ENGINEERING PLANS SHALL TAKE PRECEDENCE
 5. ALL GRADES SHALL BE NEAT, RAKED, AND FREE OF DEBRIS PRIOR TO SUBSTANTIAL COMPLETION
- SOIL AND PLANTING PREPARATION**
 1. MINIMUM OF 12" OF TOPSOIL IS REQUIRED IN ALL PLANTING AREAS
 2. MINIMUM OF 4" OF TOPSOIL IS REQUIRED IN ALL TURF PLANTING AREAS
 3. BACKFILL FOR SHRUB AND TREE PLANTINGS SHALL BE 75% TOPSOIL/ 25% HUMUS MATERIAL
 4. GROUNDCOVER PLANTINGS TO RECEIVE WEED PRE-EMERGENT
 5. SOILS REPORT SHALL TAKE PRECEDENCE
 5.1. TOPSOIL STANDARDS
 SAND - 20%-70%
 5.2. CLAY - 20%-70%
 5.3. #10 SIEVE @ 15% MAXIMUM
 5.4. PH 6 TO 8.5

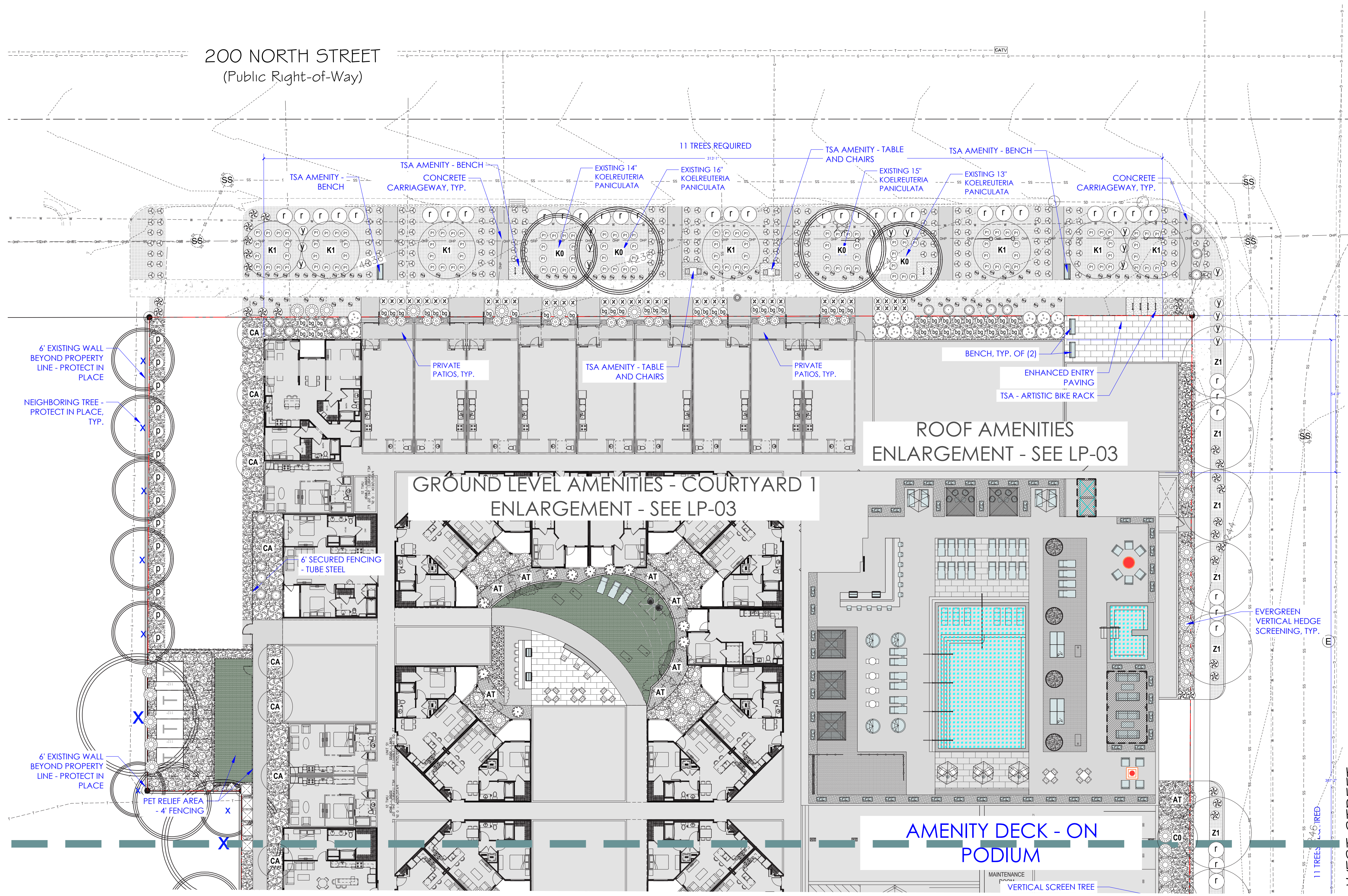
- PLANTING**
 1. ALL PLANT MATERIALS SHALL BE HEALTHY, TYPICAL GROWING HABITS EXPECTED OF THE SELECTED SPECIES. PLANT MATERIALS MAY BE REJECTED BY OWNER, CONTRACTOR, AND/OR LANDSCAPE ARCHITECT PENDING REVIEW
 2. PLANTING HOLES SHALL BE DUG 2X THE WIDTH OF THE ROOTBALL
 3. ADD AGRI-FORM OR EQUIVALENT TABLETS AT A RATE OF 1 PER 1-4 GALLONS // 2 PER 5-10 GALLON // 3 PER 11-20 GALLON // 4 PER TREE
 4. NO PLANTING SUBSTITUTIONS WITHOUT WRITTEN APPROVAL FROM OWNER'S REPRESENTATIVE AND/OR LANDSCAPE ARCHITECT
 5. NO PLANTING SHALL OCCUR UNTIL ROUGH GRADING IS COMPLETE
 6. PRE-EMERGENT HERBICIDE SHALL BE APPLIED TO ALL PLANTING AREAS PRIOR TO TOP DRESSING
 7. ALL PLANTING LOCATIONS TO BE STAKED BY CONTRACTOR PRIOR TO INSTALLATION
 8. CONTRACTOR SHALL MAINTAIN A PLANT WARRANTY, TYPICAL OF (1) YEAR, WITH A (90) DAY MAINTENANCE PERIOD.



DISTRICT NORTH
 SALT LAKE CITY, UT



L2



PLANTING SCHEDULE - DESIGN REVIEW
 *SUBJECT TO CHANGE. ALL FUTURE SELECTIONS PER SLC BMP WATER WISE MANUAL -OR- SELECTED FOR SPECIFIC HYDROZONE AREAS FOR SITE TOLERANT CONDITIONS SUCH AS FULL SHADE

GROUNDCOVERS

QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES
264	1 GAL.	ACTOSTAPHYLOS X C. 'PANCHITO'	PANCHITO MANZANITA	10"x30"	GV3	HARDY G.C.
10	5 GAL.	CAMPISIS RADICANS	TRUMPET VINE	10'+	GV1	VERTICAL CLIMBER
18	1 GAL.	HEUCHERA 'LIME'	GREEN HEUCHERA	24"x24"	SHADE	FULL SHADE
106	1 GAL.	EUONYMUS FORTUNEI 'COLORATA'	LOW EUONYMUS	24"x60"	GV4	EVERGREEN G.C. / VINE
48	1 GAL.	RHUS AROMATICA 'GRO LOW'	CLIMBING WINTERCREEPER	24"x72"	GV1	EVERGREEN G.C.

ORNAMENTAL GRASSES

QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES
195	1 GAL.	BOUTELOUA 'BLOND AMBITION'	BLONDE AMBITION GRASS	18"x18"	TW0	WINTER INTEREST
47	2 GAL.	MISCANTHUS SINENSIS 'ADAGIO'	DWARF MAIDEN GRASS	42"x42"	TW2	LIGHT SHADE
52	1 GAL.	CAREX DIVULSA	GRASSLAND SEDGE	16"x20"	TW3	SHADE TOLERANT

PERENNIALS

QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES
52	1 GAL.	LAVANDULA 'MUNSTEAD'	MUNSTEAD LAVENDER	18"x18"	P2	SUSTAINED BLOOMS
120	1 GAL.	PEROVSKIA ATRIPLICIFOLIA	RUSSIAN SAGE	48"x42"	P1	FULL SUN

DECIDUOUS SHRUBS

QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES
4	2 GAL.	BERBERIS THUNBERGII 'BAILANNA'	GREEN BARBERRY	30"x48"	SD3	FULL SUN // PART SHADE
10	2 GAL.	CORNUS ALBA 'BAILHALO' 'IVORY'	VARIEGATED DOGWOOD	6'x6'	SHADE	SHADE TOLERANT
15	2 GAL.	CORNUS 'KELSY'	KELSEY DOGWOOD	3'x3'	SHADE	SHADE TOLERANT
29	2 GAL.	COTONEASTER LUCIDUS	PEKING CONTEASTER	6'x5'	SD4	FULL SUN
11	2 GAL.	RHAMNUS FRANGULA 'RON WILLIAMS'	COLUMNAR BUCKTHORN	8'x3'	SD3	COLUMNAR ACCENT

EVERGREEN SHRUBS

QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES
52	2 GAL.	BUXUS 'GLENCOE'	BOXWOOD	24"x36"	SHADE	SHADE HEDGE
24	2 GAL.	BUXUS SEMP. 'GRAHAM BLANDY'	COLUMNAR BOXWOOD	84"x46"	SHADE	SHADE - VERTICAL
34	2 GAL.	EUONYMUS FORTUNEI 'WHITE-N-GOLD'	VARIEGATED WINTERCREEPER	24"x42"	SE4	SUN / SHADE
70	2 GAL.	HESPERALOE PARVIFLORA	RED YUCCA	3'x3'	SE0	LOW MAINTENANCE
23	2 GAL.	LIGUSTRUM VULGARE 'LODENSE'	LODENSE PRIVET	5'x5'	SE4	LOW MAINTENANCE
52	5 GAL.	FINUS MUGO 'MOPS'	DWARF PINE SHRUB	4'x4'	SE2	EVERGREEN CONIFER
14	5 GAL.	PRUNUS LAUROCERASUS 'NANA'	COMPACT LAUREL CHERRY	4'x4'	SE2	EVERGREEN CONIFER
81	5 GAL.	TAXUS X MEDIA 'DENSIFORMIS'	DENSE YEW	4'x4'	SE3	SUN / SHADE
17	3 GAL.	YUCCA FILAMENTOSA 'COLORGUARD'	VARIEGATED ADAM'S NEEDLE	3'x3'	SE0	FULL SUN ACCENT

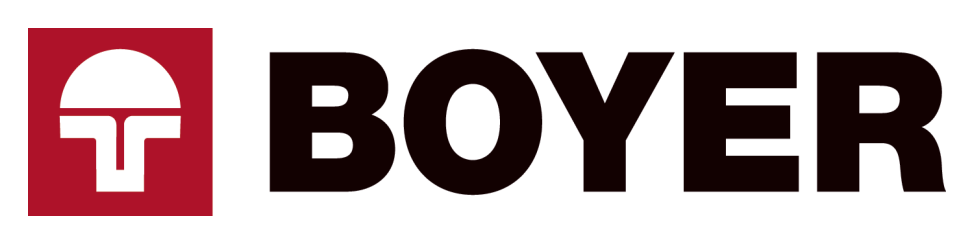
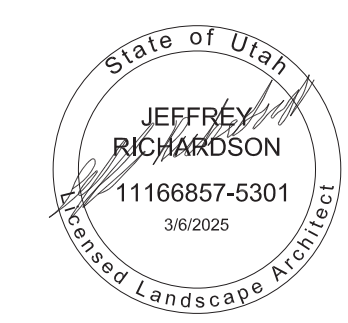
PARKWAY PLANTING - URBAN FORESTRY REQUIREMENTS

ALL TREES IN PUBLIC R.O.W. TO BE 2" CALLIPER - LOCATED:
 5' from water meter and/or utility box
 10' from fire hydrant
 5-10' from residential driveway
 5-10' from property line of adjoining parcel
 5-10' from non-traffic conducting signage
 5-10' from utility pole and/or light
 20' from an unregulated intersection (20' back from intersecting sidewalks)
 30' from stop signs
 30' from commercial driveway and/or alley
 40' from an intersection with traffic lights (40' back from intersecting sidewalks)
 20-30' from a tree that is medium in size at maturity (30 to 50' tall)

PLANTING SCHEDULE - DESIGN REVIEW
 *SUBJECT TO CHANGE. ALL FUTURE SELECTIONS PER SLC BMP WATER WISE MANUAL -OR- SELECTED FOR SPECIFIC HYDROZONE AREAS FOR SITE TOLERANT CONDITIONS SUCH AS FULL SHADE

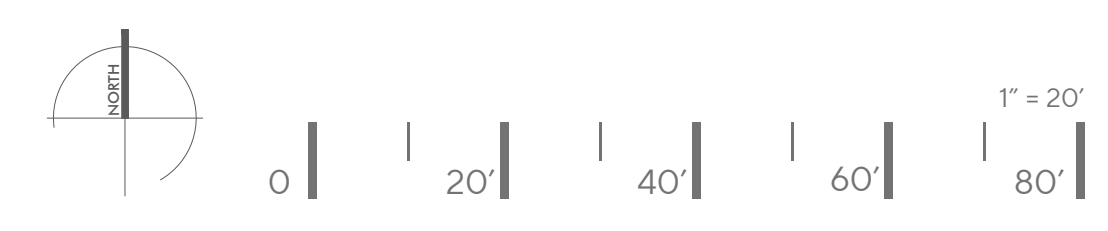
TREES

'MBOL	QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES	CANOPY COVERAGE
AT	20	1.5" CAL.	ACER TATARICUM 'HOT WINGS' - MULTI	HOT WINGS MULTI STEM MAPLE	15'x15'	TD3	PRIVATE ACCENT TREE	100X37 = 3,700
CO	3	1.5" CAL.	CARPINUS BETULUS 'FRANS FONTAINE'	VERTICAL HORNBEAM	35'x15'	TD3	PRIVATELY INSTALLED SCREEN 'URBAN FORESTRY TREE	100X5 = 500
CA	15	1.5" CAL.	CERCIS CANADENSIS 'ACE OF HEARTS'	EASTERN REDBUD	12'x12'	TD4	PRIVATE ACCENT TREE	80X15 = 1,200
K0	4	EXISTING	KOELREUTERIA PANICULATA	GOLDENRAIN TREE	NA	NA	*STREET TREE, EXISTING	2,200 S.F.
K1	7	2" CAL.	KOELREUTERIA PANICULATA	GOLDENRAIN TREE	25'x25'	TD2	*STREET TREE, UF APPROVED	200X7 = 1,400
PC	6	6'-8" BB	PICEA PUNGENS - COLORADO GREEN	GREEN SPRUCE	45'x20'	TE3	PRIVATE CONIFER	150X6 = 900
P1	8	6" BB	PICEA ABIES 'CUPRESSINA'	COLUMNAR NORWAY SPRUCE	20'x8'	TE3	PRIVATE CONIFER - VERTICAL	20X8 = 160
Z1	11	2" CAL.	ZELKOVA SERRATA 'SCHMIDFLOW'	WIRELESS ZELKOVA	30'x35'	TD4	*STREET TREE, UF APPROVED *UNDER POWER LINES	250X11 = 2,750



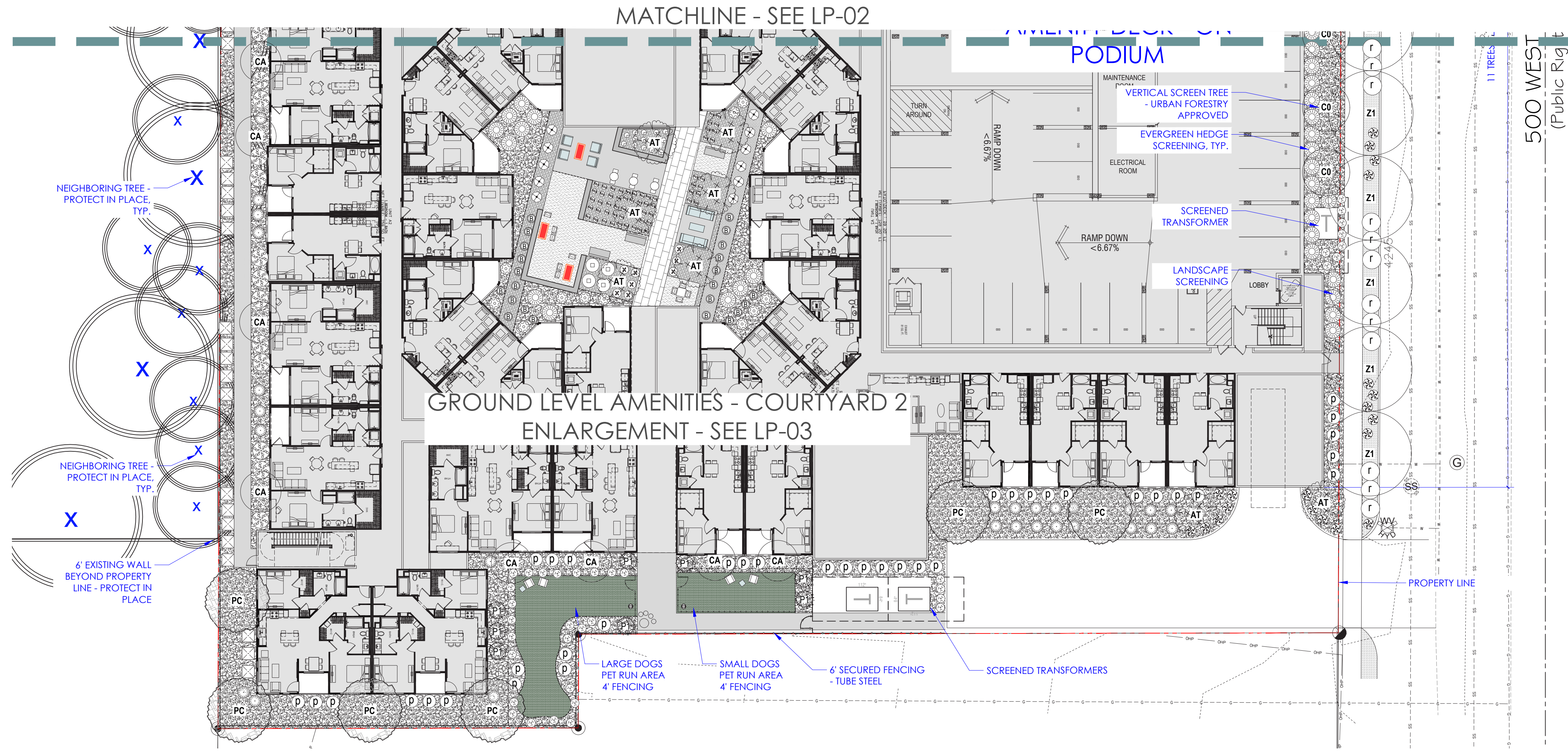
DISTRICT NORTH
 SALT LAKE CITY, UT

PRELIMINARY PLANTING



L3

JOB NO. 2024-0651
 DATE 03-06-2025
 3/18



PLANTING SCHEDULE - DESIGN REVIEW
 *SUBJECT TO CHANGE, ALL FUTURE SELECTIONS PER SLC BMP WATER WISE MANUAL -OR- SELECTED FOR SPECIFIC HYDROZONE AREAS FOR SITE TOLERANT CONDITIONS SUCH AS FULL SHADE

SYMBOL	QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES
AT	20	1.5" CAL.	ACER TATARICUM 'HOT WINGS' - MULTI	HOT WINGS MULTI STEM MAPLE	15'x15'	TD3	PRIVATE ACCENT TREE
CO	3	1.5" CAL.	CARPINUS BETULUS 'FRANS FONTAINE'	VERTICAL HORNBREAM	35'x15'	TD3	PRIVATELY INSTALLED SCREEN *URBAN FORESTRY TREE
CA	15	1.5" CAL.	CERCIS CANADENSIS 'ACE OF HEARTS'	EASTERN REDBUD	12'x12'	TD4	PRIVATE ACCENT TREE
K0	4	EXISTING	KOELREUTERIA PANICULATA	GOLDENRAIN TREE	NA	NA	*STREET TREE, EXISTING
K1	7	2" CAL.	KOELREUTERIA PANICULATA	GOLDENRAIN TREE	25'x25'	TD2	*STREET TREE, UF APPROVED
PC	6	6'-8" BB	PICEA PUNGENS - COLORADO GREEN	GREEN SPRUCE	45'x20'	TE3	PRIVATE CONIFER
P1	8	6" BB	PICEA ABIES 'CUPRESSINA'	COLUMNAR NORWAY SPRUCE	20'x8'	TE3	PRIVATE CONIFER - VERTICAL
Z1	11	2" CAL.	ZELKOVA SERRATA 'SCHMIDTLOW'	WIRELESS ZELKOVA	30'x35'	TD4	*STREET TREE, UF APPROVED *UNDER POWER LINES

GROUNDCOVERS							
SYMBOL	QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES
⊕	264	1 GAL.	ACTOSTAPHYLOS X C. 'PANCHITO'	PANCHITO MANZANITA	10'x30"	GV3	HARDY G.C.
⊕	10	5 GAL.	CAMPISIS RADICANS	TRUMPET VINE	10'+	GV1	VERTICAL CLIMBER
⊕	18	1 GAL.	HEUCHERA 'LIME'	GREEN HEUCHERA	24'x24"	SHADE	FULL SHADE
⊕	106	1 GAL.	EUONYMUS FORTUNEI 'COLORATA'	LOW EUONYMUS	24'x60"	GV4	EVERGREEN G.C. / VINE
⊕	48	1 GAL.	RHUS AROMATICA 'GRO LOW'	CLIMBING WINTERCREEPER	24'x72"	GV1	EVERGREEN G.C.

ORNAMENTAL GRASSES							
SYMBOL	QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES
⊕	195	1 GAL.	BOUTELOUA 'BLOND AMBITION'	BLONDE AMBITION GRASS	18'x18"	TW0	WINTER INTEREST
⊕	47	2 GAL.	MISCANTHUS SINENSIS 'ADAGIO'	DWARF MAIDEN GRASS	42'x42"	TW2	LIGHT SHADE
⊕	52	1 GAL.	CAREX DIVULSA	GRASSLAND SEDGE	16'x20"	TW3	SHADE TOLERANT

PERENNIALS							
SYMBOL	QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES
⊕	52	1 GAL.	LAVANDULA 'MUNSTEAD'	MUNSTEAD LAVENDER	18'x18"	P2	SUSTAINED BLOOMS
⊕	120	1 GAL.	PEROVSKIA ATRIPLICIFOLIA	RUSSIAN SAGE	48'x42"	P1	FULL SUN

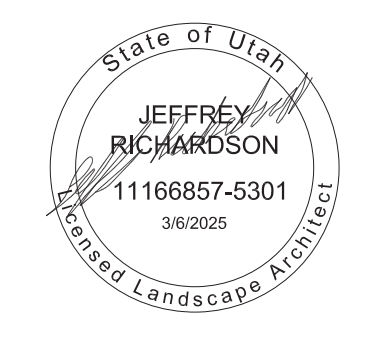
DECIDUOUS SHRUBS							
SYMBOL	QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES
⊕	4	2 GAL.	BERBERIS THUNBERGII 'BAILANNA'	GREEN BARBERRY	30'x30"	SD3	FULL SUN // PART SHADE
⊕	10	2 GAL.	CORNUS ALBA 'BAILHALO' IVORY'	VARIEGATED DOGWOOD	6'x6'	SHADE	SHADE TOLERANT
⊕	15	2 GAL.	CORNUS 'KELSEY'	KELSEY DOGWOOD	3'x3'	SHADE	SHADE TOLERANT
⊕	29	2 GAL.	COTONEASTER LUCIDUS	PEKING CONTEASTER	6'x5'	SD4	FULL SUN
⊕	11	2 GAL.	RHAMNUS FRANGULA 'RON WILLIAMS'	COLUMNAR BUCKTHORN	8'x3'	SD3	COLUMNAR ACCENT

EVERGREEN SHRUBS							
SYMBOL	QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES
⊕	52	2 GAL.	BUXUS 'GLENCOE'	BOXWOOD	24'x36"	SHADE	SHADE HEDGE
⊕	24	2 GAL.	BUXUS SEMP. 'GRAHAM BLANDY'	COLUMNAR BOXWOOD	84'x46"	SHADE	SHADE - VERTICAL
⊕	34	2 GAL.	EUONYMUS FORTUNEI 'WHITE-N-GOLD'	VARIEGATED WINTERCREEPER	24'x42"	SE4	SUN / SHADE
⊕	70	2 GAL.	HESPERALOE PARVIFLORA	RED YUCCA	3'x3'	SE0	LOW MAINTENANCE
⊕	23	2 GAL.	LIGUSTRUM VULGARE 'LODENSE'	LODENSE PRIVET	5'x5'	SE4	LOW MAINTENANCE
⊕	52	5 GAL.	PINUS MUGO 'MOPS'	DWARF PINE SHRUB	4'x4'	SE2	EVERGREEN CONIFER
⊕	14	5 GAL.	PRUNUS LAUROCERASUS 'NANA'	COMPACT LAUREL CHERRY	4'x4'	SE2	EVERGREEN CONIFER
⊕	81	5 GAL.	TAXUS X MEDIA 'DENSIFORMIS'	DENSE YEW	4'x4'	SE3	SUN / SHADE
⊕	17	3 GAL.	YUCCA FILAMENTOSA 'COLORGUARD'	VARIEGATED ADAM'S NEEDLE	3'x3'	SE0	FULL SUN ACCENT

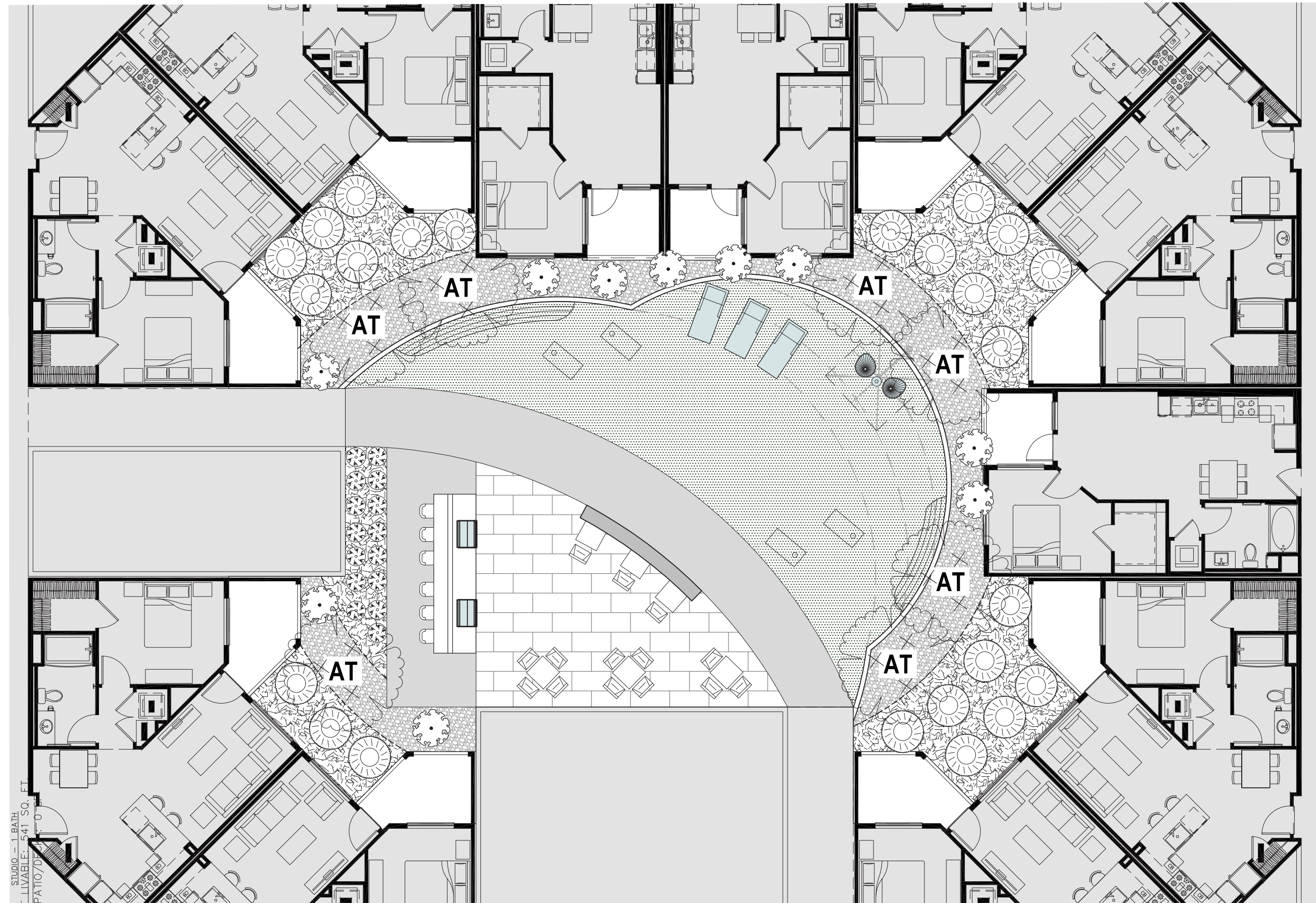
PARKWAY PLANTING - URBAN FORESTRY REQUIREMENTS

ALL TREES IN PUBLIC R.O.W. TO BE 2" CALLIPER - LOCATED:

- 5' from water meter and/or utility box
- 10' from fire hydrant
- 5-10' from residential driveway
- 5-10' from property line of adjoining parcel
- 5-10' from non-traffic conducting signage
- 5-10' from utility pole and/or light
- 20' from an unregulated intersection (20' back from intersecting sidewalks)
- 30' from stop signs
- 30' from commercial driveway and/or alley
- 40' from an intersection with traffic lights (40' back from intersecting sidewalks)
- 20-30' from a tree that is medium in size at maturity (30 to 50' tall)



ENLARGEMENT - COURTYARD 1
GROUND LEVEL



ENLARGEMENT - COURTYARD 2
GROUND LEVEL



PLANTING SCHEDULE - DESIGN REVIEW

*SUBJECT TO CHANGE, ALL FUTURE SELECTIONS PER SLC BMP WATER WISE MANUAL -OR- SELECTED FOR SPECIFIC HYDROZONE AREAS FOR SITE TOLERANT CONDITIONS SUCH AS FULL SHADE

SYMBOL	QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES
AT	20	1.5" CAL.	ACER TATARICUM 'HOT WINGS' - MULTI	HOT WINGS MULTI STEM MAPLE	15x15'	TD3	PRIVATE ACCENT TREE
CO	3	1.5" CAL.	CARPINUS BETULUS 'FRANS FONTAINE'	VERTICAL HORNBEAM	35x15'	TD3	PRIVATELY INSTALLED SCREEN / URBAN FORESTRY TREE
CA	15	1.5" CAL.	CERCIS CANADENSIS 'ACE OF HEARTS'	EASTERN REDBUD	12x12'	TD4	PRIVATE ACCENT TREE
K0	4	EXISTING	KOELREUTERIA PANICULATA	GOLDENRAIN TREE	NA	NA	*STREET TREE, EXISTING
K1	7	2" CAL.	KOELREUTERIA PANICULATA	GOLDENRAIN TREE	25x25'	TD2	*STREET TREE, UF APPROVED
PC	6	6'-8" BB	PICEA PUNGENS - COLORADO GREEN	GREEN SPRUCE	45x20'	TE3	PRIVATE CONIFER
P1	8	6" BB	PICEA ABIES 'CUPRESSINA'	COLUMNAR NORWAY SPRUCE	20x8'	TE3	PRIVATE CONIFER - VERTICAL
Z1	11	2" CAL.	ZELKOVA SERRATA 'SCHMIDTLOW'	WIRELESS ZELKOVA	30x35'	TD4	*STREET TREE, UF APPROVED / UNDER POWER LINES

GROUNDCOVERS

QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES
264	1 GAL.	ACTOSTAPHYLOS X C. 'PANCHITO'	PANCHITO MANZANITA	10"x30"	GV3	HARDY G.C.
10	5 GAL.	CAMPIS RADICANS	TRUMPET VINE	10'+	GV1	VERTICAL CLIMBER
18	1 GAL.	HEUCHERA 'LIME'	GREEN HEUCHERA	24"x24"	SHADE	FULL SHADE
106	1 GAL.	EUONYMUS FORTUNEI 'COLORATA'	LOW EUONYMUS	24"x60"	GV4	EVERGREEN G.C. / VINE
48	1 GAL.	RHUS AROMATICA 'GRO LOW'	CLIMBING WINTERCREEPER	24"x72"	GV1	EVERGREEN G.C.

ORNAMENTAL GRASSES

QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES
195	1 GAL.	BOUTELOUA 'BLOND AMBITION'	BLONDE AMBITION GRASS	18"x18"	TW0	WINTER INTEREST
47	2 GAL.	MISCANTHUS SINENSIS 'ADAGIO'	DWARF MAIDEN GRASS	42"x42"	TW2	LIGHT SHADE
52	1 GAL.	CAREX DIVULSA	GRASSLAND SEDGE	16"x20"	TW3	SHADE TOLERANT

PERENNIALS

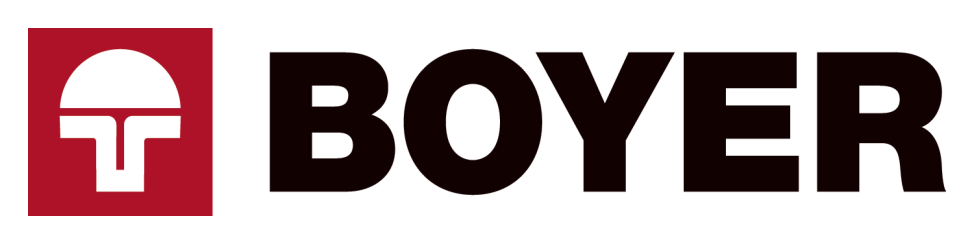
QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES
52	1 GAL.	LAVANDULA 'MUNSTEAD'	MUNSTEAD LAVENDER	18"x18"	P2	SUSTAINED BLOOMS
120	1 GAL.	PEROVSKIA ATRIPLICIFOLIA	RUSSIAN SAGE	48"x42"	P1	FULL SUN

DECIDUOUS SHRUBS

QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES
4	2 GAL.	BERBERIS THUNBERGII 'BAILANNA'	GREEN BARBERRY	30"x48"	SD3	FULL SUN // PART SHADE
10	2 GAL.	CORNUS ALBA 'BAILHALO' IVORY'	VARIEGATED DOGWOOD	6'x6'	SHADE	SHADE TOLERANT
15	2 GAL.	CORNUS 'KELSY'	KELSEY DOGWOOD	3'x3'	SHADE	SHADE TOLERANT
29	2 GAL.	COTONEASTER LUCIDUS	PEKING CONTEASTER	6'x5'	SD4	FULL SUN
11	2 GAL.	RHAMNUS FRANGULA 'RON WILLIAMS'	COLUMNAR BUCKTHORN	8'x3'	SD3	COLUMNAR ACCENT

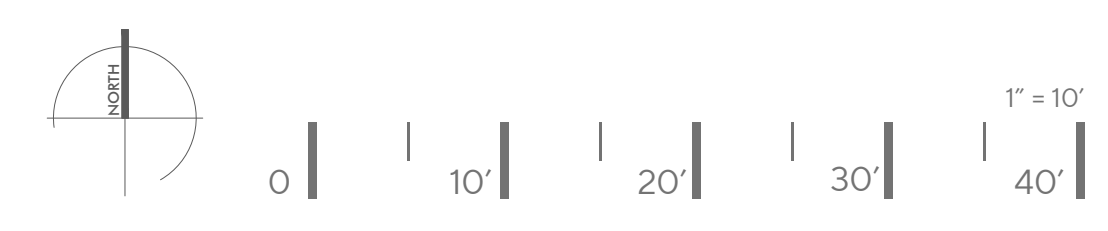
EVERGREEN SHRUBS

QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES
52	2 GAL.	BUXUS 'GLENCOE'	BOXWOOD	24"x36"	SHADE	SHADE HEDGE
24	2 GAL.	BUXUS SEMP. 'GRAHAM BLANDY'	COLUMNAR BOXWOOD	84"x46"	SHADE	SHADE - VERTICAL
34	2 GAL.	EUONYMUS FORTUNEI 'WHITE-N-GOLD'	VARIEGATED WINTERCREEPER	24"x42"	SE4	SUN / SHADE
70	2 GAL.	HESPERALOE PARVIFLORA	RED YUCCA	3'x3'	SE0	LOW MAINTENANCE
23	2 GAL.	LIGUSTRUM VULGARE 'LODENSE'	LODENSE PRIVET	5'x5'	SE4	LOW MAINTENANCE
52	5 GAL.	PINUS MUGO 'MOPS'	DWARF PINE SHRUB	4'x4'	SE2	EVERGREEN CONIFER
14	5 GAL.	PRUNUS LAUROCERASUS 'NANA'	COMPACT LAUREL CHERRY	4'x4'	SE2	EVERGREEN CONIFER
81	5 GAL.	TAXUS X MEDIA 'DENSIFORMIS'	DENSE YEW	4'x4'	SE3	SUN / SHADE
17	3 GAL.	YUCCA FILAMENTOSA 'COLORGUARD'	VARIEGATED ADAM'S NEEDLE	3'x3'	SE0	FULL SUN ACCENT



DISTRICT NORTH
SALT LAKE CITY, UT

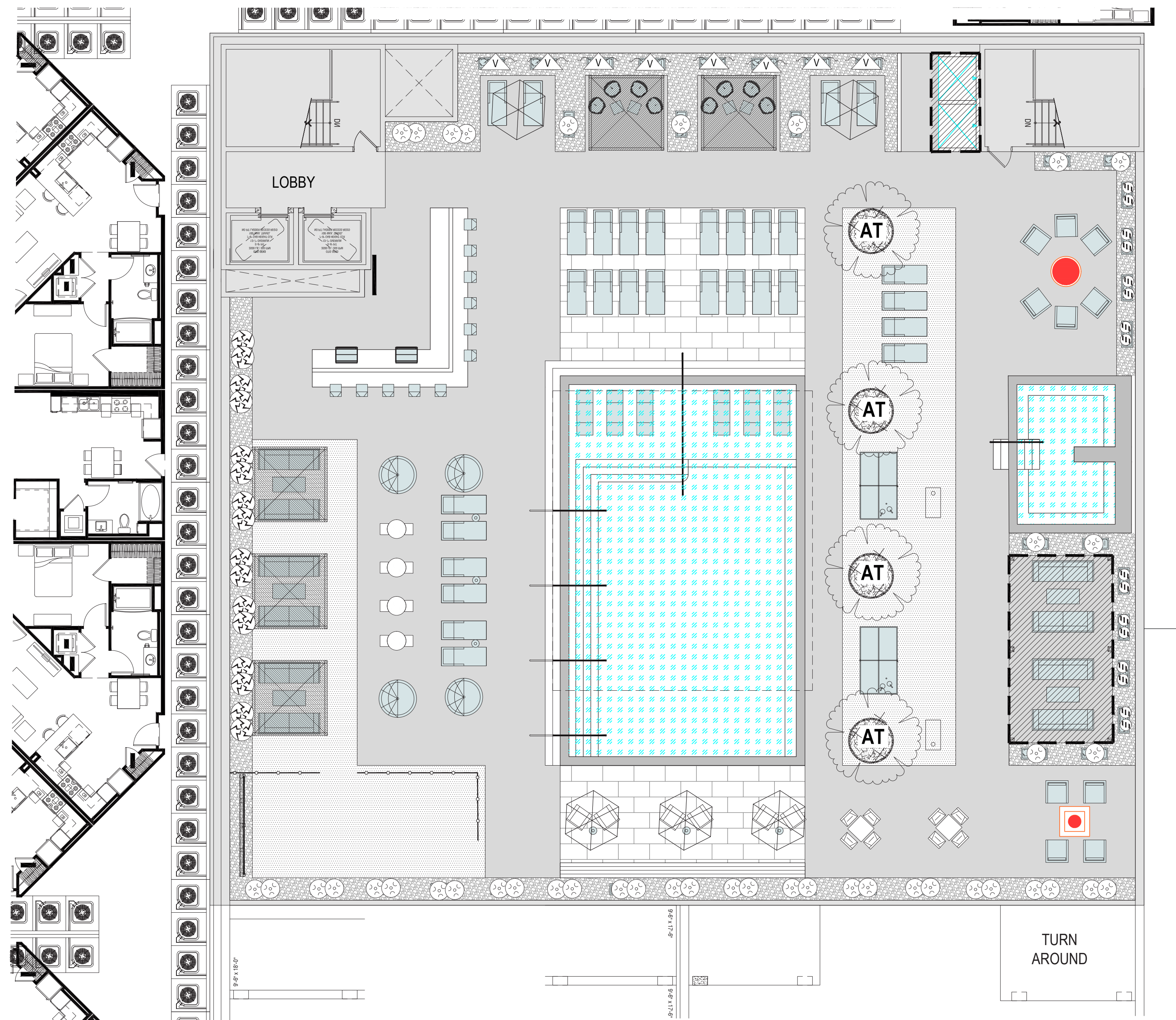
COURTYARD ENLARGEMENT



L5

JOB NO. 2024-0651
DATE 03-06-2025
3/18

ENLARGEMENT - ROOF DECK AMENITIES



PLANTING SCHEDULE - DESIGN REVIEW
 *SUBJECT TO CHANGE. ALL FUTURE SELECTIONS PER SLC BMP WATER WISE MANUAL -OR- SELECTED FOR SPECIFIC HYDROZONE AREAS FOR SITE TOLERANT CONDITIONS SUCH AS FULL SHADE

TREES							
MBOL	QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES
AT	20	1.5" CAL.	ACER TATARICUM 'HOT WINGS' - MULTI	HOT WINGS MULTI STEM MAPLE	15x15'	TD3	PRIVATE ACCENT TREE
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PC	6	6'-8" BB	PICEA PUNGENS - COLORADO GREEN	GREEN SPRUCE	45x20'	TE3	PRIVATE CONIFER
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Z1	11	2" CAL.	ZELKOVA SERRATA 'SCHMIDLOW'	WIRELESS ZELKOVA	30'x35'	TD4	*STREET TREE, UF APPROVED *UNDER POWER LINES

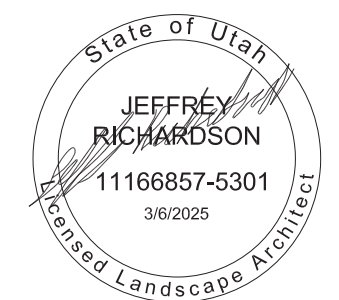
GROUNDCOVERS							
QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES	
264	1 GAL.	ACTOSTAPHYLOS X C. 'PANCHITO'	PANCHITO MANZANITA	10'x30"	GV3	HARDY G.C.	
10	5 GAL.	CAMPISIS RADICANS	TRUMPET VINE	10'+	GV1	VERTICAL CLIMBER	
18	1 GAL.	HEUCHERA 'LIME'	GREEN HEUCHERA	24'x24"	SH4	FULL SHADE	
106	1 GAL.	EUONYMUS FORTUNEI 'COLORATA'	LOW EUONYMUS	24'x60"	GV4	EVERGREEN G.C. / VINE	
48	1 GAL.	RHUS AROMATICA 'GRO LOW'	CLIMBING WINTERCREEPER	24'x72"	GV1	EVERGREEN G.C.	

ORNAMENTAL GRASSES							
QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES	
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PERENNIALS							
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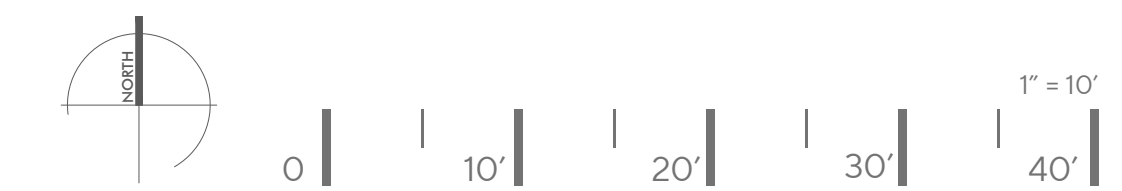
DECIDUOUS SHRUBS							
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10	2 GAL.	CORNUS ALBA 'BAILHALO' IVORY'	VARIEGATED DOGWOOD	6'x6'	SHADE	SHADE TOLERANT	
15	2 GAL.	CORNUS 'KELSEY'	KELSEY DOGWOOD	3'x3'	SHADE	SHADE TOLERANT	
29	2 GAL.	COTONEASTER LUCIDUS	PEKING CONTEASTER	6'x5'	SD4	FULL SUN	
11	2 GAL.	RHAMNUS FRANGULA 'RON WILLIAMS'	COLUMNAR BUCKTHORN	8'x3'	SD3	COLUMNAR ACCENT	

EVERGREEN SHRUBS							
QTY	SIZE	BOTANICAL NAME	COMMON NAME	MATURE HxW	ZONE	NOTES	
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24	2 GAL.	BUXUS SEMP. 'GRAHAM BLANDY'	COLUMNAR BOXWOOD	84'x46"	SHADE	SHADE - VERTICAL	
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17	3 GAL.	YUCCA FILAMENTOSA 'COLORGUARD'	VARIEGATED ADAM'S NEEDLE	3'x3'	SE0	FULL SUN ACCENT	



DISTRICT NORTH
 SALT LAKE CITY, UT

ROOF DECK ENLARGEMENT



L6

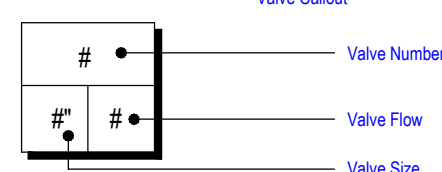
JOB NO. 2024-0651
 DATE 03-06-2025
 318

IRRIGATION SCHEDULE

SYMBOL	MANUFACTURER/MODEL/DESCRIPTION	PSI	DETAIL
	Hunter MP Corner PROS-06-PRS30-CV-F Turf Rotator, 6in. pop-up with factory installed check valve, floguard, pressure regulated to 30 psi, MP Rotator nozzle on PRS30 body. T=Turquoise adj arc 45-105	30	1/LI-05
	Hunter MP800SR PROS-06-PRS30-CV-F Turf Rotator, 6in. pop-up with check valve, floguard, pressure regulated to 30 psi, MP Rotator nozzle on PRS30 body. ADJ=Orange and Gray (arc 90-210), 360-Lime Green and Gray (arc 360)	30	1/LI-05
	Hunter MP815 PROS-06-PRS30-CV-F Turf Rotator, 6in. pop-up with check valve, floguard, pressure regulated to 30 psi, MP Rotator nozzle on PRS30 body. M=Maroon and Gray adj arc 90 to 210, L=Light Blue and Gray 210 to 270 arc, O=Olive and Gray 360 arc.	30	1/LI-05

SYMBOL	MANUFACTURER/MODEL/DESCRIPTION	PSI	DETAIL
	Wide Flow IVM Drip Control Kit for Commercial Applications. 1in. Ball Valve with 1in. PESBIVM Smart Valve w/ factory installed IVM-SOL 0.3-20 gpm and 1in. Pressure Regulating 40psi Quick-Check Basket Filter 0.3-20 gpm Netafim TLSOV	2/LI-05	
	Netafim TLSOV- 1/2in. manual flush valve, barbed insert. Install in 10in. box, with adequate blank or in-cobrain. tubing to extend valve out of valve box.	3/LI-05	
	2/3 in fits Techline HCVR, HCVR-RW/RWP, CV, DL, RW and RWP driplines, and PE irrigation hose Hunter ECO-ID	4/LI-05	
	Free Drip Ring Netafim TLCV-06-12 tree ring, 6 GPH, 12" spacing.	30	5/LI-05
	Area to Receive Dripline Netafim TLCV-06-12 Techline Pressure Compensating Landscape Dripline with Check Valve. 0.6 GPH emitters at 12" O.C. Dripline laterals spaced at 24" apart, with emitters offset for triangular pattern. 17mm.	20	6/LI-05

SYMBOL	MANUFACTURER/MODEL/DESCRIPTION	DETAIL
	Rain Bird PESB-IVM 1in., 1-1/2in., 2in. Plastic Industrial Smart Valves w/ Factory Installed IVM-SOL. Low Flow Operating Capability, Globe Configuration. With Scrubber Technology for Reliable Performance in Dirty Water Irrigation Applications.	7/LI-05
	1in. Brass Quick-Coupling Valve, with Corrosion-Resistant Stainless Steel Spring, Locking Thermoplastic Rubber Cover, and 1-Piece Body. Shut Off Valve - Mainline	8/LI-05
	Nibco T-113-K - Class 125 bronze gate shut off valve with cross handle, same size as mainline pipe diameter at valve location.	9/LI-05
	Rain Bird PESB-IVM (MV) 1-1/2" 1in., 1-1/2in., 2in. Plastic Industrial Smart Valves w/ Factory Installed IVM-SOL. Low Flow Operating Capability, Globe Configuration. With Scrubber Technology for Reliable Performance in Dirty Water Irrigation Applications.	1/LI-06
	Manual Drain Valve Nibco T-113-K - Class 125 bronze gate shut off valve with cross handle, 3/4" size.	2/LI-06
	Stop & Waste Valve Mueller H-10284N Mark II Oriseal Curb Valve, size per line.	3/LI-06
	Febco 825Y 1" Reduced Pressure Backflow Preventer Rain Bird ESPXIVM	4/LI-06
	60 Station, 2-Wire Controller w/ Smart Valve Technology. (1) ESPLXIVM 60-Station, Indoor/Outdoor, Plastic Wall-Mount Cabinet. System Requirements: Rain Bird LXIVM-XXX Integrated Valve Modules & 2-Wire Devices. Use Paige Electric Cable P7072D & Rain Bird WC20 Dry Splices ONLY. Ground System w/ (X) LXIVMSD Surge Device in Rain Bird Round Valve Boxes. Install Per Manufacturer's Recommendations.	5/LI-06
	1in. Ultrasonic Flow Sensors, with Glass Filled Nylon Body. Operating Range .3 GPM-50 GPM. Size for Flow Not According to Pipe Size.	6/LI-06
	Rain Bird CLP05/BASCI 5 HP suction boost model. Up to 53 psi and 120 GPM.	To Be Determined
	Mainline Penetration Point Landscape Contractor shall connect to stub provided by Plumbing Contractor, coordinate as needed and see Plumbing Plans for additional information.	7/LI-06
	Water Meter 1" Dedicated Irrigation Meter - Point of Connection Equipment located in the Mechanical Room within the parking garage.	See Civil Plans
	Point of Connection Pool Deck Penetration	See Plumbing Plans
	Irrigation Lateral Line: PVC Schedule 40	8/LI-06
	Irrigation Mainline: PVC Schedule 40	8/LI-06
	Irrigation Mainline: Hi-Temp CPVC Pipe Sch 40-NP Installed by Plumbing Contractor, shown for referenc only, see Plumbing Plans for additional information. Color: Purple.	See Plumbing Plans
	Irrigation Mainline: Type K Copper Pipe Installed by Plumbing Contractor, from Meter to Mechanical Room	See Plumbing Plans
	Pipe Sleeve: PVC Schedule 40 See "Irrigation Sleeving Notes" for additional information.	9/LI-06



VALVE SCHEDULE

NUMBER	MODEL	SIZE	TYPE	GPM	DESIGN PSI	PSI	PSI @ POC	PRECIP
1	Rain Bird	1"	Area for Dripline	7.38	20	33.3	60.8	0.48 in/h
2	RGZ-B00-IVMQ	1"	Area for Dripline	6.76	20	30.2	57.4	0.48 in/h
3	RGZ-B00-IVMQ	1"	Area for Dripline	9.12	20	33.1	58.1	0.48 in/h
4	RGZ-B00-IVMQ	1"	Area for Dripline	9.78	20	33.9	65.1	0.48 in/h
5	RGZ-B00-IVMQ	1"	Turf Rotary	14.0	30	33.4	67.3	1.0 in/h
6	Rain Bird	1"	Area for Dripline	9.11	20	34.0	68.0	0.48 in/h
7	RGZ-B00-IVMQ	1"	Area for Dripline	10.93	20	36.1	71.1	0.48 in/h
8	RGZ-B00-IVMQ	1"	Area for Dripline	13.52	20	40.7	75.7	0.48 in/h
9	RGZ-B00-IVMQ	1"	Turf Rotary	6.08	30	32.0	67.1	0.96 in/h
10	Rain Bird	1"	Area for Dripline	7.69	20	30.7	66.1	0.48 in/h
11	RGZ-B00-IVMQ	1"	Area for Dripline	11.48	20	35.3	70.3	0.48 in/h
12	RGZ-B00-IVMQ	1"	Area for Dripline	5.06	20	28.8	63.6	0.49 in/h
13	RGZ-B00-IVMQ	1"	Area for Dripline	7.89	20	31.5	66.2	0.48 in/h
14	RGZ-B00-IVMQ	1"	Area for Dripline	10.91	20	34.5	68.4	0.48 in/h
15	RGZ-B00-IVMQ	1"	Area for Dripline	10.46	20	34.2	66.2	0.48 in/h
16	RGZ-B00-IVMQ	1"	Area for Dripline	7.06	20	30.1	54.0	0.48 in/h
17	RGZ-B00-IVMQ	1"	Drip Ring	0.72	30	37.0	60.9	0.59 in/h
18	RGZ-B00-IVMQ	1"	Turf Rotary	14.38	30	33.4	59.1	1.1 in/h
19	Rain Bird	1"	Area for Dripline	4.76	20	27.5	53.3	0.48 in/h
20	RGZ-B00-IVMQ	1"	Drip Ring	0.84	30	37.0	62.8	0.59 in/h
21	RGZ-B00-IVMQ	1"	Area for Dripline	6.48	20	29.4	29.6	0.48 in/h
Unknown	Use as indicated for future use)		Cap (as indicated for future use)	12		70	94.1	Unknown

CRITICAL ANALYSIS

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P.O.C. NUMBER: 01
Water Source Information: Dedicated Irrigation Meter - Point of Connection
Equipment located in the Mechanical Room within the parking garage.

FLOW AVAILABLE
Water Meter Size: 1"
Flow Available: 37.5 GPM

PRESSURE AVAILABLE
Static Pressure at POC: 70 PSI
Elevation Change: 5.00 ft
Service Line Size: 2"
Length of Service Line: 20 ft
Booster Pump pressure provided: 35 PSI
Pressure Available: 103 PSI

DESIGN ANALYSIS
Maximum Multi-valve Flow: 30 GPM
Flow Available at POC: 37.5 GPM
Residual Flow Available: 7.5 GPM

Design Pressure: Unknown
Friction Loss: Unknown
Fittings Loss: Unknown
Elevation Loss: 0 PSI
Loss through Valve: Unknown
Pressure Req. at Critical Station: 70.0 PSI
Loss for Fittings: 0.43 PSI
Loss for Main Line: 4.26 PSI
Loss for POC to Valve Elevation: 0 PSI
Loss for Backflow: 10.5 PSI
Loss for Master Valve: 3.6 PSI
Loss for Water Meter: 5.3 PSI
Critical Station Pressure at POC: 94.1 PSI

IRRIGATION POINT OF CONNECTION NOTES

- POINT OF CONNECTION:** THIS PROJECT HAS A DEDICATED 1" IRRIGATION METER. CONTRACTOR SHALL LOCATE AND CONNECT TO THE STUB (PROVIDED BY OTHERS) INSIDE THE BUILDING MECHANICAL ROOM. LANDSCAPE CONTRACTOR SHALL EXTEND SCH 40 PVC TO ONE GATE VALVE, ONE MANUAL DRAIN, ONE BACKFLOW PREVENTER, ONE MASTER VALVE, ONE FLOW SENSOR, ONE MANUAL DRAIN VALVE, ONE GATE VALVE, AND CONNECT TO THE STUB ENTERING THE WALL (PROVIDED BY OTHERS).
 - THE CONTRACTOR SHALL CONFORM TO ALL LOCAL CODES. THE CONTRACTOR SHALL OBTAIN AND PROVIDE PAYMENT FOR ALL PERMITS ASSOCIATED WITH THIS WORK.
- CONTROLLER LOCATION:** THERE IS ONE CONTROLLER ON THIS PROJECT. THE CONTROLLER SHALL BE WALL MOUNTED IN THE APPROXIMATE LOCATION INDICATED. ON THE PLANS IS FOR REFERENCE ONLY, FINAL LOCATION TO BE DETERMINED. THE CONTRACTOR SHALL REFER TO THE ELECTRICAL PLANS AND COORDINATE 120 VOLT POWER AND ALL NECESSARY CONDUIT AND SLEEVING WITH THE OWNER'S REPRESENTATIVE PRIOR TO START OF WORK. ALL EQUIPMENT AND CONNECTIONS SHALL CONFORM TO ALL LOCAL CODES. REFER TO ELECTRICAL ENGINEERING AND ARCHITECTURAL PLANS FOR ADDITIONAL INFORMATION REGARDING POWER FOR THE CONTROLLER.
 - CONTRACTOR SHALL PROVIDE DIFFERENT COLORED WIRE RUNS TO THE FOUR (4) DIFFERENT LANDSCAPE AREAS OF THE PROJECT (PERIMETER, (2) COURTYARDS, AND ROOF).
- RAIN/FREEZE SENSOR:** MOUNT RAIN BIRD WR2 SENSOR IN CLOSE PROXIMITY OF THE CONTROLLER. SENSOR SHALL BE MOUNTED IN LOCATION IN FULL SUN, OPEN TO RAINFALL, AND AWAY FROM HEAT SOURCES. SENSOR SHALL BE LOCATED NOR MORE THAN 700' (LINE OF SIGHT) FROM THE CONTROLLER. WALL MOUNT THE RECEIVER ADJACENT TO THE CONTROLLER PRIOR TO SENSOR INSTALLATION AND DISCRETELY RUN WIRING TO CONTROLLER. SENSOR SHALL BE SET TO TURN OFF AT 1/4" RAINFALL.
- FLOW SENSOR:** THE RAIN BIRD ULTRASONIC FLOW SENSOR REQUIRES A MINIMUM FLOW OF .3 GPM FOR PROPER FUNCTION. MULTIPLE ZONES MAY BE REQUIRED TO RUN AT THE SAME TIME TO ACHIEVE THE FLOW SENSOR'S MINIMUM FLOW REQUIREMENT. **MAXIMUM FLOW NOT TO EXCEED 50 GPM.**
- SYSTEM PRESSURE & FLOW:** THIS PROJECT HAS BEEN DESIGNED FOR A REQUIRED MINIMUM STATIC PRESSURE OF XX PSI AND A MAXIMUM SAFE FLOW OF 30 GPM. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE OPERATING PRESSURE IN THE FIELD AT THE POINT OF CONNECTION BEFORE CONSTRUCTION BEGINS. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE OWNER'S REPRESENTATIVE IF DEFICIENT EQUIPMENT, LOW PRESSURE OR LOW FLOW CONDITIONS ARE ENCOUNTERED. IF THE CONTRACTOR FAILS TO NOTIFY OWNER'S REPRESENTATIVE OF SUCH DISCREPANCIES, THEN THE CONTRACTOR ASSUMES ALL LIABILITY AND COSTS ASSOCIATED WITH SYSTEM MODIFICATIONS TO ACCOMMODATE THE ACTUAL PRESSURE.

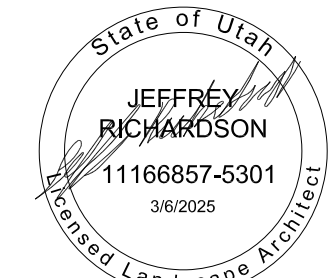
IRRIGATION SLEEVING NOTES

- INSTALLATION OF SLEEVING IS THE RESPONSIBILITY OF THE GENERAL CONTRACTOR. SLEEVES SHALL BE INSTALLED PRIOR TO THE START OF PAVING OPERATIONS. THE GENERAL CONTRACTOR SHALL COORDINATE WITH THE IRRIGATION CONTRACTOR FOR LOCATION AND SIZING OF SLEEVES PRIOR TO THE START OF WORK.
- THE CONTRACTOR SHALL SLEEVE ALL IRRIGATION DISTRIBUTION LINES, VALVE CONTROL WIRES AND COMMUNICATION WIRES, UNDER ALL PAVED SURFACES, WALL FOOTERS, DRAINAGE CHANNELS, INLETS, CATCH BASINS, ETC.
- ALL SLEEVES SHALL EXTEND A **MINIMUM OF 1 FOOT BEYOND EDGE OF ALL OBSTRUCTIONS. NO TEES, ELLS OR OTHER TURNS IN PIPING SHALL BE LOCATED UNDER ANY OBSTRUCTIONS.**
- SLEEVING SHALL BE INSTALLED PER THE SIZES AND QUANTITIES SHOWN ON THE PLANS BASED ON THE CHART BELOW. ALL MAINLINE, VALVE CONTROL AND COMMUNICATION WIRES, LATERALS AND 3/4" POLYETHYLENE DRIP TUBING UNDER PAVED SURFACES ARE TO BE INSTALLED IN SEPARATE SLEEVING.**

SLEEVING PIP. SIZE/WIRE QTY	REQUIRED SLEEVE SIZE AND QTY.
3/4" - 1" PIPING	2" PVC (1)
1-1/4" - 1-1/2" PIPING	3" PVC (1)
CONTROL WIRES	2" PVC (1)

GENERAL IRRIGATION NOTES

- THE CONTRACTOR SHALL PROVIDE ALL LABOR, MATERIALS AND EQUIPMENT NECESSARY TO INSTALL THE IMPROVEMENTS SHOWN ON THE PLANS.
- THE CONTRACTOR SHALL COORDINATE AS NECESSARY WITH THE GENERAL CONTRACTOR AND OWNER'S REPRESENTATIVE FOR SUCCESSFUL COMPLETION OF THIS WORK.
- THE CONTRACTOR ASSUMES ALL LIABILITY ASSOCIATED WITH THE MODIFICATION OF THE IRRIGATION SYSTEM DESIGN WITHOUT NOTIFYING OWNER'S REPRESENTATIVE.
- ALL IRRIGATION EQUIPMENT IS TO BE AS SPECIFIED OR APPROVED EQUAL PER THE DISCRETION OF THE OWNER'S REPRESENTATIVE.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO CONDUCT A THOROUGH SITE INSPECTION AND REVIEW OF THE PROJECT CONSTRUCTION DOCUMENTS INCLUDING BUT NOT LIMITED TO THE FOLLOWING: LANDSCAPE PLAN, UTILITY PLAN, CIVIL PLAN, ARCHITECTURAL PLANS, ELECTRICAL PLANS, GRADING AND DRAINAGE AND ALL ASSOCIATED PLANS THAT AFFECT THIS WORK PRIOR TO BEGINNING CONSTRUCTION. IF THE CONTRACTOR OBSERVES ANY DISCREPANCIES AMONG THE CONSTRUCTION DOCUMENTS AND THE EXISTING CONDITIONS ON SITE, IT IS THE CONTRACTOR'S RESPONSIBILITY TO CONTACT THE OWNER'S REPRESENTATIVE IMMEDIATELY.
- THE CONTRACTOR SHALL CONFORM TO ALL LOCAL AND STATE REGULATIONS TO INSTALL THE IRRIGATION SYSTEM AND ITS COMPONENTS PER THE MANUFACTURER'S SPECIFICATIONS AS SHOWN ON THESE DOCUMENTS. THE CONTRACTOR SHALL OBTAIN AND PROVIDE PAYMENT FOR ALL PERMITS REQUIRED BY ANY AND ALL LOCAL AND STATE AGENCIES AND UTILITY COMPANIES HAVING JURISDICTION OVER THIS SITE.
- THE CONTRACTOR MUST VERIFY THE LOCATION OF ALL UNDERGROUND UTILITIES PRIOR TO CONSTRUCTION. IF THE CONTRACTOR FAILS TO DO SO AND DAMAGES ANY UNDERGROUND UTILITIES THROUGH THE COURSE OF HIS WORK, THE CONTRACTOR SHALL PAY FOR ANY/AND ALL REPAIR WORK ASSOCIATED WITH SAID DAMAGES.
- IT IS THE INTENT OF THIS DESIGN THAT ALL IRRIGATION EQUIPMENT BE INSTALLED WITHIN THE PROJECT LIMITS AND WITHIN LANDSCAPE AREAS. ANY EQUIPMENT SHOWN OUTSIDE OF THESE LIMITS IS SHOWN FOR GRAPHICAL CLARITY ONLY. IF THERE IS A QUESTION REGARDING THE LOCATION OF ANY COMPONENT OF THE IRRIGATION SYSTEM, IT IS THE CONTRACTOR'S RESPONSIBILITY TO CONTACT THE OWNER'S REPRESENTATIVE. IF THE CONTRACTOR NEGLECTS TO NOTIFY THE NECESSARY PARTIES, THE CONTRACTOR SHALL PAY FOR ANY REPLACEMENT OR MODIFICATION TO INSURE PROPER LOCATION AND OPERATION OR THE IRRIGATION SYSTEM AND IT'S COMPONENTS.
- PLANT MATERIAL LOCATIONS TAKE PRECEDENTS OVER IRRIGATION LINES. THE CONTRACTOR SHALL COORDINATE INSTALLATION OF IRRIGATION EQUIPMENT SO THAT IT DOES NOT INTERFERE WITH THE PLANTING OF TREES OR OTHER LANDSCAPE MATERIAL.
- IF IRRIGATION SLEEVING IS REQUIRED A MINIMUM OF 10' HORIZONTAL SEPARATION BETWEEN BANKS OF SLEEVES USED FOR OTHER ON-SITE UTILITIES. IT IS THE CONTRACTOR'S RESPONSIBILITY TO INSURE ADEQUATE VERTICAL SEPARATION BETWEEN ALL IRRIGATION DISTRIBUTION LINES AND ALL UTILITIES (EXISTING OR PROPOSED) CONDUIT, STORM WATER COMPONENTS, DRAINS, ETC.
- ALL VALVE BOXES / LIDS SHALL BE PLASTIC, COLOR GREEN OR TAN, WITH LOCKING COVERS, PER THE CONSTRUCTION DETAILS. ALL VALVE BOXES SHALL BE INSTALLED A MINIMUM OF 1'-0" FROM THE EDGE OF PAVED SURFACES AND 3'-0" FROM THE CENTERLINE OF DRAINAGE SWALES OR RETENTION BASINS. THE CONTRACTOR SHALL ADJUST ALL VALVE BOXES TO BE FLUSH FINISH GRADE AS PER THE CONSTRUCTION DETAIL.
- THE CONTRACTOR SHALL COORDINATE INSTALLATION OF DRIP IRRIGATION LINES PRIOR TO INSTALLATION OF PLANT MATERIAL WITH THE OWNER'S REPRESENTATIVE.
- LAYOUT EMITTER TUBING PARALLEL TO TOPOGRAPHY WHEREVER POSSIBLE. INSTALL FLUSHABLE TYPE END CAP AT ENDS OF ALL 3/4" PVC DRIP LATERALS AND FLUSH THOROUGHLY BEFORE INSTALLING EMITTERS.
- SHRUBS SHALL BE IRRIGATED BY POINT SOURCE EMITTERS. SEE IRRIGATION LEGEND AND PLANS FOR DETAILS.
- TREES SHALL BE IRRIGATED BY TWO (2) POP-UP SPRAY HEADS. SEE IRRIGATION LEGEND AND PLANS FOR DETAILS.
- THE CONTRACTOR SHALL FINE TUNE / ADJUST THE IRRIGATIONS SYSTEM TO AVOID / REDUCE OVER-SPRAY ONTO HARD SURFACES BY ADJUSTING NOZZLE DIRECTION AND NOZZLE RADIUS.
- THE CONTRACTOR SHALL EXTEND THREE (3) SPARE CONTROL WIRES (ONE (1) COMMON AND TWO (2) CONTROL WIRES) FROM THE CONTROLLER TO THE END OF THE MAINLINE OR AS SHOWN ON THE PLANS. INSTALL SPARE WIRES IN A 10" ROUND VALVE BOX WITH A QUICK COUPLING VALVE. USE ONLY #14 OR #12 (WHEN NECESSARY) DIRECT BURY COPPER WIRE FOR ALL UNDERGROUND WIRING.
- GROUNDING FOR THE IRRIGATION CONTROLLER TO BE INSTALLED PER THE MANUFACTURER'S SPECIFICATIONS AND PER THE AMERICAN SOCIETY OF IRRIGATION CONSULTANTS GUIDELINE 100-2002 FOR EARTH GROUNDING ELECTRONIC EQUIPMENT IN IRRIGATION SYSTEMS FOUND AT www.asic.org/Design_Guides.aspx. CONTACT THE MANUFACTURER FOR ADDITIONAL TECHNICAL ASSISTANCE.

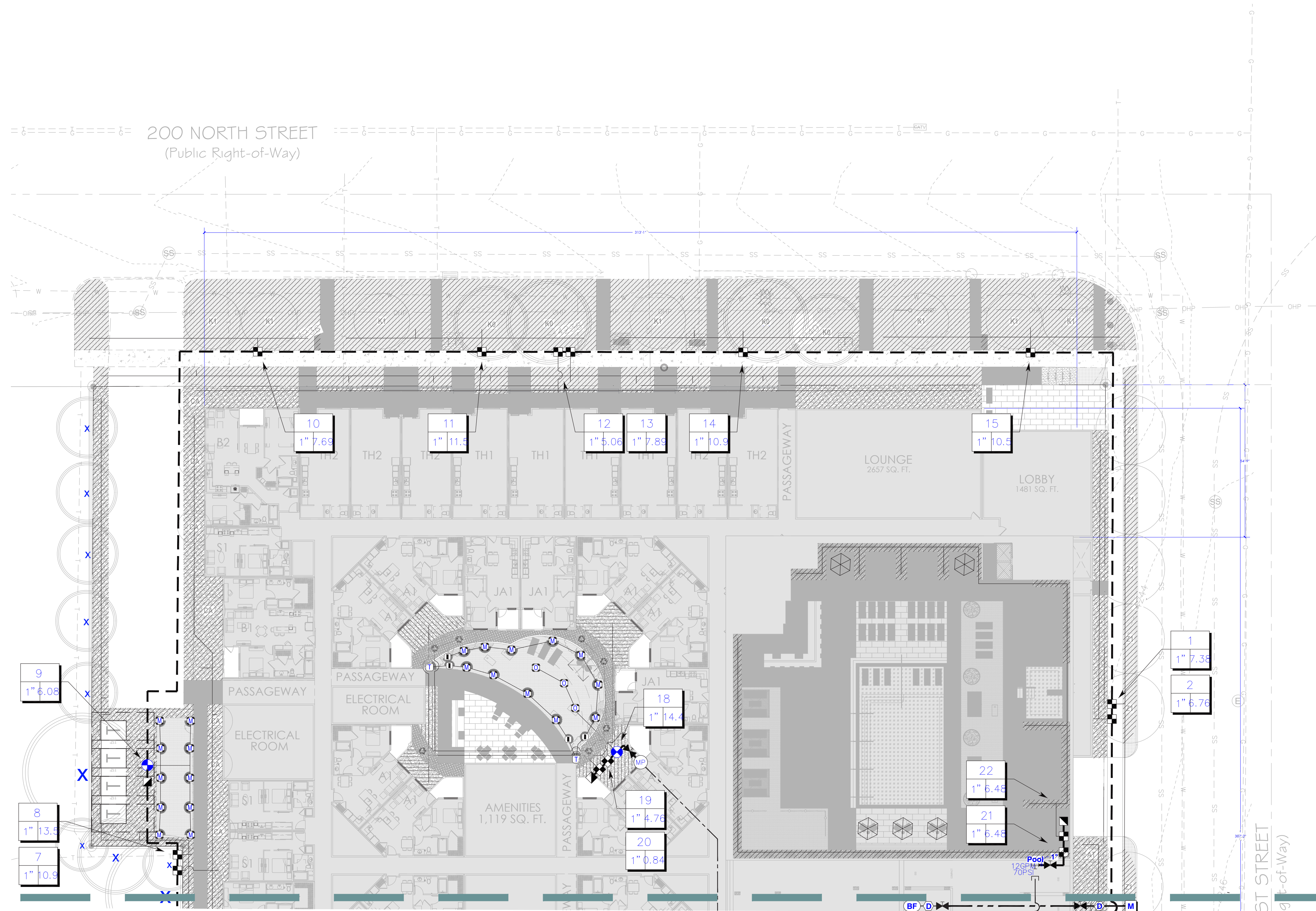


DISTRICT NORTH
SALT LAKE CITY, UT

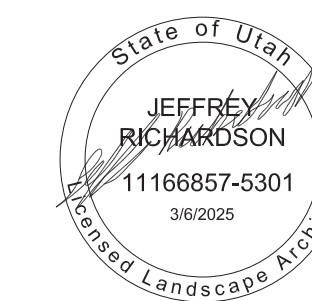
IRRIGATION -
SCHEDULE AND NOTES



L7



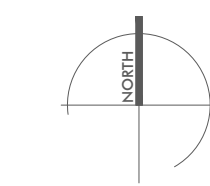
MATCHLINE - SEE LI-02



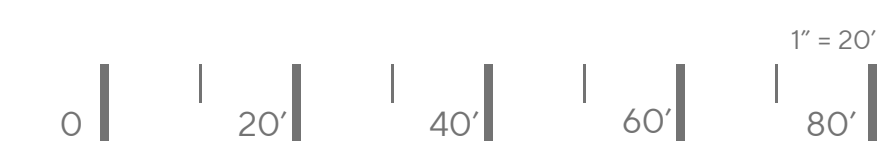
DISTRICT NORTH

SALT LAKE CITY, UT

PLN/PCM2024-01232
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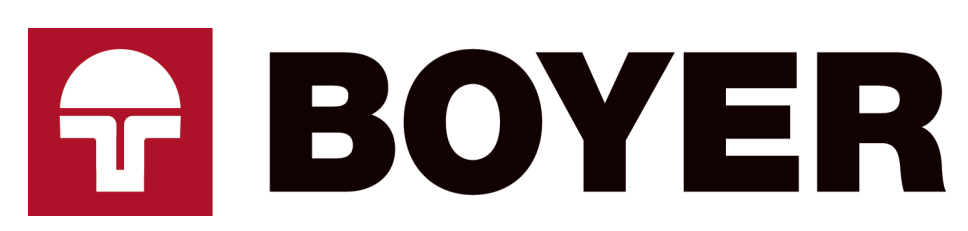
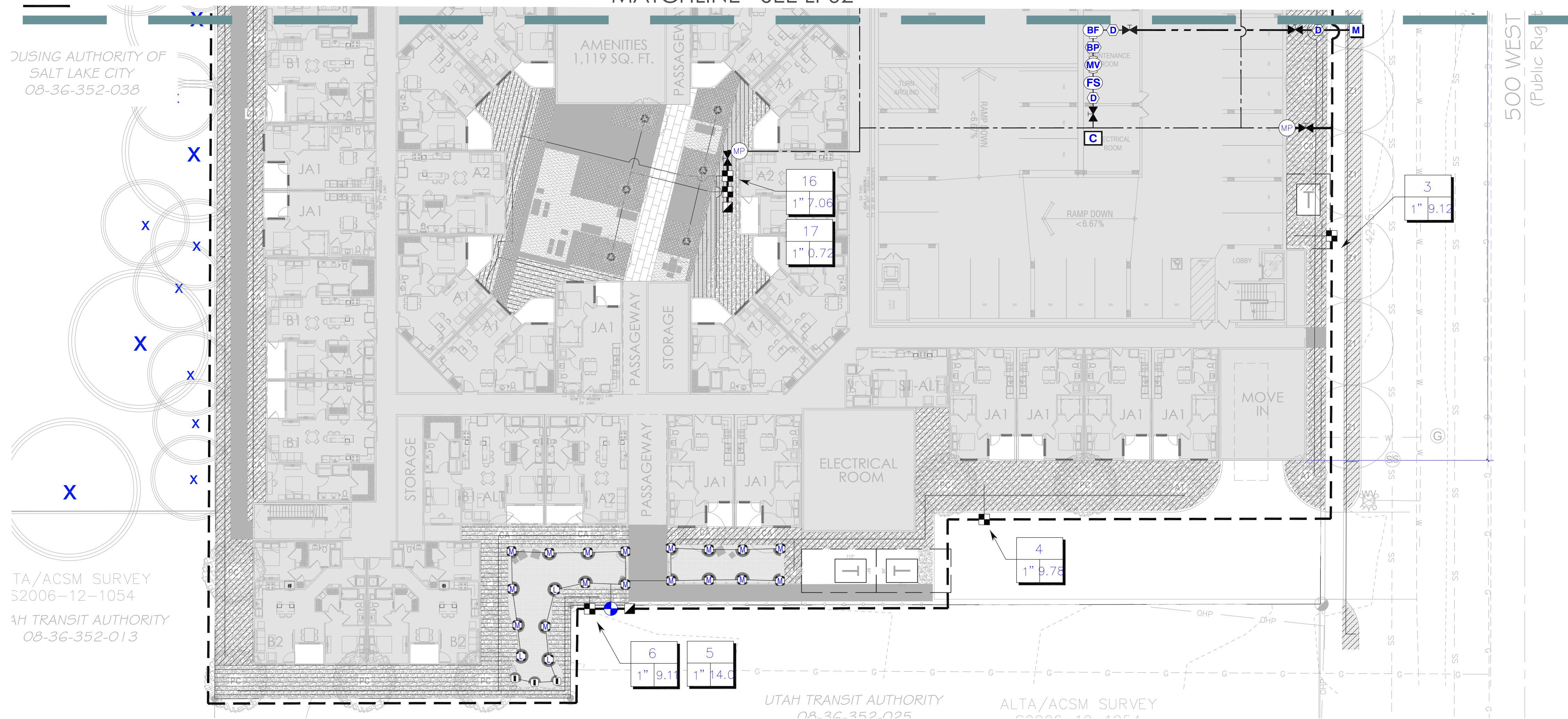
IRRIGATION -
PRELIMINARY IRRIGATION



L8

JOB NO. 2024-0651
DATE 03-06-2025
3/18

MATCHLINE - SEE LI-02



DISTRICT NORTH
SALT LAKE CITY, UT

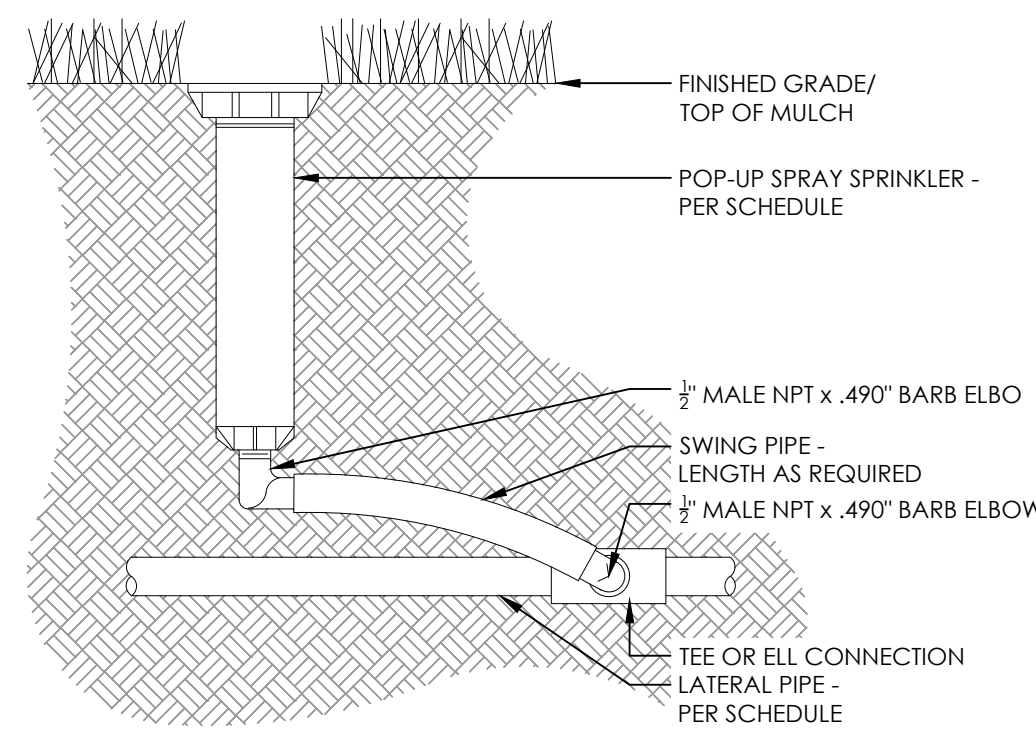


IRRIGATION - PRELIMINARY IRRIGATION
1" = 20'



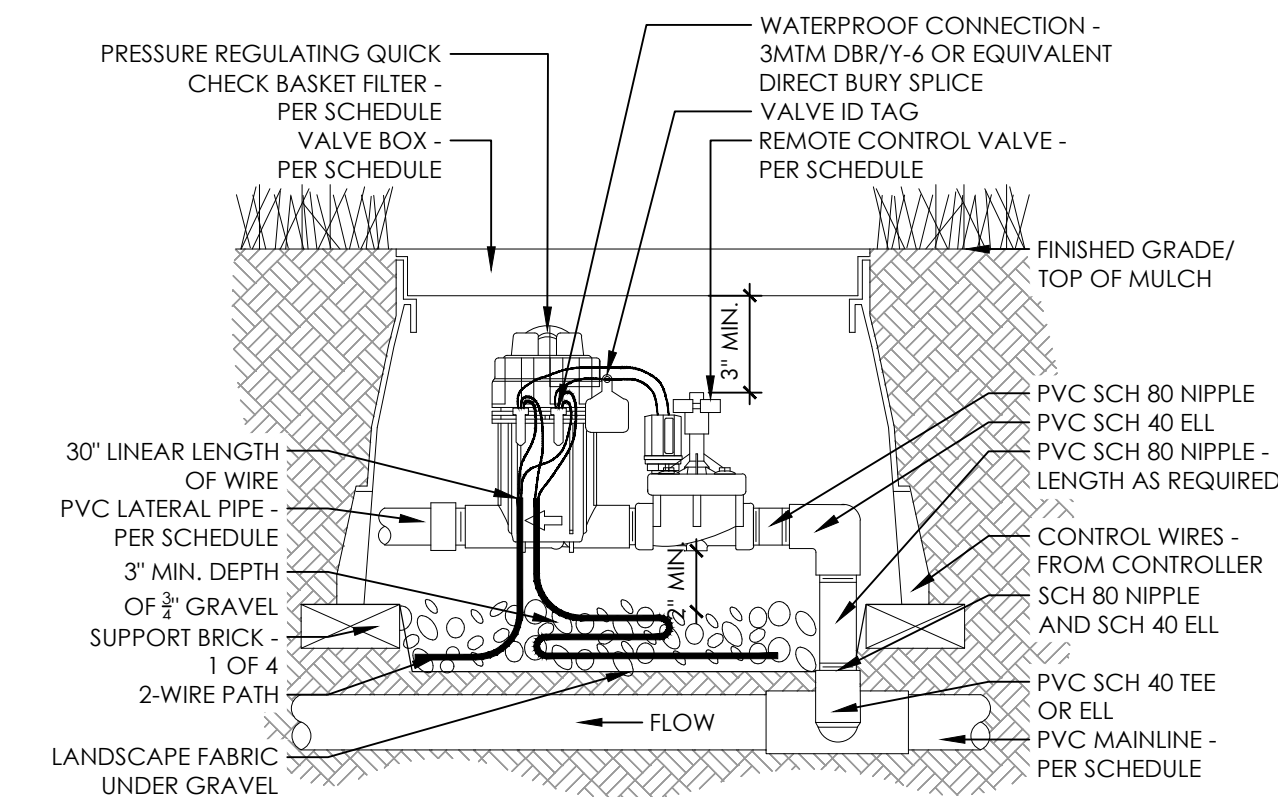
L9
JOB NO. 2024-0651
DATE 03-06-2025
318

- NOTES:**
1. ALL SPRAY HEADS **MUST** HAVE BUILT IN PRESSURE REGULATING FEATURE.
 2. AFTER FLUSHING HEADS, REGRADE AND COMPACT AS NEEDED TO RETURN TO FINISH GRADE CONDITION.
 3. SPRINKLERS SHALL BE MIN. 12" FROM ANY WALLS OR BUILDINGS.
 4. ADJUST ALL SPRINKLER HEADS TO AVOID OVERSPRAY ON ANY WALLS, HARDSCAPE, ETC.
 5. **DO NOT** USE SEALANT OR TAPE ON THREADED INSERTS TO SPRINKLER HEAD.

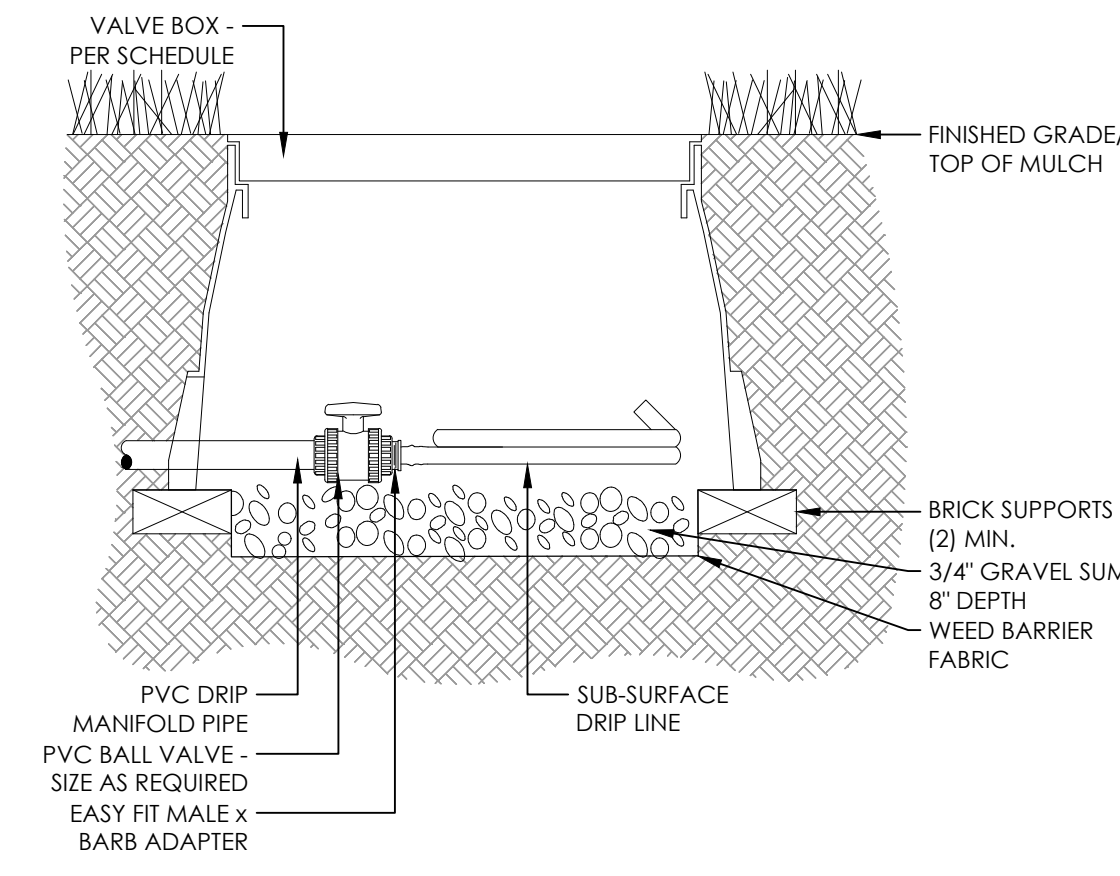


1 POP-UP SPRAY - SWING PIPE ASSEMBLY
NOT TO SCALE P-BWIC-HEA-02

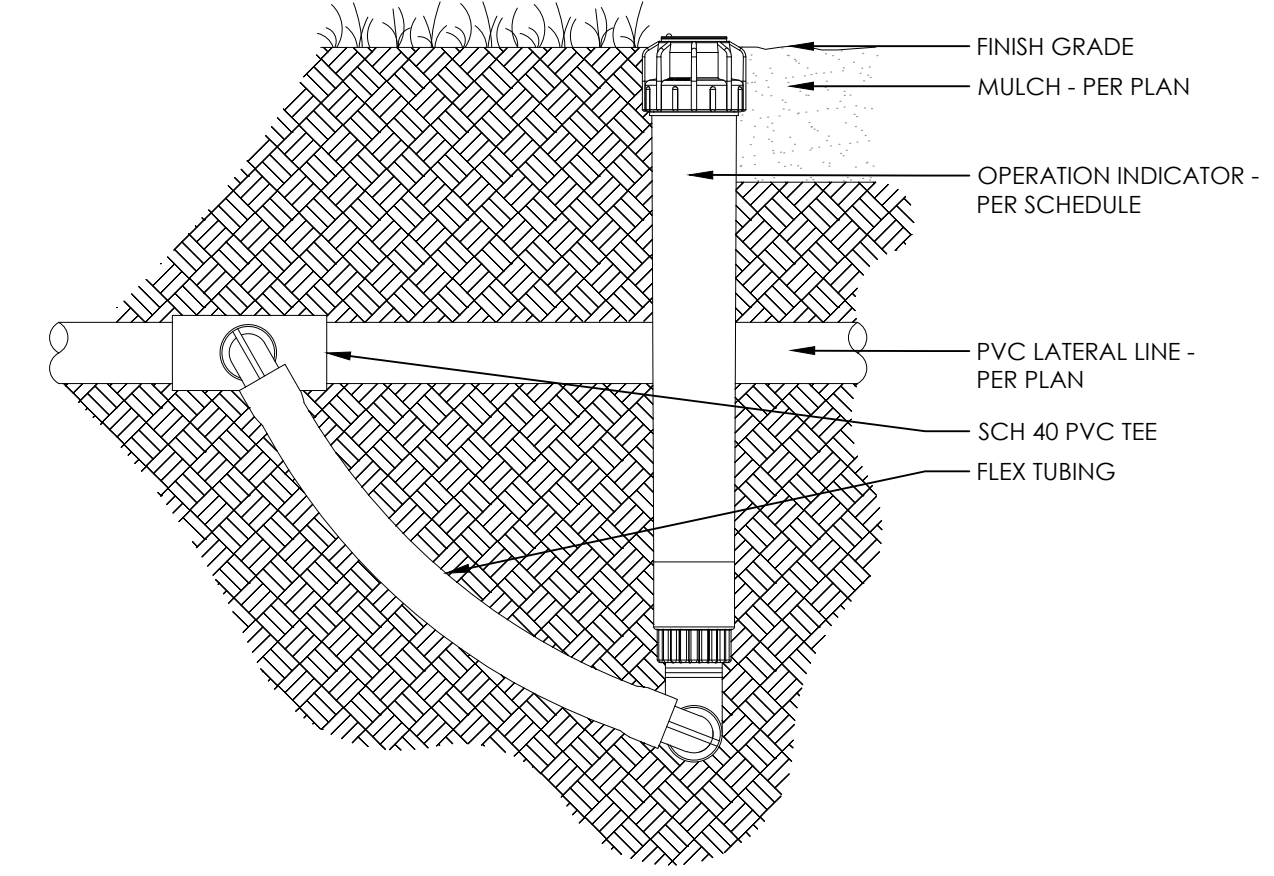
- NOTES:**
1. SEAL WIRE ENDS WITH WATERPROOF SPLICING MATERIAL.
 2. 30' MIN. LENGTH OF PATH WIRE. COIL AND PLACE IN BOX AT WATERPROOF CONNECTION TO DECODER AND SOLENOID.
 3. INSTALL DECODERS PER MANUFACTURER'S SPECIFICATIONS FOR WIRING AND GROUNDING.
 4. MOUNT DECODERS TO SIDE OF VALVE BOX WITH STAINLESS STEEL SCREWS AND LABEL WITH VALVE ID.



2 DRIP VALVE W/PRS BASKET & IVM DECODER
NOT TO SCALE P-BWIC-DRI-24

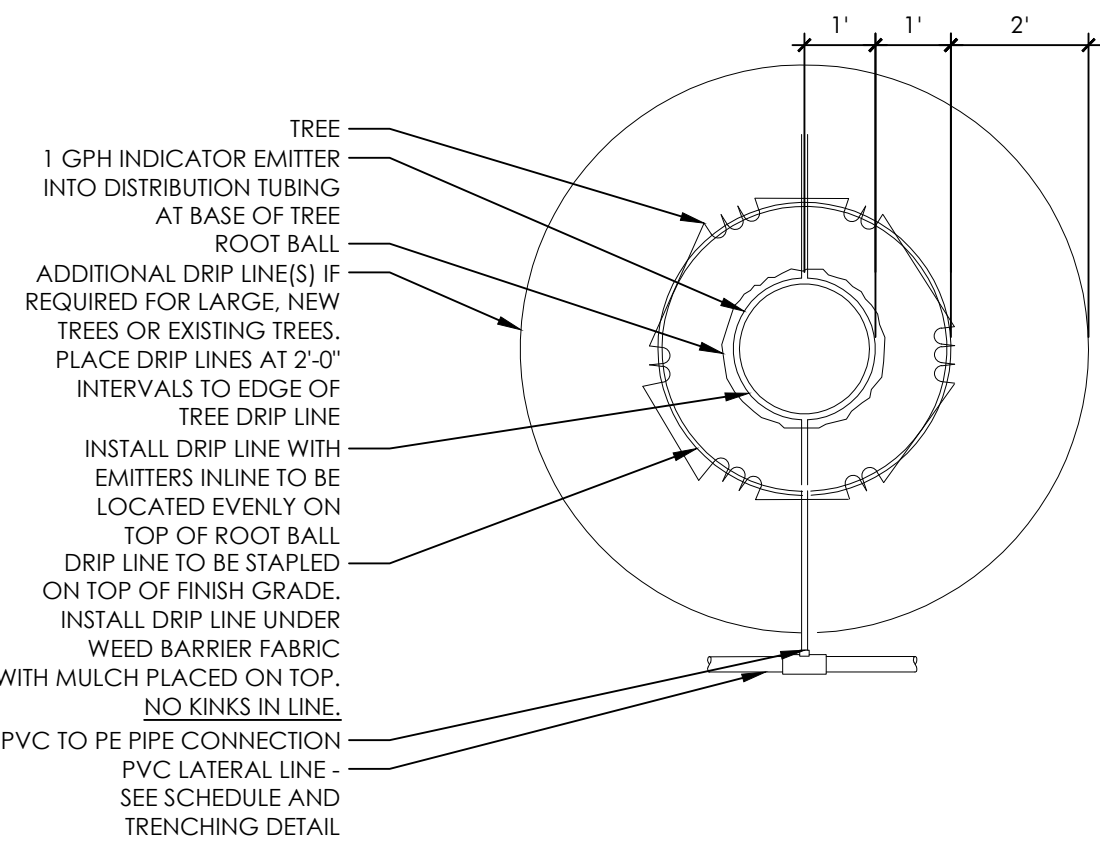


3 DRIPLINE FLUSH POINT WITH BALL VALVE
NOT TO SCALE P-BWIC-DRI-11

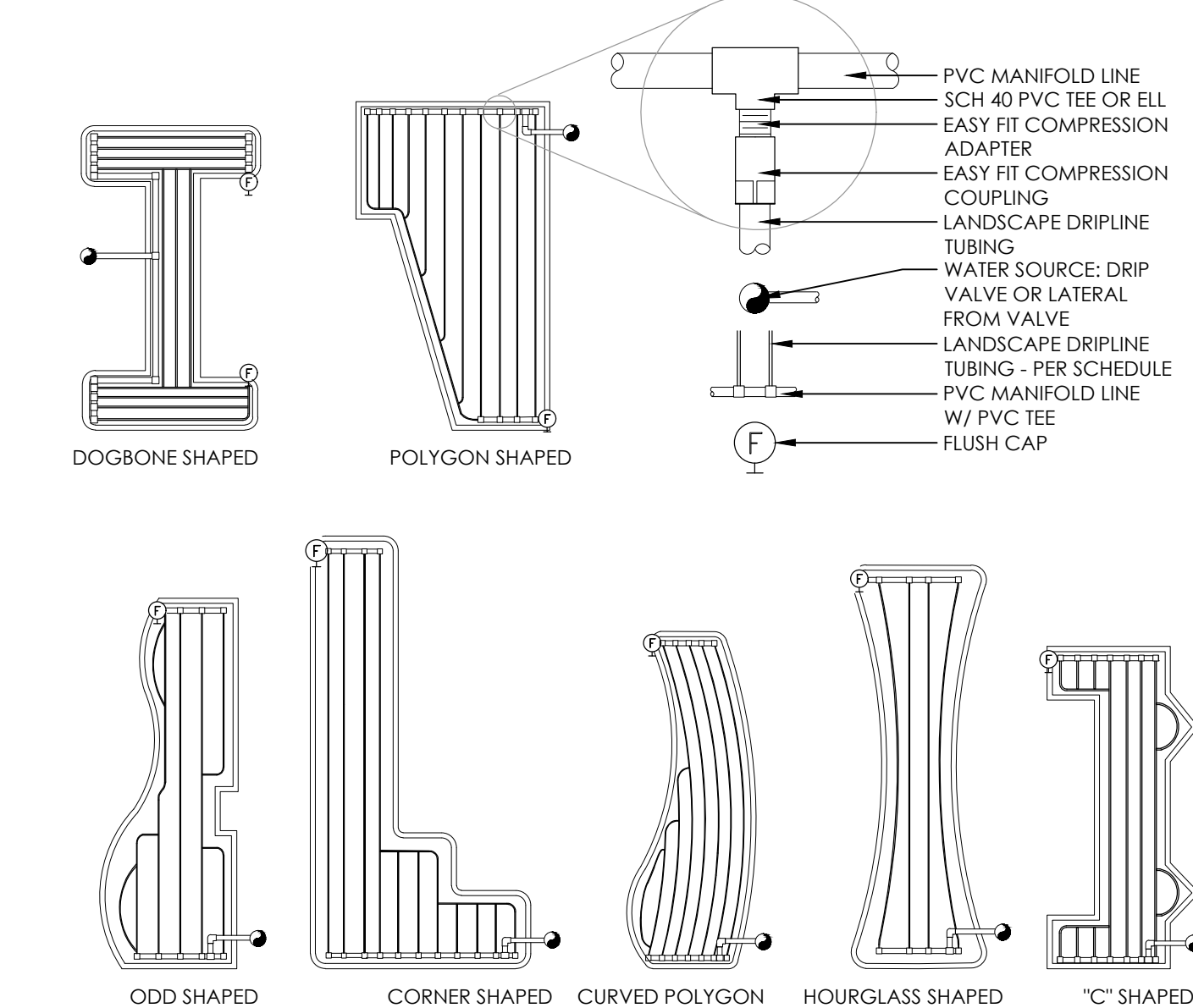


4 DRIPLINE OPERATION INDICATOR (HUNTER)
NOT TO SCALE P-BWIC-DRI-17

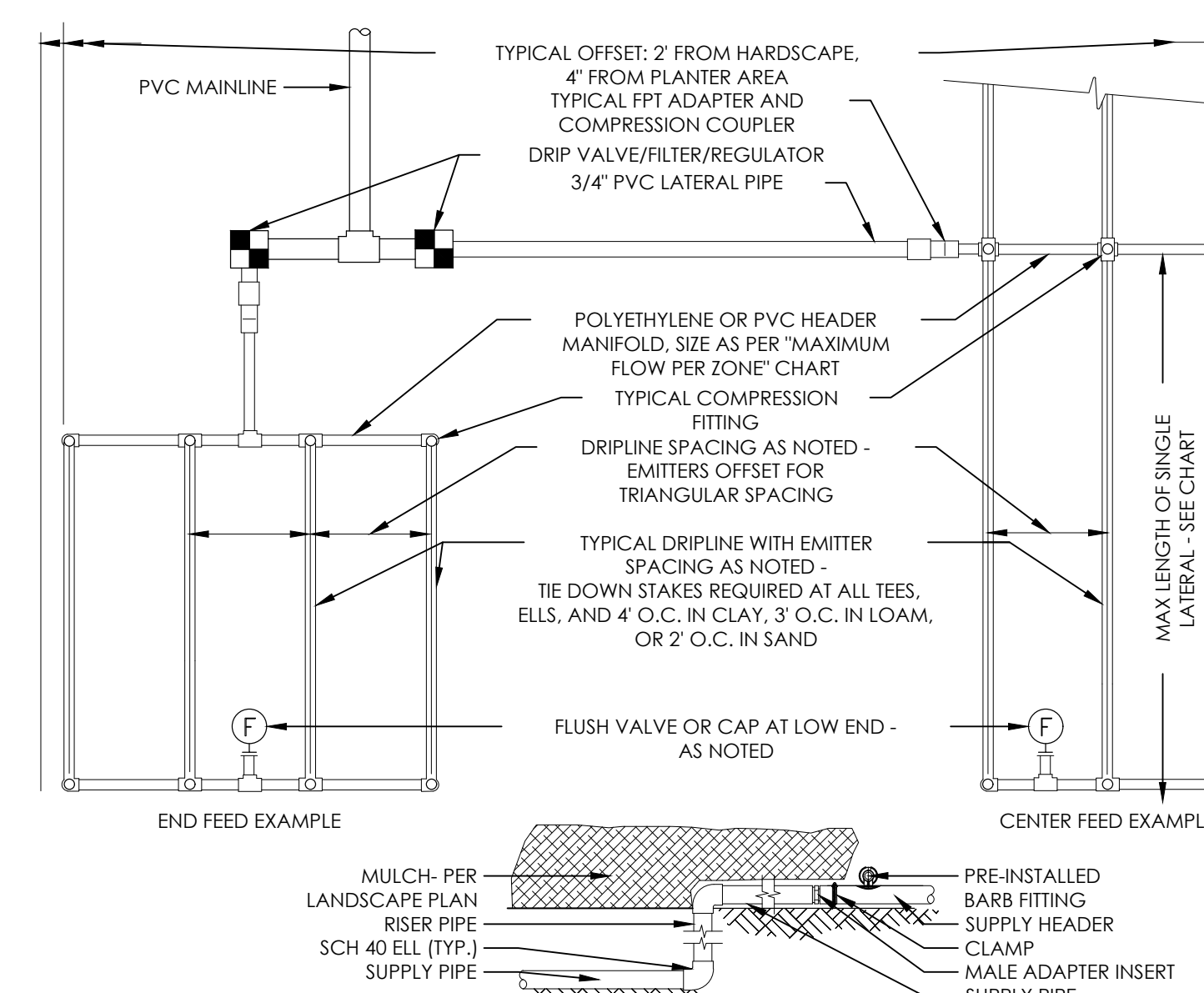
- NOTES:**
1. FOR EVERGREEN TREES, LOCATE INDICATOR EMITTERS ON OUTSIDE OF OUTER DRIP RING.
 2. ALL FITTINGS TO IN-LINE DRIP TUBING TO BE COMPRESSION FITTINGS. IF MALE INSERTS ARE NEEDED, INSTALL WITH OETIKER CLAMPS.



5 TREE DRIP RINGS - PLAN VIEW (PLANTER AREAS)
NOT TO SCALE P-BWIC-DRI-23



6 INLINE ON-SURFACE DRIP
NOT TO SCALE



SLOPED CONDITION NOTES:

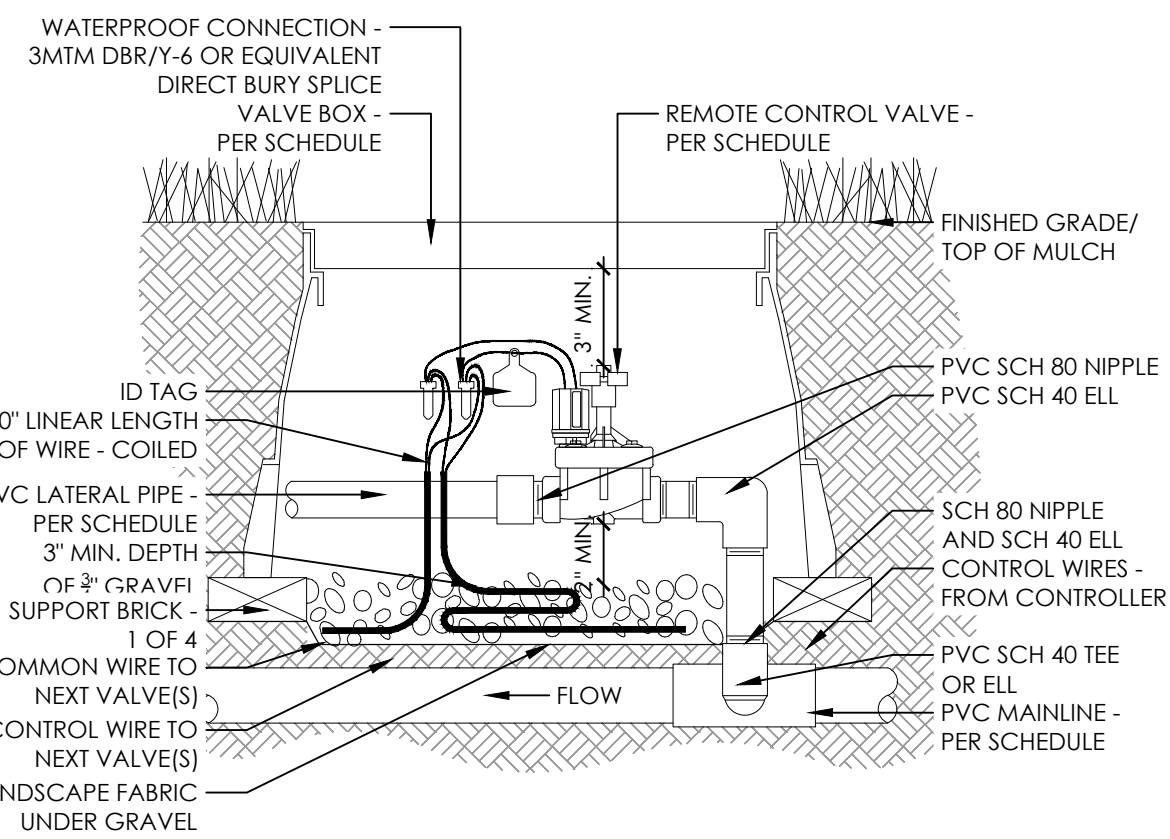
1. DRIPLINE LATERALS SHOULD FOLLOW THE CONTOURS OF THE SLOPE WHENEVER POSSIBLE.
2. INSTALL AIR RELIEF VALVE AT THE HIGHEST POINT.
3. NORMAL SPACING WITHIN THE TOP 2/3 OF SLOPE.
4. INSTALL DRIPLINE AT 25% GREATER SPACING AT THE BOTTOM 1/3 OF THE SLOPE.
5. WHEN ELEVATION CHANGE IS 10 FT OR MORE, ZONE THE BOTTOM 3 ON A SEPARATE VALVE.

GRID PRECIPITATION RATES (IN/HR)		EMITTER FLOW RATE		MAXIMUM FLOW PER ZONE	
EMITTER SPACING	LATERAL SPACING	0.6	0.9	SCHEDULE 40 PVC HEADER SIZE	MAX GPM PSI LOSS
12	12	0.96	1.44	1/2"	4.7 GPM 7.7 PSI
18	18	0.69	1.03	3/4"	8.3 GPM 5.6 PSI
24	24	0.28	0.41	1"	13.5 GPM 4.2 PSI
				1-1/2"	33.9 GPM 2.9 PSI
				2"	52.4 GPM 1.9 PSI

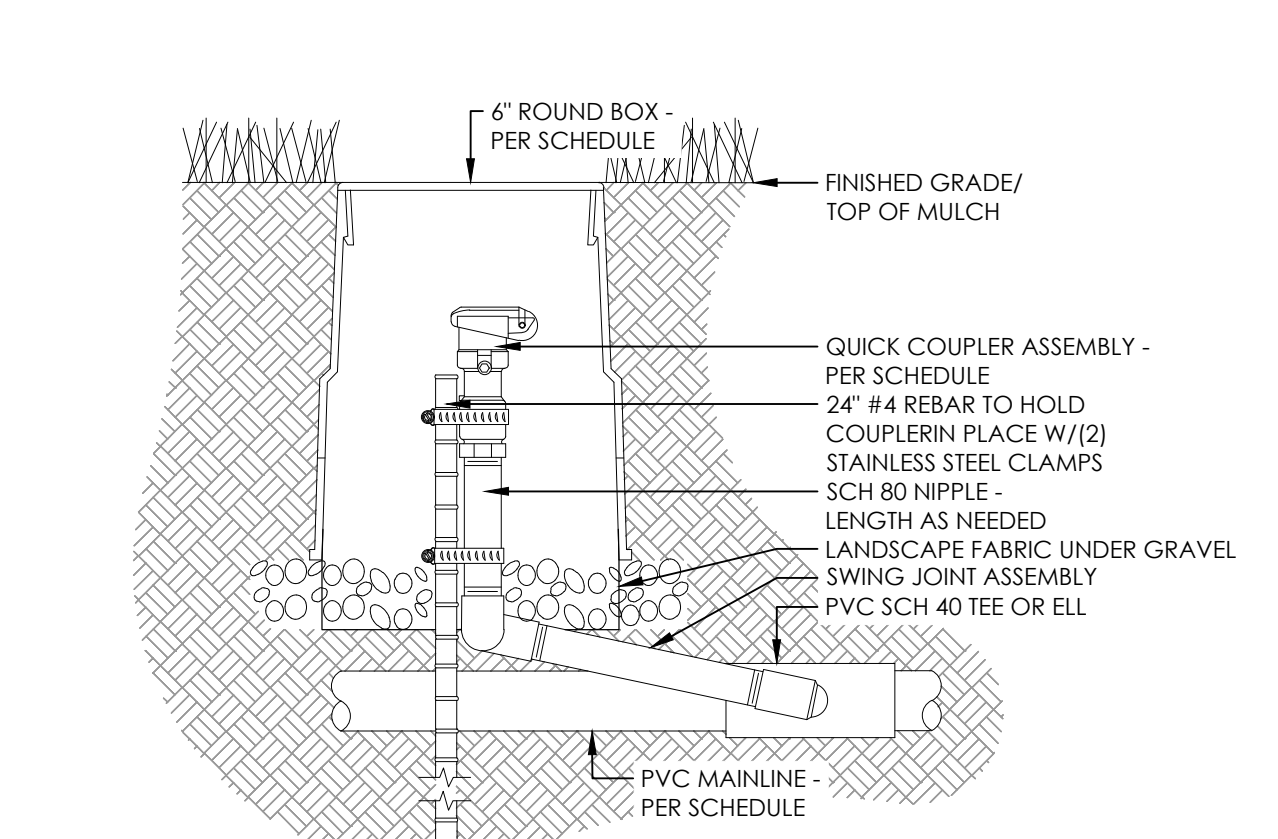
LATERAL FLOW PER 100 FT (GPM)		POLY PIPE HEADER SIZE	
EMITTER SPACING	12" SPACING	18" SPACING	24" SPACING
0.6 GPH	1.0 GPM	0.67 GPM	0.50 GPM
0.9 GPH	1.5 GPM	1.0 GPM	0.75 GPM

MAXIMUM LATERAL LENGTH (FEET)		EMITTER FLOW RATE GPH	
PSI	12" SPACING	18" SPACING	24" SPACING
0.6	0.9	0.6	0.9
10	125	96	175
20	249	191	350
30	307	236	434
40	350	268	495
50	125	96	175
60	125	96	175

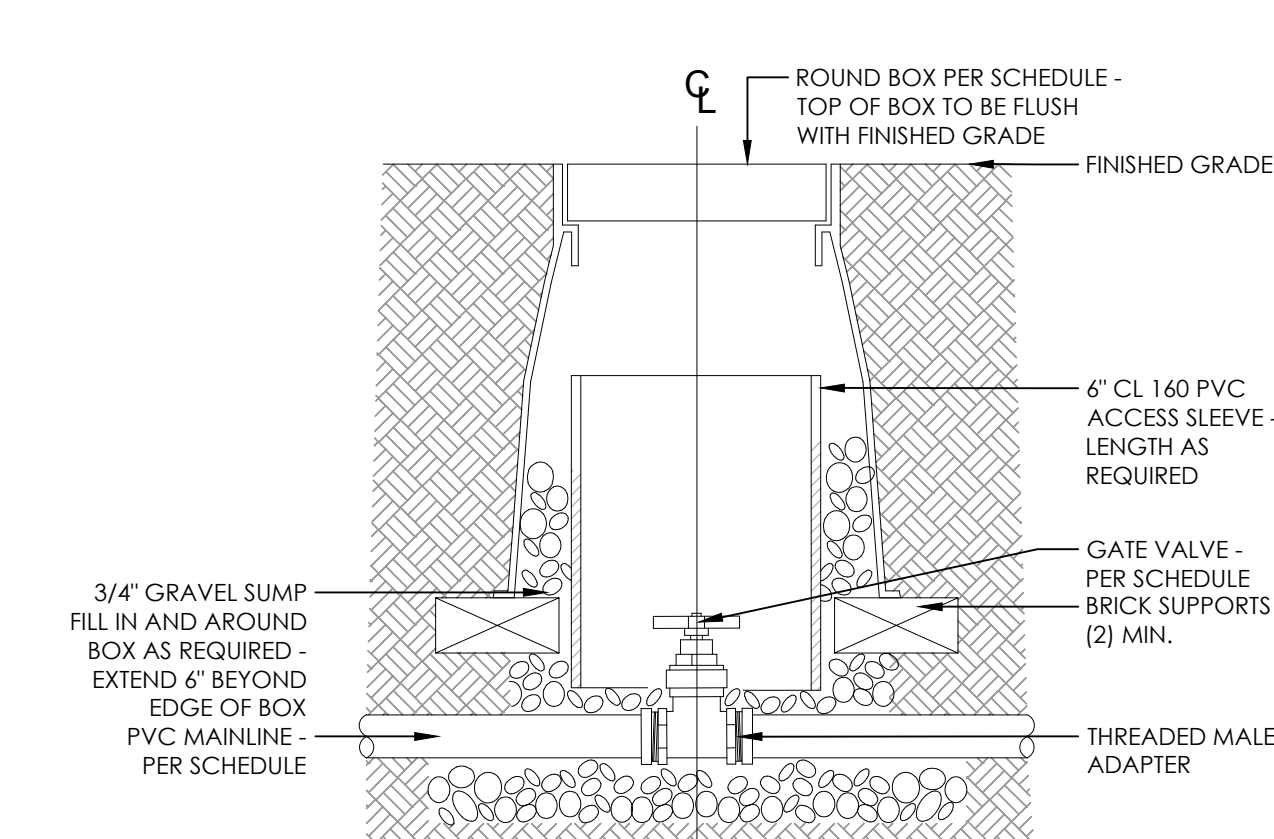
- NOTES:**
1. COMPACT SOIL AROUND GATE VALVE ASSEMBLY TO THE SAME DENSITY AS ADJACENT, UNDISTURBED SUBGRADE.
 2. DO NOT REST VALVE BOX OR ACCESS SLEEVE ON MAINLINE OR LATERAL LINE.
 3. PROVIDE GATE VALVE KEY - LENGTH AS NEEDED.



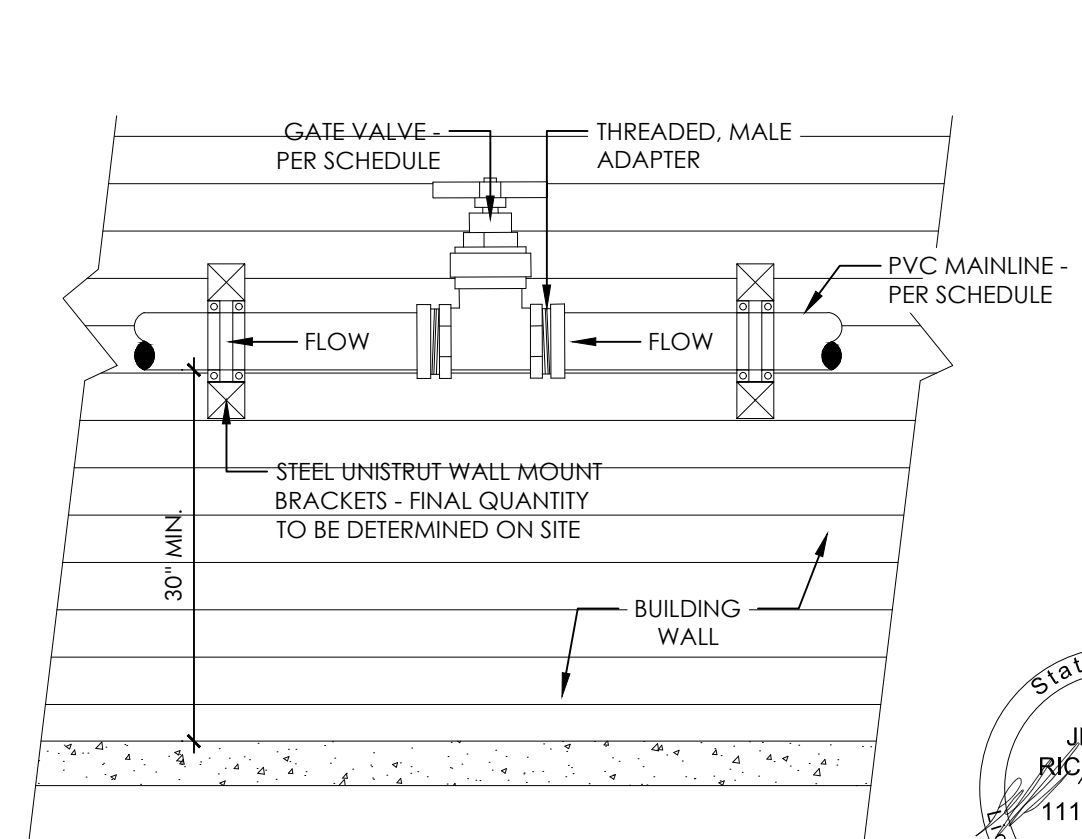
7 TURF VALVE ASSEMBLY W/IVM DECODER
NOT TO SCALE P-BWIC-VAL-03



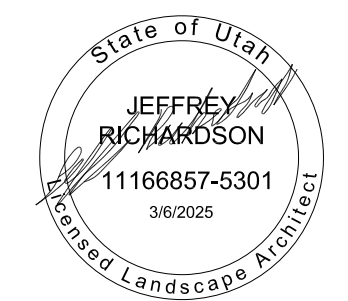
8 QUICK COUPLER
NOT TO SCALE P-BWIC-VAL-04

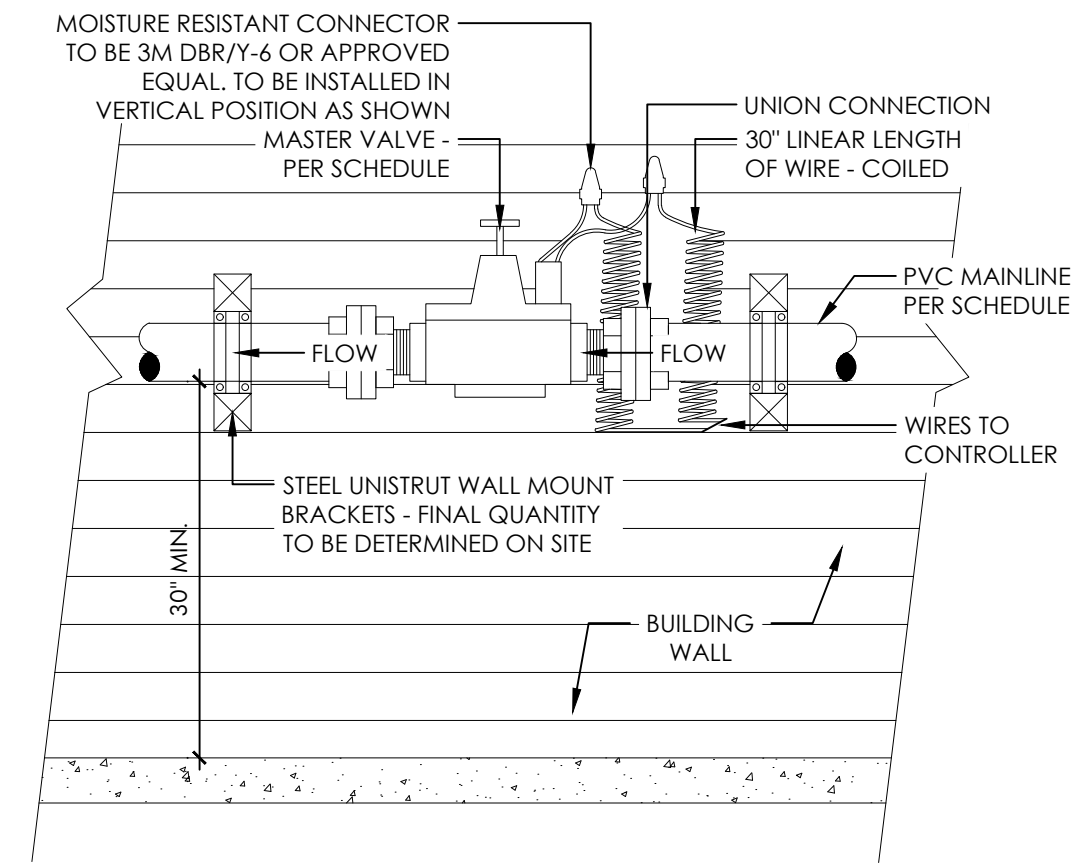


9 GATE VALVE - 3" AND SMALLER
NOT TO SCALE P-BWIC-VAL-10



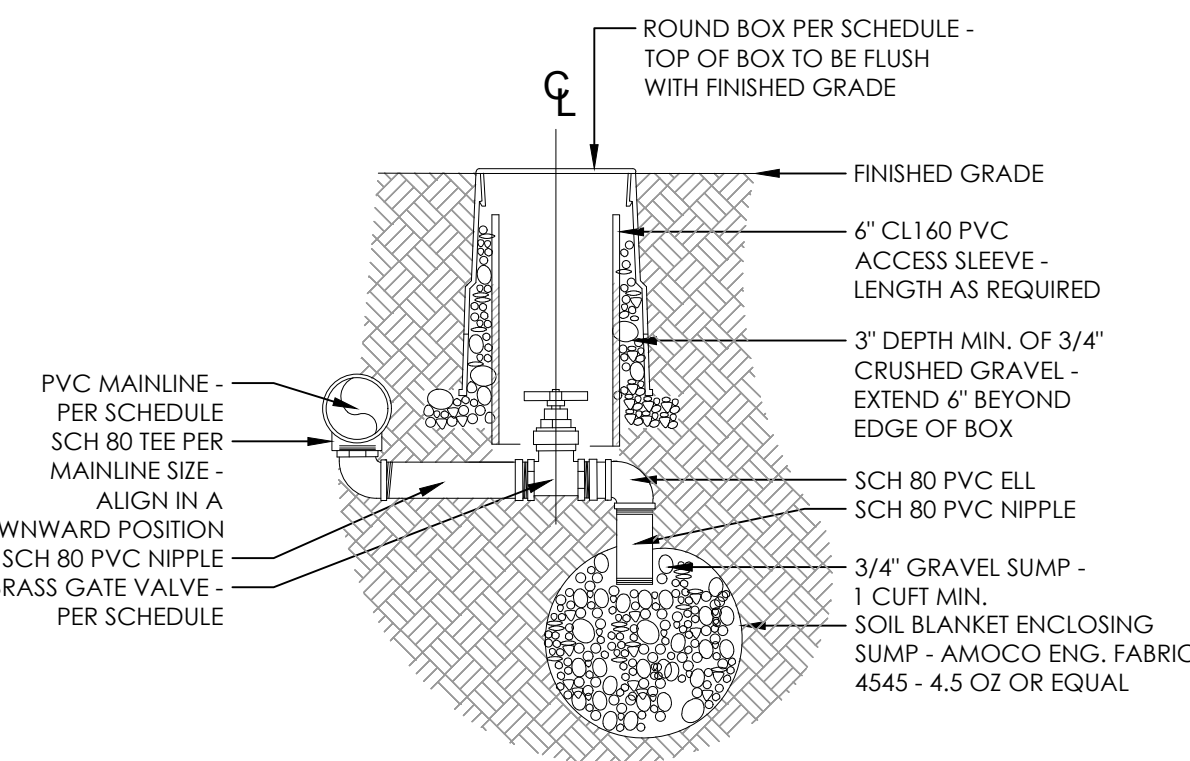
10 GATE VALVE - WALL MOUNT
NOT TO SCALE P-BWIC-VAL-A-08





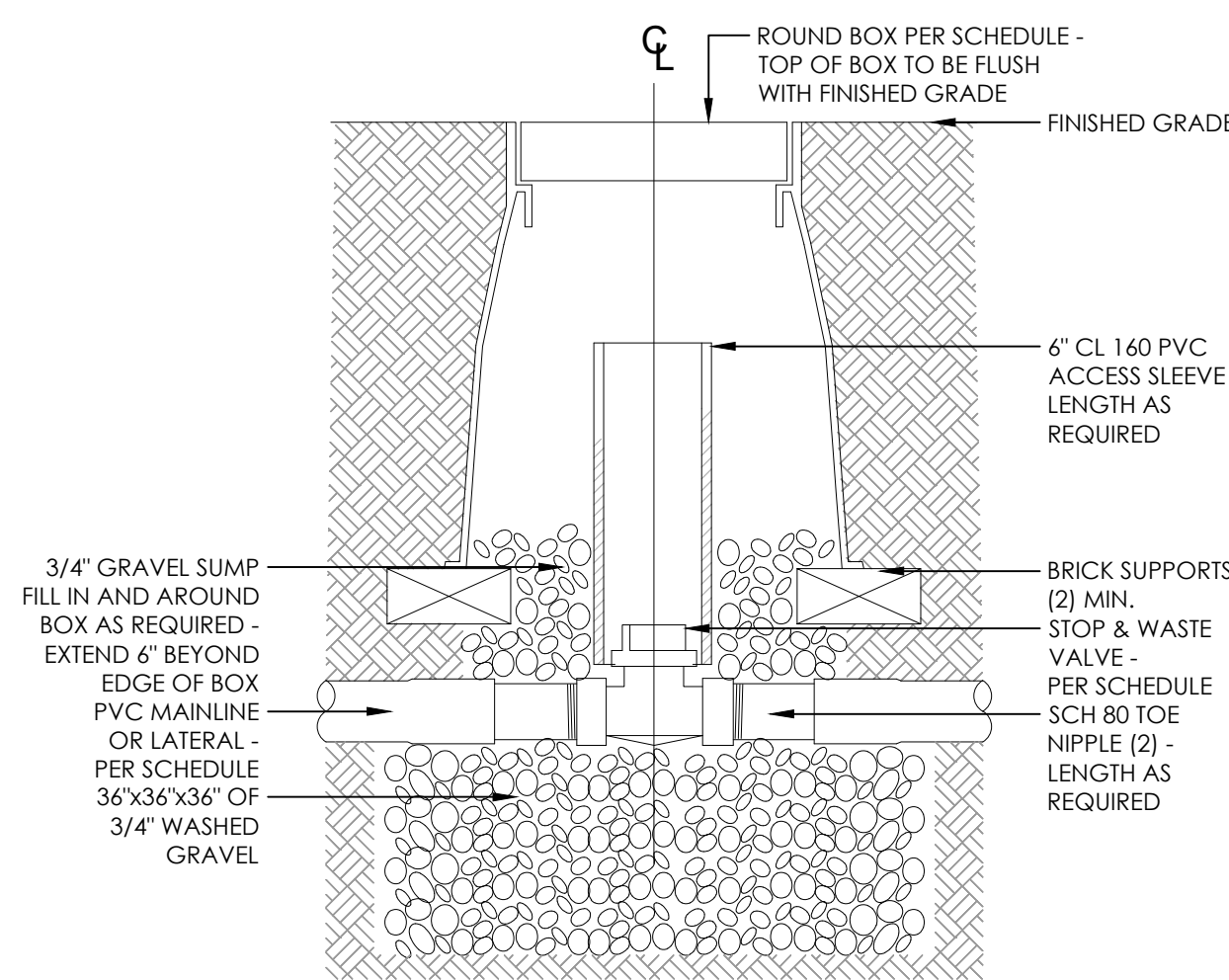
1 MASTER VALVE ASSEMBLY - WALL MOUNT
NOT TO SCALE P-BWIC-VAL-16

- NOTES:**
- LOCATE DRAIN VALVE AS INDICATED ON PLANS. TYPICAL LOCATIONS ARE AT POINT OF CONNECTION AND AT ALL LOW POINT(S) ALONG THE IRRIGATION MAINLINE AS NEEDED.
 - ALL THREADED CONNECTIONS SHALL BE COATED WITH RECTORSEAL PIPE SEALANT OR EQUIVALENT.



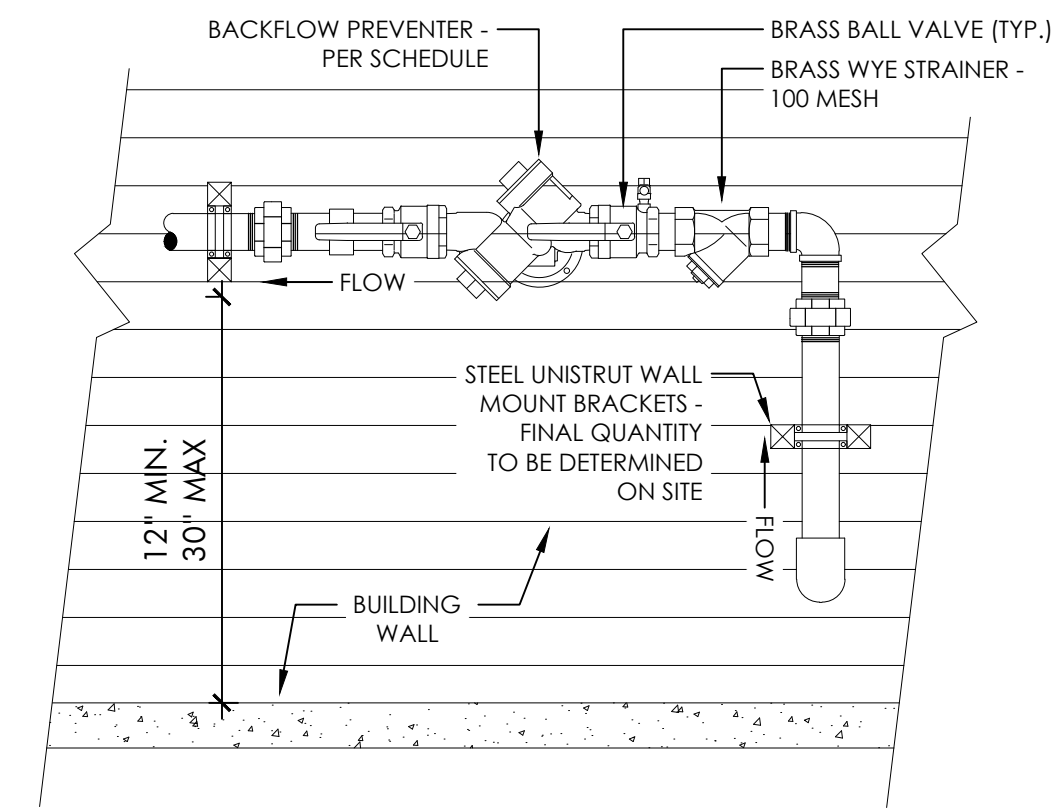
2 MANUAL DRAIN
NOT TO SCALE P-BWIC-VAL-08

- NOTES:**
- COMPACT SOIL AROUND VALVE ASSEMBLY TO THE SAME DENSITY AS ADJACENT, UNDISTURBED SUBGRADE.
 - DO NOT REST VALVE BOX OR ACCESS SLEEVES ON MAINLINE OR LATERAL LINE.
 - PROVIDE VALVE KEY - LENGTH AS NEEDED.



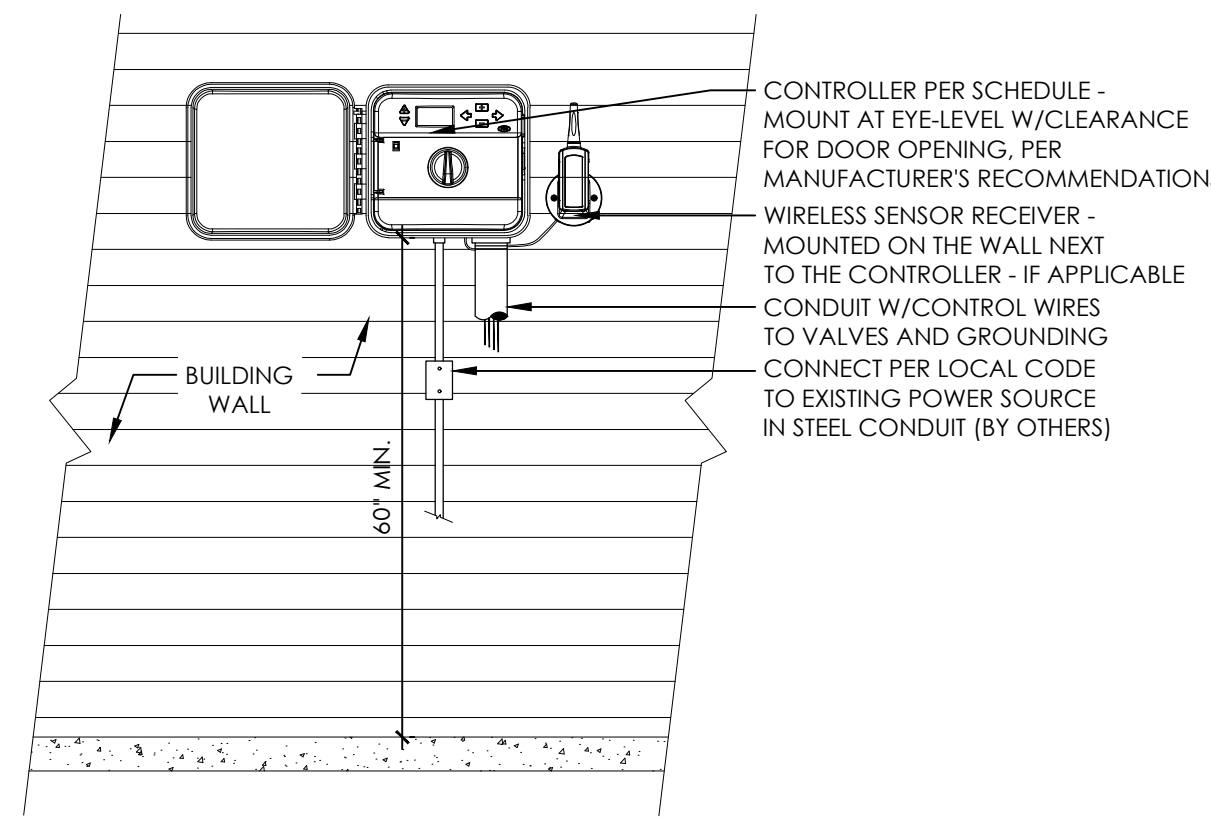
3 STOP & WASTE VALVE
NOT TO SCALE P-BWIC-VAL-A-09

- NOTES:**
- INSTALL PER LOCAL CODES AND MANUFACTURER'S SPECIFICATIONS.
 - PROVIDE PVC PIPE PROTECTION AROUND SUPPLY LINES AS THEY GO THROUGH THE WALL OR CONCRETE SLAB BASE.
 - ALL BOLTS FOR MOUNTING BRACKETS SHALL BE ZINC PLATED AND TAMPER PROOF.



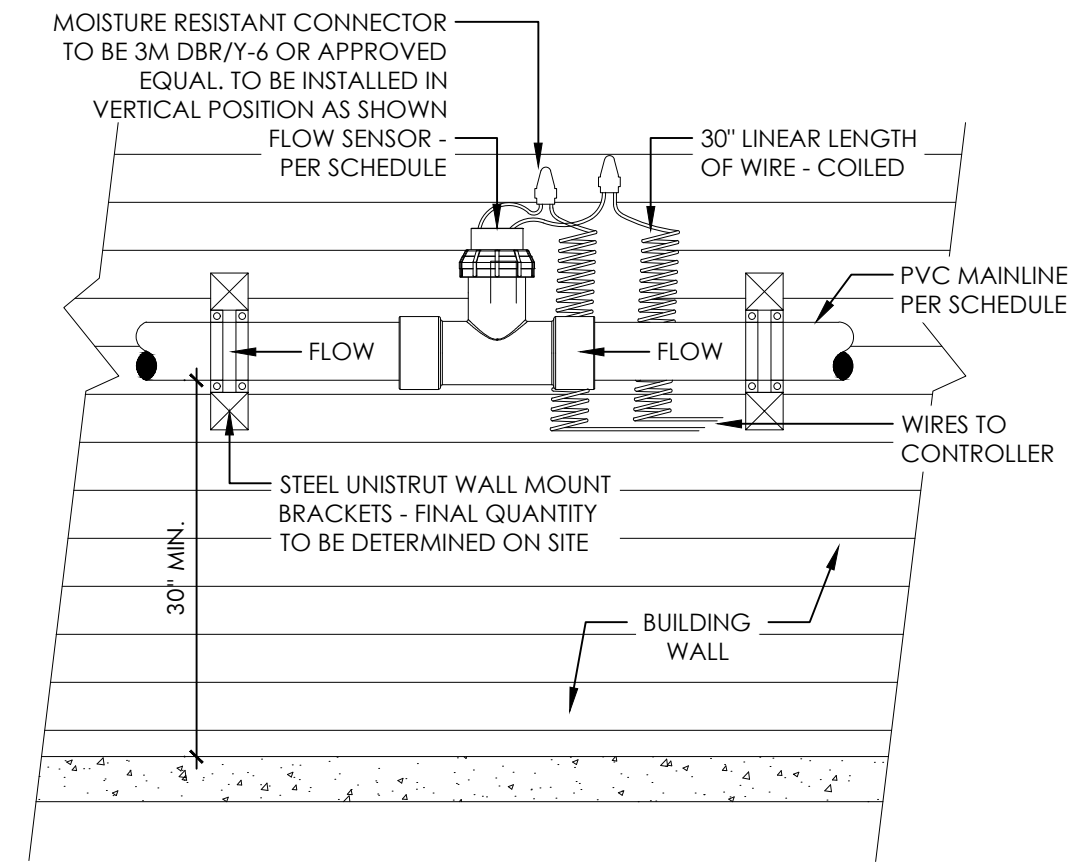
4 BACKFLOW PREVENTER - WALL MOUNT
NOT TO SCALE P-BWIC-AUXE-A-07

- NOTES:**
- ALL ELECTRICAL AND CONTROLLER WIRE TO BE INSTALLED PER LOCAL CODE AND MANUFACTURER'S SPECIFICATIONS.
 - ALL ELECTRICAL MATERIALS SHALL BE U.L. APPROVED FOR USE AS SHOWN.
 - GROUND CONTROLLER PER LOCAL CODE AND MANUFACTURER'S SPECIFICATIONS.
 - PROVIDE WATERPROOF SEALANT FOR ALL CONDUIT AND WIRE ACCESS POINTS.
 - PROVIDE LOCK AND KEY FOR ENCLOSURE (IF APPLICABLE).



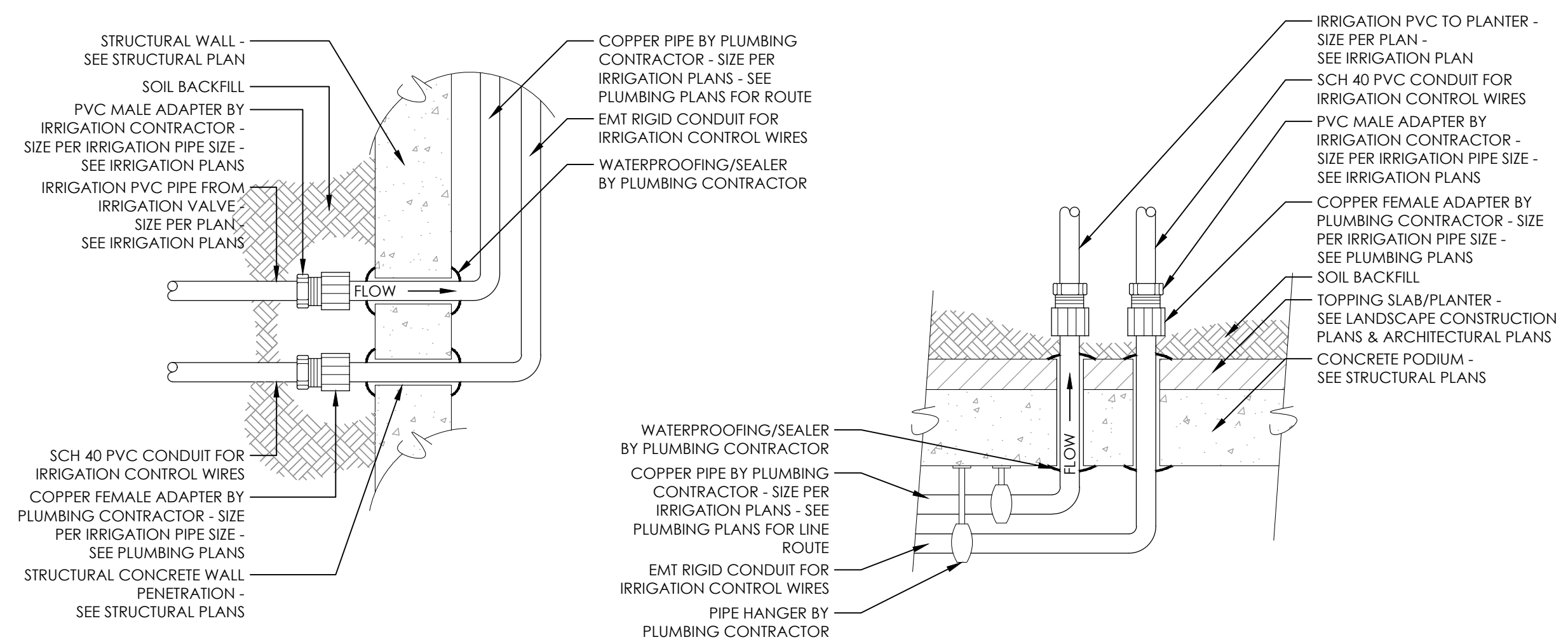
5 CONTROLLER - WALL MOUNTED - INTERIOR
NOT TO SCALE P-BWIC-AUXE-08

- NOTES:**
- STRAIGHT PIPE - 10 PIPE DIAMETERS UPSTREAM & 5 PIPE DIAMETERS DOWNSTREAM



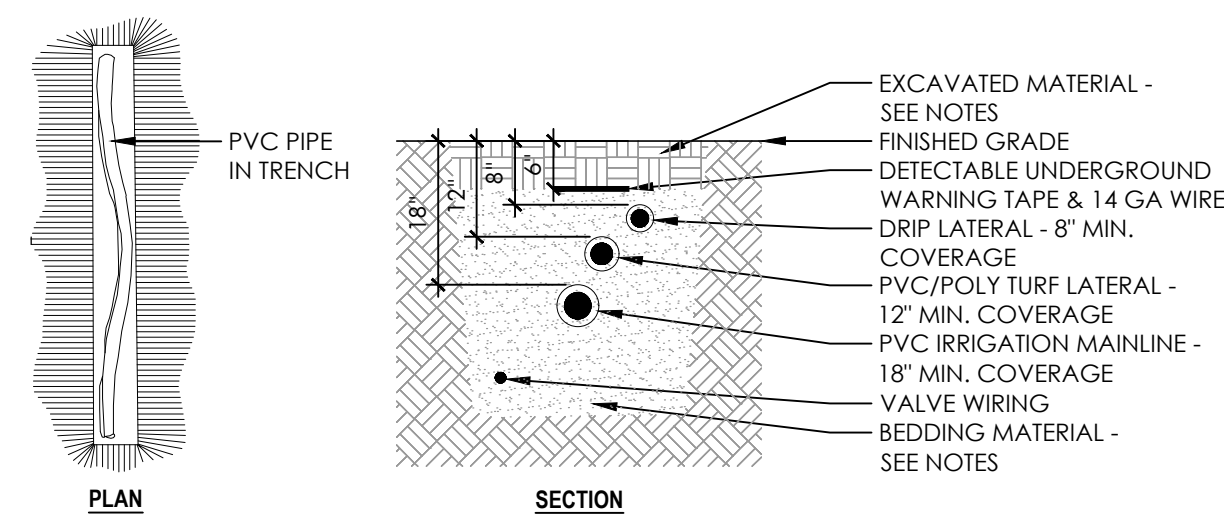
6 FLOW SENSOR - WALL MOUNT
NOT TO SCALE P-BWIC-AUXE-07

- NOTES:**
- SEE LANDSCAPE CONSTRUCTION PLAN FOR PLANTER LOCATIONS.
 - REFER TO PLUMBING DRAWINGS FOR SPECIFIC COPPER PIPE SPECIFICATIONS.
 - REFER TO CIVIL/STRUCTURAL DRAWINGS FOR IRRIGATION PENETRATIONS THROUGH PODIUM DECK.



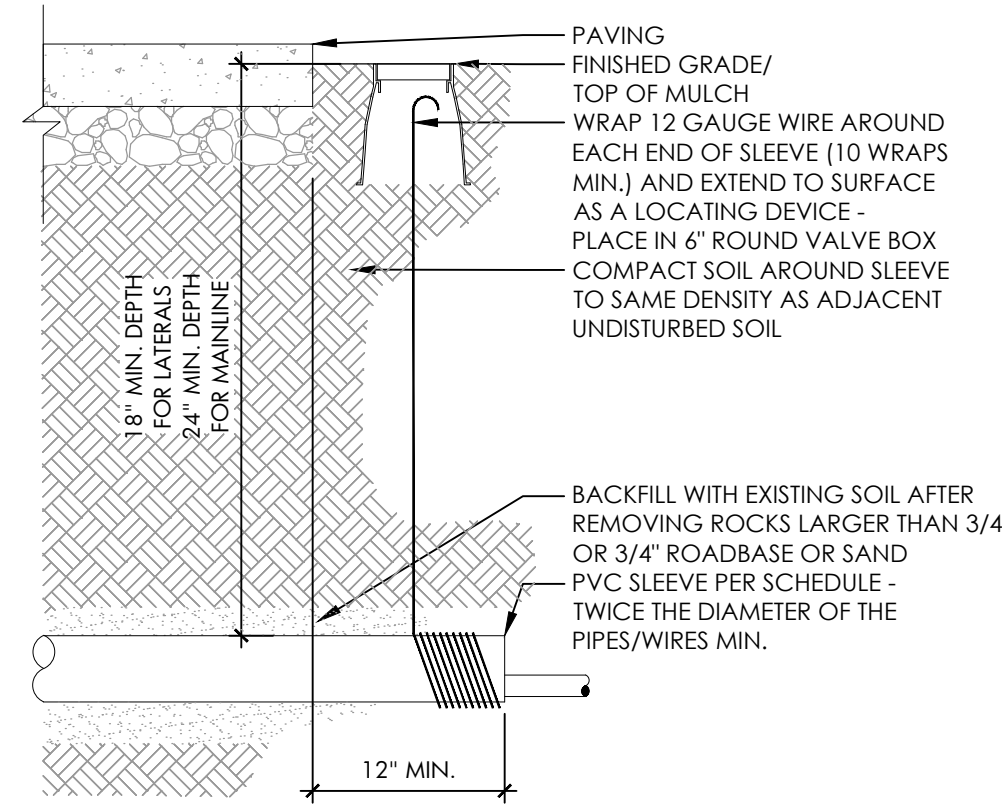
7 IRRIGATION PIPE PODIUM PENETRATION
NOT TO SCALE P-BWIC-PIP-04

- NOTES:**
- ALL MAINLINES TO BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS.
 - ALL PVC PIPING TO BE SNAKED IN TRENCHES AS SHOWN IN PLAN VIEW.
 - ALL 120 VOLT WIRING IN CONDUIT TO BE INSTALLED AS PER LOCAL CODES.
 - ALL ELECTRICAL WIRE CONNECTIONS TO VALVES AND SPLICES TO BE INSTALLED WITHIN A VALVE BOX AND MADE WITH 3MTM DBRY-6 WATERPROOF CONNECTORS, OR APPROVED EQUAL.
 - BUNDLE AND TAPE WIRING IN 10' INTERVALS.
 - VALVE WIRES TO BE INSTALLED WITHIN MAINLINE TRENCH WHEREVER POSSIBLE.
 - BEDDING MATERIAL SHALL BE 3/4" BASE OR SAND AND SHALL BE 3" BELOW LOWEST PIPE OR WIRE AND 3" ABOVE HIGHEST PIPE OR WIRE WITHIN TRENCH.
 - BEDDING MATERIAL SHALL BE IN MAINLINE TRENCH ONLY.
 - BEDDING IS NOT REQUIRED IN POLYETHYLENE TUBING TRENCHES BUT NO ROCKS LARGER THAN 3/4".
 - EXCAVATED COVER MATERIAL SHALL BE FREE FROM DEBRIS AND ROCKS 3/4" OR GREATER.
 - PIPE BEDDING MATERIAL TO BE ROCK AND DEBRIS FREE, BACKFILL IN 6" LIFTS, PUDDLE WITH WATER BETWEEN LIFTS.



8 IRRIGATION TRENCH
NOT TO SCALE P-BWIC-PIP-01

- NOTES:**
- ALL SLEEVES SHALL BE INSPECTED PRIOR TO BACKFILLING.
 - CAP SLEEVES WITH 90° FITTING AND EXTEND PIPE TO SURFACE UNTIL USE.
 - MULTIPLE SLEEVES REQUIRE 4" HORIZONTAL SEPARATION WITHIN SAME SLEEVE TRENCH.
 - IRRIGATION PIPE AND WIRE SHALL NOT SHARE THE SAME SLEEVED.
 - MARK/STAMP - 'X' AND/OR INSTALL PLACARD AT BACK OF CURB OR ROOFING NAIL IN ASPHALT



9 IRRIGATION SLEEVE
NOT TO SCALE P-BWIC-PIP-02

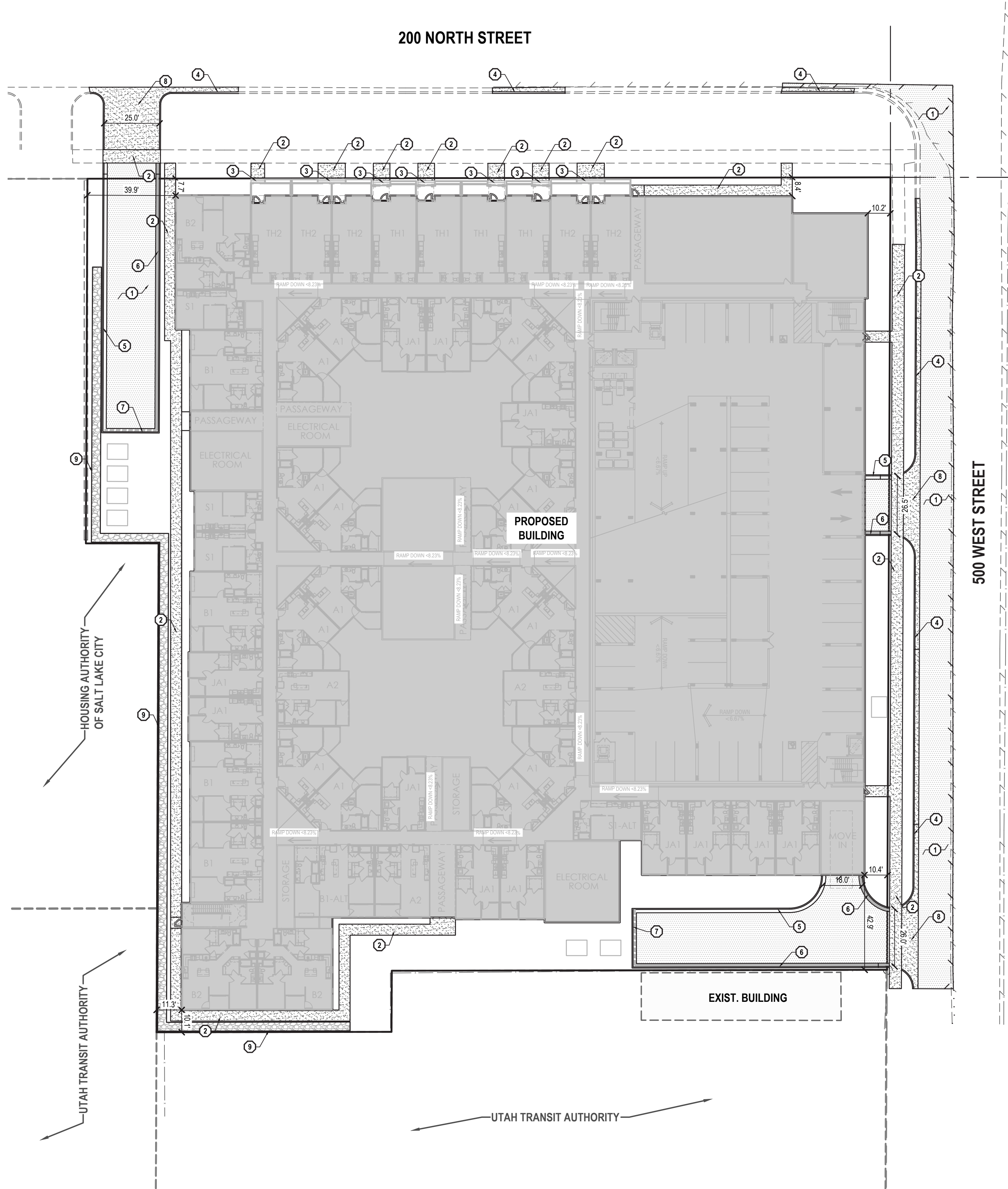


BENCHMARK
 SOUTH QUARTER CORNER OF SECTION 1,
 TOWNSHIP 3 SOUTH, RANGE 1 WEST
 SALT LAKE BASE AND MERIDIAN
 ELEV = 4385.14'

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SITE SUMMARY TABLE		
DESCRIPTION	AREA (SF)	PERCENTAGE
PAVEMENT	13,697	11%
ROOF	100,328	79%
LANDSCAPING	12,792	10%
TOTAL SITE	126,817	100%
	2.91 ACRES	



GENERAL NOTES

1. ALL WORK TO COMPLY WITH SALT LAKE CITY STANDARDS AND SPECIFICATIONS.
2. ALL IMPROVEMENTS MUST COMPLY WITH ADA STANDARDS AND RECOMMENDATIONS.
3. SEE LANDSCAPE/ARCHITECTURAL PLANS FOR CONCRETE MATERIAL, COLOR, FINISH, AND SCORE PATTERNS THROUGHOUT SITE.
4. ALL PAVEMENT MARKINGS SHALL CONFORM TO THE LATEST EDITION OF THE M.U.T.C.D. (MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES).
5. ALL SURFACE IMPROVEMENTS DISTURBED BY CONSTRUCTION SHALL BE RESTORED OR REPLACED, INCLUDING TREES AND DECORATIVE SHRUBS, SOD, FENCES, WALLS AND STRUCTURES, WHETHER OR NOT THEY ARE SPECIFICALLY SHOWN ON THE CONTRACT DOCUMENTS.
6. NOTIFY ENGINEER OF ANY DISCREPANCIES IN DESIGN OR STAKING BEFORE PLACING CONCRETE OR ASPHALT.
7. THE CONTRACTOR IS TO PROTECT AND PRESERVE ALL EXISTING IMPROVEMENTS, UTILITIES, AND SIGNS, ETC. UNLESS OTHERWISE NOTED ON THESE PLANS.

SCOPE OF WORK:

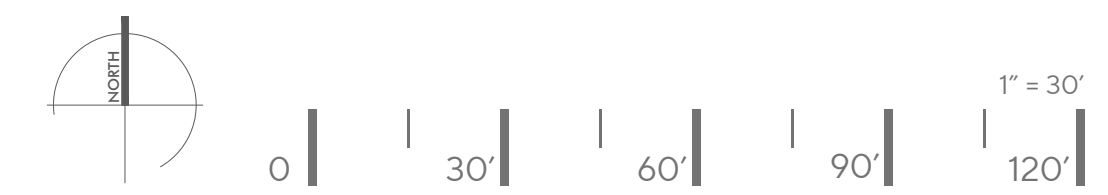
PROVIDE, INSTALL AND/OR CONSTRUCT THE FOLLOWING PER THE SPECIFICATIONS GIVEN OR REFERENCED, THE DETAILS NOTED, AND/OR AS SHOWN ON THE CONSTRUCTION DRAWINGS:

- ① ASPHALT PAVEMENT PER GEOTECHNICAL REPORT
- ② 4" THICK CONCRETE SIDEWALK PER APWA STANDARD PLAN NO. 231.
- ③ STAIRS IN SIDEWALK. SEE GRADING PLAN FOR ELEVATION INFORMATION. SEE ARCHITECTURAL PLANS FOR HANDRAIL INFORMATION.
- ④ 30" TYPE 'A' CURB AND GUTTER PER APWA STANDARD PLAN NO. 205.
- ⑤ 24" COLLECTION CURB AND GUTTER
- ⑥ 24" REVERSE PAN CURB AND GUTTER
- ⑦ TRANSITION BETWEEN COLLECTION CURB AND GUTTER AND REVERSE PAN CURB AND GUTTER.
- ⑧ OPEN DRIVEWAY APPROACH PER APWA STANDARD PLAN NO. 225.
- ⑨ RETAINING WALL.



DISTRICT NORTH
 SALT LAKE CITY, UT

SITE PLAN



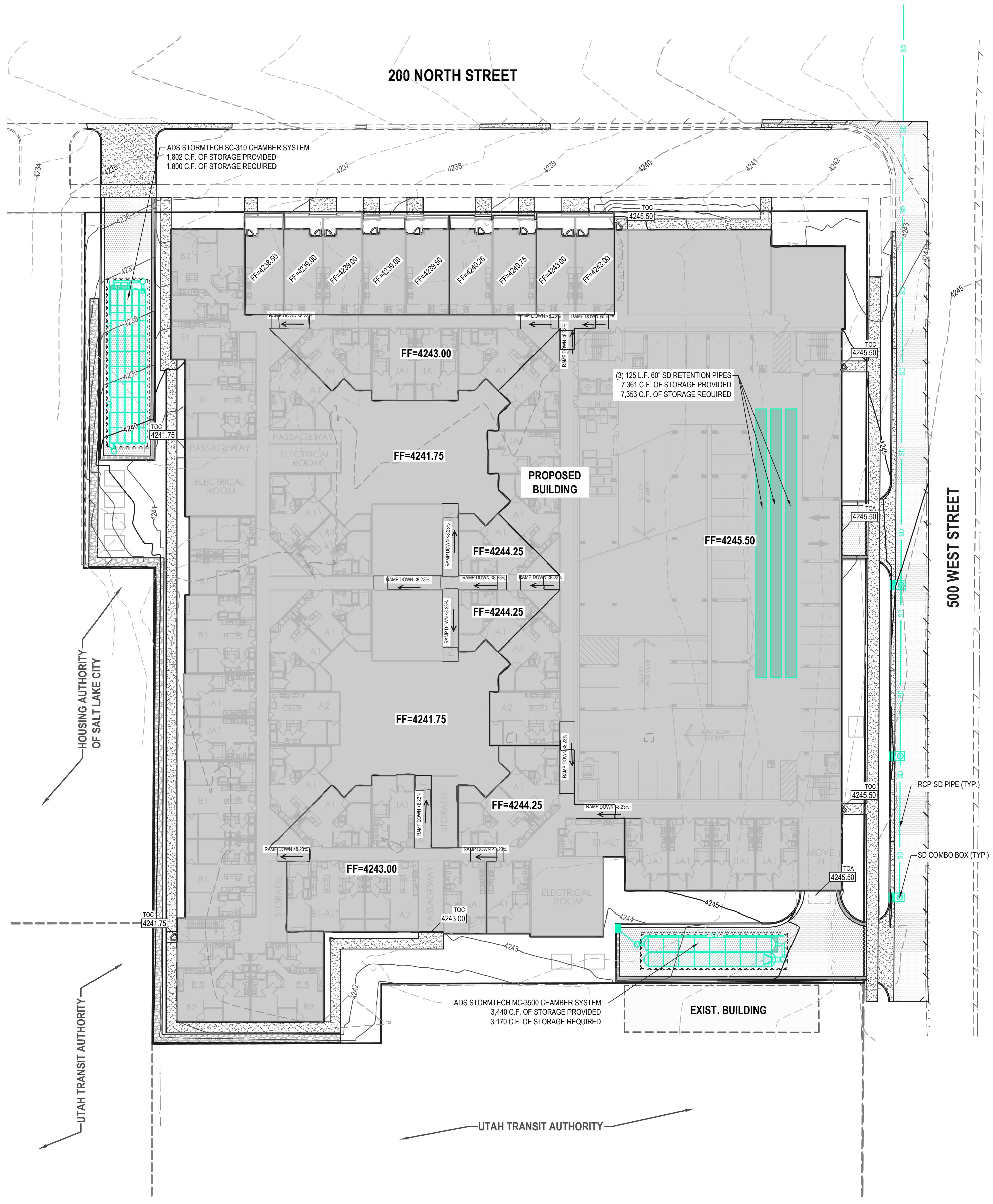
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JOB NO. 2024-0651
 DATE 03-06-2025
 3/18

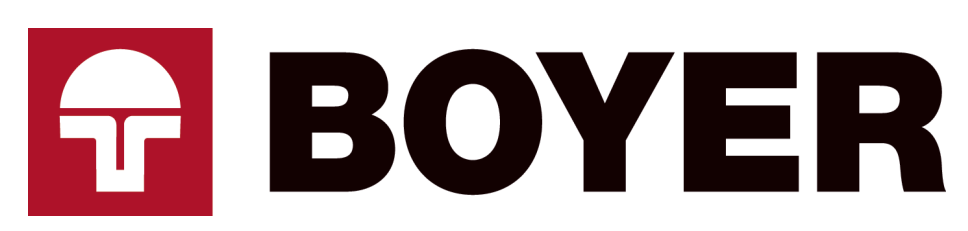
BENCHMARK
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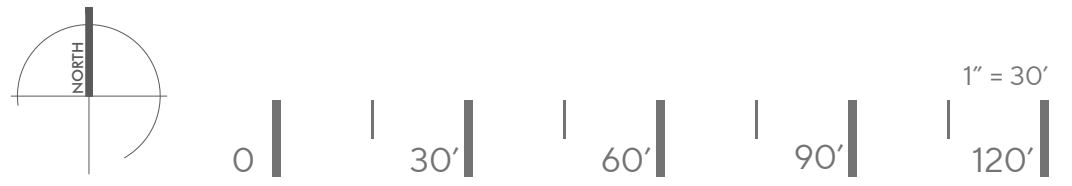


- GENERAL NOTES**
1. ALL WORK TO COMPLY WITH SALT LAKE CITY STANDARDS AND SPECIFICATIONS.
 2. ALL IMPROVEMENTS MUST COMPLY WITH ADA STANDARDS AND RECOMMENDATIONS.
 3. ALL WORK SHALL COMPLY WITH THE RECOMMENDATIONS OF THE GEOTECHNICAL ENGINEER POSSIBLY INCLUDING, BUT NOT LIMITED TO, REMOVAL OF UNCONSOLIDATED FILL, ORGANICS, AND DEBRIS, PLACEMENT OF SUBSURFACE DRAIN LINES AND GEOTEXTILE, AND OVEREXCAVATION OF UNSUITABLE BEARING MATERIALS AND PLACEMENT OF ACCEPTABLE FILL MATERIAL.
 4. THE CONTRACTOR SHALL BECOME FAMILIAR WITH THE EXISTING SOIL CONDITIONS.
 5. ELEVATIONS HAVE BEEN TRUNCATED FOR CLARITY. XXXX REPRESENTS AN ELEVATION OF 42XX.XX ON THESE PLANS.
 6. LANDSCAPED AREAS REQUIRE SUBGRADE TO BE MAINTAINED AT A SPECIFIC ELEVATION BELOW FINISHED GRADE AND REQUIRE SUBGRADE TO BE PROPERLY PREPARED AND SCARIFIED. SEE LANDSCAPE PLANS FOR ADDITIONAL INFORMATION.
 7. SLOPE ALL LANDSCAPED AREAS AWAY FROM BUILDING FOUNDATIONS TOWARD CURB AND GUTTER OR STORM DRAIN INLETS.
 8. EXISTING UNDERGROUND UTILITIES AND IMPROVEMENTS ARE SHOWN IN THEIR APPROXIMATE LOCATIONS BASED UPON RECORD INFORMATION AVAILABLE AT THE TIME OF PREPARATION OF THESE PLANS. LOCATIONS MAY NOT HAVE BEEN VERIFIED IN THE FIELD AND NO GUARANTEE IS MADE AS TO THE ACCURACY OR COMPLETENESS OF THE INFORMATION SHOWN. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THE EXISTENCE AND LOCATION OF THE UTILITIES SHOWN ON THESE PLANS OR INDICATED IN THE FIELD BY LOCATING SERVICES. ANY ADDITIONAL COSTS INCURRED AS A RESULT OF THE CONTRACTOR'S FAILURE TO VERIFY THE LOCATIONS OF EXISTING UTILITIES PRIOR TO THE BEGINNING OF CONSTRUCTION IN THEIR VICINITY SHALL BE BORNE BY THE CONTRACTOR AND ASSUMED INCLUDED IN THE CONTRACT. THE CONTRACTOR IS TO VERIFY ALL CONNECTION POINTS WITH THE EXISTING UTILITIES. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE CAUSED TO THE EXISTING UTILITIES AND UTILITY STRUCTURES THAT ARE TO REMAIN. IF CONFLICTS WITH EXISTING UTILITIES OCCUR, THE CONTRACTOR SHALL NOTIFY THE ENGINEER PRIOR TO CONSTRUCTION TO DETERMINE IF ANY FIELD ADJUSTMENTS SHOULD BE MADE.
 9. ALL STORM DRAIN INFRASTRUCTURE TO BE INSTALLED PER SALT LAKE CITY OR APWA STANDARD PLANS AND SPECIFICATIONS.
 10. ENSURE MINIMUM COVER OVER ALL STORM DRAIN PIPES PER MANUFACTURER'S RECOMMENDATIONS. NOTIFY ENGINEER IF MINIMUM COVER CANNOT BE ATTAINED.
 11. ALL FACILITIES WITH DOWNSPOUTS/ROOF DRAINS SHALL BE CONNECTED TO THE STORM DRAIN SYSTEM. SEE PLUMBING PLANS FOR DOWNSPOUT/ROOF DRAIN LOCATIONS AND SIZES. ALL ROOF DRAINS TO HAVE MINIMUM 1% SLOPE.
 12. THE CONTRACTOR SHALL ADJUST TO GRADE ALL EXISTING UTILITIES AS NEEDED PER LOCAL GOVERNING AGENCY'S STANDARDS AND SPECIFICATIONS.
 13. NOTIFY ENGINEER OF ANY DISCREPANCIES IN DESIGN OR STAKING BEFORE PLACING CONCRETE, ASPHALT, OR STORM DRAIN STRUCTURES OR PIPES.
 14. THE CONTRACTOR IS TO PROTECT AND PRESERVE ALL EXISTING IMPROVEMENTS, UTILITIES, AND SIGNS, ETC. UNLESS OTHERWISE NOTED ON THESE PLANS.



DISTRICT NORTH
 SALT LAKE CITY, UT

GRADING AND DRAINAGE PLAN



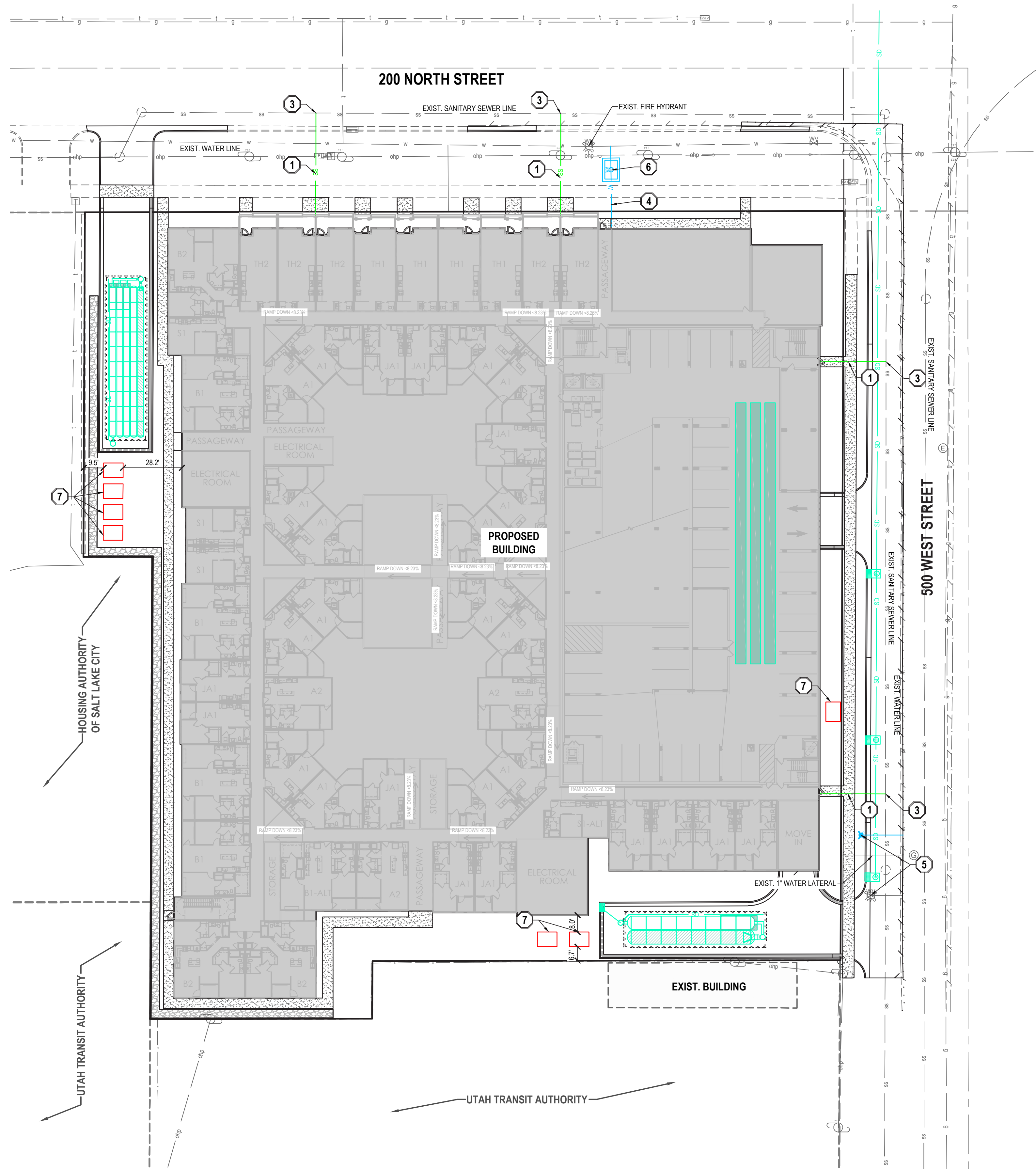
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JOB NO. 2024-0651
 DATE 03-06-2025
 3/18

BENCHMARK
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3. ALL SANITARY SEWER INFRASTRUCTURE TO BE INSTALLED PER SALT LAKE CITY STANDARD PLANS AND SPECIFICATIONS.
4. ALL WATER INFRASTRUCTURE TO BE INSTALLED PER SALT LAKE CITY OR APWA STANDARD PLANS AND SPECIFICATIONS.
5. DEFLECT OR LOOP ALL WATERLINES TO AVOID CONFLICTS WITH OTHER UTILITIES PER GOVERNING AGENCY'S STANDARDS AND SPECIFICATIONS.
6. PROJECT SHALL COMPLY WITH ALL UTAH DIVISION OF DRINKING WATER RULES AND REGULATIONS INCLUDING, BUT NOT LIMITED TO, THOSE PERTAINING TO BACKFLOW PROTECTION AND CROSS CONNECTION PREVENTION.
7. THE CONTRACTOR IS TO COORDINATE ALL UTILITIES WITH MECHANICAL/PLUMBING PLANS.
8. NOTIFY ENGINEER OF ANY DISCREPANCIES IN DESIGN OR STAKING BEFORE PLACING UTILITY STRUCTURES OR PIPES.
9. THE CONTRACTOR SHALL ADJUST TO GRADE ALL EXISTING UTILITIES AS NEEDED PER LOCAL GOVERNING AGENCY'S STANDARDS AND SPECIFICATIONS.
10. THE CONTRACTOR IS TO PROTECT AND PRESERVE ALL EXISTING IMPROVEMENTS, UTILITIES, AND SIGNS, ETC. UNLESS OTHERWISE NOTED ON THESE PLANS.

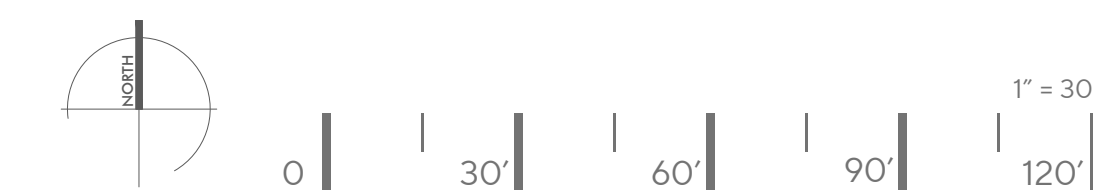
SCOPE OF WORK:
 PROVIDE, INSTALL AND/OR CONSTRUCT THE FOLLOWING PER THE SPECIFICATIONS GIVEN OR REFERENCED, THE DETAILS NOTED, AND/OR AS SHOWN ON THE CONSTRUCTION DRAWINGS:

- 1 6" SDR-35 PVC SANITARY SEWER LATERAL, INCLUDING CLEANOUTS AT MAXIMUM 100-FOOT SPACING, PER GOVERNING AGENCY'S STANDARDS AND SPECIFICATIONS. LENGTH AND SLOPE PER PLAN.
- 2 SANITARY SEWER CLEANOUT PER SALT LAKE CITY PUBLIC UTILITIES STANDARDS AND SPECIFICATIONS.
- 3 CONNECT TO EXISTING SEWER MAIN PER SALT LAKE CITY PUBLIC UTILITIES STANDARDS AND SPECIFICATIONS.
- 4 4" C-900 PVC POTABLE WATER LINE PER SALT LAKE CITY PUBLIC UTILITIES STANDARDS AND SPECIFICATIONS.
- 5 RELOCATE FIRE HYDRANT ASSEMBLY PER APWA STANDARD PLAN NO. 546 AND SPECIFICATIONS.
- 6 4" CULINARY WATER METER AND CONCRETE VAULT PER APWA STANDARD PLAN NO. 523 AND SPECIFICATIONS.
- 7 ELECTRICAL TRANSFORMER BOX.



DISTRICT NORTH
 SALT LAKE CITY, UT

UTILITY PLAN



C3

JOB NO. 2024-0651
 DATE 03-06-2025
 3/18

ATTACHMENT C: Site & Vicinity Photos



© 2025

View Looking South East



View Looking North West



N 5th W

200 N

View Looking South

ATTACHMENT D: TSA-UC-T & Base Zoning Standards

TSA-UC-T (Transit Station Area – Urban Center – Transition) Zoning District

Purpose Statement: The purpose of the TSA Transit Station Area District is to provide an environment for efficient and attractive transit and pedestrian oriented commercial, residential and mixed use development around transit stations. Redevelopment, infill development and increased development on underutilized parcels should include uses that allow them to function as part of a walkable, mixed use district. Existing uses that are complementary to the district, and economically and physically viable, should be integrated into the form and function of a compact, mixed use pedestrian oriented neighborhood. Each transit station is categorized into a station type. These typologies are used to establish appropriate zoning regulations for similar station areas.

TSA-UN-T Standards (21A.26.078)			
Standard	Requirement	Proposed	Finding
Land Use	Any permitted use in 21A.33.050	Residential	Complies
Lot Size/Width	Minimum 2,500 SF/40' width	2.91 acres (126,760 SF)/360' width	Complies
Maximum Building Height	60'	63'9"	Planned Development approval required
Front/Corner/Side/Rear Yard Setbacks	<p>Front (with RMF-35 across the street): Minimum: 25% of the lot depth, up to 25'. For buildings taller than 25', setback shall increase 2' for every 1' of building height above 25' and is applied to the portion of the building over 25' in height.</p> <p>Corner: At least 50% of the street facing building facade shall be within 5' of the front or corner side property line.</p>	<p>Front (200 N): 8'4" Corner Side (500 W): 4'9" Interior Side: 15-40' from western property line Rear: 10'-20' from southern property line</p>	<p>Front: Planned Development approval required Corner: Complies Side: Complies Rear: Complies</p>

	Interior Side Yard/Rear: None		
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General Zoning Standards			
Standard	Requirement	Proposed	Finding
Refuse Control (21A.30.010)	Refuse to be covered and enclosed or screened from public view	Located within the building	Complies
Lighting (21A.30.010)	Lighting to not cast onto adjacent properties	Lighting will be designed in accordance with the SLC Lighting Master Plan	Complies
Off Street Parking & Loading (21A.44)	Studio- no minimum 1 bedroom- .5 per DU (2 max) 2+ bedroom- 1 per DU (3 max)	Studio – 28 units, 28 stalls 1 bedroom – 237 units, 237 stalls 2 bedroom – 85 units, 170 stalls Total: 439 total stalls (104 EV ready)	Complies
Landscaping & Street Trees (21A.48)	All setbacks to be landscaped. Street trees at 1 per 20' for small, 1 per 30' for medium, 1 per 40' for large	<u>11 trees required on 200 N:</u> 7 new Goldenrain & 4 existing Goldenrain provided on 200 N <u>11 trees required on 500 W:</u> 11 Zelkova along 500 W 4 Green Spruces & 7 Eastern Redbuds to be planted along side/rear property lines	Complies
Signage (21A.46.110)	n/a	n/a	To be submitted with building permit

Design Standards (21A.37)			
Standard	Requirement	Proposed	Finding

Ground Floor Use	80% - Can be reduced by 15% if the ground floor has residential units	<u>200 N</u> : 67% - residential units count towards the percentage, but the lounge and lobby do not <u>500 W</u> : 52% of the building façade is parking, the remainder is portions of the lobby and bike storage	Design Review required
Building Materials: Ground Floor	90%	200 N: 92% 500 W: 31% The lower durable material number is due to the 500 W façade being a parking structure.	Design Review required
Building Materials: Upper Floors	60%	200 N: 70% 500 W: 13% Materials include brick veneer and vertical shading screens with foliage. Architectural plaster (EIFS) is also used, but it not considered a durable material.	Design Review required
Ground Floor Glass	60% 45% for residential uses	200 N: 53% 500 W: 15%	Design Review required
Building Entrances	40'	200 N: There are 11 entry points, 9 to the individual townhomes and 2 for the lounge and lobby. The spacing between the lounge and lobby entrances is more than 40'. 500 W: The façade is primarily a parking structure. There are 3 total entrances; 2 to utility spaces and 1 into the bike/ski repair space.	Design Review required
Maximum Length of Blank Wall	15'	No wall exceeds 15' without providing windows, doors, or architectural detailing on the ground floor.	Complies
Maximum Length of Street Facing Facade	200'	200 N: 319' 500 W: 360'	Design Review required
Exterior Lighting	Yes	Exterior lighting will be shielded and directed downwards.	Reviewed for compliance during the building permit process

Parking Lot Lighting	Yes	Not applicable, there is not a surface level parking lot proposed.	N/A
Screening of Mechanical Equipment	Yes	Mechanical equipment is located on the roof or within the side/rear yard area. The transformers will be screened.	Complies
Screening of Service Areas	Yes	All service areas, loading docks, refuse containers and similar areas are fully screened from public view.	Complies

ATTACHMENT E: Planned Development Standards

21A.55.050: Standards for Planned Developments: The planning commission may approve, approve with conditions, or deny a planned development based upon written findings of fact according to each of the following standards. It is the responsibility of the applicant to provide written and graphic evidence demonstrating compliance with the following standards.

The Finding for each standard is the recommendation of the Planning Division based on the facts associated with the proposal, the discussion that follows, and the input received during the engagement process. Input received after the staff report is published has not been considered in this report.

A. Planned Development Objectives: The planned development shall meet the purpose statement for a planned development (section 21A.55.010 of this chapter) and will achieve at least one of the objectives stated in said section. To determine if a planned development objective has been achieved, the applicant shall demonstrate that at least one of the strategies associated with the objective are included in the proposed planned development. The applicant shall also demonstrate why modifications to the zoning regulations are necessary to meet the purpose statement for a planned development. The Planning Commission should consider the relationship between the proposed modifications to the zoning regulations and the purpose of a planned development, and determine if the project will result in a more enhanced product than would be achievable through strict application of the land use regulations.

Planned Development Purpose Statement: A planned development is intended to encourage the efficient use of land and resources, promoting greater efficiency in public and utility services and encouraging innovation in the planning and building of all types of development. Further, a planned development implements the purpose statement of the zoning district in which the project is located, utilizing an alternative approach to the design of the property and related physical facilities. A planned development incorporates special development characteristics that help to achieve City goals identified in adopted Master Plans and that provide an overall benefit to the community as determined by the planned development objectives. A planned development will result in a more enhanced product than would be achievable through strict application of land use regulations, while enabling the development to be compatible with adjacent and nearby land developments.

Discussion: The proposed development is directly aligned with the purpose statement for the TSA-UC-T zone. The project addresses the need for additional medium-density housing near transit. The development also aligns with many of the objectives of Plan Salt Lake and the North Temple Boulevard Plan. See Consideration 2.

Finding: Meets Purpose Statement Does Not Meet Purpose Statement

A. Open Space And Natural Lands: Preserving, protecting or creating open space and natural lands:

1. Inclusion of community gathering places or public recreational opportunities, such as new trails or trails that connect to existing or planned trail systems, playgrounds or other similar types of facilities.

2. Preservation of critical lands, watershed areas, riparian corridors and/or the urban forest.
3. Development of connected greenways and/or wildlife corridors.
4. Daylighting of creeks/water bodies.
5. Inclusion of local food production areas, such as community gardens.
6. Clustering of development to preserve open spaces.

Discussion: The applicant is not intending to meet this objective. Only one Planned Development objective must be fulfilled.

Finding: Objective Satisfied Objective Not Satisfied

B. Historic Preservation:

1. Preservation, restoration, or adaptive reuse of buildings or structures that contribute to the character of the City either architecturally and/or historically, and that contribute to the general welfare of the residents of the City.
2. Preservation of, or enhancement to, historically significant landscapes that contribute to the character of the City and contribute to the general welfare of the City's residents.

Discussion: The applicant is not intending to meet this objective. Only one Planned Development objective must be fulfilled.

Finding: Objective Satisfied Objective Not Satisfied

C. Housing: Providing affordable housing or types of housing that helps achieve the City's housing goals and policies:

1. At least twenty percent (20%) of the housing must be for those with incomes that are at or below eighty percent (80%) of the area median income.
2. The proposal includes housing types that are not commonly found in the existing neighborhood but are of a scale that is typical to the neighborhood.

Discussion: The applicant is not intending to meet this objective. Only one Planned Development objective must be fulfilled.

Finding: Objective Satisfied Objective Not Satisfied

D. Mobility: Enhances accessibility and mobility:

1. Creating new interior block walkway connections that connect through a block or improve connectivity to transit or the bicycle network.
2. Improvements that encourage transportation options other than just the automobile.

Discussion: The applicant is not intending to meet this objective. Only one Planned Development objective must be fulfilled.

Finding: Objective Satisfied Objective Not Satisfied

E. Sustainability: Creation of a project that achieves exceptional performance with regards to resource consumption and impact on natural systems:

- 1. Energy Use And Generation: Design of the building, its systems, and/or site that allow for a significant reduction in energy usage as compared with other buildings of similar type and/or the generation of energy from an on-site renewable resource.**
- 2. Reuse Of Priority Site: Locate on a brownfield where soil or groundwater contamination has been identified, and where the local, State, or national authority (whichever has jurisdiction) requires its remediation. Perform remediation to the satisfaction of that authority.**

Discussion: The applicant is not intending to meet this objective. Only one Planned Development objective must be fulfilled.

Finding: Objective Satisfied Objective Not Satisfied

F. Master Plan Implementation: A project that helps implement portions of an adopted Master Plan in instances where the Master Plan provides specific guidance on the character of the immediate vicinity of the proposal:

- 1. A project that is consistent with the guidance of the Master Plan related to building scale, building orientation, site layout, or other similar character-defining features. (Ord. 8-18, 2018)**

Discussion: The project is within the Guadalupe neighborhood of the North Temple Boulevard Plan. The project is in the Northwest Quadrant of the Viaduct Transfer Station Area Plan, which is characterized by low-density residential, manufacturing uses along 500 West, and a lack of connectivity between 400-600 West due to the rail. The plan calls for mid-rise housing (7-10 stories) with very high densities, over 100 units per acre. The proposed building is 6 stories, the maximum height allowed within the Transition area is 60 feet, and provides 120 units per acre. The plan also calls for buildings located close to the street with active uses on the ground floor and structured parking with little or no surface parking lots. Both of these characteristics have been met, while still taking into consideration the lack of connectivity along 500 W that will not support retail uses now or likely in the future.

Finding: Objective Satisfied Objective Not Satisfied

B. Master Plan Compatibility: The proposed planned development is generally consistent with adopted policies set forth in the Citywide, community, and/or small area Master Plan that is applicable to the site where the planned development will be located.

Finding: Complies

Discussion: The project is within the Viaduct Transfer Station Area of the North Temple Boulevard Plan. As discussed in Consideration 2, the project aligns with the adopted policies set forth by the City.

Condition(s):

C. Design And Compatibility: The proposed planned development is compatible with the area the planned development will be located and is designed to achieve a more enhanced product than would be achievable through strict application of land use regulations. In determining design and compatibility, the Planning Commission should consider:

1. Whether the scale, mass, and intensity of the proposed planned development is compatible with the neighborhood where the planned development will be located and/or the policies stated in an applicable Master Plan related to building and site design;

Finding: Complies

Discussion: The scale, mass, and intensity of the development is compatible with newer structures built across the railroad tracks to the East and is in line with the existing TSA-UC-T zoning. The development potential of surrounding properties is the same as the subject property and the proposed height of 63'8" is comparable to much of the city's podium style development.

Condition(s):

2. Whether the building orientation and building materials in the proposed planned development are compatible with the neighborhood where the planned development will be located and/or the policies stated in an applicable Master Plan related to building and site design;

Finding: Complies

Discussion: The building is oriented towards 200 North. Building materials include brick, architectural plaster and EIFS. The chosen materials are representative of newer development across the city.

Condition(s):

3. Whether building setbacks along the perimeter of the development:
- a. Maintain the visual character of the neighborhood or the character described in the applicable Master Plan.
 - b. Provide sufficient space for private amenities.
 - c. Provide sufficient open space buffering between the proposed development and neighboring properties to minimize impacts related to privacy and noise.
 - d. Provide adequate sight lines to streets, driveways and sidewalks.
 - e. Provide sufficient space for maintenance.

Finding: Complies

Discussion:

- a. The building setbacks support the character of the neighborhood and are appropriate for the site. The building is of similar scale to newer development and will be comparable to future development within the area.
- b. The proposal includes two large ground level courtyards and a rooftop amenity space oriented to the east.
- c. The proposal has a wide 40-foot setback from the western lot line (adjacent to a 3-story multifamily apartment building) and between 10-40 feet from the northern interior lot line. The property provides sufficient buffering from other uses, while still maintaining an engaging street frontage along 200 N.
- d. Only one driveway is proposed along 500 W. It does not conflict with pedestrian or vehicular traffic.
- e. The building has large interior setbacks and adequate setbacks from both streets. All of the setback areas are fully landscaped with trees along both street frontages. Transformers are located at the end of the fire and loading area drive aisles, and are easily accessible.

Condition(s):

- 4. Whether building facades offer ground floor transparency, access, and architectural detailing to facilitate pedestrian interest and interaction;

Finding: Complies

Discussion: The applicant is requesting a reduction in ground floor transparency along 500 W. The 200 N ground floor maximizes the fenestration for both the lobby/lounge and residential uses, while still providing privacy for residents. The 500 W façade consists of mainly a parking structure, which lacks typical fenestration. While that is not provided, the setback area is fully landscaped to help soften the streetscape and provide some pedestrian interest.

Condition(s):

- 5. Whether lighting is designed for safety and visual interest while minimizing impacts on surrounding property;

Finding: Complies With Conditions

Discussion: All street lighting will adhere to the Salt Lake City Lighting Master Plan.

Condition(s): Staff is recommending that final details of the building lighting be delegated to staff for approval.

- 6. Whether dumpsters, loading docks and/or service areas are appropriately screened;

Finding: Complies

Discussion: All dumpsters and services areas are within the structure. The residential loading area is along the norther façade and is not highly visible from the street.
Condition(s):
7. Whether parking areas are appropriately buffered from adjacent uses.
Finding: Complies
Discussion: All parking is within the structure and accessed via one entry point on 500 W.
Condition(s):

D. Landscaping: The proposed planned development preserves, maintains or provides native landscaping where appropriate. In determining the landscaping for the proposed planned development, the Planning Commission should consider:
1. Whether mature native trees located along the periphery of the property and along the street are preserved and maintained;
Finding: Complies
Discussion: The subject property currently houses a storage facility. There are 4 street trees along 200 N that will be preserved.
Condition(s):
2. Whether existing landscaping that provides additional buffering to the abutting properties is maintained and preserved;
Finding: Complies
Discussion: The applicant’s landscaping plans delineate existing trees from new ones. The existing trees adjacent to the site will be protected during construction.
Condition(s):
3. Whether proposed landscaping is designed to lessen potential impacts created by the proposed planned development;
Finding: Complies
Discussion: The landscaping proposal incorporates native plants to reduce the developments impact on the natural environment. The proposal adds 19 street trees and 14 trees against the parking structure, in addition to shrubs and ground cover.

Condition(s):
4. Whether proposed landscaping is appropriate for the scale of the development.
Finding: Complies
Discussion: The proposed landscaping is appropriate for the scale of the development.
Condition(s):

E. Mobility: The proposed planned development supports Citywide transportation goals and promotes safe and efficient circulation within the site and surrounding neighborhood. In determining mobility, the Planning Commission should consider:
1. Whether drive access to local streets will negatively impact the safety, purpose and character of the street;
Finding: Complies
Discussion: There is one proposed vehicular entry off of 500 W. The street currently does not have any infrastructure improvements, so the proposal will increase safety and enhance the character of the street by adding sidewalks and a park strip.
Condition(s):
2. Whether the site design considers safe circulation for a range of transportation options including: <ul style="list-style-type: none"> a. Safe and accommodating pedestrian environment and pedestrian oriented design; b. Bicycle facilities and connections where appropriate, and orientation to transit where available; and c. Minimizing conflicts between different transportation modes;
Finding: Complies
Discussion: The proposal adds sidewalks, park strips, and a bike lane where there is currently no public improvement. The building is also located near the North Temple TRAX station, facilitating the use of transit. Bike storage is provided within the development.
Condition(s):
3. Whether the site design of the proposed development promotes or enables access to adjacent uses and amenities;
Finding: Complies

Discussion: The development is less than a block from a major public transit hub. The Gateway mall and the Delta Center are located across North Temple on 400 West.

Condition(s):

4. Whether the proposed design provides adequate emergency vehicle access;

Finding: Complies

Discussion: The proposal was reviewed by all city departments. There were no concerns brought up about emergency vehicle access. The applicant's AM&M was approved by the Fire Department.

Condition(s):

5. Whether loading access and service areas are adequate for the site and minimize impacts to the surrounding area and public rights-of-way.

Finding: Complies

Discussion: The loading area is on the south side of the building, within an interior side yard setback. There will be no conflicts between the loading areas and existing or proposed improvements to the public right-of-way.

Condition(s):

F. Existing Site Features: The proposed planned development preserves natural and built features that significantly contribute to the character of the neighborhood and/or environment.

Finding: Not Applicable

Discussion: The existing site does not have any natural or built features that contribute to the character of the area. The current site features a storage building and parking lot. This site is slated for redevelopment; the storage building will be replaced by a new multifamily structure.

Condition(s):

G. Utilities: Existing and/or planned utilities will adequately serve the development and not have a detrimental effect on the surrounding area.

Finding: Complies

Discussion: The proposed and existing utilities will adequately serve the development and not have a detrimental effect on the surrounding area. The applicant will be burying the existing power lines to help create a more engaging streetscape.

Condition(s):

ATTACHMENT F: Design Review Standards

21A.59.050: Standards for Design Review: In addition to standards provided in other sections of this title for specific types of approval, the following standards shall be applied to all applications for design review:

The Finding for each standard is the recommendation of the Planning Division based on the facts associated with the proposal, the discussion that follows, and the input received during the engagement process. Input received after the staff report is published has not been considered in this report.

A. Any new development shall comply with the intent of the purpose statement of the zoning district and specific design regulations found within the zoning district in which the project is located as well as the City's adopted "urban design element" and adopted master plan policies and design guidelines governing the specific area of the proposed development.

Finding: Complies

Discussion: The proposed development is directly aligned with the purpose statement for the TSA-UC-T zone. The project addresses the need for additional housing and retail to support the more intensive Core districts and the Downtown area. The development also aligns with many of the objectives of the North Temple Boulevard Plan. See Consideration 2 of the staff report.

Condition(s):

B. Development shall be primarily oriented to the sidewalk, not an interior courtyard or parking lot.

- 1. Primary entrances shall face the public sidewalk (secondary entrances can face a parking lot).**
- 2. Building(s) shall be sited close to the public sidewalk, following and responding to the desired development patterns of the neighborhood.**
- 3. Parking shall be located within, behind, or to the side of buildings.**

Finding: Complies

Discussion:

- 1.** The development and primary building entrances are oriented towards 200 N and the public sidewalk. The proposal does not include a parking lot.
- 2.** The building setbacks of the street facing facades are sited close to the sidewalk, consistent with the desired development pattern identified in the Viaduct Transfer Station Area plan.
- 3.** An interior parking garage is located within the footprint of the building and consists of 439 parking stalls. The parking garage is oriented towards 500 W, keeping the residential units away from the rail lines directly east of the property.

Condition(s):

C. Building facades shall include detailing and glass in sufficient quantities to facilitate pedestrian interest and interaction.

- 1. Locate active ground floor uses at or near the public sidewalk.**
- 2. Maximize transparency of ground floor facades.**
- 3. Use or reinterpret traditional storefront elements like sign bands, clerestory glazing, articulation, and architectural detail at window transitions.**
- 4. Locate outdoor dining patios, courtyards, plazas, habitable landscaped yards, and open spaces so that they have a direct visual connection to the street and outdoor spaces.**

Finding: Complies With Conditions

Discussion:

1. Residential units, which count as an active use, are along 200 N. 500 W is not conducive to retail uses, but the residential lobby and lounge are situated at the northeastern corner, which will bring some activity to that space.
2. The corner lobby is approximately 21'6" in height and is primarily composed of glass and storefront window systems. The glass extends from the floor to ceiling and wraps around the northeastern façade. The 500 W façade lacks in transparency due to the parking use along the street.
3. The applicant is proposing floor to ceiling storefront window systems with steel overhead window awnings and storefront signage. The windows are recessed, adding depth and dimension to the facades.
4. The applicant is proposing two ground level interior courtyards, which are not connected to the street but do provide an amenity to the residents. The top of the parking structure will include additional amenity space and a pool that faces eastward.

Condition(s): Design Review approval is requiring for reduced transparency along 500 W.

D. Large building masses shall be divided into heights and sizes that relate to human scale.

- 1. Relate building scale and massing to the size and scale of existing and anticipated buildings, such as alignments with established cornice heights, building massing, step-backs and vertical emphasis.**
- 2. Modulate the design of a larger building using a series of vertical or horizontal emphases to equate with the scale (heights and widths) of the buildings in the context and reduce the visual width or height.**

3. **Include secondary elements such as balconies, porches, vertical bays, belt courses, fenestration and window reveals.**
4. **Reflect the scale and solid-to-void ratio of windows and doors of the established character of the neighborhood or that which is desired in the master plan.**

Finding: Complies

Discussion:

1. The proposed building is 6 stories, and measures approximately 64 feet to the top of the roof. While the TSA zoning is in place for surrounding properties, they have yet to redevelop. The building is in scale with TSA development on the east side of the railroad tracks.
2. The proposed building modulates well to relate to the scale of pedestrians and to the surrounding buildings. The ground floor of the building's north street facing facade consists of residential units and storefront window systems, with delineated entrances for each use. The visual massing of the building is reduced through the application of differentiating exterior materials and colors.
3. The building massing is visually and structurally broken down into smaller masses through implementation of projecting and recessed wall planes, recessed balconies, and defined building entries.
4. The scale and solid-to-void ratio of windows and doors along 200 N reflects the small scale development along the street, which includes 3-story apartments and two single-family dwellings. The 500 W façade is substantially similar to the development across the street, with the parking structure facing the rail lines rather than an interior lot line. The structure is open but includes screening and plants to help mitigate its appearance.

Condition(s):

E. Building facades that exceed a combined contiguous building length of two hundred feet (200') shall include:

1. **Changes in vertical plane (breaks in facade)**
2. **Material changes; and**
3. **Massing changes.**

Finding: Complies

1. The 200 N street facing façade of the building measures approximately 319' in length. The façade features recessed and projecting architectural features such as patios, balconies and stepbacks, which create breaks in the massing. The 500 W façade is primarily a parking structure. The lobby on the corner and ski/bike repair space are closer to the property line than the parking structure, which is meant to visually reduce the scale of the structure.

2. Both the 200 N and 500 W facades have changes in material. Brick highlights the lobby and lounge spaces and the use of smooth and rough architectural plaster is used for the townhomes along 200 N.
3. The northern façade of the building is visually and structurally broken down into smaller masses through implementation of recessed balconies, and façade stepbacks at level 3.

Condition(s):

F. If provided, privately-owned public spaces shall include at least three (3) of the six (6) following elements:

1. **Sitting space of at least one sitting space for each two hundred fifty (250) square feet shall be included in the plaza. Seating shall be a minimum of sixteen inches (16") in height and thirty inches (30") in width. Ledge benches shall have a minimum depth of thirty inches (30");**
2. **A mixture of areas that provide seasonal shade;**
3. **Trees in proportion to the space at a minimum of one tree per eight hundred (800) square feet, at least two inch (2") caliper when planted;**
4. **Water features or public art;**
5. **Outdoor dining areas; and**
6. **Other amenities not listed above that provide a public benefit.**

Finding: Complies

Discussion: The project does not provide privately-owned public spaces.

Condition(s):

G. Building height shall be modified to relate to human scale and minimize negative impacts. In downtown and in the CSHBD Sugar House Business District, building height shall contribute to a distinctive City skyline.

1. **Human scale:**
 - a. **Utilize stepbacks to design a building that relate to the height and scale of adjacent and nearby buildings, or where identified, goals for future scale defined in adopted master plans.**
 - b. **For buildings more than three (3) stories or buildings with vertical mixed use, compose the design of a building with distinct base, middle and top sections to reduce the sense of apparent height.**
2. **Negative impacts:**
 - a. **Modulate taller buildings vertically and horizontally so that it steps up or down to its neighbors.**
 - b. **Minimize shadow impacts of building height on the public realm and semi-public spaces by varying building massing. Demonstrate impact from shadows due to building height for the portions of the building that are subject to the request for additional height.**

- c. **Modify tall buildings to minimize wind impacts on public and private spaces, such as the inclusion of a wind break above the first level of the building.**

3. Cornices and rooflines:

- a. **Cohesiveness: Shape and define rooflines to be cohesive with the building's overall form and composition.**
- b. **Complement Surrounding Buildings: Include roof forms that complement the rooflines of surrounding buildings.**
- c. **Green Roof And Roof Deck: Include a green roof and/or accessible roof deck to support a more visually compelling roof landscape and reduce solar gain, air pollution, and the amount of water entering the stormwater system.**

Finding: Complies

Discussion:

Human Scale

The building is designed with a base (townhomes), middle, and top (defined cornice). The first two floors are human scale and reflect the 1-3 story development along 200 N with the townhouse building form. The middle of the building is stepped back from the top of the townhomes, which softens the street wall.

Negative Impacts

The building is not stepped down to its western neighbor, a 3-story apartment complex, but it is setback from the property line by 40 feet. There is no other development abutting the property that would be impacted by the height or shadows of the building.

Rooflines and Cornices

The roofline of the building varies in height and architectural design. There are no other buildings within the vicinity for the development to architecturally complement.

Condition(s):

H. Parking and on site circulation shall be provided with an emphasis on making safe pedestrian connections to the sidewalk, transit facilities, or midblock walkway.

Finding: Complies

Discussion: The sole parking entrance is located along 500 W. The entrance does not conflict with vehicle or pedestrian circulation and is less than the maximum 30' width allowable for garage entrances.

Condition(s):

I. **Waste and recycling containers, mechanical equipment, storage areas, and loading docks shall be fully screened from public view and shall incorporate building materials and detailing compatible with the building being served.**

Service uses shall be set back from the front line of building or located within the structure. (See subsection 21A.37.050K of this title.)

Finding: Complies

Discussion: Waste and recycling containers, mechanical equipment, storage areas are located inside the parking structure. The loading area is located along the southern property line and hidden from public view.

Condition(s):

J. Signage shall emphasize the pedestrian/mass transit orientation.

- 1. Define specific spaces for signage that are integral to building design, such as commercial sign bands framed by a material change, columns for blade signs, or other clearly articulated band on the face of the building.**
- 2. Coordinate signage locations with appropriate lighting, awnings, and other projections.**
- 3. Coordinate sign location with landscaping to avoid conflicts.**

Finding: Complies With Conditions

Discussion: Signage will be reviewed with the building permit. All signage location will be designed with the existing and new landscaping in mind so as not to obstruct wayfinding and views.

Condition(s): Staff is recommending that final details of the building signage be delegated to staff for approval.

K. Lighting shall support pedestrian comfort and safety, neighborhood image, and dark sky goals.

- 1. Provide street lights as indicated in the Salt Lake City Lighting Master Plan.**
- 2. Outdoor lighting should be designed for low-level illumination and to minimize glare and light trespass onto adjacent properties and uplighting directly to the sky.**
- 3. Coordinate lighting with architecture, signage, and pedestrian circulation to accentuate significant building features, improve sign legibility, and support pedestrian comfort and safety.**

Finding: Complies With Conditions

Discussion: The street lighting will be designed in accordance with the Salt Lake City Lighting Master Plan. Outdoor lighting will be designed for low-level illumination and to minimize glare and light trespass onto adjacent properties and uplighting directly to the sky. Lighting will coordinate with signage locations to provide safe pedestrian walkways along 200 N and 500 W.

Condition(s): Staff is recommending that final details of the building lighting be delegated to staff for approval.

L. Streetscape improvements shall be provided as follows:

1. One street tree chosen from the street tree list consistent with the City's urban forestry guidelines and with the approval of the City's Urban Forester shall be placed for each thirty feet (30') of property frontage on a street. Existing street trees removed as the result of a development project shall be replaced by the developer with trees approved by the City's Urban Forester.
2. Hardscape (paving material) shall be utilized to differentiate privately-owned public spaces from public spaces. Hardscape for public sidewalks shall follow applicable design standards. Permitted materials for privately-owned public spaces shall meet the following standards:
 - a. Use materials that are durable (withstand wear, pressure, damage), require a minimum of maintenance, and are easily repairable or replaceable should damage or defacement occur.
 - b. Where practical, as in lower-traffic areas, use materials that allow rainwater to infiltrate into the ground and recharge the water table.
 - c. Limit contribution to urban heat island effect by limiting use of dark materials and incorporating materials with a high Solar- Reflective Index (SRI).
 - d. Utilize materials and designs that have an identifiable relationship to the character of the site, the neighborhood, or Salt Lake City.
 - e. Use materials (like textured ground surfaces) and features (like ramps and seating at key resting points) to support access and comfort for people of all abilities.
 - f. Asphalt shall be limited to vehicle drive aisles.

Finding: Complies

Discussion:

Street trees have been provided and approved by Urban Forestry and hardscape will meet the standards listed above.

Condition(s):

ATTACHMENT G: Public Process & Comments

Public Notice, Meetings, Comments

The following is a list of public meetings that have been held, and other public input opportunities, related to the proposed project since the applications were submitted:

- January 3, 2025 – The 45-day required notice for recognized community organizations was sent to both the Fairpark and Capitol Hill community councils. The comment period ended on February 18, 2025.
- January 10, 2025 – Property owners and residents within 300 feet of the development were provided early notification of the proposal.

Notice of the public hearing for the proposal included:

- March 14, 2025
 - Public hearing notice signage posted on the property.
- March 14, 2025
 - Public hearing notice mailed.
 - Public notice posted on City and State websites and Planning Division list serve.

Public Input:

At the time of publication, comments were received from one community group. Comments were shared with the applicant and are provided below.

ATTACHMENT H: Department Review Comments

This proposal was reviewed by the following departments. Any requirement identified by a City Department is required to be complied with.

Building: Comments provided by Bryan Romney 1/21/25

1/21/2025: No specific comments in regard to the Design Review and Planned Development application.

All construction within the corporate limits of Salt Lake City shall be per the State of Utah adopted construction codes and to include any state or local amendments to those codes. RE: Title 15A State Construction and Fire Codes Act.

Existing structures on this or adjacent parcels shall not be made less complying to the construction codes than it was before the development and/or construction.

Any building code corrective action comments will be posted under the building permit application review.

Where there are practical difficulties involved in carrying out the provisions of the code, the code official shall have the authority to grant modifications for individual cases, provided that the code official shall first find that special individual reason makes the strict letter of this code impractical and the modification is in compliance with the intent and purpose of this code and that such modification does not lessen health, life and fire safety requirements.

An alternative material, design or method of construction shall be approved where the code official finds that the proposed design is satisfactory and complies with the intent of the provisions of the code, and that the material, method or work offered is, for the purpose intended, not less than the equivalent of that prescribed in this code in quality, strength, effectiveness, fire resistance, durability and safety.

Engineering: Comments provided by Scott Weiler on 1/23/25

APWA Standards apply to the work in the public way. A bond for the estimated value of public way improvements should accompany the application for a Permit to Work in the Public Way.

Fire: Comments provided by Douglas Bateman on 1/28/25

The comments provided may not include all items necessary to meet minimum requirements of fire and life safety codes. Unable to verify some information due to lack of dimensioning.

*Maximum distance of 150-feet from a fire access road to all ground level exterior walls has been exceeded.

*Fire access roads shall meet minimum width requirements

*Fire Department Connection shall be located on street address side of building with a hydrant within 100-feet which is on the same side of the street and will not block or obstruct driveways

or roads when hose is deployed.

*Verify Fire Pump is not needed to meet sprinkler demands.

*Aerial access shall be provided in accordance with IFC Appendix D 105 up to and including road widths, proximity distances and obstructions. Powerlines, utility lines and trees are obstructions and not allowed

*Hydrants need to provide the required fire flow and be spaced according to Appendix B and C

*Verify all interior portions of the building are located within 200-feet of standpipe connections

Transportation: Comments provided by Jena Carver on 1/28/25

1. A minimum of 2 short loading berths are required. The plans appear to show only 1 loading berth. Please clearly label and dimension 2 short loading berths turning paths to show that all loading and truck maneuvering can be completed on site. Show access from rear of loading berths to residence area.

2. All roadway improvements will require a separate public way permit from SLC Engineering. Additional review of the public way improvements, including signing and striping, will be completed in conjunction with the public way permit.

3. Parking is conceptually approved. Final review of parking, including ADA, EV, EV ready, and bicycle parking, will be completed with the building permit application.

Police: Comments provided by Lt. Andrew Cluff on 1/6/25

No comments. Development would benefit the public safety of the area and add needed housing.

Public Utilities: Comments provided by Kristeen Beitel on 1/22/25

Public Utilities comments have been provided to assist in the future development of the property. The following comments are provided for information only and do not provide official project review or approval.

With increased densification, the applicant must consider the potential increase in construction costs resulting from required offsite utility improvements, potentially downstream of the subject property. Densification may place greater demands on water, sewer, and storm drain systems, which could exceed the capacity of the existing infrastructure. Property owners and developers will be required to upgrade the offsite public utilities to ensure sufficient capacity for the new development.

- Public Utility permit, connection, survey, and inspection fees will apply.
- All utility design and construction must comply with APWA Standards and SLCPU Standard Practices.
- All utilities must meet horizontal and vertical clearance requirements. Water and sewer lines require 10 ft minimum horizontal separation and 18" minimum vertical separation. Sewer must maintain 5 ft minimum horizontal separation and 12" vertical separation from

any non-water utilities. Water must maintain 3 ft minimum horizontal separation and 12” vertical separation from any non-sewer utilities.

- Public street light requirements are determined during building permit review.
- Utilities cannot cross property lines without appropriate easements and agreements between property owners.
- Site utility and grading plans will be required for building permit review. Site utility plans should include all existing and proposed utilities, including water, irrigation, fire, sewer, stormwater, street lighting, power, gas, and communications. Please refer to APWA, SLCDPU Standard Practices, and the SLC Design Process Guide for utility design requirements.
- Applicant must provide fire flow, culinary water, and sewer demand calculations to SLCDPU for review. The public sewer and water system will be modeled with these demands. If the demand is not adequately delivered or if one or more reaches of the sewer system reach capacity as a result of the development, a water/sewer main upsizing will be required at the property owner’s expense. Required improvements on the public water and sewer system will be determined by the Development Review Engineer and may be downstream of the project. Additionally, any new fire hydrants or relocated fire hydrants will automatically require a water main upsize, as State Law prohibits fire hydrants being served by water mains smaller than 8”.
- **Please note that this site is served by 6” water mains that do not meet State requirements for fire hydrants (see above) and likely will be unable to provide the required fire hydrant demand required for this building per IFC Appendix B. A water main upsize is anticipated to be required for this project.**
- One culinary water meter is permitted per parcel and fire services, as required, will be permitted for this property. If the parcel is larger than 0.5 acres, a separate irrigation meter is also permitted. Each service must have a separate tap to the main. There are multiple existing water meters to the site. These will need consolidated to a single culinary water meter and service.
- A minimum of one sewer lateral is required per building. The laterals must be 4” or 6” and meet minimum slope requirements (2% for 4" laterals, 1% for 6" laterals). Any unused sewer laterals must be capped and plugged at the main.
- A minimum of one exterior cleanout is required on the sewer lateral within 5 feet of the building. Additional cleanouts are required at each bend and at least one every 50 feet for 4" laterals and every 100 feet for 6" laterals.
- Covered parking area drains area drains are required to be treated to remove solids and oils prior to discharge to the sanitary sewer. These drains cannot be discharged to the storm drain. Use a sand/oil separator or similar device. A 4ft diameter sampling manhole must be located downstream of the device and upstream of any other connections.
- Site stormwater must be collected on site and routed to the public storm drain system. Stormwater cannot discharge across property lines or public sidewalks.

- Site stormwater must gravity drain to the public storm drain system. No force connections are allowed to the public system.
- Stormwater treatment is required prior to discharge to the public storm drain. Utilize stormwater Best Management Practices (BMP's) to remove solids and oils. Green Infrastructure should be used whenever possible. Green Infrastructure and LID treatment of stormwater is a design requirement and required by the Salt Lake City UPDES permit for Municipal Separate Storm Sewer System (MS4). The applicant will need to provide options for stormwater treatment and retention for the 80th percentile storm. If additional property is not available, there are other options such as green roof or other BMP's. Lack of room or cost is generally not an exception for this requirement. If green infrastructure is not used, then applicant must provide documentation of what green infrastructure measures were considered and why these were not deemed feasible. Please verify that plans include appropriate treatment measures. Please visit the following websites for guidance with Low Impact Development: <https://deq.utah.gov/water-quality/low-impact-development?form=MY01SV&OCID=MY01SV> and <https://documents.deq.utah.gov/water-quality/stormwater/updes/DWQ-2019-000161.pdf?form=MY01SV&OCID=MY01SV>.
- Stormwater detention is required for this project. The allowable release rate is 0.2 cfs per acre. Detention must be sized using the 100-year 3-hour design storm using the farmer Fletcher rainfall distribution. Provide a complete Technical Drainage Study including all calculations, figures, model output, certification, summary, and discussion.
- Projects larger than one acre require that a Stormwater Pollution Prevention Plan (SWPPP) and Technical Drainage Study are submitted for review.

Urban Forestry: Comments provided by Rick Nelson 1/7/25

Urban Forestry approves of this proposal. To help the Boyer Company navigate the Urban Forestry portion of the building permit review process I am attaching some documents that describe our requirements for their review. A Planting Permit will be required for the proposed new trees in the public ROW parkstrip that is available online at <https://www.sl.gov/parks/urban-forestry/> and scroll down to find Tree Work Permit Application, click, and fill out the application. Once the application has been reviewed and approved, an e-mail will be sent to you with the required permit. This permit must be issued prior to the approval of the building permit. The four existing Golden Rain trees in the public ROW parkstrip along 200 N must be preserved and protected as described in the attached documents.

Zoning: Comments provided by Amanda Roman

- Provide a site plan with property dimensions – not just building length. The building dimensions are also off on page A1.
- How tall are the walls on 200 N? Provide dimensions on the building elevations on page A17. *“Walls up to three feet (3') in height, patios and other similar elements intended to activate the sidewalk can be located to the property line.”*
- The site is less than 1,000 feet from a rail line, requiring a diagnostic review and UDOT approval. Please reach out to Jena Carver in Transportation for more details (Jena.Carver@slc.gov). The information should've been provided to you at your DRT meeting.

The project can move forward to PC prior to UDOT approval, but building permits won't be issued until the diagnostic review is complete.

- To address the Fire comments you need to provide a site plan with road and fire access road dimensions.
- Confirm the total length of the 500 West building façade. The portion of the building setback from the lot line should be included unless a different parcel is located in front of the boundary line.

This portion (blue) of the building does not need to be included in the building length calc. because there is a parcel in front of it.



This portion (red) of the building needs to be included in the building length calculation, even if setback from the property line.