



Staff Report

PLANNING DIVISION
DEPARTMENT of COMMUNITY and NEIGHBORHOODS

To: Salt Lake City Planning Commission
From: Aaron Barlow, Principal Planner, aaron.barlow@slcgov.com, 801-535-6182
Date: November 16, 2022
Re: PLNPCM2022-00708 & PLNPCM2022-00709 – SPARK! Mixed-Use Development

Design Review & Planned Development

PROPERTY ADDRESS: 1500 West North Temple

PARCEL ID: 08-34-476-017-0000

MASTER PLAN: [North Temple Boulevard Plan](#)

ZONING DISTRICT: TSA-MUEC-C Transit Station Area Mixed-Use Employment Center Core

COUNCIL DISTRICT: District 1, Victoria Petro-Eschler

REQUEST:

KTGY Architects, on behalf of Brinshore Development, has requested Design Review and Planned Development approval for the SPARK! Mixed Use Development project to be located at approximately 1500 W North Temple. The 2.1-acre (90,000-square-foot) parcel is owned by the Redevelopment Agency of Salt Lake City. The proposed project is for a mixed-use building that will include 200 living units as well as ground-floor retail space and a daycare center. The building will consist of 6 stories with 5 stories of living space located over the parking, retail, and daycare. The applicant is seeking Design Review approval for modifications to the following requirements:

1. Minimum Setbacks in TSA districts [21A.26.078.E.3]
2. Ground floor use on street-facing facades [21A.37.050.A.1]
3. Ground floor glass on street-facing facades [21A.37.050.C.1]
4. Building entrances on street-facing facades [21A.37.050.D]
5. Blank walls on street-facing facades [21A.37.050.E]
6. Maximum building façade length [21A.37.050.F]

They are also seeking Planned Development approval for the following modifications:

1. The minimum height of a North Temple-facing façade [21A.26.078.E.2.a]
2. The maximum height for a fence in a front yard [21A.40.120.E.1.b(1) & (4)]

RECOMMENDATION:

Based on the information and findings listed in the staff report, it is the Planning Staff's opinion that the request generally meets the applicable standards of approval and therefore recommends the Planning Commission approve the request with the following conditions:

1. Final review of the plans is delegated to Staff. In addition to showing compliance with zoning regulations not modified through this request, the plans shall show compliance with the Design Review standards F (Privately Owned Public Spaces), K (Lighting), and L (Streetscape Improvements).

ATTACHMENTS:

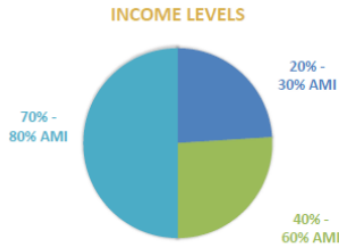
- | | |
|---|--|
| A. Vicinity Map | F. Design Review Standards |
| B. Plan Set | G. Planned Development Standards |
| C. Applicant Narratives | H. Public Process & Comments |
| D. Property & Vicinity Photos | I. Department Review Comments |
| E. Zoning & Design Standards | |

PROJECT DESCRIPTION

Located at approximately 1500 West North Temple, the Spark! project is a proposed mixed-use development located on property owned by the Redevelopment Agency of Salt Lake City (RDA) on the block between Cornell Street and 1460 West. The property is currently vacant and the Overniter Inn Motel that historically occupied the site was demolished in late 2021. The six-story proposed development includes ground-floor retail and a daycare. The upper five stories will house 200 mixed-income residential units with a mix of studio, one-, two-, three-, and four-bedrooms. The following graphic shows the breakdown of the units:

IMPACTFUL UNIT MIX

Affordable workforce & family size housing units



200 Total Units

- 63 family-sized units
- 50% of the units (100 total) affordable to households at or below 60% AMI (ranging from 20-60% AMI)
- 50% of the units (100 total) affordable to households between 70% and 80% AMI (currently market rate range for the neighborhood)
- 100% deed restricted for affordability into the future
- Unit mix maximizes ~\$42.5 Million LIHTC equity to be invested in the North Temple area

Parking, located on the ground floor behind the retail and daycare, will be accessed from 1460 West. The proposed building would face the existing Power Station served by the Green Trax line. The development would also feature a mid-block park that connects Cornell Street and 1460 West.

Neighborhood Context & Current Conditions

The project site is bound on three sides by public streets and, as mentioned above, was occupied by the Overniter Inn Motel before its demolition in late 2021. Currently, the site sits vacant. A fence was set up around the perimeter earlier this year after the City received complaints about encampments on the site. The site sits adjacent to Power Station served by the Green Trax line, accessed by the crosswalk for the intersection of 1460 West and North Temple.

An eclectic mix of uses surrounds the site. To the east sits the roughly 20-acre Kampground of America's (KOA) Salt Lake City Holiday campsite. Vacant property sits to the north between the project site and the parking lot serving the Utah Department of Health Building. The property to the west (across Cornell Street) is also vacant but was historically occupied by the old Diamond Lil's steak house. Across North Temple sits Rocky Mountain Power's (RMP) headquarters. Currently, a parking lot takes up the space opposite the Subject Property. While RMP has proposed to redevelop the site, no publicly available plans or timelines have been provided to the Planning Division at this point.

Quick Facts

Height: ~70 feet (6 stories)

Number of Residential Units: 200 units

Ground Floor Uses: Retail, daycare, parking

Upper Floor Uses: Residential units, second-floor courtyard

Exterior Materials: Glass, brick, corrugated metal, fiber cement panel

Parking: 100 stalls

Review Process & Standards: Design Review, Planned Development TSA zoning standards, design standards, and general zoning standards.

Project History

Salt Lake City's RDA acquired the property in later 2017 and began the developer selection process in early 2018. Brinshore Development was selected later that year. The applicant (representing Brinshore Development) submitted applications for both Design Review and Planned Development in July and has worked with Planning Staff since then on bringing the proposal more into compliance with zoning and design review regulations. Since July, staff has also been helping the applicant bring the proposal more in line with the intent of the Design Review and Planned Development objectives.

Building Details

Layout

The proposed 5-over-1 construction building will consist of five stories of residential units over ground-floor retail and parking. The ground floor will also include a daycare with an outdoor play area. The building would sit close to the public right of way on all street-facing sides; however, the building is set back farther than what is permitted for street-facing facades in TSA districts, and the applicants have asked for modifications to this standard through the design review process (see Key Consideration 1 for discussion on the requested modifications). Along the north part of the property, the applicant has proposed a mid-block walkway connecting Cornell Street and 1460 West.

The center of the building along the North Temple street frontage is one-story in height. The remainder of the building wraps around an inner courtyard. This creates the impression of two building masses on the North Temple façade. This façade does not meet the minimum height requirements for North-Temple-facing facades in TSA districts, so the applicant has requested a modification through Planned Development (see Key Consideration 1).



Materials

The majority of the ground floor will be wrapped in a gray brick veneer. The North Temple-facing façade includes large amounts of glass that will provide transparency and a view into the retail space. Because the proposal has set aside the rear (north) part of the property as a mid-block walkway, the east (1460 West-facing) side of the building would be used as the primary point of access for parking and utilities. As a result, it features far less glass than the other two street-facing facades. The west façade features some proposed glass elements (obscured for the safety of the daycare); however, they are only present on the south (front) half of the façade. A fence screening the daycare's outdoor play area and some transformers would block the remaining north (rear) half of the façade.

The proposed plans show the upper stories wrapped by materials designed to differentiate sections of the building. Bronze corrugated metal and orange laminate wrap around the majority of the proposed building's west wing, while

fiber cement (of varying textures and shades of gray with yellow accents) wraps the east and central sections of the building. Ample windows dot the upper stories.

Design

The submitted plans illustrate how the building's long street-facing facades (all over 200 feet in length) will be broken up (as required by the Design Review Standards, found in [Attachment F](#)). Along the South (front) façade, the proposed building's mass has been broken up by the 215-foot deep inner courtyard that makes the middle of the building appear to have only one story. The larger masses on either side will feature different materials and appear as different buildings. On the west (daycare) façade, the building has been separated into two masses with different materials that are also on different planes. The rear half of the façade sits further back from the street in order to accommodate the proposed play area for the daycare. The east (parking entrance) façade is the least varied of the three and features a single massing break three-quarters of the way back from North Temple.

Transportation and Parking

The proposed building would face the PowerTrax Station, which provides access to Downtown, the Airport, and other transit lines. While transit access is a key feature of this proposal, off-street parking is still included. The interior parking garage will be accessed from 1460 West by a single entrance. The parking area includes sufficient ADA and electrical charging spaces. Trash access and residential loading areas will be located within the parking area. Bike Storage will also be located within the parking area. Loading for the proposed daycare will be located along Cornell street with a proposed cut-in that will require approval from the Transportation Division. Finally, as mentioned earlier, a landscaped mid-block walkway (along the north side of the property) would connect Cornell Street and 1460 West.

APPROVAL PROCESS AND COMMISSION AUTHORITY

Review Process: Design Review

The applicant has requested Design Review approval for modifications to the following design standards required for new development within the TSA zoning districts:

1. **Minimum Setbacks in TSA districts:** 21A.26.078.E.3, which requires at least 50% of the street-facing facades to be within 5 feet of the property line.
2. **Ground floor use on street-facing facades:** 21A.37.050.A.1, which requires active uses along all street-facing facades.
3. **Ground floor glass on street-facing facades:** 21A.37.050.C.1, which requires at least 60% of street-facing facades to have glass between three and eight feet above grade.
4. **Building Entrances on street-facing facades:** 21A.37.050.C.1, which requires building entrances every 40 feet along street-facing facades.
5. **Blank walls on street-facing facades:** 21A.37.050.E, which limits the maximum length of blank walls uninterrupted by windows, doors, art or architectural detailing at the ground floor to 15 feet.
6. **Maximum building façade length:** 21A.37.050.F, which prohibits street-facing facades longer than 200 feet.

In making a decision for the Design Review the Planning Commission should consider whether the proposal meets the standards in Section [21A.59.050](#) of the zoning code. The standards of review are found in this report as [Attachment F](#).

Review Process: Planned Development

The applicant has requested Planned Development approval from the Planning Commission to waive the following requirements:

1. **Minimum building height:** [21A.26.078.E.2.a](#), which requires at least 50% of all street-facing building walls to meet the minimum building height (25 feet in this case). The applicant has requested a modification to this standard to allow flexibility in the design of the North Temple building facade.
2. **Maximum fence height in a front yard:** [21A.40.120.E.1.b\(1\) & \(4\)](#), which requires fences between the front property line and street-facing façade—or within 10 feet of the front property line—to be no taller than four feet. The applicant has requested a modification to this standard for additional security for the daycare play area and as a safety precaution for the ground-mounted utility boxes.

The proposed project will need to meet the Planned Development standards found in section [21A.55.050](#) of the zoning ordinance (An analysis of these standards can be found in [Attachment G](#)) in addition to all other relevant zoning requirements.

KEY CONSIDERATIONS

The key considerations listed below were identified through the analysis of this project:

1. Requested Modifications
2. Master Plan Compatibility
3. City Benefits

Consideration 1: Requested Modifications

The following summarizes the modifications requested through Design Review. The applicant has provided information on the proposed modifications in [Attachment C](#).

- **Minimum Front Setback in TSA districts:** At least 50% of the street-facing facades must be within 5 feet of the property line.
 - The project has frontage on three streets; North Temple, Cornell, and 1460 West
 - The setbacks along these streets at the ground floor are as follows:
 - North Temple = 8 feet to 24 feet. The setback allows for plaza space.
 - Cornell Street = 10 feet to approx. 28 feet. The 10 foot setback allows for space along the street to be used for the daycare drop-off and pick-up and the 28 foot area allows for an outdoor daycare courtyard.
 - 1460 West = approx. 14 feet. The parking entrance is located along this façade. Additional setback allows additional space for autos to maneuver out of the parking garage, reducing conflicts with pedestrians.
- **Ground floor use on street-facing facades:** At least 80% of the uses along the street-facing façade must be considered active uses.
 - The ground floor uses along North Temple Street meets this requirement.
 - The majority (approx. 60%) of the ground floor uses along the east façade is occupied by parking so the east façade does not meet the requirement.
 - Along the west façade, approximately 37.5% of the internal space is occupied by a daycare, which is considered an active use. The remainder of the space along the street frontage is occupied by an outdoor play area for the daycare. Although this space will be active during certain parts of the day, outdoor space does not meet the building active use requirement.
- **Ground floor glass on street-facing facades:** At least 60% of street-facing facades must have glass between three and eight feet above grade.
 - The façade along North Temple is comprised of approximately 61% glass; therefore, it meets the requirement.
 - The east façade is comprised of approximately 24% glass due to the parking garage.
 - The west façade is comprised of approximately 29% glass. This is due to the outdoor play area for the daycare area being located along the street frontage and a limit on the glass along the daycare area for privacy purposes.
- **Building Entrances on street-facing facades:** Building entrances are required every 40 feet along street-facing facades.
 - The façade along North Temple meets the requirement.
 - The east and west facades do not meet this requirement due to the parking garage located on the ground level and the limited access to the daycare facility.
- **Blank walls on street-facing facades:** The maximum length of blank walls uninterrupted by windows, doors, art or architectural detailing at the ground floor can be no longer than 15 feet.
 - The south (North Temple) facade meets this requirement.
 - The west (Cornell) façade has a segment of blank wall at the north end that is approximately 30 feet in length. This portion of the wall will be obscured by a fence screening utility structures.
 - There are four wall segments on the East elevation that exceed 15 feet.
- **Maximum building façade length:** Street-facing facades are limited to 200 feet in length.
 - All street-facing facades exceed 200 feet in length:
 - South Elevation = 245 feet

- East Elevation = 263 feet
- West Elevation = 280 feet

The following summarizes the modifications requested through the Planned Development process:

- **Minimum building height:** In the TSA-MUEC-C zoning district at least 50% of all street-facing building walls must be at least 25 feet in height. The following are the proposed building heights:
 - North, East, and West elevation building heights range from approximately 55 feet to 69 feet in height.
 - The South elevation is split into three different segments with the end segments approximately 69 feet in height. The middle segment accounts for approximately 56% of the building length and is approximately 18 feet in height; therefore, the south elevation does not meet the minimum building height requirement.
 - The applicant has requested a modification to this standard to allow flexibility in the design of the North Temple building facade.
- **Maximum fence height in a front yard:** The fence regulations in the Zoning Ordinance limits the height of fences between the front property line and street-facing façade—or within 10 feet of the front property line—to be no taller than four feet. The applicant has requested a modification to this standard to allow a fence up to six feet in height that would screen utility boxes. A diagram showing the fence is in [Attachment C](#).

Consideration 2: Master Plan Compatibility

The proposed development is generally consistent with the adopted policies within the following plans:

- Plan Salt Lake (2015)
- Downtown Master Plan (2016)

A discussion of the relevant plans and policies can be found below:

[Plan Salt Lake \(2015\)](#)

Applicable initiatives from the plan are below:

Growth:

- *Locate new development in areas with existing infrastructure and amenities, such as transit and transportation corridors*
- *Promote infill and redevelopment of underutilized land*
- *Accommodate and promote an increase in the City's population*

The proposed development will be able to rely on the ample existing infrastructure (including the adjacent Trax Station) and will not require new public roads or utilities. Additionally, the subject site is currently vacant and underutilized. New residential development on sites like this limits the amount of displacement that might happen if the proposal was in an established residential neighborhood.

Housing:

- *Direct new growth toward areas with existing infrastructure and services that have the potential to be people-oriented.*
- *Promote high density residential in areas served by transit.*

This development would place new high-density residential development immediately adjacent to a Trax station. It would also bring new growth to a location that already has available public infrastructure.

Transportation and Mobility:

- *Reduce automobile dependency and single-occupancy vehicle trips*
- *Encourage transit-oriented development*

This development will bring significant residential density to a site that is immediately adjacent to a Trax station. The additional commercial uses add to the transit-oriented nature of this development.

[North Temple Boulevard Master Plan](#)

The proposed development supports a number of strategies relating to the Power Station area (known as “Cornell” in the Master Plan). They are listed below:

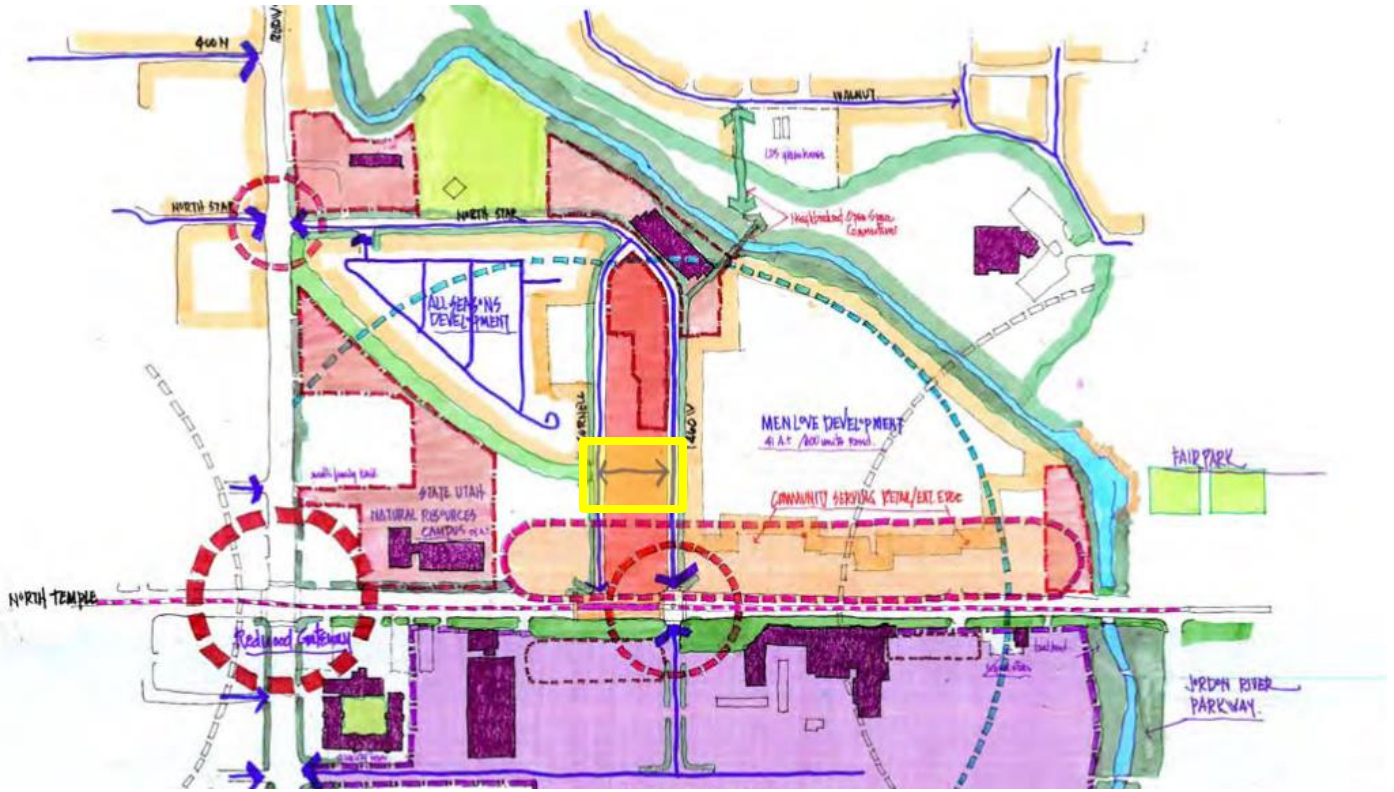
- **Policy #2: Compact Mix of Uses**
Allow for a more intense, compact mix of uses around Cornell Street and 1460 West.
- **Policy #3: Placemaking**

Improve the connections between the Cornell Station and the nearby open spaces.

- **Build a “Signature Project” at North Temple and Cornell**

The proposed development accomplishes the above policies and visions by doing the following:

- By providing retail, residential and daycare uses all on a single lot
- While the proposed development may be one of the first new projects along this section of North Temple, it will not be the only one and it will likely establish a pattern that will be followed by other development projects in the vicinity.
- By providing a mid-block walkway along the identified canal pathway and increasing pedestrian connectivity in the neighborhood.



The Power Station urban design framework shows a desired connection between Cornell Street and 1460 West (highlighted in yellow).

Consideration 3: City Benefits

The applicant is requesting several modifications to zoning regulations. This can sometimes raise a red flag when reviewing a project. In this case, Staff is of the opinion that the modifications are a good trade-off considering the difficulty in designing a project with three street frontages. The project also includes elements that are instrumental in implementing City goals, such as:

- Much needed affordable housing;
- A daycare center;
- Activation of the street frontage immediately adjacent to a light rail station; and
- A mid-block greenway connection.

NEXT STEPS

Approve the Request

If the Design Review and Planned Development requests are approved, the applicant will be able to submit building permits for the development, and the plans will need to meet any conditions of approval. Final certificates of occupancy

for the buildings will only be issued once all conditions of approval are met. Modifications beyond those identified as minor modifications in the ordinance would require additional review and approval from the Planning Commission.

Table/Continue the Request

If the Planning Commission tables the Design Review and Planned Development petitions and requests changes to the plan or additional details, the applicant will have the opportunity to make changes to the design and/or further articulate details in order to return to the Planning Commission for further review and a decision on the proposal.

Deny the Request

If the Planning Commission denies the Design Review and Planned Development requests, the applicant will still be able to develop the property by right without the requested modifications. Specifically, the proposal would need to comply with the zoning and design standards listed in this report.

ATTACHMENT A: Vicinity Map

SPARK! - Mixed-Use - 1500 W North Temple



Salt Lake City Planning Division 8/2/2022

ATTACHMENT B: Plan Set



ZONING **PROJECT DESCRIPTION**

PROJECT ADDRESS: 1500 W NORTH TEMPLE STREET
SALT LAKE CITY, UT 84116
ASSESSOR PARCEL: 08-34-476-017-0000
SITE AREA: 2.07 AC (90,169 SF)

BASELINE ZONING INFORMATION:

ZONING: TSA-MUEC-C (TRANSIT STATION AREA - MIXED USED EMPLOYMENT CENTER - CORE)
ALLOWABLE FAR: 2:1 (3:1 WITH INCENTIVES) EXCL. PKG STRUCTURE
MAXIMUM AREA: 180,338 SF (270,507 WITH INCENTIVES)
ALLOWABLE HEIGHT: 25 FT MIN. 75' MAX.
ALLOWABLE DENSITY: REGULATED THROUGH FAR AND HEIGHT
MAXIMUM UNITS: N/A
UNIT SIZE: N/A

SETBACKS:
FRONT: MIN. 5', MAX. 15' FEET (NORTH TEMPLE)
SIDE: 0 FEET
REAR: 0 FEET (ALLEY)

PARKING:
21A.44 OFF STREET PARKING, MOBILITY AND LOADING

VEHICULAR PARKING:
RESIDENTIAL: 2 PARKING STALL PER UNIT W/ 2 OR MORE BEDS
1 PARKING STALL PER UNIT W/ 1 BED & EFFICIENCY
1/2 PARKING STALL FOR SINGLE OCC. DWELLINGS (<600 SF)

*1/2 PARKING STALL PER UNIT W/10 MORE MORE UNITS W/ MIN. 25% AFFORDABLE OR SENIOR.

TSA CORE (ALL USES): NO SPACES REQUIRED
MAX. 1 PARKING STALL PER UNIT (RESIDENTIAL)
MAX. 3 SPACES/1,000 SF USABLE SPACE (NON-RES)

MU, R-MU (RESID.): 1/2 PARKING STALL PER UNIT

ELECTRICAL VEHICLES: 1 EV PARKING STALL PER 25 PARKING STALLS PROVIDED (SHALL COUNT TOWARD REQUIRED)

BICYCLE PARKING: 5% OF PARKING STALLS REQUIRED FOR RESIDENTIAL AND COMMERCIAL USE (MIN. 2 SPACES REQUIRED)

LOADING:

OPEN SPACE REQUIRED:
ONE SF FOR EVERY THEN (10) SF OF LAND AREA INCLUDED IN THE DEVELOPMENT, UP TO FIVE THOUSAND (5,000) SF FOR CORE AREAS. INCLUDES LANDSCAPED YARDS, PATIOS, PUBLIC PLAZAS, POCKET PARKS, COURTYARDS, ROOF TOP AND TERRACE GARDENS AND SIMILAR.

PROPOSED PROJECT SUMMARY:

UNITS:
V-A BUILDING: 70 UNITS
III-A BUILDING: 130 UNITS
TOTAL: 200 UNITS

SITE AREA: 2.07 AC
DENSITY: 97 DU/AC

FLOOR AREA RATIO: 233,083 SF = 2.59:1 FAR
(SEE GROSS BUILDING AREA SUMMARY TABLE)

VEHICULAR PARKING PROVIDED: 100 SPACES
BICYCLE PARKING PROVIDED: 24 SPACES

OPEN SPACE:
PROVIDED ROOF TERRACE LVL 2: 3,920 SF
PROVIDED COURTYARD LVL 2: 13,082 SF

PROJECT DESCRIPTION:
CONSTRUCT NEW 6-STORY MULTI-FAMILY BUILDING WITH 6 STORIES OF RESIDENTIAL OVER 1-STORIES OF GARAGE, COMMERCIAL & DAYCARE. CONTAINS 200 APARTMENT UNITS, APPROX. 5,000 SF OF COMMERCIAL SPACE AND 8,000 SF OF DAY CARE AREA, LEASING OFFICE, COMMUNITY COURTYARD AND ROOF TOP TERRACES.

PROPOSED BUILDING
6 STORY MULTI-FAMILY BUILDING.

5 LEVELS OF TYPE III-A WOOD CONSTRUCTION OVER
1 LEVELS OF ABOVE GROUND TYPE 1-A CONCRETE PARKING GARAGE & 5 LEVELS OF TYPE V-A WOOD CONSTRUCTION OVER 1 LEVEL OF ABOVE GROUND TYPE 1-A CONCRETE PARKING GARAGE, DAYCARE AND COMMERCIAL USES.

OCCUPANCY TYPES: R2, S2, B, A3, A2 (COMMERCIAL), E (DAYCARE)

VICINITY MAP



PARKING PROVIDED

SPARK - PARKING SUMMARY						
Parking Required		Min.	Max.			
Residential:	0.5 space per unit	100	200			
Non-residential:	None	0	37			
Total Required:		100	237			
PARKING PROVIDED / RESIDENTIAL / NON-RESIDENTIAL						
LEVEL 1	STANDARD	ADA	ADA VAN	ADA EV	STANDARD EV	TOTAL
	93	2	1	1	3	100
TOTAL PROVIDED	93	2	1	1	3	100

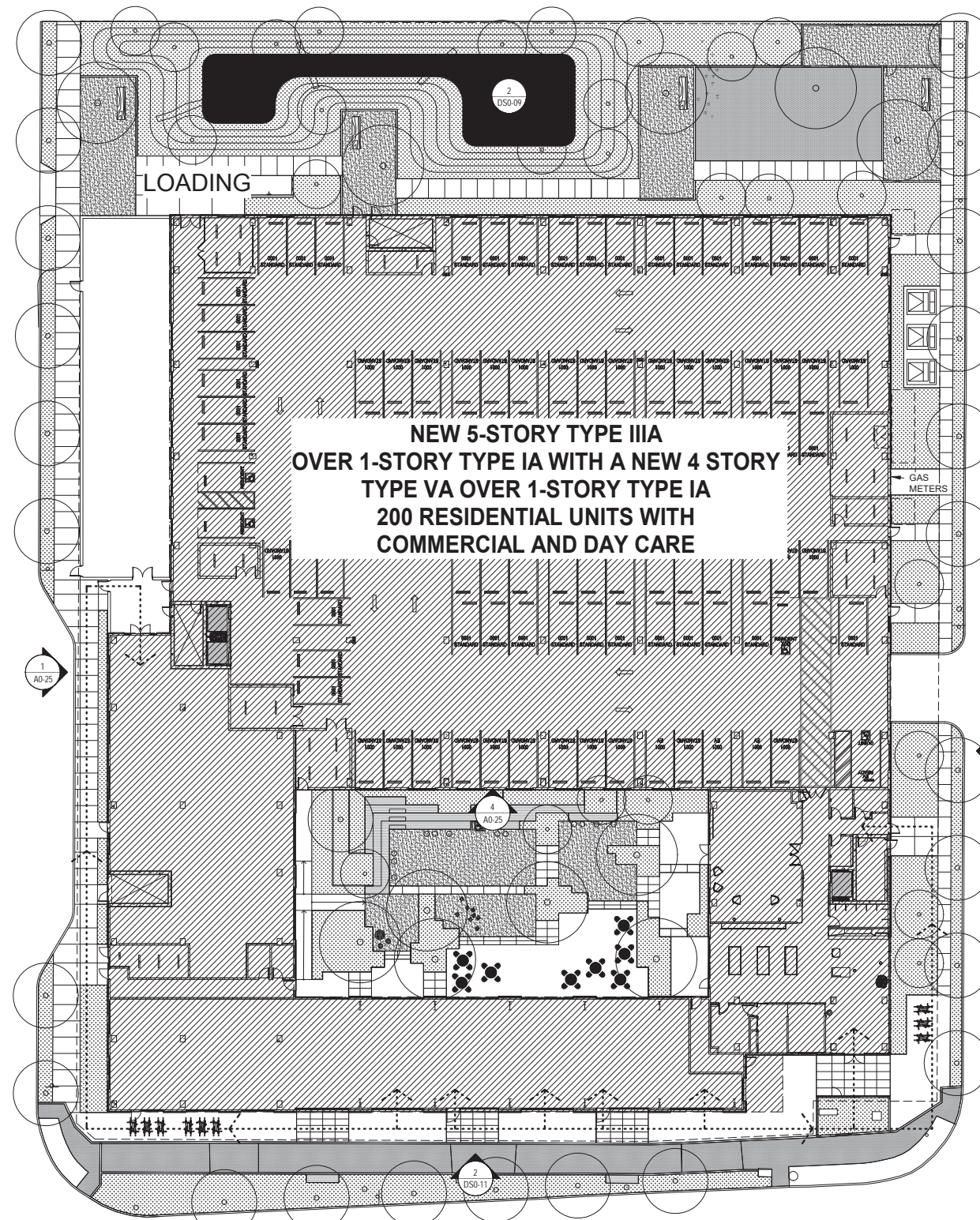
UNIT MIX SUMMARY

UNIT COUNT MIX	
UNIT NAME	COUNT
UNIT S1	13
UNIT S2	2
UNIT S2.1	3
UNIT S3	5
RESIDENTIAL-0 BED (Studio)	23
UNIT 1A	98
UNIT 1A.1	1
UNIT 1B	9
UNIT 1B.1	5
UNIT 1C	1
RESIDENTIAL-1 BED	114
UNIT 2A	2
UNIT 2A.1	11
UNIT 2A.2	2
UNIT 2A.3	2
UNIT 2A.4	13
UNIT 2B	4
UNIT 2C	3
UNIT 2D	4
UNIT 2E	2
UNIT 2E.1	2
RESIDENTIAL-2 BED	45
UNIT 3A	3
UNIT 3A.1	5
UNIT 3A.2	2
UNIT 3A.3	3
RESIDENTIAL-3 BED	13
UNIT 4A	2
UNIT 4A.1	3
RESIDENTIAL-4 BED	5
Grand total: 200	200

AREA SUMMARY

BUILDING AREA SCHEDULE - GROSS SF		
Level		Total Area
LEVEL 1		53736 SF
LEVEL 2		39333 SF
LEVEL 3		39333 SF
LEVEL 4		39333 SF
LEVEL 5		39333 SF
LEVEL 6		21940 SF
		233005 SF

COMMON AREA SCHEDULE - GROSS SF			
	USE	OCCUP ANCY	AREA
Not Placed			0 SF
LEVEL 1			
	TYPE IA / PARKING		0 SF
	TYPE IA / EGRESS	A-2	183 SF
	TYPE IA / EGRESS	A-2	337 SF
	TYPE IA / RESD AMENITIES	A-3 / B	4430 SF
	TYPE IA / DAYCARE	I-4	5145 SF
	TYPE IA / COMMERCIAL	A-2	6232 SF
	TYPE IA / PARKING	S-2	37406 SF
LEVEL 2			
	TYPE VA / RESD AMENITIES	A-3	930 SF
	TYPE VA / RESIDENTIAL	R-2	16370 SF
	TYPE IIB / RESIDENTIAL	R-2	22032 SF
LEVEL 3			
	TYPE VA / RESIDENTIAL	R-2	17319 SF
	TYPE IIB / RESIDENTIAL	R-2	22014 SF
LEVEL 4			
	TYPE VA / RESIDENTIAL	R-2	17319 SF
	TYPE IIB / RESIDENTIAL	R-2	22014 SF
LEVEL 5			
	TYPE VA / RESIDENTIAL	R-2	17309 SF
	TYPE IIB / RESIDENTIAL	R-2	22023 SF
LEVEL 6			
	TYPE VA / RESIDENTIAL	R-2	0 SF
	TYPE IIB / RESIDENTIAL	R-2	21940 SF
Grand total			233005 SF



W. NORTH TEMPLE STREET

N. CORNELL STREET

VEHICULAR ENTRY/EXIT

N. 1460 W

**NEW 5-STORY TYPE IIIA
OVER 1-STORY TYPE IA WITH A NEW 4 STORY
TYPE VA OVER 1-STORY TYPE IA
200 RESIDENTIAL UNITS WITH
COMMERCIAL AND DAY CARE**

LOADING



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433 S. Spring St.
Suite 750
Los Angeles, CA 90013
ktgy.com
310.394.2623

SPARK!
1500 WEST NORTH TEMPLE STREET 20211149
SALT LAKE CITY, UT 84116

PLANNING SUBMITTAL

Developer
SPARK!
Brinshore Development, LLC.
666 Dundee Road, Suite 1102
Northbrook, IL 60062
<https://www.brinshore.com>
847.562.9400

PLOT PLAN DS0-01

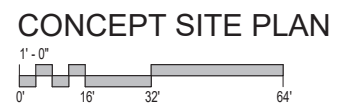


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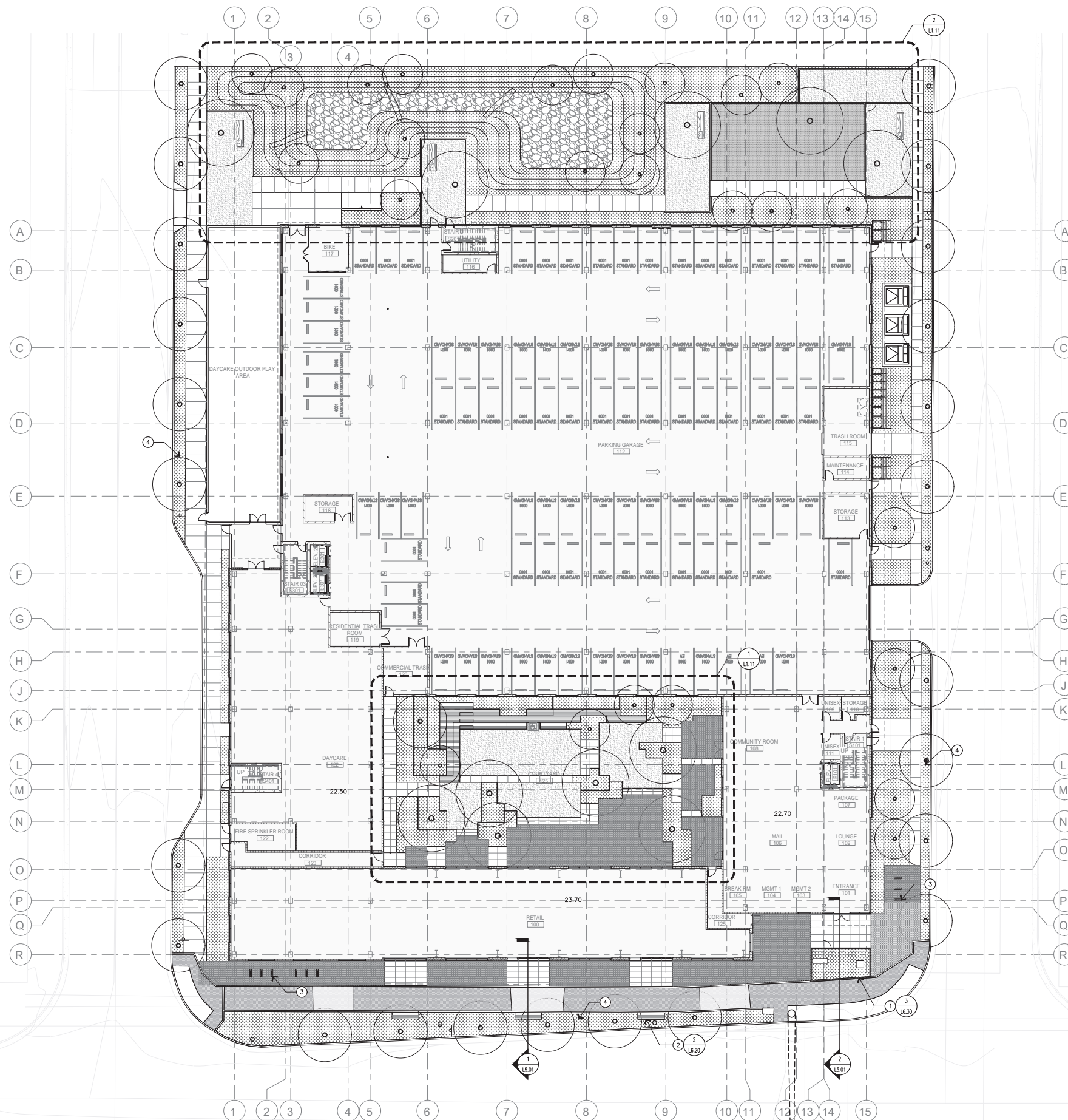
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 1500 WEST NORTH TEMPLE STREET 20211149
 SALT LAKE CITY, UT 84116

PLANNING SUBMITTAL

Developer
 SPARK!
 Brinshore Development, LLC.
 666 Dundee Road, Suite 1102
 Northbrook, IL 60062
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 847.562.9400



DS0-02

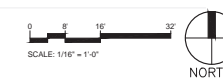


KEY NOTES - LEVEL 1

- ① NEW FENCE AROUND THE EXISTING UTILITIES. SEE DETAIL
- ② SIDEWALK BENCHES - SEE DETAIL
- ③ BIKE RACK - SEE DETAIL
- ④ PARKWAY PLANTING

LEGEND

- PLANT BED
- PROPOSED TREE

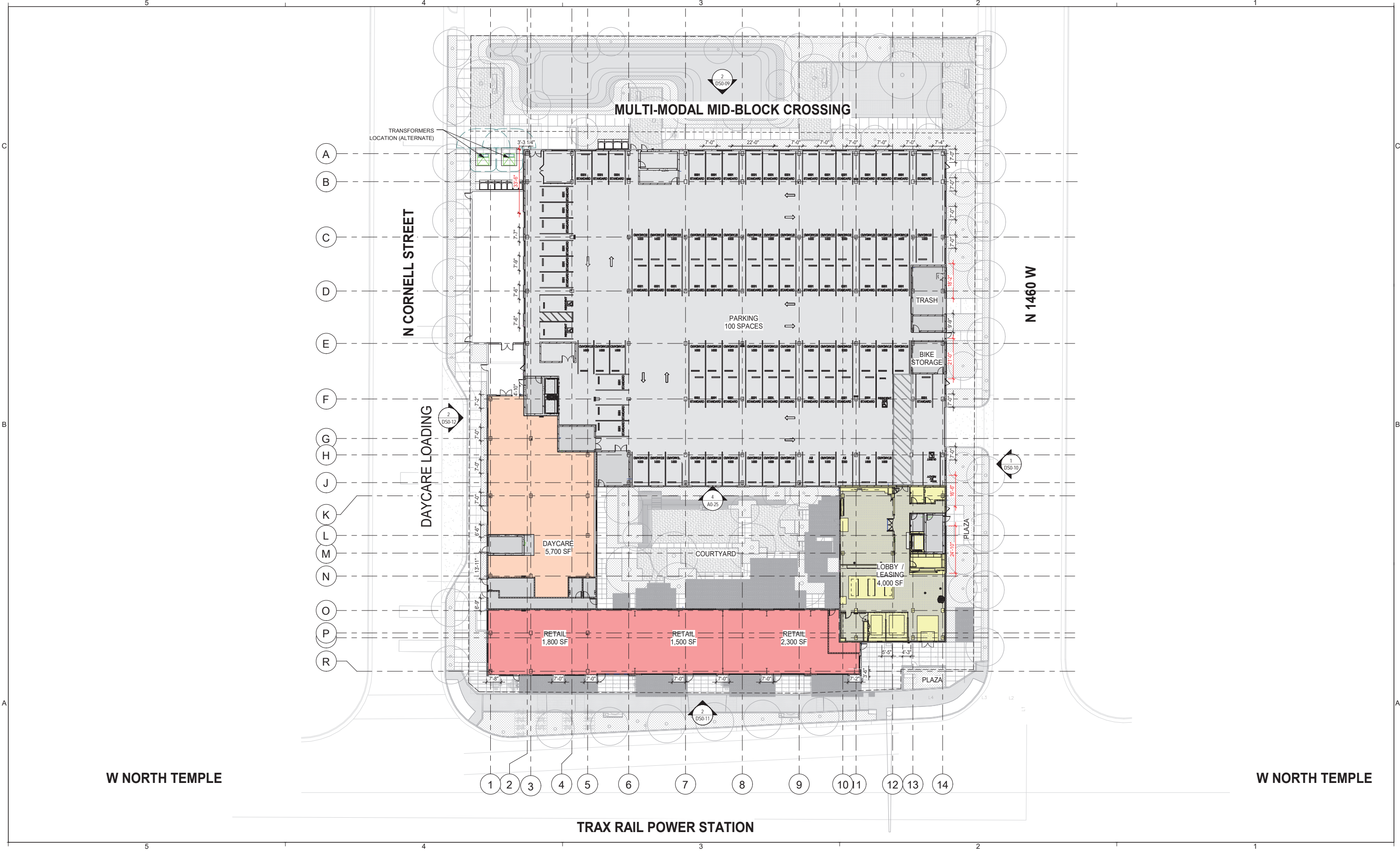


No.	Date	Description
	8/19/22	PLAN CHECK SUBMITTAL

It is the client's responsibility prior to or during construction to verify the architect in writing all proposed areas of construction in the plans and specifications of work to contractor through knowledgeable with the building codes and methods of construction should be verified by owner. The contractor is responsible for any errors or omissions in construction. The client will be responsible for any delays in construction if these procedures are not followed.



LANDSCAPE PLAN
- LEVEL 01



W NORTH TEMPLE

W NORTH TEMPLE

TRAX RAIL POWER STATION



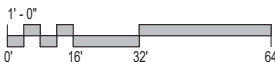
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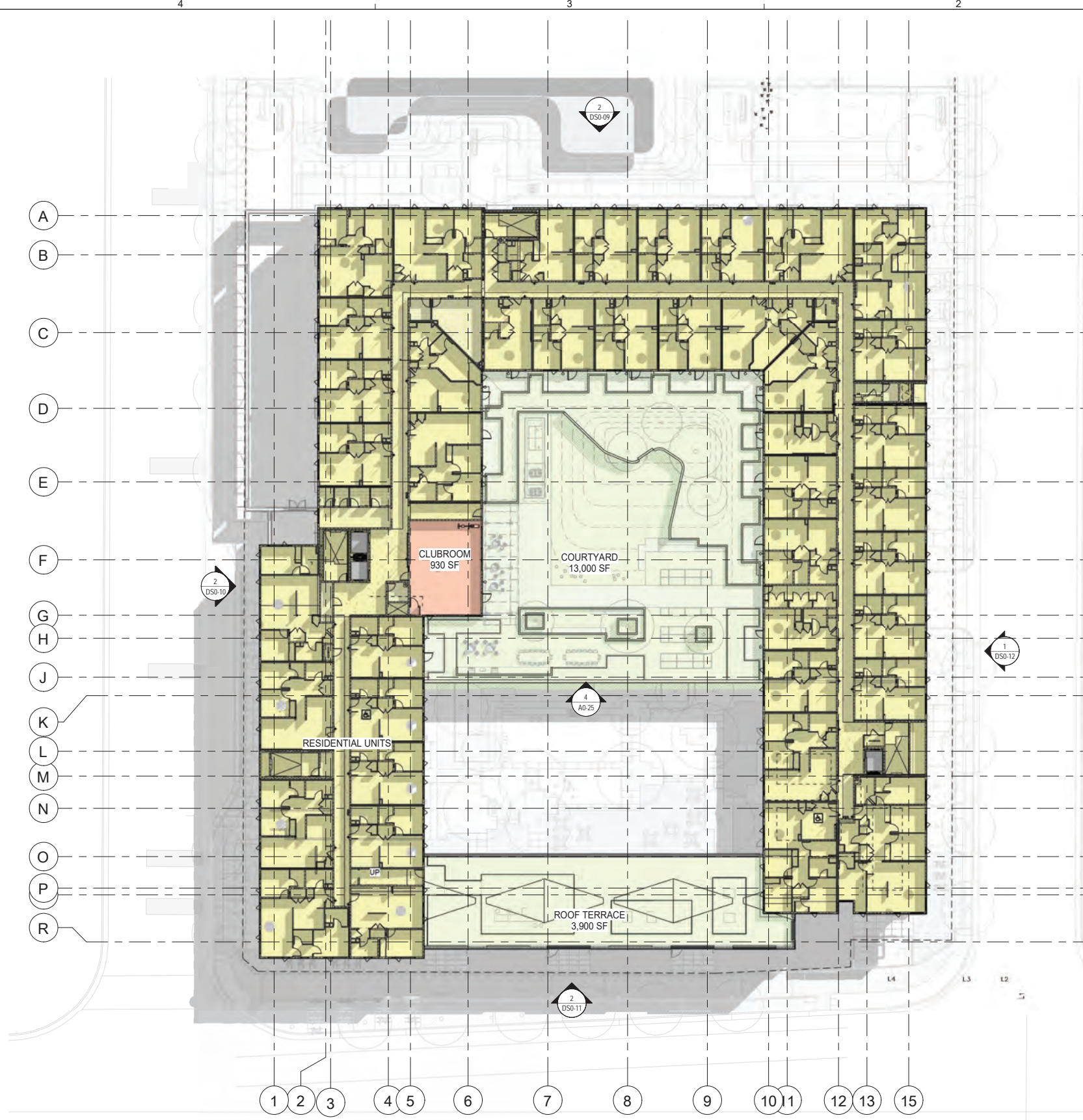
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LEVEL 01 - FLOOR PLAN



DS0-03



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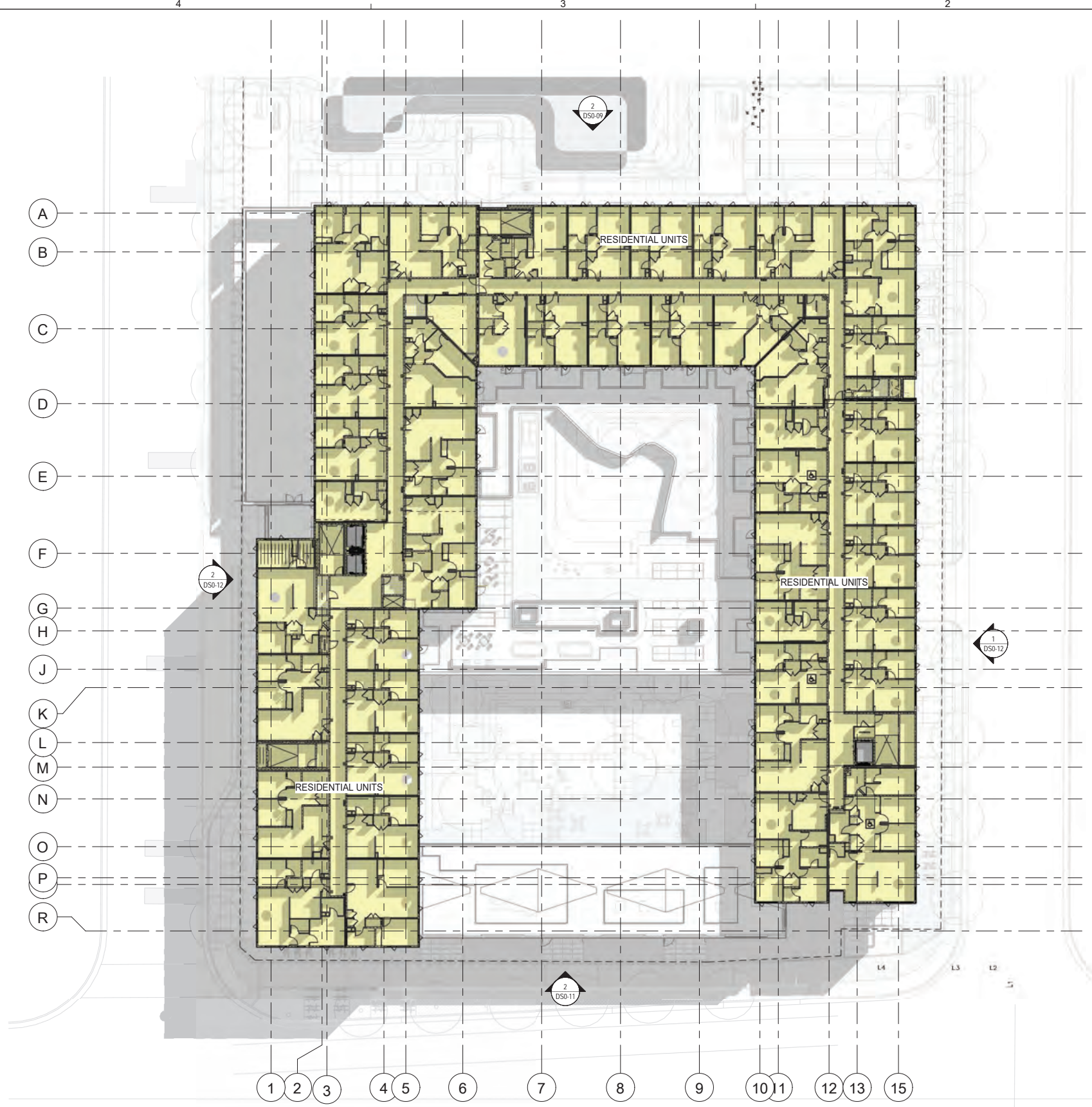
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LEVEL 02 - FLOOR PLAN
 1" = 0'
 0' 16' 32' 64'

DS0-04



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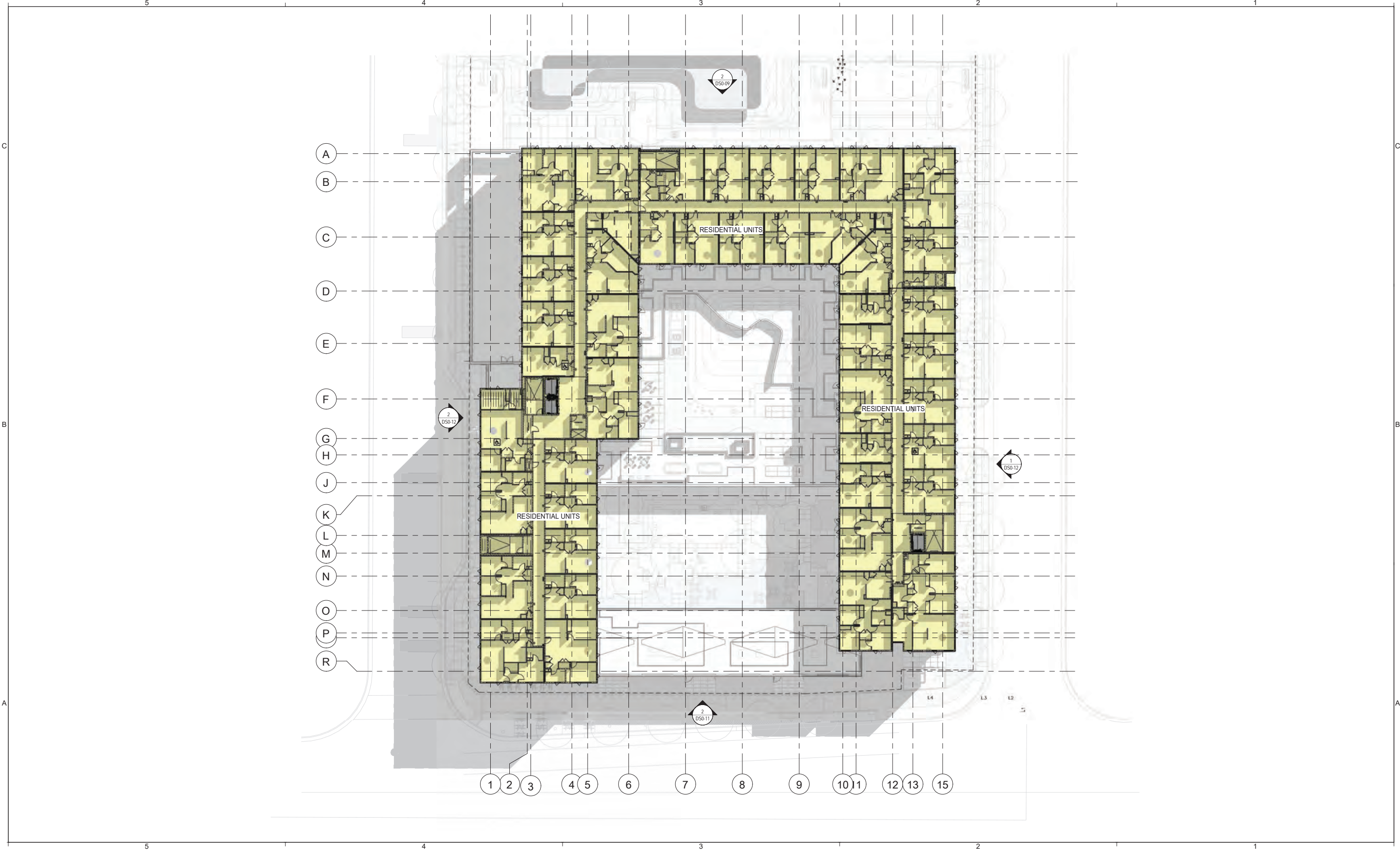
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LEVEL 03 - FLOOR PLAN
 1" = 0'
 0' 16' 32' 64'

DS0-05



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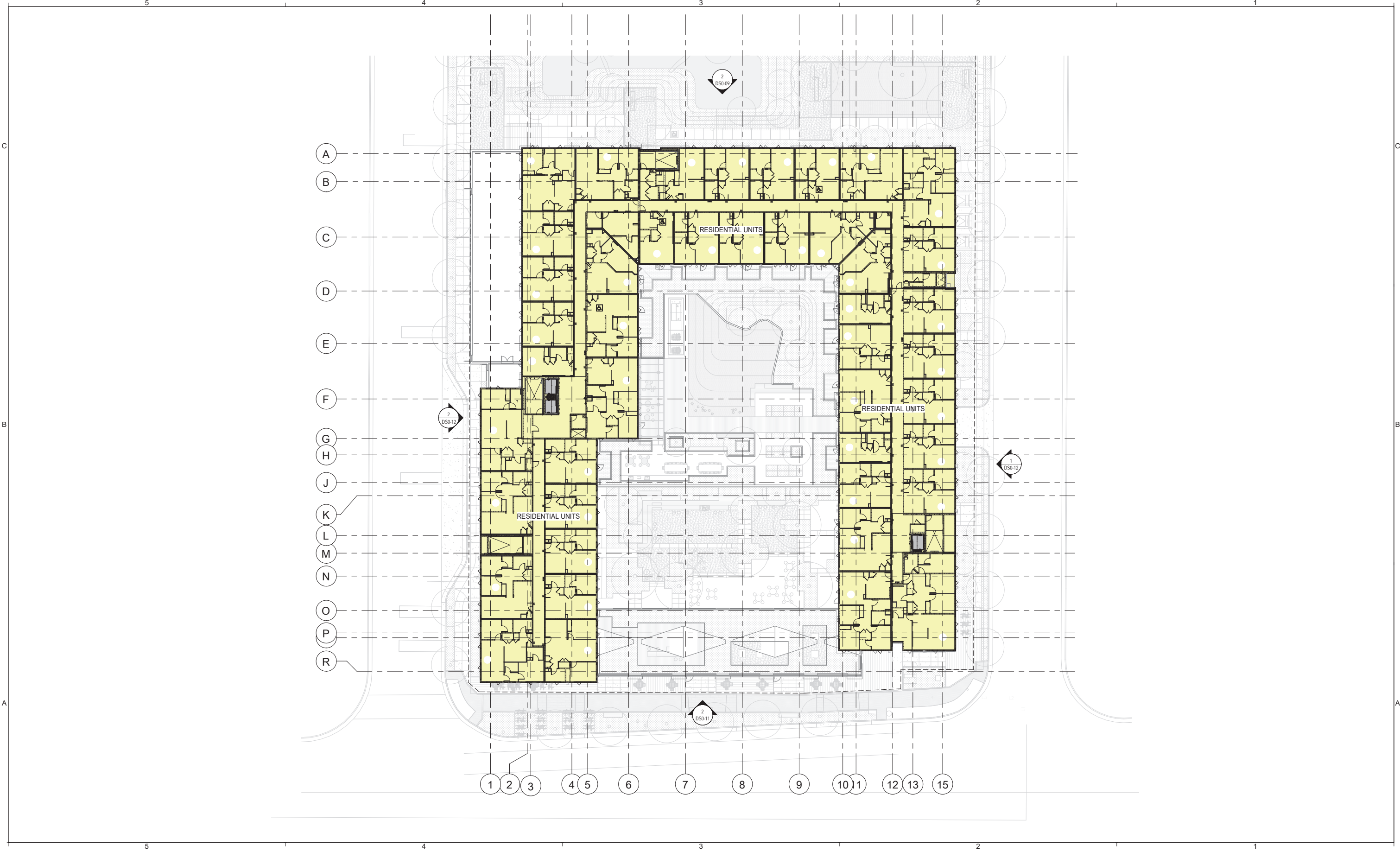
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LEVEL 04 - FLOOR PLAN
 1" = 0'
 0' 16' 32' 64'

DSO-06



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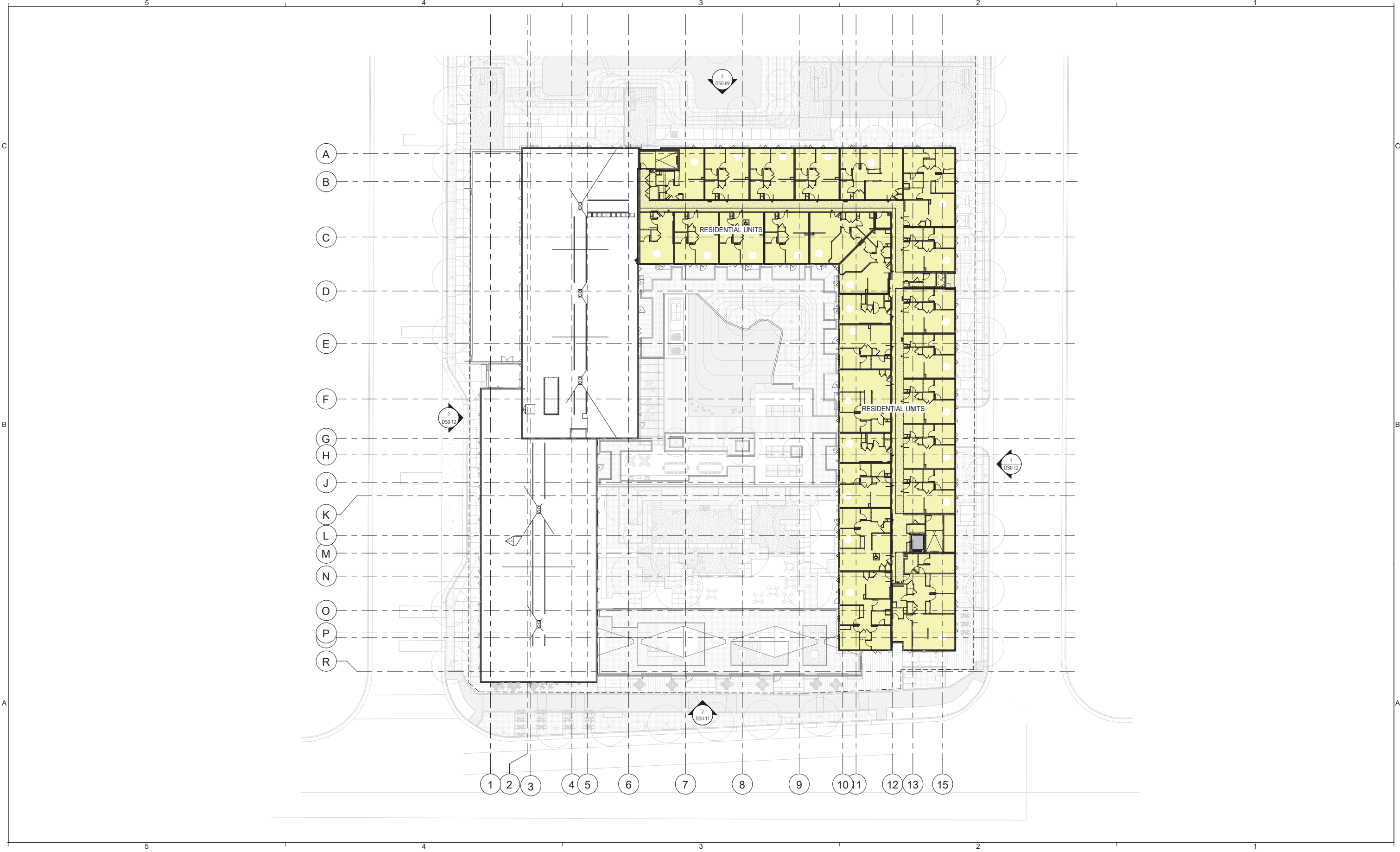
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LEVEL 05 - FLOOR PLAN
 1" = 0'
 0' 16' 32' 64'

DS0-07



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LEVEL 06 - FLOOR PLAN
 1" = 0'
 0' 16' 32' 64'

DSO-08

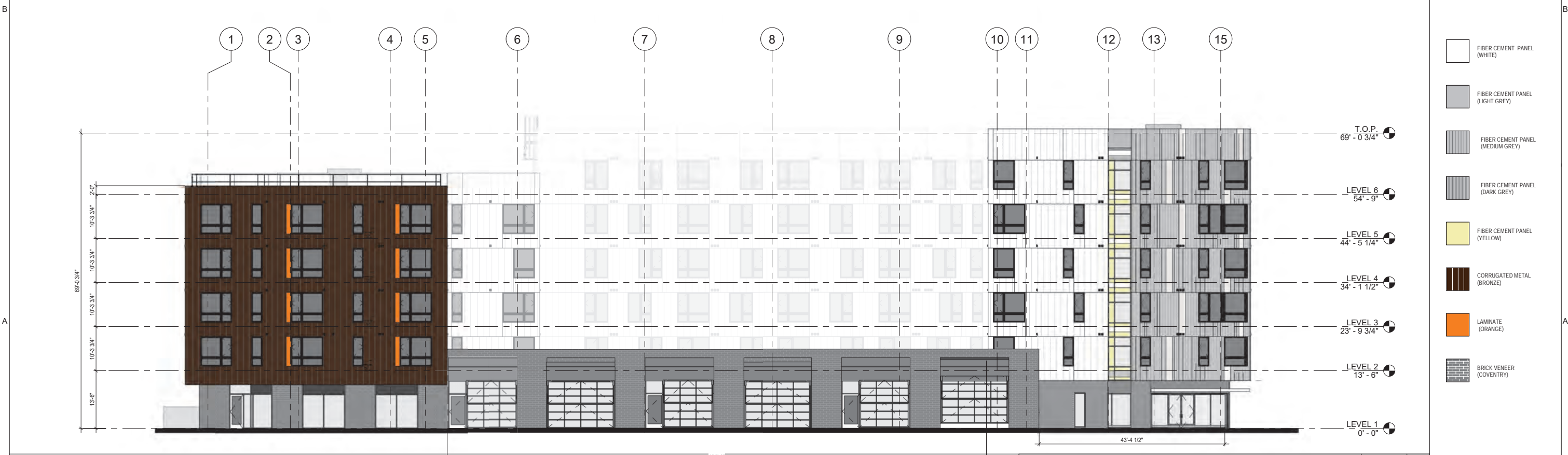


NORTH EXTERIOR ELEVATION

SCALE: 3/32" = 1'-0"

2









MATERIALS LEGEND



SOUTH EXTERIOR ELEVATION

SCALE: 3/32" = 1'-0"

1

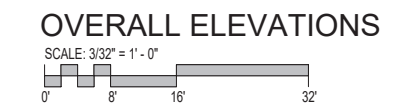
-  FIBER CEMENT PANEL (WHITE)
-  FIBER CEMENT PANEL (LIGHT GREY)
-  FIBER CEMENT PANEL (MEDIUM GREY)
-  FIBER CEMENT PANEL (DARK GREY)
-  FIBER CEMENT PANEL (YELLOW)
-  CORRUGATED METAL (BRONZE)
-  LAMINATE (ORANGE)
-  BRICK VENEER (COVENTRY)

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DS0-09

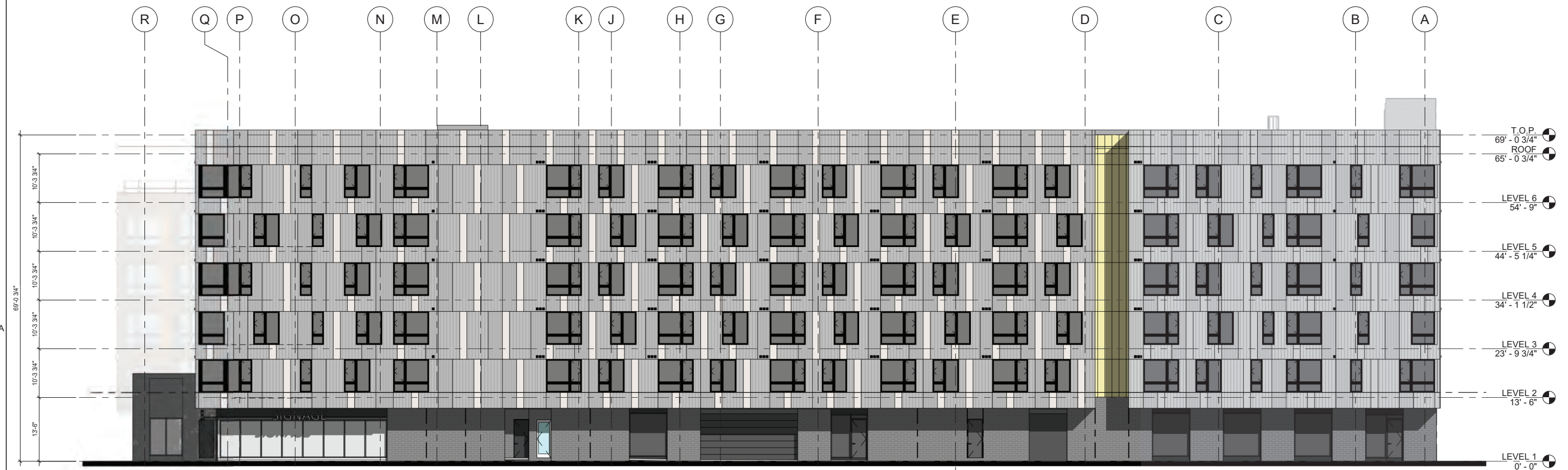


WEST EXTERIOR ELEVATION

SCALE: 3/32" = 1'-0"

2









MATERIALS LEGEND



EAST EXTERIOR ELEVATION

SCALE: 3/32" = 1'-0"

1

-  FIBER CEMENT SIDING - CONTEMPORARY SMOOTH (SILK)
-  FIBER CEMENT SIDING - CONTEMPORARY SMOOTH (SILVER GREY)
-  FIBER CEMENT SIDING - 8 REVEAL (SILVER GREY)
-  FIBER CEMENT SIDING - MODERN STRIPE (SILVER GREY)
-  FIBER CEMENT PANEL (YELLOW)
-  CORRUGATED METAL (BRONZE)
-  LAMINATE (ORANGE)
-  BRICK VENEER (COVENTRY)

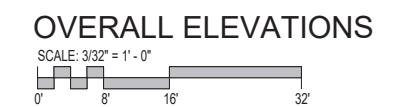


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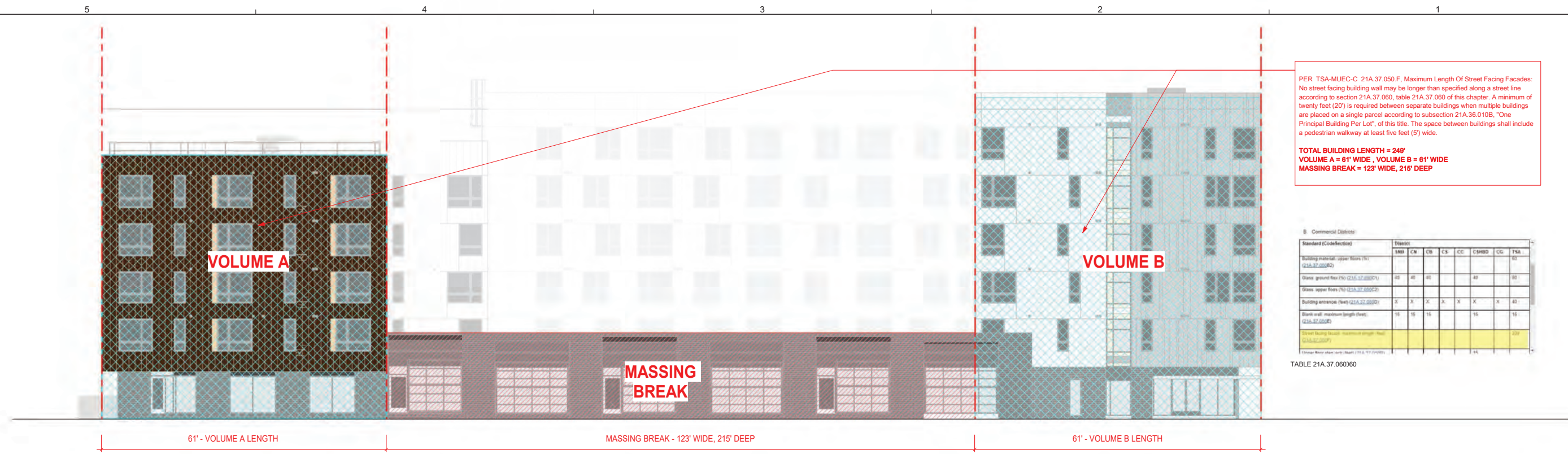
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DS0-10



PER TSA-MUEC-C 21A.37.050.F, Maximum Length Of Street Facing Facades:
 No street facing building wall may be longer than specified along a street line according to section 21A.37.060, table 21A.37.060 of this chapter. A minimum of twenty feet (20') is required between separate buildings when multiple buildings are placed on a single parcel according to subsection 21A.36.010B, "One Principal Building Per Lot", of this title. The space between buildings shall include a pedestrian walkway at least five feet (5') wide.

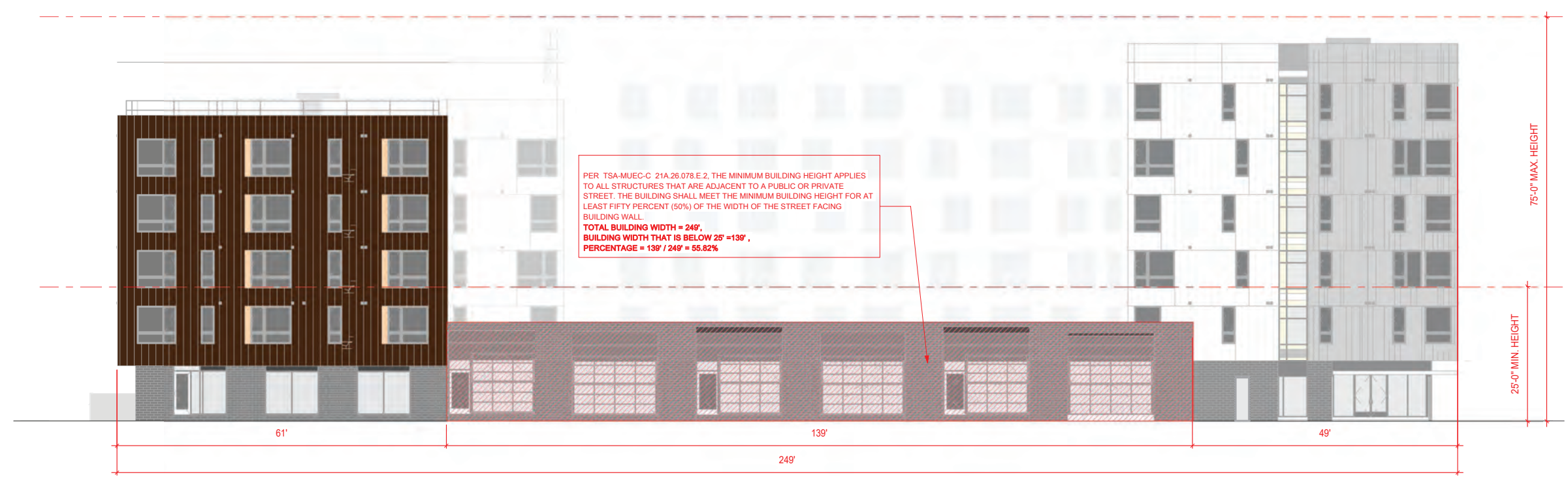
TOTAL BUILDING LENGTH = 249'
VOLUME A = 61' WIDE, VOLUME B = 61' WIDE
MASSING BREAK = 123' WIDE, 215' DEEP

B Commercial Districts

Standard (CodeSection)	Element						
	SMB	CK	CS	CC	CSMB	CC	TSA
Building material, upper floors (%) (21A.37.050E2)							80
Glass ground floor (%) (21A.37.050C1)	40	40	40			40	80
Glass upper floors (%) (21A.37.050C2)							
Building entrance (feet) (21A.37.050D)	X	X	X	X	X	X	40
Blank wall, maximum length (feet) (21A.37.050E)	15	15	15			15	15
Street facing facade, maximum length (feet) (21A.37.050F)							200

TABLE 21A.37.060B0

BUILDING LENGTH DIAGRAM - SOUTH ELEVATION SCALE: 3/32" = 1'-0" **2**



PER TSA-MUEC-C 21A.26.078.E.2, THE MINIMUM BUILDING HEIGHT APPLIES TO ALL STRUCTURES THAT ARE ADJACENT TO A PUBLIC OR PRIVATE STREET. THE BUILDING SHALL MEET THE MINIMUM BUILDING HEIGHT FOR AT LEAST FIFTY PERCENT (50%) OF THE WIDTH OF THE STREET FACING BUILDING WALL.

TOTAL BUILDING WIDTH = 249'
BUILDING WIDTH THAT IS BELOW 25' = 139'
PERCENTAGE = 139' / 249' = 55.82%

BUILDING HEIGHT DIAGRAM - SOUTH ELEVATION SCALE: 3/32" = 1'-0" **1**

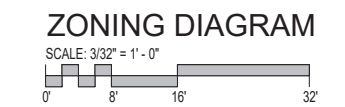


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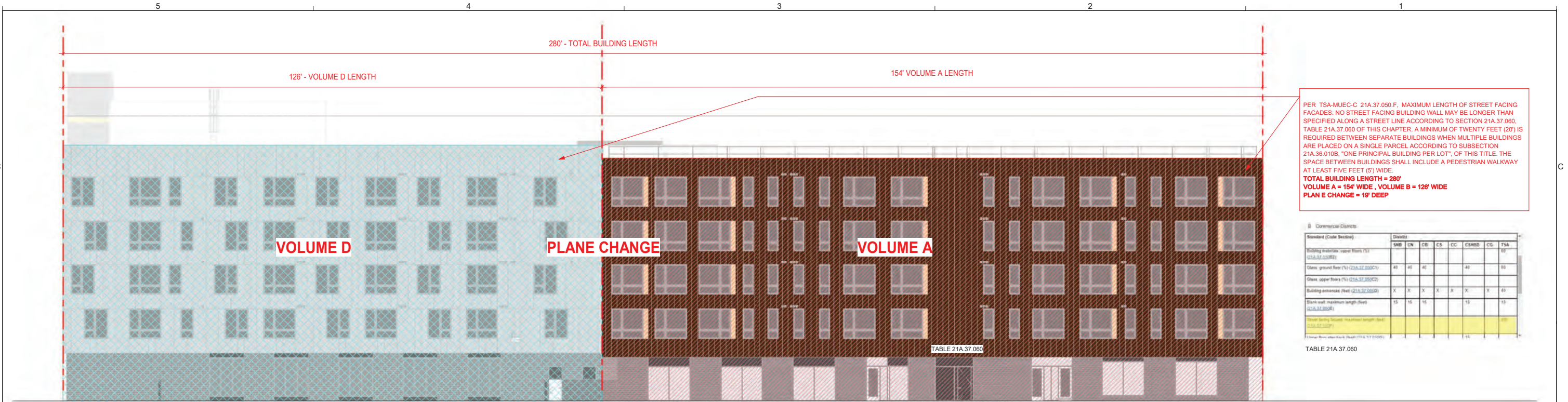
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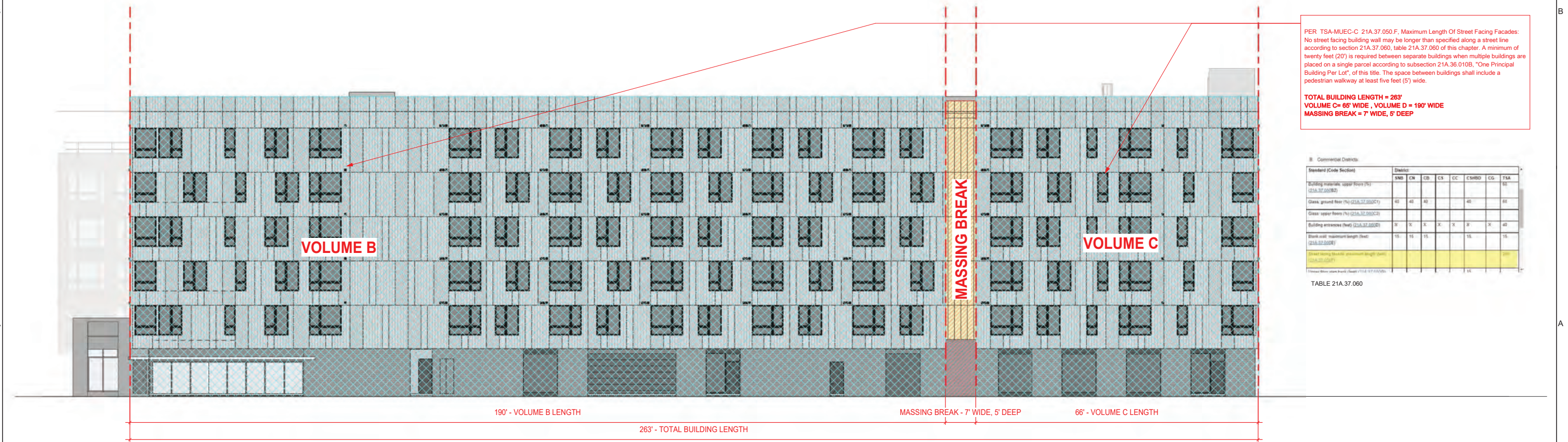
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DS0-11



BUILDING LENGTH DIAGRAM - WEST ELEVATION SCALE: 3/32" = 1'-0" 2



BUILDING LENGTH DIAGRAM - EAST ELEVATION SCALE: 3/32" = 1'-0" 1

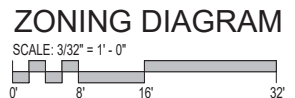


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DS0-12

3. Setbacks:

a. General Standards For Front/Corner Side Yards:

- (1) All portions of the yard not occupied by building, driveways, walkways or other similar features must be landscaped or include an active outdoor use, such as outdoor dining, plazas, courtyards or other similar outdoor use. See subsection F of this section for specific front yard design requirements.
- (2) Parking is prohibited in the front and corner side yards.
- (3) Walls up to three feet (3') in height, patios and other similar elements intended to activate the sidewalk can be located to the property line.
- (4) Awnings or canopies may be located within any portion of the yard and are not subject to the front or corner side yard restrictions in subsection 21A.36.020B, table 21A.36.020B of this title.
- (5) Balconies may project up to two feet (2') into the required yards and are not subject to the front or corner side yard restrictions in subsection 21A.36.020B, table 21A.36.020B of this title.
- (6) All front and corner side yard standards in table 21A.26.078E3b of this subsection E may be modified through the design review process of chapter 21A.59 of this title, except that the front and corner side yard setback for 400 South shall not be reduced below the minimum.

North Temple

Minimum: 5', and at least 50% of the street facing building facade must be built to the minimum.

Maximum: 15', but may be increased if the additional setback is used for plazas, courtyards, or outdoor dining areas.

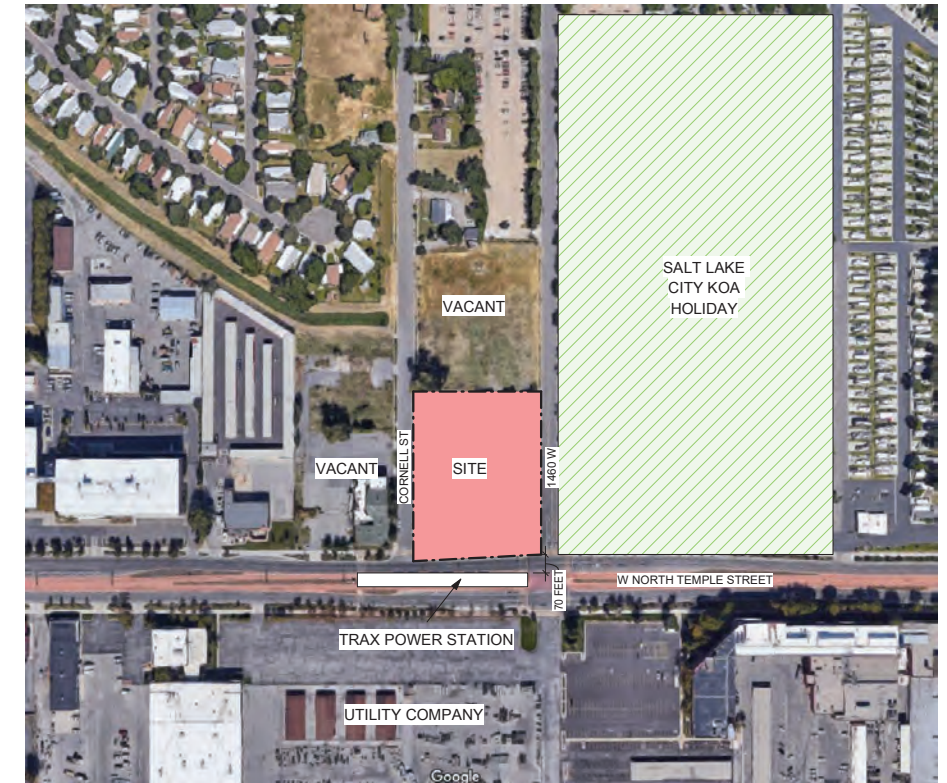
In locations where the sidewalk is not a minimum of 10' wide, additional sidewalk width shall be installed by the developer so there is a minimum width sidewalk of 10'. This applies to new buildings and to additions that increase the gross building square footage by more than 50%. This standard does not require removal of existing buildings or portions thereof.

21A.26.078 SETBACK REQUIREMENT 6

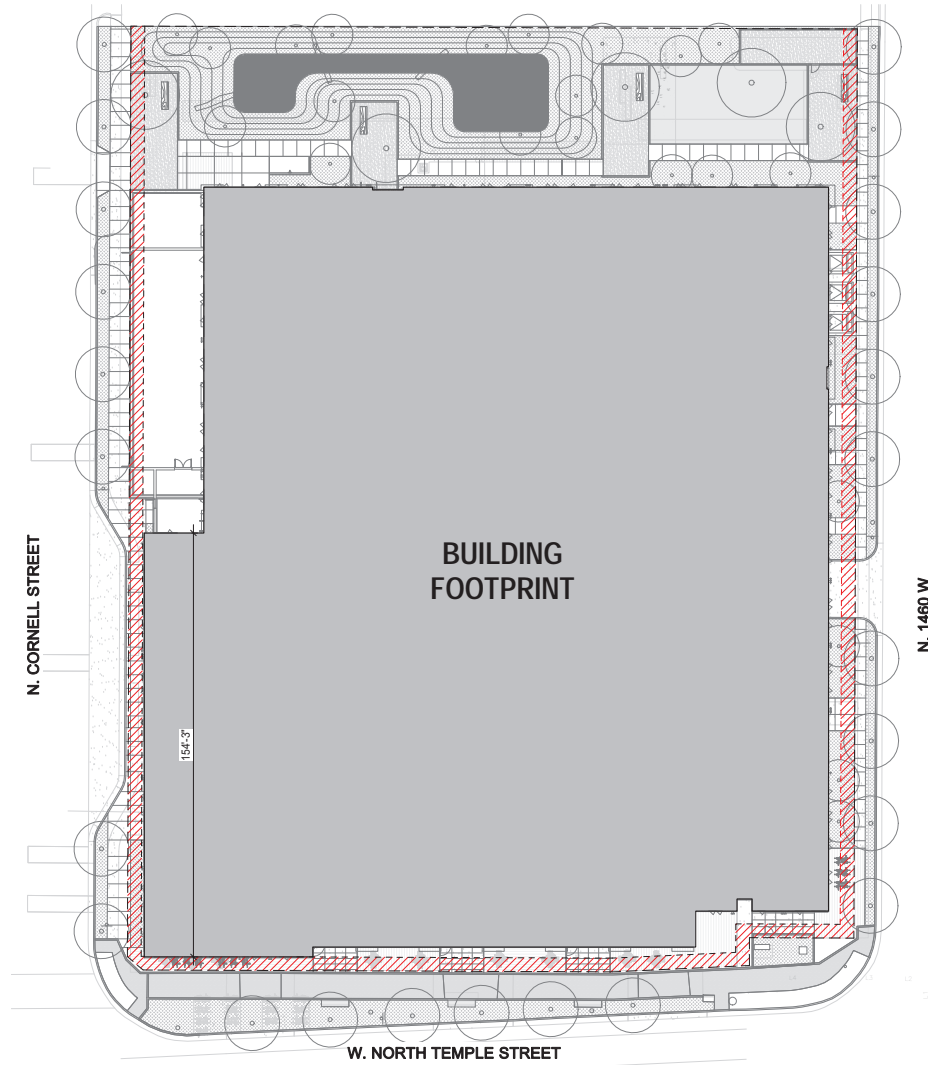
TABLE 21A.26.078E3B 5



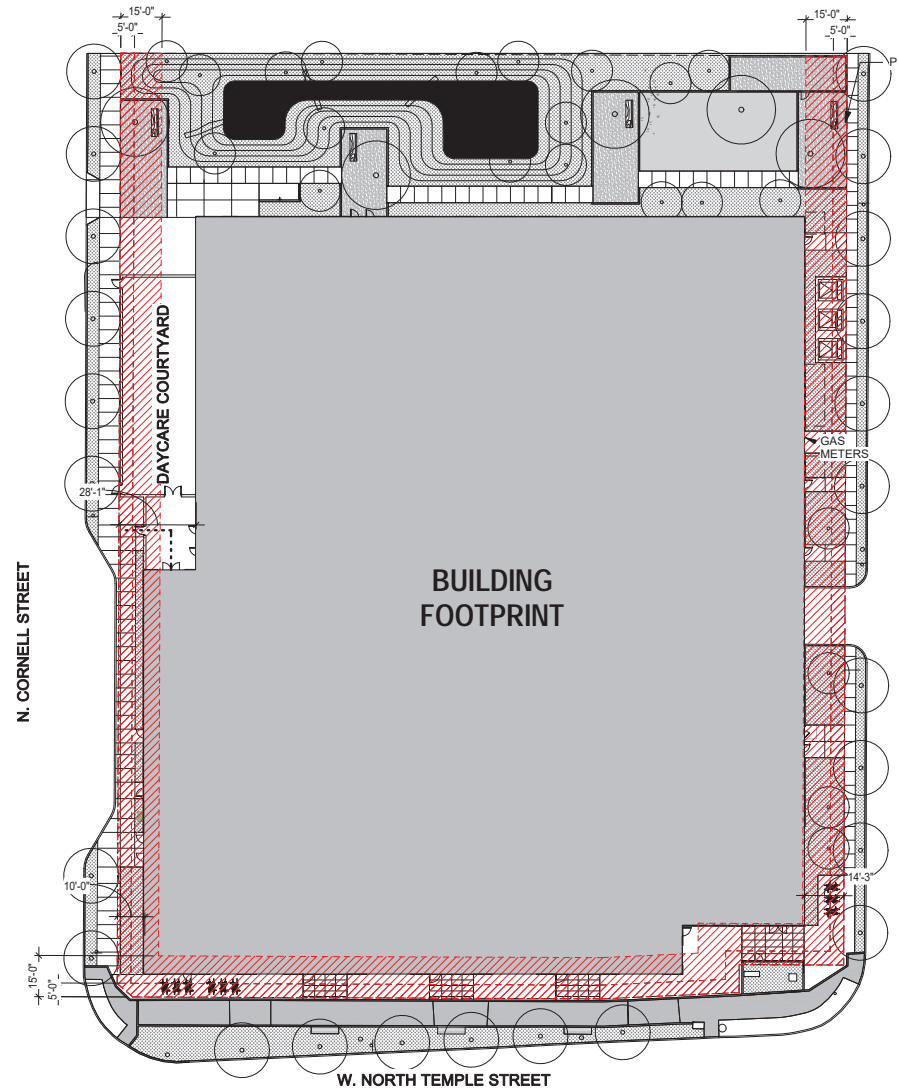
PROJECT FOOTPRINT DIAGRAM 2



PROJECT ADJAENCY DIAGRAM 1



PROJECT SETBACK DIAGRAM - L06 PLAN 4



PROJECT SETBACK DIAGRAM - L01 PLAN 3



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ZONING DIAGRAM

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PERSPECTIVE VIEW

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PERSPECTIVE VIEW

DS0-15



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PERSPECTIVE VIEW

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PERSPECTIVE VIEW

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PERSPECTIVE VIEW

DS0-18

ATTACHMENT C: Applicant Narratives

SPARK – DESIGN REVIEW PROJECT NARRATIVE

1500 W. North Temple Street Salt Lake City, UT 84116



PROJECT VISION

Spark has been conceived as a pivotal site that will set a new precedent for transit-oriented communities in Salt Lake City. The project is situated immediately adjacent to the Power Station TRAX stop which will provide vital affordable/workforce housing for Salt Lake City and will combine it with essential childcare and retail programs to provide the community with an engaging mix of on-site programs for residents and the public. The project site design looks to replace the under-utilized motel building with a vibrant new mixed-use community consisting of affordable and market-rate housing, street-facing retail, and a ground floor day care center.

Assessor parcel: 08-34-476-017-0000

Site area: 2.07 AC (90,169 SF)

Zoning: TSA-MUEC-C

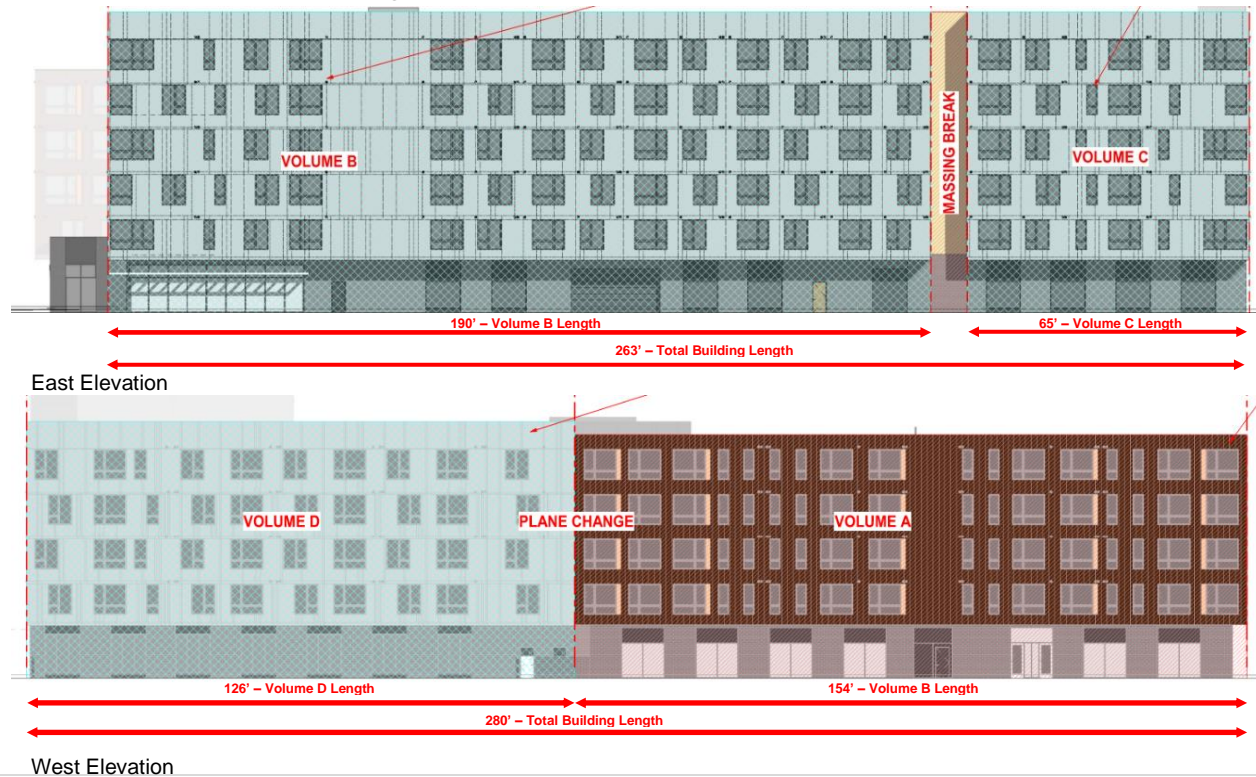
*Initial Zoning Review – SPARK – 1500 W North Temple – Based on Plans
Submitted with TSA Application PLNTSD2022-00629*

Review Date: July 5, 2022 Planner: David J. Gellner, Senior Planner

A Design Review application is required per the procedures and requirements outlined in Chapter 21A.59.

- 21A.37.050.F Maximum Length Of Street Facing Facades:** No street facing building wall may be longer than specified along a street line according to section [21A.37.060](#), table [21A.37.060](#) of this chapter. A minimum of twenty feet (20') is required between separate buildings when multiple buildings are placed on a single parcel according to subsection [21A.36.010B](#), "One Principal Building Per Lot", of this title. The space between buildings shall include a pedestrian walkway at least five feet (5') wide. **(200 Feet Maximum in the TSA Zone)**

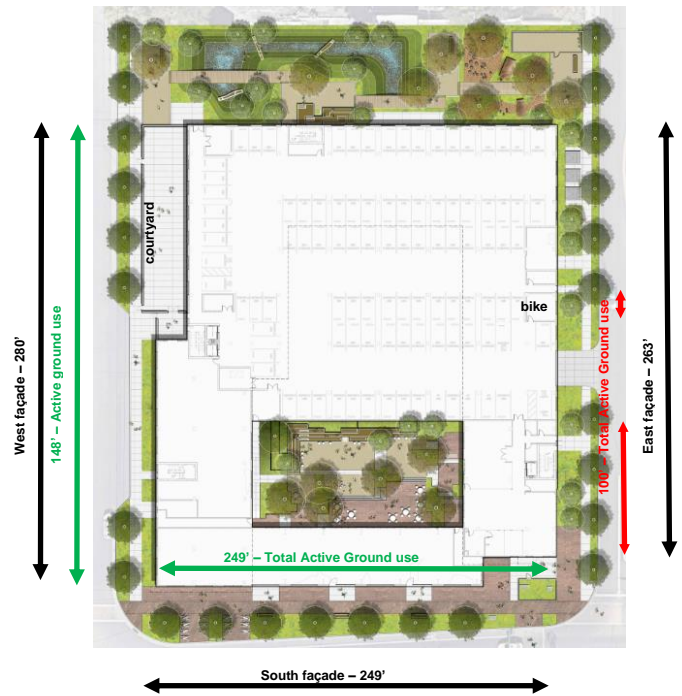
The South façade is not fully complying with the requirement, but the majority of the façade on this side is either set back or low in height. Due to the existing site dimensions, the East and West façades, facing Cornell Street and 1460 W exceed the 200ft maximum. On both façades, we have proposed design solutions to help break up the façade visually. On the East façade, there will be a 5' deep, 8' wide massing break that separates volume B (190' wide) and volume C (68' wide). On the West façade, there will be a 20' deep plane change between volume A (154' wide) and volume D (126' wide). Please refer to DS0-12 in Planning Submittal Package for more details. Moreover, the project provides a 40'+ deep through-block open space feature that would include a multi-modal trail for pedestrians and cyclists. This connective element would include some edible landscape elements such as fruit-bearing trees, rosemary, sage, and lavender, along with some areas for residents and the public to stop and rest and soak in the aromas of the garden. In conclusion, although the proposed project may not fully comply with the façade length requirement in chapter 21A.37.050, the project team has explored several avenues that mitigate the issue, and we believe that the proposed building adequately addresses the concerns about building length through its mitigation measures, in order to provide a solution that integrates well into the public realm.



- **21A.37.050.A Ground Floor Use and Visual Interest:** This standard's purpose is to increase the amount of active uses and/or visual interest on the ground floor of a building. There are two (2) options for achieving this, one dealing solely with the amount of ground floor use, and the other combining a lesser amount of ground floor use with increased visual interest in the building facade's design.

1. **Ground Floor Use Only:** This option requires that on the ground floor of a new principal building, a permitted or conditional use other than parking shall occupy a minimum portion of the length of any street facing building facade according to section [21A.37.060](#), table [21A.37.060](#) of this chapter. All portions of such ground floor spaces shall extend a minimum of twenty five feet (25') into the building. Parking may be located behind these spaces. **(80% of frontage in TSA zone)**

East Façade Length = 263', Active use = 100' (38%)
South Façade Length = 249', Active use = 227' (92%)
West Façade Length = 280', Active use = 258' (92%)



While the east elevation does not meet the 80% ground floor use, we have provided active programs along the majority of the south and west elevation to allow the parking garage to be located at the north east corner to free up for the on-grade semi-public courtyard at the south. Our goal is to create diverse open spaces that offer a variety of experiences at various levels throughout the project. The project provides the opportunity for an on-grade courtyard, which will offer the public and residents a semi-private space to gather and relax. This would help the project to provide an engaging street experience that would not have been possible with the garage occupying this part of the project site.

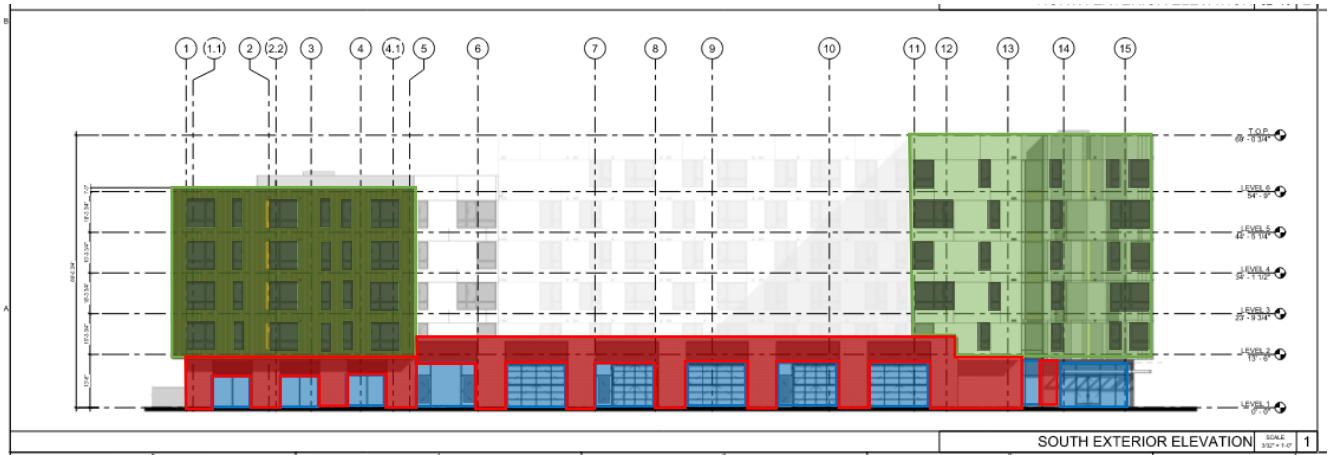


- **21A.37.050 B. Building Materials:**

1. **Ground Floor Building Materials:** Other than windows and doors, a minimum amount of the ground floor facade's wall area of any street facing facade shall be clad in durable materials according to section [21A.37.060](#), table [21A.37.060](#) of this chapter. Durable materials include stone, brick, masonry, textured or patterned concrete, and fiber cement board. Other materials may be used for the remainder of the ground floor facade adjacent to a street. Other materials proposed to satisfy the durable requirement may be approved at the discretion of the Planning Director if it is found that the proposed material is durable and is appropriate for the ground floor of a structure. (**90% Required in the TSA Zone**)
2. **Upper Floor Building Materials:** Floors above the ground floor level shall include durable materials on a minimum amount of any street facing building facade of those additional floors according to section [21A.37.060](#), table [21A.37.060](#) of this chapter. Windows and doors are not included in that minimum amount. Durable materials include stone, brick, masonry, textured or patterned concrete, and fiber cement board. Other materials may be approved at the discretion of the Planning Director if it is found that the proposed material is durable and is appropriate for the upper floor of a structure. (**60% Required in the TSA Zone**)

- **21A.37.050 C Glass:**

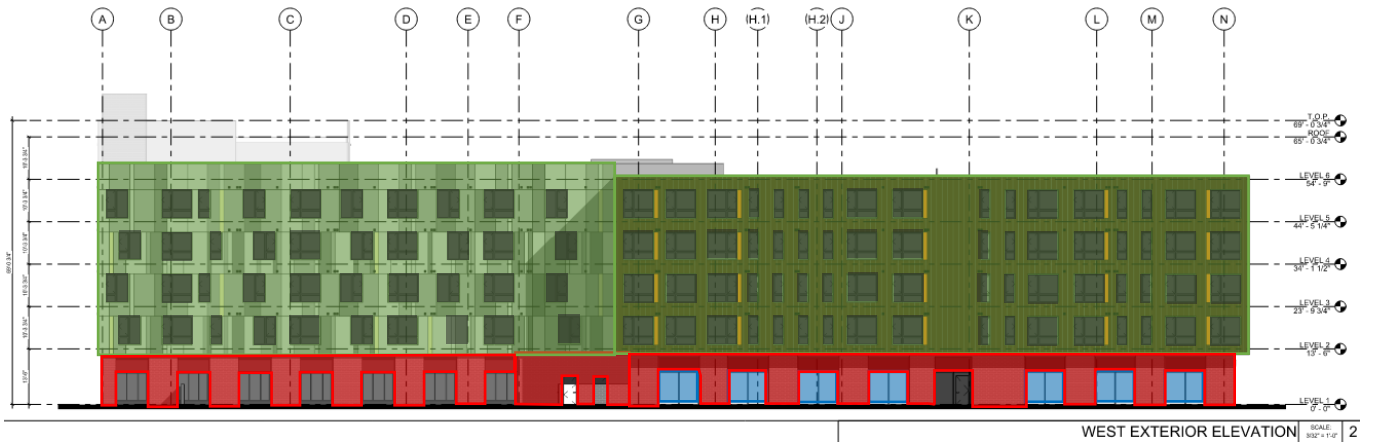
1. **Ground Floor Glass:** The ground floor building elevation of all new buildings facing a street, and all new ground floor additions facing a street, shall have a minimum amount of glass, or within a specified percentage range, between three feet (3') and eight feet (8') above grade according to section [21A.37.060](#), table [21A.37.060](#) of this chapter. All ground floor glass shall allow unhampered and unobstructed visibility into the building for a depth of at least five feet (5'), excluding any glass etching and window signs when installed and permitted in accordance with [chapter 21A.46](#), "Signs", of this title. (**60% Required in the TSA Zone**)



Ground floor façade = 2,720 sf, 249' in length, Upper floor façade = 5,840 sf

Ground floor façade excluding opening= 2,109 sf; Upper floor façade excluding opening = 4,255 sf

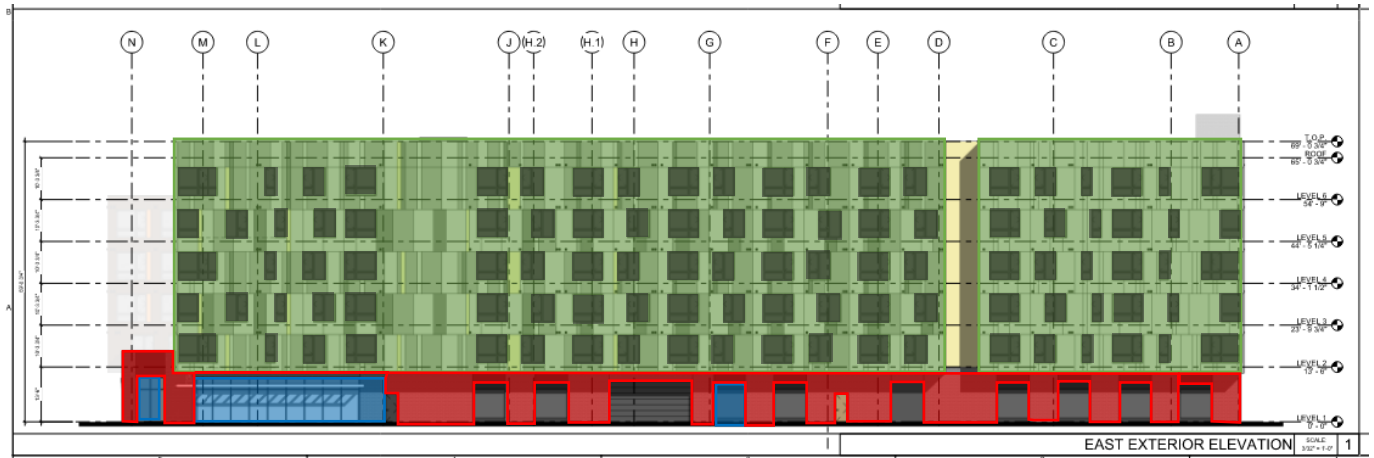
- Ground Floor Durable Material Area– (Brick) = 2,109 sf (100%)
- Glass - Ground Floor area = 1,650 SF (61%)
- Upper Floor Durable material – (Fiber Cement Panel + Metal Panel) 4,255 sf(100%)



Ground floor façade = 3,360 sf, 280' in length, Upper floor façade = 12,605 sf

Ground floor façade excluding opening= 2,252 SF; Upper floor façade excluding opening = 8,321 SF

- Ground Floor Durable Material Area– (Brick) = 2,252 sf (100%)
- Glass - Ground Floor ratio = 80' in length (29% of building length)
- Upper Floor Durable material – (Fiber Cement Panel + Metal Panel) 8,321 sf (100%)



Ground floor façade = 3,084 sf, 263' in length, Upper floor façade = 14,558 sf

Ground floor façade excluding opening= 2,153 sf; Upper floor façade excluding opening = 10,211 sf

■ Durable material – (brick) Ground Floor area = 2,153 sf (100%)

■ Durable material – (Fiber Cement Panel) Upper Floor area = 10,211 sf (100%)

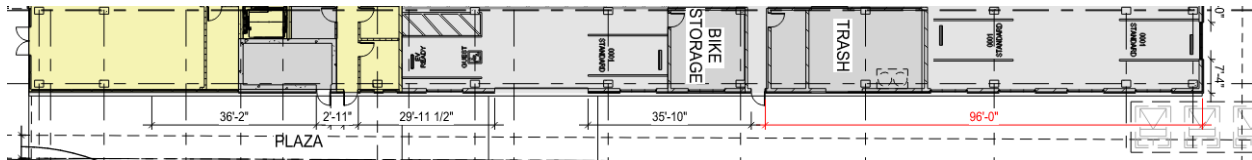
■ Glass - Ground Floor ratio = 62' in length (24% of building length)

Overall, the project is largely consistent with the requirements of chapter **21A.37**. The only areas that deviate from the requirements are noted in red above. As mentioned, we rotated the parking garage and left the center of the project for an on-grade courtyard. Therefore, a portion of the façade has to be designed to meet standard parking garage requirements. In order to provide sufficient ventilation to the garage, architectural screens have been included to ensure air flow inside. As a result, certain area of the façade may have deviated from the required standards.

- 21A.37.050 D Building Entrance:** At least one operable building entrance on the ground floor is required for every street facing facade. Additional operable building entrances shall be required, at a minimum, at each specified length of street facing building facade according to section [21A.37.060](#), table [21A.37.060](#) of this chapter. The center of each additional entrance shall be located within six feet (6') either direction of the specified location. Each ground floor nonresidential leasable space facing a street shall have an operable entrance facing that street and a walkway to the nearest sidewalk. Corner entrances, when facing a street and located at approximately a forty five degree (45°) angle to the two (2) adjacent building facades (chamfered corner), may count as an entrance for both of the adjacent facades.
(40 feet required in the TSA Zone)

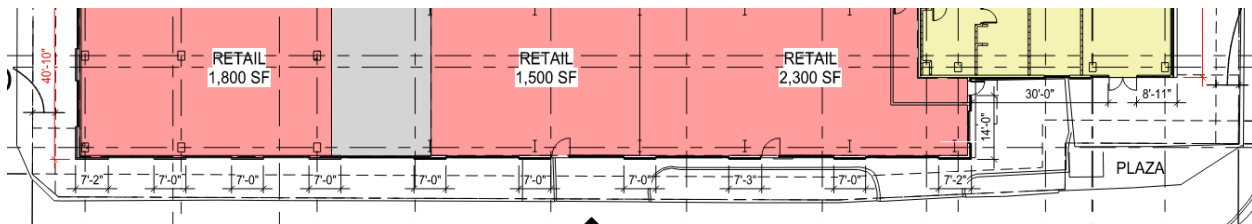
East Elevation

As indicated in the enlarged floor plan, there is one portions of the wall that exceeds 40' entrance spacing requirement. Due to security reason, the project only provides a single entrance to the parking garage on this façade.



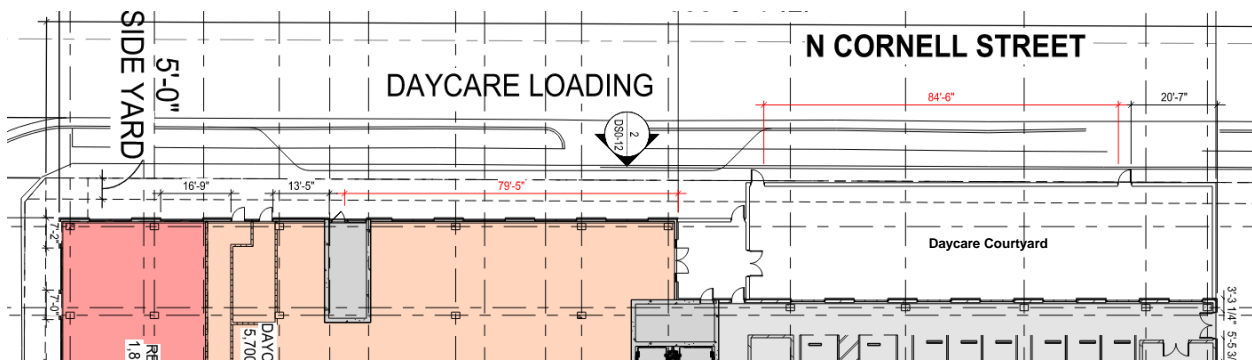
South Elevation

As indicated in the enlarged floor plan, the entire façade satisfies the 40' entrance spacing requirement.



West Elevation

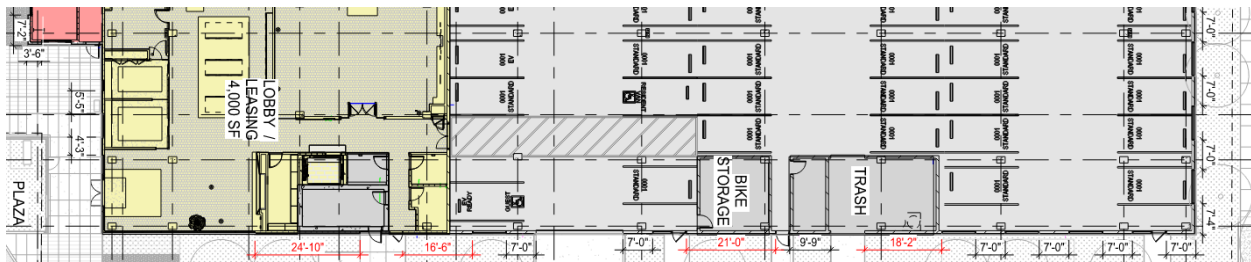
As indicated in the enlarged floor plan, there are two portions of the wall that exceeds 40' entrance spacing requirement. The areas that require deviation would be the day care center façade. Due to security reason and the nature of the daycare, limited entrances are advised. Currently, in order to enhance the condition, entrances are provided on both ends of the daycare center and the daycare courtyard.



- 21A.37.050 E Maximum Length of Blank Wall:** The maximum length of any blank wall uninterrupted by windows, doors, art or architectural detailing at the ground floor level along any street facing facade shall be as specified according to section [21A.37.060](#), table [21A.37.060](#) of this chapter. Changes in plane, texture, materials, scale of materials, patterns, art, or other architectural detailing are acceptable methods to create variety and scale. This shall include architectural features such as bay windows, recessed or projected entrances or windows, balconies, cornices, columns, or other similar architectural features. The architectural feature shall be either recessed a minimum of twelve inches (12") or projected a minimum of twelve inches (12"). (**15 feet maximum in the TSA Zone**)

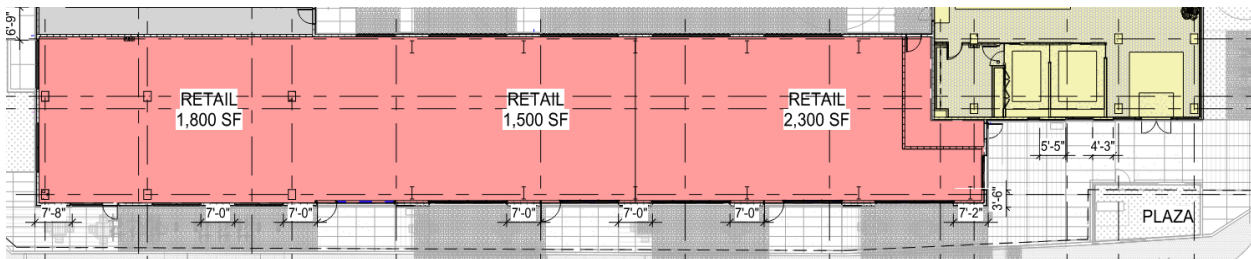
East Elevation

As indicated in the enlarged floor plan, there are four portions of the wall that exceeds 15'-0" blank wall requirement. This is part of the core enclosure, and the wall will be clad with patterned brick and therefore it should not have too big of an impact overall.



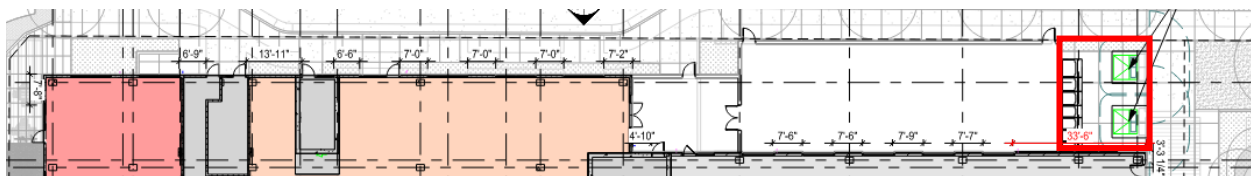
South Elevation

As indicated in the enlarged floor plan below, this street-facing façade complies with the maximum 15'-0" blank wall requirement.



West Elevation

As indicated in the enlarged floor plan, there is one portions of the wall that exceeds 15'-0" blank wall requirement. This is wall is introduced because of the transformer. However, there will be screening (please see reference exhibits) around the transformer (red box), which would essentially hide the blank wall behind





3 UTILITY FENCING
N/A

SPARK – Planned Development Project Overall Narrative

1500 W. North Temple Street Salt Lake City, UT 84116



*Assessor parcel: 08-34-476-017-0000
Site area: 2.07 AC (90,169 SF)
Zoning: TSA-MUEC-C*

*Initial Zoning Review – SPARK – 1500 W North Temple – Based on Plans
Submitted with TSA Application PLNTSD2022-00629
Review Date: July 5, 2022 Planner: David J. Gellner, Senior Planner*



A. PURPOSE AND OBJECTIVES OF A PLANNED DEVELOPMENT (21A.55.010)

1. Open Space and Natural Lands

One benefits of the proposed project massing is the creation of diverse open spaces that offer a variety of experiences at various levels throughout the project. The ground floor contains the corner plaza, public retail court, mid-block connection park, and the children's outdoor play area. The 2nd floor podium courtyard will offer residents protected and secured outdoor recreation where one can overlook the activity below while enjoying the outdoor BBQ grills, fire pit, or other amenities that will be provided at this deck. Also at level 2, and at the roof of the retail volume below is where the honey bee 'farm' and butterfly conservatory will be located. Please refer to outdoor diagram.

2. Mobility

The project site is conveniently situated between the light rail station and the Jordan River. Cornell and 1460 West both run North towards the river and provide connectivity to Cottonwood Park and the Jordan River Pkwy Trail, but currently there's no convenient E/W connection between the new streets. The proposed project would provide a 40'+ wide open space element that would consist of a 10ft wide multi-modal trail for pedestrians and cyclists. This connective element would include some edible landscapes such as fruit-bearing trees, rosemary, sage, and lavender, along with some areas for residents and the public to stop and rest and soak in the aromas of the garden. As currently envisioned, this mid-block passage could also contain play elements for young children and spaces for pets to get outdoors

OUTDOOR PROGRAM



B. STANDARDS FOR PLANNED DEVELOPMENTS (21A.55.050)

1. Planned Development Objectives

As set forth directly above, the proposed project meets multiple planned development objectives. Layers of open spaces are provided through out the project for both residence as well as pedestrian. Light rail station can also conveniently be accessed from the project site. Lastly, mid-block connection is also proposed to enhance accessibility across the project.



2. Master Plan Compatibility

The proposed project is largely compatible with the City's Master Plan. The project site design looks to replace the under-utilized motel building with a vibrant new mixed-use community consisting of affordable and market-rate housing, street-facing retail, and a ground floor day care center. The project aims to create an active, integrated development where residents, retail customers and childcare students can share all that the development has to offer.

3. Design and Compatibility

The plaza and adjacent streetscape along Temple meld functional paving with softer green parkways and planting along the public street. The public realm will be sprinkled with on-grade planting, tree wells, benches, street lighting, acting as the foreground to the linear retail volume that runs parallel to Temple. The low-slung building mass is kept down intentionally to allow for views up into the heart of the project, and to create a mass that is visually welcoming. The 4,000sf retail space fronting Temple is envisioned as a 'food-centric' program that will benefit from the project's edible landscapes, community gardens, bee hives and butterfly conservatory. On the opposite corner at the SW, the street-facing façade will be made up of the child care frontage for neighborhood house. Holding the corner will allow for the child care facility to have visibility and easy access, while the massing itself will wrap around and protect the outdoor play area at the Northwest corner. These ground floor programs along with the resident lobby and lounge spaces will provide continuous street activation while also wrapping and concealing the structured parking behind them.

4. Landscaping

The current project site is an empty site and there do not appear to have any existing trees within the site. There are currently landscape strips along the peripheries of project site. Our site plan above shows that our design would be respectful to the existing condition as most of the existing landscaping shall be incorporated in the new site design. Moreover, as indicated on the site plan, landscape buffers will be provided at the parking lot entry as required by ordinance. Please refer to site images.



5. Mobility

One of the most important goals of the project is to create a user-friendly environment for different groups. As indicated earlier, the project aims to enhance mobility around the site by providing a 40'+ wide mid-block connection that would consist of a 10ft wide multi-modal trail for pedestrians and cyclists. On North Temple, as shown in the following rendering, plenty of storefront retail as well as a semi-public on grade courtyard are introduced to increase visibility and accessibility. To further emphasize the connectivity between the proposed project and the Trax, main residential lobby is located at the corner right opposite to the Power Trax station. Additionally, Cottonwood Park and the Jordan River Pkwy Trail are within walking distance from the project.



6. Existing Site Features

The original site for the project was an under-utilized motel building which was already been demolished. Currently, the project site is an empty lot and therefore there would not be any community significant features that have to be retained for this project

7. Utilities

The immediate adjacent sites are an empty lot and a car wash facility, which will expected to have a low utilities demand. Therefore, existing utilities will be sufficient to supply for the new proposed project.



C. Long Term Maintenance of All Private Infrastructure

Section 21A.55.110 of City code requires provisions for disclosure of private infrastructure maintenance and placement costs to unit owners. The SPARK! project will consist of for-rent apartment housing, with the entirety of the units under single ownership. Therefore, any maintenance or replacement costs for proposed infrastructure will be the responsibility of the project owner. The project will be contained within an existing parcel, so a subdivision plat is not anticipated for this site.

Based on preliminary feedback from two DRT meetings with City staff, the SPARK! Project will rely on the water main upsizing in Cornell that is included in the design and permit drawings for The Gauge project to the immediate west of the SPARK! site. The new waterline will be adequately sized for both domestic flows and fire flows for this project. The existing public sewer and storm drain infrastructure requires no upsizing to serve this development.

The infrastructure improvements that will require regular maintenance and upkeep will include the aboveground stormwater retention/detention basin, the newly paved areas around the building (including sidewalk and brick pavers), and the landscaped areas along all three street frontages. An Operations & Maintenance (O&M) manual will be generated for this project as part of the City permitting process. The O&M manual will outline the required maintenance intervals for each aspect of the project, notifying the owner of their responsibilities.

Lastly, please kindly refer to the system impact study report from Rocky Mountain Power for electrical service distribution for the proposed project.

ATTACHMENT D: Property & Vicinity Photos





ATTACHMENT E: Zoning & Design Standards

TSA Transit Station Area Purpose Statement ([21A.26.078.A](#))

The purpose of the TSA Transit Station Area District is to provide an environment for efficient and attractive transit and pedestrian oriented commercial, residential and mixed use development around transit stations. Redevelopment, infill development and increased development on underutilized parcels should include uses that allow them to function as part of a walkable, Mixed-Use District. Existing uses that are complementary to the district, and economically and physically viable, should be integrated into the form and function of a compact, mixed-use pedestrian-oriented neighborhood. Each transit station is categorized into a station type. These typologies are used to establish appropriate zoning regulations for similar station areas. Each station area will typically have two (2) subsections: the core area and the transition area. Due to the nature of the area around specific stations, the restrictions of Overlay Zoning Districts, and the neighborhood vision, not all station areas are required to have a core area and a transition area.

Standard	Proposed	Finding
Minimum Lot Area: 2,500 sq. ft.	90,169 sq. ft.	Complies
Minimum Lot Width: 40 ft	264' 4"	Complies
<p>Maximum Building Height: 75' + one additional floor</p> <p>Minimum Building Height: 25' (for at least 50% of the width of the street facing façade)</p>	<p>North Elevation = 69' ¾"</p> <p>East Elevation = 69' ¾"</p> <p>South Elevation = Top is 69' ¾" & middle portion is less than 25' in height – middle portion accounts for 55.82% of the width of the street facing facade</p> <p>West Elevation = 54' 9"</p>	<p>North, East, and West Elevations – Complies</p> <p>South Elevation – Requires PD approval</p>
<p>Front/Corner Yard Setback – North Temple:</p> <p>Minimum: 5', and at least 50% of the street facing building facade must be built to the minimum.</p> <p>Maximum: 15', but may be increased if the additional setback is used for plazas, courtyards, or outdoor dining areas.</p> <p>Sidewalk Requirement: In locations where the sidewalk is not a minimum of 10' wide, additional sidewalk width shall be installed by the developer so there is a minimum width sidewalk of 10'.</p>	South Yard = 8' to 24'	Requires DR Approval
<p>Front/Corner Yard Setback – other streets</p> <p>Minimum: None</p> <p>Maximum: At least 50% of the street facing building facade shall be within 5' of the front or corner side property line.</p>	<p>West Yard = 9' 7" to 28'</p> <p>East Yard = Approx. 14'</p>	Requires DR Approval
Interior Side Setback: none	Not applicable	Complies

Rear Yard Setback: none	59' 6" for midblock connection	Complies
Open Space: 1 sq-ft per 10 sqft of lot area, up to 5,000 square feet in core areas	Approx. 17,002 square feet (roof terrace and courtyard)	Complies
Building Material Limitation: Use of Exterior Insulation and Finishing System (EIFS) or traditional stucco is not allowed as a building material on the ground floor of street facing building facades. Use of EIFS and stucco is allowed for up to ten percent (10%) of the upper level street facing facades.	No EIFS or stucco used	Complies
Landscaping: At least fifty percent (50%) of the front or corner side yards shall be covered in live plant material. This can include raised planter boxes. This percentage can be reduced to thirty percent (30%) if the yard includes outdoor dining, patios, outdoor public space, or private yards for ground floor residential uses that cover at least fifty percent (50%) of the provided front or corner side yard.	Landscaping covers at least 50% of the Corner side yards along Cornell Street and 1460 West. Planters and outdoor public space fulfill this requirement for the North Temple side of the building	Complies
Landscaping: At least 1 shade tree per 30 ft in yards deeper than 10 feet	Tress Shown in Landscape Plans	Complies
Outdoor Public Space: At least thirty percent (30%) of the front or corner side yard shall be occupied by outdoor dining areas, patios, outdoor public space, or private yards for ground floor residential uses.	More than 30% of front and corner side yards would be used as publicly available open space	Complies
Entries: All required building entrances shall include at least one of the features in 21A.26.078.F.2.c	Entries meet this standard	Complies
Parking: No minimum parking requirement. Maximum parking: 1 space per dwelling and 3 spaces per 1,000 usable square feet of nonresidential	100 spaces provided.	Complies
Ground Floor Use Requirement (North Temple): When facing 400 South or North Temple Boulevard, the ground floor use area required by chapter 21A.37 of this title shall be built to accommodate an allowed commercial, institutional, or public use.	Retail takes up the majority of the North Temple-facing Ground Floor. The Residential Lobby meets the additional standards required by this section.	Complies

Applicable Design Standards (see table [21A.37.060.B](#))

Requirement	Standard	Proposed	Finding
Ground Floor Use (21A.37.050.A)	Option 1: Use other than parking must occupy at least 80% of ground-floor façade length (excluding parking access) Special provisions along North Temple (See 21A.26)	South façade = 92% East façade = 38% West façade = 37.5%	Requires DR approval
Building Materials, ground floor (21A.37.050.B.1)	At least 90% of street-facing facades must be clad in durable materials (excluding doors and windows)	South = 100% East = 100% West = 100%	Complies

Building Materials, upper floors (21A.37.050.B.2)	At least 60% of street-facing facades must be clad in durable materials (excluding doors and windows)	South = 100% East = 100% West = 100%	Complies
Glass: ground floor (21A.37.050.C.1)	60% of street-facing façades must have transparent glass	South = 61% East = 24% West = 29%	Requires DR approval
Building Entrances (21A.37.050.D)	Required every 40 feet	South = Complies East = Does not comply West = Does not Comply	Requires DR approval
Blank wall Maximum Length (21A.37.050.E)	15 feet	South and west facades comply with this standard. East façade does not.	Requires DR approval
Max Length of Street-facing Façade (21A.37.050.F)	200 ft	All street facing facades exceed 200'.	Requires DR approval
Lighting: exterior (21A.37.050.H)	All proposed exterior lighting must be shielded and directed downward.	No lighting plan has been submitted.	Defer to Staff
Lighting: parking lot (21A.37.050.J)	All lighting for parking lots cannot exceed 16 feet in height and must be directed downward when adjacent to a residential zoning district	Not applicable	Not Applicable
Screening of mechanical equipment (21A.37.050.J)	All mechanical equipment must be screened from view	Location of mechanical equipment shown on Northwest corner of Property, proposed to be screened with 6-foot fence.	Complies
Screening of service areas (21A.37.050.K)	Screened from public view	Dumpsters located within parking structure.	Complies

ATTACHMENT F: Design Review Standards

21A.59.050: Standards for Design Review: In addition to standards provided in other sections of this title for specific types of approval, the following standards shall be applied to all applications for design review:

The Finding for each standard is the recommendation of the Planning Division based on the facts associated with the proposal, the discussion that follows, and the input received during the engagement process. Input received after the staff report is published has not been considered in this report.

A. Any new development shall comply with the intent of the purpose statement of the zoning district and specific design regulations found within the zoning district in which the project is located as well as the City's adopted "urban design element" and adopted master plan policies and design guidelines governing the specific area of the proposed development.

TSA Transit Station Area Purpose Statement (21A.26.078.A): *The purpose of the TSA Transit Station Area District is to provide an environment for efficient and attractive transit and pedestrian oriented commercial, residential and mixed use development around transit stations. Redevelopment, infill development and increased development on underutilized parcels should include uses that allow them to function as part of a walkable, Mixed-Use District. Existing uses that are complementary to the district, and economically and physically viable, should be integrated into the form and function of a compact, mixed-use pedestrian-oriented neighborhood. Each transit station is categorized into a station type. These typologies are used to establish appropriate zoning regulations for similar station areas. Each station area will typically have two (2) subsections: the core area and the transition area. Due to the nature of the area around specific stations, the restrictions of Overlay Zoning Districts, and the neighborhood vision, not all station areas are required to have a core area and a transition area.*

Core Area Purpose Statement (21A.26.078.A.1): *The purpose of the core area is to provide areas for comparatively intense land development with a mix of land uses incorporating the principles of sustainable, transit oriented development and to enhance the area closest to a transit station as a lively, people oriented place. The core area may mix ground floor retail, office, commercial and residential space in order to activate the public realm.*

Mixed Use Employment Center Station Purpose Statement (21A.26.078.B.3): *A mixed use employment station is an area with a high concentration of jobs that attract people from the entire region. Buildings are often large-scale in nature and may have large footprints. Land uses that support the employment centers such as retail sales and service and restaurants are located throughout the station area and should occupy ground floor space in multi-story buildings oriented to the pedestrian and transit user. A mix of housing types and sizes are appropriate to provide employees with the choice to live close to where they work. Building types should trend toward more flexible building types over time. Connectivity for all modes of travel is important due to the limited street network.*

Design Standards Purpose Statement (21A.37.010): *The design standards identified in this chapter are intended to utilize planning and architecture principles to shape and promote a walkable environment in specific zoning districts, foster place making as a community and economic development tool, protect property values, assist in maintaining the established character of the City, and implementing the City's master plans.*

Finding: Complies

Discussion: The proposed Spark! development project conforms with the intended purpose of the TSA District as well as the corresponding station typology (Mixed Use Employment Center) and subsection (Core

Area). The proposal will develop a property that is currently vacant (previously occupied by the Overniter Motel) and within an area that the Salt Lake City’s Redevelopment Agency has found to have historically experienced disinvestment and neglect. The *Mixed Use Employment Center* typology calls for buildings that are “large-scale in nature” with large footprints, and the proposed building meets this requirement.

Condition(s): None

B. Development shall be primarily oriented to the sidewalk, not an interior courtyard or parking lot.

- 1. Primary entrances shall face the public sidewalk (secondary entrances can face a parking lot).**
- 2. Building(s) shall be sited close to the public sidewalk, following and responding to the desired development patterns of the neighborhood.**
- 3. Parking shall be located within, behind, or to the side of buildings.**

Finding: Complies

Discussion: The project is primarily oriented to the sidewalk and is consistent with the following factors:

1. The subject property has three street frontages. Along North Temple there are entrances to commercial spaces and the lobby space for the residential units. Additional entrances to the project are included in the secondary streets including entrance to a daycare center.
2. The building is sited close to the sidewalk and the project includes plaza space between the building and the sidewalk. This is consistent with the desired development pattern in the area.
3. Parking is located within the building.

Condition(s): None

C. Building facades shall include detailing and glass in sufficient quantities to facilitate pedestrian interest and interaction.

- 1. Locate active ground floor uses at or near the public sidewalk.**
- 2. Maximize transparency of ground floor facades.**
- 3. Use or reinterpret traditional storefront elements like sign bands, clerestory glazing, articulation, and architectural detail at window transitions.**
- 4. Locate outdoor dining patios, courtyards, plazas, habitable landscaped yards, and open spaces so that they have a direct visual connection to the street and outdoor spaces.**

Finding: Complies

Discussion: The project facilitates pedestrian interest and interaction considering the following factors:

1. Commercial spaces, a daycare center, and the lobby for the residential component of the project are located along the primary street (North Temple) and wrap around the corners of the building onto the two secondary streets.
2. North Temple Street should be treated as a primary street and the North Temple façade is the primary façade of the development. The North Temple façade meets the minimum ground floor glass requirement of the zoning district. The facades along the two secondary streets do not meet the minimum glass requirement overall; however, they do maximize the transparency at the street intersections.

3. The North Temple Street frontage incorporates storefront elements as part of the retail/commercial spaces. The lobby space and commercial space/daycare center located at the corners of the building include sufficient glazing and articulation to facilitate pedestrian interest.
4. The project incorporates landscaping and hardscape between the building and the sidewalk to create plazas and habitable spaces.

Condition(s): None

D. Large building masses shall be divided into heights and sizes that relate to human scale.

1. **Relate building scale and massing to the size and scale of existing and anticipated buildings, such as alignments with established cornice heights, building massing, step-backs and vertical emphasis.**
2. **Modulate the design of a larger building using a series of vertical or horizontal emphases to equate with the scale (heights and widths) of the buildings in the context and reduce the visual width or height.**
3. **Include secondary elements such as balconies, porches, vertical bays, belt courses, fenestration and window reveals.**
4. **Reflect the scale and solid-to-void ratio of windows and doors of the established character of the neighborhood or that which is desired in the master plan.**

Finding: Complies

Discussion: Large building masses are divided into heights and sizes that relate to a human scale.

1. There are currently no buildings near the subject property to relate the massing to. The project is consistent with anticipated future buildings in the area. Although the façade along North Temple does not meet the minimum height requirement along the street frontage, it provides active uses and relates to human scale with taller portions of the building located at the corners.
2. Although the building frontages exceed the maximum building length, the building is designed with distinct segments according to their function. The application submittal shows the massing breaks at each street frontage.
3. Each street facing façade contains vertical elements, modulations in height and front setbacks, and other elements that break up the horizontal building mass.
4. There is currently no development that establishes a specific character. The pattern of windows and doors along North Temple Street is consistent with development patterns anticipated for future development. The facades along the secondary streets do not fully comply with the minimum glass and entrance requirements; however, these elements are present on these facades near the street intersections.

Condition(s): None

E. Building facades that exceed a combined contiguous building length of two hundred feet (200') shall include:

1. **Changes in vertical plane (breaks in facade)**
2. **Material changes; and**
3. **Massing changes.**

Finding: Complies

Discussion: The application submittal contains diagrams showing how the street facing facades have changes in materials and massing to create a visual break in the facades.

Condition(s): None

F. If provided, privately-owned public spaces shall include at least three (3) of the six (6) following elements:

- 1. Sitting space of at least one sitting space for each two hundred fifty (250) square feet shall be included in the plaza. Seating shall be a minimum of sixteen inches (16") in height and thirty inches (30") in width. Ledge benches shall have a minimum depth of thirty inches (30");**
- 2. A mixture of areas that provide seasonal shade;**
- 3. Trees in proportion to the space at a minimum of one tree per eight hundred (800) square feet, at least two-inch (2") caliper when planted;**
- 4. Water features or public art;**
- 5. Outdoor dining areas; and**
- 6. Other amenities not listed above that provide a public benefit.**

Finding: Complies

Discussion: The additional front setback areas along North Temple Street and 1460 West will be utilized as plaza space. The plans and diagrams show landscaping, seating and trees that are appropriate. A midblock greenway is also proposed. Staff will review the building plans to ensure that these elements are included in the final design.

Condition(s): Staff review of building permit plans for full compliance.

G. Building height shall be modified to relate to human scale and minimize negative impacts. In downtown and in the CSHBD Sugar House Business District, building height shall contribute to a distinctive City skyline.

- 1. Human scale:**
 - a. Utilize stepbacks to design a building that relate to the height and scale of adjacent and nearby buildings, or where identified, goals for future scale defined in adopted master plans.**
 - b. For buildings more than three (3) stories or buildings with vertical mixed use, compose the design of a building with distinct base, middle and top sections to reduce the sense of apparent height.**
- 2. Negative impacts:**
 - a. Modulate taller buildings vertically and horizontally so that it steps up or down to its neighbors.**
 - b. Minimize shadow impacts of building height on the public realm and semi-public spaces by varying building massing. Demonstrate impact from shadows due to building height for the portions of the building that are subject to the request for additional height.**
 - c. Modify tall buildings to minimize wind impacts on public and private spaces, such as the inclusion of a wind break above the first level of the building.**
- 3. Cornices and rooflines:**
 - a. Cohesiveness: Shape and define rooflines to be cohesive with the building's overall form and composition.**
 - b. Complement Surrounding Buildings: Include roof forms that complement the rooflines of surrounding buildings.**

c. Green Roof and Roof Deck: Include a green roof and/or accessible roof deck to support a more visually compelling roof landscape and reduce solar gain, air pollution, and the amount of water entering the stormwater system.

Finding: Not Applicable

Discussion: The only portion of the building that does not meet the height requirements is along North Temple Street. The subject portion of the building is under the 25-foot minimum height requirement. This standard is related to buildings that exceed the by-right maximum height limitation; therefore, this standard is not applicable.

Condition(s): None

H. Parking and on-site circulation shall be provided with an emphasis on making safe pedestrian connections to the sidewalk, transit facilities, or midblock walkway.

Finding: Complies

Discussion: Parking is located within the structure. The building façade where the access is located is setback to provide sufficient space for drivers of vehicles to see pedestrians while the vehicles exit the building.

Condition(s): None

I. Waste and recycling containers, mechanical equipment, storage areas, and loading docks shall be fully screened from public view and shall incorporate building materials and detailing compatible with the building being served. Service uses shall be set back from the front line of building or located within the structure. (See subsection 21A.37.050K of this title.)

Finding: Complies

Discussion: Service uses are located within the parking garage.

Condition(s): None

J. Signage shall emphasize the pedestrian/mass transit orientation.

- 1. Define specific spaces for signage that are integral to building design, such as commercial sign bands framed by a material change, columns for blade signs, or other clearly articulated band on the face of the building.**
- 2. Coordinate signage locations with appropriate lighting, awnings, and other projections.**
- 3. Coordinate sign location with landscaping to avoid conflicts.**

Finding: Complies With Conditions

Discussion: A specific sign package has not been submitted. Staff is of the opinion that compliance with the sign regulations for the TSA zoning district will result in signage that is consistent with this standard.

Condition(s): Compliance with TSA signage regulations.

K. Lighting shall support pedestrian comfort and safety, neighborhood image, and dark sky goals.

- 1. Provide streetlights as indicated in the Salt Lake City Lighting Master Plan.**
- 2. Outdoor lighting should be designed for low-level illumination and to minimize glare and light trespass onto adjacent properties and uplighting directly to the sky.**
- 3. Coordinate lighting with architecture, signage, and pedestrian circulation to accentuate significant building features, improve sign legibility, and support pedestrian comfort and safety.**

Finding: Complies With Conditions

Discussion: A detailed lighting plan has not been provided. Staff recommends that the lighting plan be reviewed during the building permit phase of the project to ensure compliance with this standard.

Condition(s): Staff to review building permit plans for compliance.

L. Streetscape improvements shall be provided as follows:

- 1. One street tree chosen from the street tree list consistent with the City's urban forestry guidelines and with the approval of the City's Urban Forester shall be placed for each thirty feet (30') of property frontage on a street. Existing street trees removed as the result of a development project shall be replaced by the developer with trees approved by the City's Urban Forester.**
- 2. Hardscape (paving material) shall be utilized to differentiate privately-owned public spaces from public spaces. Hardscape for public sidewalks shall follow applicable design standards. Permitted materials for privately-owned public spaces shall meet the following standards:**
 - a. Use materials that are durable (withstand wear, pressure, damage), require a minimum of maintenance, and are easily repairable or replaceable should damage or defacement occur.**
 - b. Where practical, as in lower-traffic areas, use materials that allow rainwater to infiltrate into the ground and recharge the water table.**
 - c. Limit contribution to urban heat island effect by limiting use of dark materials and incorporating materials with a high Solar- Reflective Index (SRI).**
 - d. Utilize materials and designs that have an identifiable relationship to the character of the site, the neighborhood, or Salt Lake City.**
 - e. Use materials (like textured ground surfaces) and features (like ramps and seating at key resting points) to support access and comfort for people of all abilities.**
 - f. Asphalt shall be limited to vehicle drive aisles.**

Finding: Complies

Discussion: The plans show compliance with the tree requirement. The plans submitted show materials that differentiate between the public and private space. Detailed plans have not been provided that show compliance with the other material standards. Staff will review for compliance when complete plans have been submitted for building permits.

Condition(s): Staff to review building permit plans for compliance.

ATTACHMENT G: Planned Development Standards

Planned Development Standards

21A.55.050: Standards for Planned Developments: The planning commission may approve, approve with conditions, or deny a planned development based upon written findings of fact according to each of the following standards. It is the responsibility of the applicant to provide written and graphic evidence demonstrating compliance with the following standards.

The Finding for each standard is the recommendation of the Planning Division based on the facts associated with the proposal, the discussion that follows, and the input received during the engagement process. Input received after the staff report is published has not been considered in this report.

A. Planned Development Objectives: The planned development shall meet the purpose statement for a planned development (section 21A.55.010 of this chapter) and will achieve at least one of the objectives stated in said section. To determine if a planned development objective has been achieved, the applicant shall demonstrate that at least one of the strategies associated with the objective are included in the proposed planned development. The applicant shall also demonstrate why modifications to the zoning regulations are necessary to meet the purpose statement for a planned development. The Planning Commission should consider the relationship between the proposed modifications to the zoning regulations and the purpose of a planned development, and determine if the project will result in a more enhanced product than would be achievable through strict application of the land use regulations.

Planned Development Purpose Statement: A planned development is intended to encourage the efficient use of land and resources, promoting greater efficiency in public and utility services and encouraging innovation in the planning and building of all types of development. Further, a planned development implements the purpose statement of the zoning district in which the project is located, utilizing an alternative approach to the design of the property and related physical facilities. A planned development incorporates special development characteristics that help to achieve City goals identified in adopted Master Plans and that provide an overall benefit to the community as determined by the planned development objectives. A planned development will result in a more enhanced product than would be achievable through strict application of land use regulations, while enabling the development to be compatible with adjacent and nearby land developments.

Discussion: The Planned Development process is being requested because a portion of one façade of the building does not meet the minimum height requirement and additional fence height is requested in order to screen utility boxes. Modification to the height requirement allows modulation of the front façade. The two ends of the building are taller and meet the height regulation. Allowing the center portion to be slightly lower than what is required will provide a bit of an open feeling. Allowing the additional fence height would allow for an artistic element to screen utility boxes. Overall, the project helps to implement many planning goals by providing affordable housing, providing a daycare, and incorporating a mid-block greenway.

Finding: Meets Purpose Statement Does Not Meet Purpose Statement

A. Open Space And Natural Lands: Preserving, protecting or creating open space and natural lands:

1. Inclusion of community gathering places or public recreational opportunities, such as new trails or trails that connect to existing or planned trail systems, playgrounds or other similar types of facilities.

2. Preservation of critical lands, watershed areas, riparian corridors and/or the urban forest.
3. Development of connected greenways and/or wildlife corridors.
4. Daylighting of creeks/water bodies.
5. Inclusion of local food production areas, such as community gardens.
6. Clustering of development to preserve open spaces.

Discussion:

The project includes a midblock greenway and an outdoor plaza.

Finding: Objective Satisfied Objective Not Satisfied

C. Housing: Providing affordable housing or types of housing that helps achieve the City's housing goals and policies:

1. At least twenty percent (20%) of the housing must be for those with incomes that are at or below eighty percent (80%) of the area median income.
2. The proposal includes housing types that are not commonly found in the existing neighborhood but are of a scale that is typical to the neighborhood.

Discussion: The project includes affordable housing.

Finding: Objective Satisfied Objective Not Satisfied

D. Mobility: Enhances accessibility and mobility:

1. Creating new interior block walkway connections that connect through a block or improve connectivity to transit or the bicycle network.
2. Improvements that encourage transportation options other than just the automobile.

Discussion: Project includes a midblock walkway.

Finding: Objective Satisfied Objective Not Satisfied

F. Master Plan Implementation: A project that helps implement portions of an adopted Master Plan in instances where the Master Plan provides specific guidance on the character of the immediate vicinity of the proposal:

1. A project that is consistent with the guidance of the Master Plan related to building scale, building orientation, site layout, or other similar character-defining features. (Ord. 8-18, 2018)

Discussion: The project implements the goals of the North Temple Boulevard Plan.

Finding: Objective Satisfied Objective Not Satisfied

B. Master Plan Compatibility: The proposed planned development is generally consistent with adopted policies set forth in the Citywide, community, and/or small area Master Plan that is applicable to the site where the planned development will be located.

Finding: Complies

Discussion:

The project is consistent with Plan Salt Lake and the North Temple Boulevard Master Plan as stated in the Key Considerations section of this report.

Condition(s): Complies.

C. Design And Compatibility: The proposed planned development is compatible with the area the planned development will be located and is designed to achieve a more enhanced product than would be achievable through strict application of land use regulations. In determining design and compatibility, the Planning Commission should consider:

1. Whether the scale, mass, and intensity of the proposed planned development is compatible with the neighborhood where the planned development will be located and/or the policies stated in an applicable Master Plan related to building and site design;

Finding: Complies

Discussion:

Overall, the project is consistent with the TSA zoning regulations as they related to scale, mass, and intensity. Although the project requires a modification to the minimum height standards for street facing facades, the ask is minimal. The ordinance regulation requires at least 50% of the street facing façade to be at least 25 feet in height. In this case, the under-height segment accounts for approximately 56%, which is a minimal adjustment.

Condition(s): None

2. Whether the building orientation and building materials in the proposed planned development are compatible with the neighborhood where the planned development will be located and/or the policies stated in an applicable Master Plan related to building and site design;

Finding: Complies

Discussion: The project has frontage along three public streets. North Temple is treated as the primary frontage (as it should) and the building includes commercial spaces and the lobby space for the residential units along this frontage. While the other two streets are secondary streets, the building still incorporates active uses at the corners. This design is consistent with the vision for the area as stated in the adopted master plan. The project also meets the durable building material requirements for projects located in the TSA zoning district.

Condition(s): None

3. Whether building setbacks along the perimeter of the development:

- a. Maintain the visual character of the neighborhood or the character described in the applicable Master Plan.
- b. Provide sufficient space for private amenities.
- c. Provide sufficient open space buffering between the proposed development and neighboring properties to minimize impacts related to privacy and noise.
- d. Provide adequate sight lines to streets, driveways and sidewalks.
- e. Provide sufficient space for maintenance.

Finding: Complies

Discussion:

The applicant has requested modifications to the required setbacks through the Design Review process. Although the setbacks along the street frontages are slightly larger than what is required, Staff is of the opinion that they are appropriate. They provide extra space for plazas, landscaping and other pedestrian improvements. The additional building setback along 1460 West provides additional space between the parking garage access and the sidewalk, which increases pedestrian safety. The building is also setback considerably in the rear in order to construct a midblock greenway.

Condition(s): None

4. Whether building facades offer ground floor transparency, access, and architectural detailing to facilitate pedestrian interest and interaction;

Finding: Complies

Discussion:

Ground floor access, transparency, and architectural detailing have been addressed in the Design Review standards. Staff is of the opinion that the project facilitates pedestrian interest and interaction.

Condition(s): None

5. Whether lighting is designed for safety and visual interest while minimizing impacts on surrounding property;

Finding: Complies

Discussion:

A detailed lighting plan has not been provided. Staff recommends that the lighting plan be reviewed during the building permit phase of the project to ensure compliance with the specific Design Review standards.

Condition(s): Staff to review building permit plans for compliance.

6. Whether dumpsters, loading docks and/or service areas are appropriately screened;

Finding: Complies

Discussion:

The dumpsters and loading and service areas are located within the parking structure and not visible from the street.

Condition(s): None.
7. Whether parking areas are appropriately buffered from adjacent uses.
Finding: Complies
Discussion: Parking is located within an enclosed parking structure. It is appropriately buffered from adjacent uses.
Condition(s): None.

D. Landscaping: The proposed planned development preserves, maintains or provides native landscaping where appropriate. In determining the landscaping for the proposed planned development, the Planning Commission should consider:

1. Whether mature native trees located along the periphery of the property and along the street are preserved and maintained;

Finding: Complies
Discussion: There are no mature native trees along the periphery of the property or along the street that should be preserved.

Condition(s): None

2. Whether existing landscaping that provides additional buffering to the abutting properties is maintained and preserved;

Finding: Complies
Discussion: There is no existing landscaping that needs to be preserved.

Condition(s): None

3. Whether proposed landscaping is designed to lessen potential impacts created by the proposed planned development;

Finding: Complies
Discussion: Landscaping, including street trees is included along all three street frontages. The project also includes a landscaped mid-block crossing that will function as a greenway and heavily landscaped areas in the internal courtyard. Staff is of the opinion that the proposed landscaping is appropriate for this development.

Condition(s): None
4. Whether proposed landscaping is appropriate for the scale of the development.
Finding: Complies
Discussion: Landscaping, including street trees is included along all three street frontages. The project also includes a landscaped mid-block crossing that will function as a greenway and heavily landscaped areas in the internal courtyard. Staff is of the opinion that the proposed landscaping is appropriate for this development.
Condition(s): None

E. Mobility: The proposed planned development supports Citywide transportation goals and promotes safe and efficient circulation within the site and surrounding neighborhood. In determining mobility, the Planning Commission should consider:
1. Whether drive access to local streets will negatively impact the safety, purpose and character of the street;
Finding: Complies
Discussion: There is only one drive access to the development, and it is located along 1460 West. This single drive access will not negatively impact the street.
Condition(s): None
2. Whether the site design considers safe circulation for a range of transportation options including: <ul style="list-style-type: none"> a. Safe and accommodating pedestrian environment and pedestrian oriented design; b. Bicycle facilities and connections where appropriate, and orientation to transit where available; and c. Minimizing conflicts between different transportation modes;
Finding: Complies
Discussion: The project includes improvements to the sidewalk and an enhanced pedestrian experience along North Temple Street. It also includes a multi-modal mid-block crossing. Access to the parking garage is located on the east side of the building along 1460 West. The building setback along this frontage is increased. This will allow drivers of autos more room to see pedestrians on the sidewalk.
Condition(s): None
3. Whether the site design of the proposed development promotes or enables access to adjacent uses and amenities;
Finding: Complies
Discussion:

There are limited adjacent use at this time; however, the project is located directly adjacent to station TRAX. The main doors of the lobby supporting the residential units opens directly to the crosswalk to the station. The project also includes improvements to the sidewalks.

Condition(s): None

4. Whether the proposed design provides adequate emergency vehicle access;

Finding: Complies

Discussion:

A representative from the Fire Department reviewed the plans and provided the regulations that will be used when reviewing the building plans. No issues were raised concerning the existing plans.

Condition(s): None

5. Whether loading access and service areas are adequate for the site and minimize impacts to the surrounding area and public rights-of-way.

Finding: Complies

Discussion:

The service areas are located within the parking structure and do not create an impact to the surrounding area or right-of-way.

Condition(s): None

F. Existing Site Features: The proposed planned development preserves natural and built features that significantly contribute to the character of the neighborhood and/or environment.

Finding: Complies

Discussion:

There are no natural or built features that need to be preserved.

Condition(s): None

G. Utilities: Existing and/or planned utilities will adequately serve the development and not have a detrimental effect on the surrounding area.

Finding: Complies

Discussion:

The developer will be required to provide improvements to utility infrastructure in order to serve the development; however, it is not anticipated that the improvements will have a detrimental effect on the surrounding area.

Condition(s): None

ATTACHMENT H: Public Process & Comments

Public Notice, Meetings, Comments

The following is a list of public meetings that have been held, and other public input opportunities, related to the proposed project since the applications were submitted:

- September 1, 2022 – The 45-day Notice was sent to the chairs of the Jordan Meadows, Fairpark and Polar Grove Community Councils. The councils did not provide comments.
- September 2, 2022 - Property owners and residents within 300 feet of the development were provided early notification of the proposal.
- September – October 2022 – The project was posted to the Online Open House webpage.

Notice of the public hearing for the proposal included:

- November 4, 2022
 - Public hearing notice sign posted on the property
- November 3, 2022
 - Public hearing notice mailed
 - Public notice posted on City and State websites and Planning Division list serve

Public Input:

Planning staff did not receive any comments regarding this request as of the day this report was published.

ATTACHMENT I: Department Review Comments

This proposal was reviewed by the following departments. Any requirement identified by a City Department is required to be complied with.

Engineering:

I'm aware of a RMP transformer that is proposed to be relocated from private property into the public way of North Temple. The transformer serves the UTA Trax station and is not needed to serve this project, which will have its own transformers installed on private property.

The public way improvements to be installed in the public way of North Temple, 1460 West and Cornell Street must meet APWA Standards.

Building:

- Clarify whether this project involves two separate buildings. It appears that with the two different construction types for the residential portions that a Fire Wall is required between the two buildings. Please confirm.
- The 2018 IBC Section 1106.1 and Table 1106.1 does not account for EV accessible spaces. The IBC requires 4 accessible parking spaces which will include 2 van accessible space.
- The Area Summary and Common Area Schedule shall be shown for each of the two buildings separately. Two buildings are required because of the two different construction types of the residential occupancies.
- Provide a breakdown of the Type A and Type B units as per the 2018 IBC Section 1107.6.2.
- Identify the extent of the Type IA construction which is the below podium level as per 2018 IBC Section 510.2
- Clarify which building is the 5 story and which is the 4 story. Clarify what the 1 story building's the construction type.
- Identify where the 3 hour Fire Wall for buildings separation is located.
- Clarify whether the perspective drawing accurately represents the one level of parking which is the Type IA Construction and the second level is the top of the podium.

Fire:

- Approved fire apparatus access roads shall be provided for every facility, building or portion of a building hereafter constructed or moved into; and shall extend to within 150 feet of all portions of the facility and all portions of the exterior walls of the first story of the building as measured by an approved route around the exterior of the building or facility. AMM needed for increased fire protection fees
- Fire apparatus access roads shall have an unobstructed width of not less than 20 feet for buildings 30-feet or less, exclusive of shoulders, except for approved security gates in accordance with Section 503.6, and an unobstructed vertical clearance of not less than 13 feet 6 inches. Buildings greater than 30 feet shall have a road width of not less than 26 feet. Fire apparatus access roads with fire hydrants on them shall be 26-feet in width; at a minimum of 20-feet to each side of the hydrant in the direction of road travel.
- Fire apparatus access roads shall be designed and maintained to support the imposed loads of fire apparatus (80,000 pounds) and shall be surfaced to provide all-weather driving capabilities.
- The required turning radius of a fire apparatus access road shall be the following: Inside radius is 20 feet, outside is 45-feet
- Buildings or portions of buildings constructed or moved into or within the jurisdiction is more than 400 feet from a hydrant on a fire apparatus access road, as measured by an approved route around the exterior of the facility or building, on-site fire hydrants and mains shall be provided where required by the fire code official. Additional fire hydrants may be necessary dependent on total square footage and required fire flows in accordance with IFC appendix B and C
- Fire department connections shall be located on the street address side of buildings, fully visible and recognizable from the street, and have a fire hydrant within 100-feet on the same side of the street.
- Where a fire hydrant is located on a fire apparatus access road, the minimum road width shall be 26 feet, exclusive of shoulders.
- Aerial fire apparatus access roads shall be provided where the highest roof surface exceeds 30 feet measured from grade plane. For purposes of this section, the highest roof surface shall be determined by measurement

to the eave of a pitched roof, the intersection of the roof to the exterior wall, or the top of parapet walls, whichever is greater. Some exceptions have been added by SLC; those can be obtained from this office.

- Aerial fire apparatus access roads shall have a minimum unobstructed width of 26 feet, exclusive of shoulders. Aerial access routes shall be located not less than 15 feet and not greater than 30 feet from the building and shall be positioned parallel to one entire side of the building, which shall be along 1460 W to residential section of the building
- Overhead utility and power lines shall not be located over the aerial fire apparatus access road or between the aerial fire apparatus road and the building.
- Buildings or facilities exceeding 30 feet or three stories in height shall have not fewer than two means of fire apparatus access for each structure.

Urban Forestry:

I have no concerns from an Urban Forestry perspective. They will need to get an Urban Forestry Tree Planting Permit for the proposed street trees and it will be necessary to identify a specific species of tree at that time.

Housing Stability:

The Housing Stability Division's comments regarding the design review and planned development of the Spark! project, in relation to [*Growing SLC: A Five Year Housing Plan, 2018-2022*](#) are as follows.

Concerns:

- No concerns.

In Support of:

- Salt Lake City is committed to increasing mixed-income developments, increasing the number of affordable/income-restricted units, and increasing equity in all housing.
 - The developer's planned 200 residential units to be income restricted, at an income-averaging of 60% of the Area Median Income, advances the goals of the Housing Plan by increasing the number of affordable/income-restricted units in the City.
 - The developer's planned inclusion of three and four bedroom units, advances the goals of the Housing Plan by providing a wider range of rental options and supports families with children looking to live in the City.

Recommendations:

- We encourage the developer to include units with accommodations and amenities in alignment with the Americans with Disabilities Act, such as: ramps, automatic door openers, wider door frames, grab bars, and roll-in showers to benefit residents with temporary or long-term mobility difficulties.

Transportation:

The following details, as applicable to the project, are to be provided:

- ADA parking stall and access aisle dimensions, signage, and pavement markings.
- Parking stall width and length dimensions and aisle width dimensions. Any parking stalls adjacent to a wall or a column must be 1' wider than the standard width. (21A.44.020)
- Signage and/or pavement markings for electric vehicle parking spaces indicating exclusive availability for electric vehicles (see 21A.44.050.B.2).
- Bike rack installation (See SLC Transportation Standard Detail, F1.f2, "Bicycle Parking" @ <http://www.slcdocs.com/transportation/design/pdf/F1.f2.pdf>).
- Required loading berth location and dimensions.

Public Utilities:

The following comments are provided for information only and do not provide official project review or approval. Comments are provided to assist in design and development by providing guidance for project requirements.

- Public Utility permit, connection, survey, and inspection fees will apply.
- All utility design and construction must comply with APWA Standards and SLCPU Standard Practices.
- All utilities must meet horizontal and vertical clearance requirements. Water and sewer lines require 10 ft minimum horizontal separation and 18" minimum vertical separation. Sewer must maintain 5 ft minimum horizontal separation and 12" vertical separation from any non-water utilities. Water must maintain 3 ft minimum horizontal separation and 12" vertical separation from any non-sewer utilities.

- Contact SLCPU Street Light Program Manager, Dave Pearson (801-483-6738), for information regarding street lights.
- Utilities cannot cross property lines without appropriate easements and agreements between property owners.
- Site utility and grading plans will be required for building permit review. Other plans such as erosion control plans and plumbing plans may also be required, depending on the scope of work. Submit supporting documents and calculations along with the plans.
- Applicant must provide fire flow, culinary water, and sewer demand calculations to SLCDPU for review. The public sewer and water system will be modeled with these demands. If the water demand is not adequately delivered by the existing main, then a water main upsizing will be required at the property owner's expense. The expected maximum daily flow (gpd) from the development will be modeled to determine the impacts on the public sewer system. If one or more sewer lines reaches of the sewer system reach capacity as a result of the development, sewer main upsizing will be required at the property owner's expense. Required improvements on the public water and sewer system will be determined by the Development Review Engineer. A plan and profile of the new main(s) and engineer's cost estimate must be submitted for review. Design drawings and cost estimate must be stamped and signed by a professional engineer. The property owner is required to bond for the amount of the approved cost estimate.
- One culinary water meter is permitted per parcel. Fire lines are permitted, as necessary. If the parcel is larger than 0.5 acres, a separate irrigation meter is also permitted. Each service must have a separate tap to the main.
- If any food service establishments (FSE) will be located on the property, then a grease removal device will be required. Treatment must be provided such that the discharge limit of 500 mg/L of FOG (fats, oils, and grease) is achieved. Treatment device and plan for treatment and sampling must be provided that meets the discharge limit and all applicable standards set forth in the Utah plumbing code and SLCDPU Standards. Treatment device shall be located as to be readily accessible for cleaning and inspection. The treatment device must be sized by a licensed design professional. For an exterior, below grade device, a 4 foot diameter sampling manhole, per APWA 411, must be located downstream of the interceptor and upstream of any other connections. For alternative treatment methods, a sampling point must be provided immediately downstream of the device and upstream of any other connections.
- Covered parking area drains and work shop area drains are required to be treated to remove solids and oils prior to discharge to the sanitary sewer. These drains cannot be discharged to the storm drain. Use a sand/oil separator or similar device. A 4ft diameter sampling manhole must be located downstream of the device and upstream of any other connections.
- Site stormwater must be collected on site and routed to the public storm drain system. Stormwater cannot discharge across property lines or public sidewalks.
- Stormwater treatment is required prior to discharge to the public storm drain. Utilize stormwater Best Management Practices (BMP's) to remove solids and oils. Green Infrastructure should be used whenever possible. Green Infrastructure and LID treatment of stormwater is a design requirement and required by the Salt Lake City UPDES permit for Municipal Separate Storm Sewer System (MS4). This permit was updated with this requirement in June 2021. The applicant will need to provide options for stormwater treatment and retention for the 80th percentile storm. If additional property is not available, there are other options such as green roof or other BMP's. Lack of room or cost is generally not an exception for this requirement. If green infrastructure is not used, then applicant must provide documentation of what green infrastructure measures were considered and why these were not deemed feasible. Please verify that plans include appropriate treatment measures. Please visit the following websites for guidance with Low Impact Development: <https://deq.utah.gov/water-quality/low-impact-development?form=MY01SV&OCID=MY01SV> and <https://documents.deq.utah.gov/water-quality/stormwater/updes/DWQ-2019-000161.pdf?form=MY01SV&OCID=MY01SV>.
- Stormwater detention is required for this project. The allowable release rate is 0.2 cfs per acre. Detention must be sized using the 100-year 3-hour design storm using the farmer Fletcher rainfall distribution. Provide a complete Technical Drainage Study including all calculations, figures, model output, certification, summary and discussion.
- Projects larger than one acre require that a Stormwater Pollution Prevention Plan (SWPPP) and Technical Drainage Study are submitted for review.