



Staff Report

PLANNING DIVISION

DEPARTMENT of COMMUNITY and NEIGHBORHOODS

To: Salt Lake City Planning Commission
From: Amanda Roman, Principal Planner, amanda.roman@slcgov.com or 801-535-7660
Date: May 25, 2022
Re: PLNSUB2021-00866 – Preliminary Plat
PLNPCM2021-00870 – Planned Development
PLNPCM2022-00176 – Design Review

Planned Development // Design Review// Preliminary Plat

PROPERTY ADDRESS: 1012 W 200 S

PARCEL ID: 15-02-178-025-0000

MASTER PLAN: North Temple Boulevard Plan

ZONING DISTRICT: TSA-UN-T (Transit Station Area – Urban Neighborhood – Transition)

REQUEST:

NeighborWorks Salt Lake, represented by Pascal Meyer of Carpenter Stringham Architects, has submitted Planned Development, Design Review, and Preliminary Plat applications to develop the property located at 1012 W 200 S. The proposed development will consist of 16 single-family attached townhomes, each on their own lot. The subject property is currently undeveloped and is located in the TSA Urban Neighborhood Transition District (TSA-UN-T).

Planned Development: The development is required to obtain Planned Development approval for the following modifications:

- Lots without frontage on a public street
- Reduction in the side yard setback adjacent to an RMF-35 zone from 25 feet to 17 feet
- Increase in the front yard setback from 5 feet to approximately 7'3"
- Increase in the amount of permitted open space
- Reduction of the required 10-foot landscape buffer

Design Review: The development is required to go through the Design Review process because the proposal did not obtain enough TSA points to allow for an administrative approval. Projects must receive at least 125 points to be administratively approved and the Euclid Corners proposal received 88 points. The applicant is also requesting a reduction in the ground floor glass requirement, which requires Design Review approval.

Subdivision: Each townhome will be subdivided onto its own lot. Preliminary plats can be approved administratively, but staff brought the application to the Planning Commission because it is tied to the Planned Development and Design Review petitions. Other than the request for lots without frontage on a public street and modified setback standards, the preliminary plat meets the applicable subdivision standards.

RECOMMENDATION:

Based on the information and findings listed in the staff report, it is the Planning Staff's opinion that the request generally meets the applicable standards of approval and therefore recommends the Planning Commission approve the request with the following conditions:

1. The applicant must complete a Subdivision Improvement Construction Agreement with the Engineering Division for the public improvements to be installed in 1000 West, 200 South, and the public alley. The subdivider will need to execute the agreement prior to recordation of the final plat.
2. The section of alley adjacent to the property must have "No Parking" signs installed.

ATTACHMENTS

- A. [Vicinity Map](#)
- B. [Plan Set](#)
- C. [Property and Vicinity Photos](#)
- D. [TSA Zoning Standards](#)
- E. [Planned Development Standards](#)
- F. [Design Review Standards](#)
- G. [Preliminary Plat Standards](#)
- H. [Public Process & Comments](#)
- I. [Department Review Comments](#)

PROJECT DESCRIPTION

The residential development will consist of 16 two- to three-bedroom townhomes in 4 separate buildings. The subject property is approximately .53 acres (23,311 square feet) and is undeveloped. NeighborWorks Salt Lake intends to sell at least 20% of the proposed dwelling units to those whose income is 80% or less of the median household income of the city. The project was originally approved by the Planning Commission in 2017, but the applicant did not obtain building permits or apply for a final plat, so their approval expired. Since 2017, the Zoning Ordinance has been amended, which required the applicant to slightly modify their original proposal to what is shown today.

Measured from grade, the units with frontage along 1000 West (Building 1) and 200 South (Building 2) are approximately 37 feet in height. The four units (Buildings 3 and 4) with frontage on the alleyway are approximately 28'6" in height. The maximum building height in the TSA-UNT zone is 50 feet. The townhomes along 1000 West and 200 South have a private rooftop deck

and all of the units have stooped entrances and a covered front porch. The project includes private underground parking with one stall per two-bedroom unit and two stalls per three-bedroom unit. The parking garage is accessed via a 16 foot wide alleyway off 1000 West. There is a landscaped courtyard encompassing 5,756 square feet located in the middle of the development, which can be accessed from the rear of each building or from 200 South.



Quick Facts

Height: 24 feet (2 story units) – 37 feet (3 story units)

Number of Residential Units: 16 (approx. 30 units per acre)

Uses: Residential with underground parking garage

Exterior Materials: Brick, fiber cement siding, metal siding, concrete

Parking: 24 stalls

Open Space: 25% of project area

Summary of Requested Zoning Modifications

The applicant is requesting Planned Development and Design Review approval for six zoning modifications.

Planned Development Requests

The applicant is requesting approval for lots without frontage on a public street. Lots 13-16 have frontage along the alleyway. The required side yard setback is 25 feet, and the applicant has requested to modify it to 17 feet. The front yard setback may be no more than 5 feet for 50% of the street facing façade. To avoid having the buildings stoops encroach into the public right of way, the applicant is requesting a front yard setback of approximately 7'3". To accommodate the design of the interior courtyard, the applicant is requesting approval for more than 2,500 square feet of open space, which is the maximum permitted in the TSA Transition zone. Finally, the applicant is requesting to reduce the 10-foot landscape buffer that is required when a property in the TSA zone abuts a property in the RMF-35 zone.

Design Review Requests

A Design Review is also required because the project did not obtain at least 125 TSA points, which allows for an administrative approval. The project received 88 points out of the submitted 119. In addition to not receiving the minimum points, the applicant is requesting modifications to the ground floor glass requirement. Due to the internal functions of the buildings, the applicant is requesting to reduce the ground floor glass requirement from 45% to between 7-41%.

APPROVAL PROCESS AND COMMISSION AUTHORITY

Per section 21A.55.030 of the Zoning Ordinance, the Planning Commission may approve a Planned Development as proposed or may impose conditions necessary or appropriate for the Planned Development to comply with the standards. The Planning Commission may deny an application for a Planned Development if it finds that the proposal does not meet the intent of the base zoning district (TSA-UN-T), does not meet the purpose of a Planned Development, or is not consistent with the standards and factors as set forth in section 21A.55.

Design Reviews may be approved administratively or when required, by the Planning Commission. This project must be approved by the Planning Commission because it did not receive a TSA development score of over 125 points. Staff awarded the project a total of 88 points. Per section 21A.59.030 of the Zoning Ordinance, the Planning Commission shall approve a project if it finds that the proposal complies with the purpose of the zoning district and applicable Overlay District(s), the purpose of the individual design standards that are applicable to the project, and the project is compliant with the applicable design review objectives. The Commission may also add conditions or modifications if it determines the modifications are necessary to comply with applicable standards.

KEY CONSIDERATIONS

The key considerations listed below were identified through the analysis of the project:

1. Compliance with City Goals & Policies Identified in Adopted Plans
2. Requested Zoning Modifications

Consideration 1: Compliance with City Goals & Policies Identified in Adopted Plans

Plan Salt Lake (2015)

The City has an adopted citywide master plan that includes policies related to providing additional housing options. The plan includes policies related to growth and housing in Salt Lake City. Applicable initiatives from the plan are below.

Growth:

- *Locate new development in areas with existing infrastructure and amenities, such as transit and transportation corridors.*
- *Promote infill and redevelopment of underutilized land.*
- *Accommodate and promote an increase in the City's population.*

Housing:

- *Direct new growth toward areas with existing infrastructure and services that have the potential to be people-oriented.*
- *Increase the number of medium density housing types and options.*
- *Enable moderate density increases within existing neighborhoods where appropriate.*

Staff Discussion: The proposed development will provide infill housing that is compatible with the character and scale of the existing Euclid neighborhood. The historic housing stock is single-family, but recent development patterns consist of townhomes and higher density housing, much of which was approved via the TSA process. Almost half of the housing in Salt Lake City is single-family detached.

While preserving this type of housing stock is important, it is also a priority to provide new housing options with a range of densities, as increased density supports walkable, transit-oriented neighborhoods.

North Temple Boulevard Plan

The subject property is located within the 800 West Transitional Area of the North Temple Boulevard Plan. The Euclid neighborhood is located south of North Temple and is unique in that it has a mix of uses and small streets that bisect larger blocks. The 1000 West block of Euclid Avenue has well maintained single-family homes and 1000 West is the main bicycle lane in the area.

The neighborhood is anticipated to grow significantly in housing units and jobs by 2030. New housing types range from 3-4 story multi-family developments to single-family homes. Some parts of the Euclid neighborhood lack adequate infrastructure, so the city must plan for necessary infrastructure improvements and services that support the growth.

Staff Discussion: According to the master plan, the desired density within Transition Area's is 30 units per acre, which the Euclid Corners proposal achieves with a density of nearly 30 dwelling units per acre. While the neighborhood is stable, the subject property is located at the southern edge of the Transitional Area, which is expected to see some changes and has been designated as an area appropriate for mixed use and less intensive transit-oriented zoning. Increasing connectivity is one of the goals outlined in the North Temple Boulevard Plan. While not adjacent to public transit, this area is used as a buffer between the core along North Temple and the stable neighborhoods. The project accomplishes the desired neighborhood characteristics described in the plan, such as, locating buildings at or near the sidewalk and locating parking to the side or behind buildings.

Consideration 2: Requested Zoning Modifications

The applicant is requesting six zoning modifications: lots without frontage on a public street, a side yard setback reduction, a front yard setback increase, increased open space, reduction of the required landscape buffer, and a reduction in the ground floor glass requirement.

1. Modification for lots that do not front a public street (Section 21A.36.010.C)

The project consists of four buildings, two of which front the existing 16-foot wide public alleyway. The General Provisions in Chapter 21A.36.010.C requires that all lots front on a public street. Building's 3 and 4 house four separate units (lots 13-16) with the access to the underground parking garage located between them. The buildings are setback from the northern property line by approximately 25 feet and are approximately 28'6" feet in height, which is shorter than the other two structures. While the development functions as a condominium, with shared underground parking, garbage collection, and an interior courtyard, each unit will be on its own lot. Without the modification, Building's 3 and 4 would not be permitted since the lots will face the alleyway.

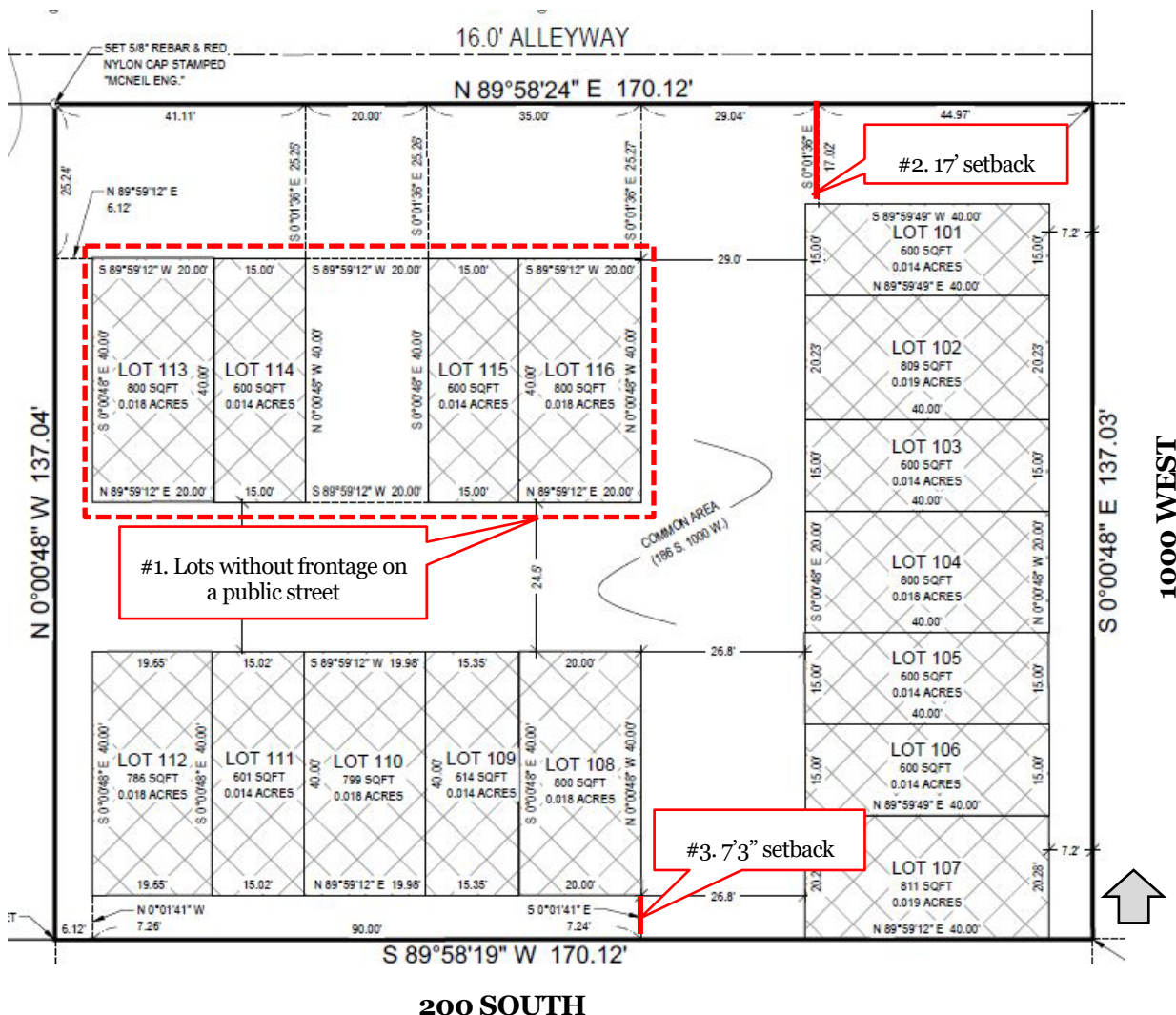
2. Reduction of the 25 foot side yard setback requirement (Table 21A.26.070.E3b)

The minimum interior side yard setback in the TSA-UN-T zone is zero, unless the building is adjacent to specific zones, such as the RMF-35, then the minimum setback is 25 feet. The minimum setback shall increase 1 foot for every 1 foot of building height above 25 feet.

An interior side yard setback modification is requested for Building 1, which has 7 units (lots 1-7) with frontage along 1000 West. The proposed side yard setback is approximately 17 feet, resulting in an 8 foot encroachment into the required setback. The abutting alley to the north is 16 feet wide, which creates a 33 foot separation between Building 1 and the residential properties to the north.

3. Increase in the minimum front yard setback (Table 21A.26.070.E3b)

While there is no minimum front yard setback in the TSA-UN-T zone, at least 50% of the street facing building façade must be within 5 feet of the front or corner property line. When the application was originally approved in 2017, a condition of approval was to obtain an encroachment permit for the stairs and landings leading to the individual units and plaza off 200 South. Since 2017, the city has changed what type of infrastructure is allowed to encroach into the public realm. Nonremovable elements, such as stairs or balconies, may no longer encroach into the public right of way. The applicant was required to move the building inward to ensure the stairs are built on private property. This adjustment increased the front yard setback to approximately 7'3". The setback from the back of sidewalk to the front façade of the building along 200 South is 13'9" and the setback to the front façade of the building along 1000 West is 14'3".



Preliminary plat - Showing #1, 2, and 3 of the PD Requests

4. Modification of maximum allowable open space (21A.26.078.E5)

The TSA zone requires one square foot of open space for every 10 feet of land area, up to 2,500 SF for transition areas. The applicant is requesting approval for more than 2,500 square feet of open space. Open space areas include landscaped yards, patios, public plazas, pocket parks, courtyards, rooftop and terrace gardens.

The proposed interior courtyard footprint is 5,756 square feet, which is 25% of the land area. The courtyard will have 11 trees and a variety of grasses. Each of the 16 units has direct access to the courtyard, which is intended to serve as a community gathering space. The additional open space lessens the impact of the development and is appropriately scaled for the site.



Landscaping plan

5. Reduction in the required landscape buffer (Section 21A.48.080.C12)

Properties that are zoned TSA and adjacent to an RMF-35 zone are required to have a 10-foot landscape buffer. City code defines a landscape buffer as, “An area of natural or planted vegetation adjoining or surrounding a land use and unoccupied in its entirety by any building, structure, paving or portion of such land use, for the purposes of screening and softening the effects of the land use.”

The northern property line of the subject property is adjacent to the RMF-35 zoning district; thus the 10-foot buffer is required. The proposal includes a 5-foot sidewalk and paving leading to Building’s 3 and 4. The remaining yard area is landscaped. The buildings are setback from the northern property line by 25 feet and when the alley is included, they are setback from the adjacent RMF-35 zone by approximately 41 feet.



Required 10-foot landscape buffer shown in yellow

6. Reduction in the ground floor glass requirement (Section 21A.37.050.C1)

Design Review approval is required for projects that don't receive the 125 TSA points or for projects that are proposing modifications to the additional design standards. The Euclid Corners project received 88 points and they are requesting a modification to the design standard that requires residential uses in the TSA zone to have 45% glass between 3-8 feet above grade. The requirement is intended to promote pedestrian interest and transparency at eye level, especially for commercial buildings.

Due to the internal functions of the building, this requirement has not been met. Approximately 4 feet of the underground parking structure is above grade, which the applicant has stated is necessary due to the slope of the parking garage access. The above ground portion of the garage includes small window wells for light and will be buffered by landscaping.

The front elevation (east) of Building 1 along 1000 West has 39.8% ground floor glass and the side (south) of the building that faces 200 South has approximately 7% glass. The front elevation (south) of Building 2, which faces 200 South, has 41% ground floor glass, Building's 3 and 4, which face the alley (north) have 21.5% ground floor glass.

While placed higher than the stipulated 3-8 feet, the applicant has added a large window in the center of each side façade and placed smaller clerestory windows across the remaining façade area. There is no upper floor glass requirement in the TSA zone.



200 South and 1000 West (south façade) elevations showing the ground floor glass between 3-8 feet

6. Other Considerations

The TSA zone requires a minimum lot area of 2,500 square feet and a minimum lot width of 40 feet. The subject property is approximately .43 acres (23,311 square feet) with a lot width of 137 feet along 1000 West and a lot width of 170 feet along 200 South. Lots subdivided for single-family attached dwellings are exempt from minimum lot area provided that:

1. Parking for units shall be rear loaded and accessed from a common drive shared by all units in a particular development;
2. Driveway access shall connect to the public street in a maximum of 2 locations; and
3. No garages shall face the primary street and front yard parking shall be strictly prohibited.

The proposed underground parking meets the three requirements above as it is accessed from a shared drive and only has one connection to the public street. So, while the units will be subdivided onto their own lots that do not meet the zones minimum lot size or width, the overall design is compliant with the TSA standards.

Staff Discussion:

While the applicant is asking for zoning modifications, the proposal meets the purpose of the Urban Neighborhood within the Transit Station Area District, which is to provide a flexible development pattern that consists of multi-level buildings that are lower in scale than what would be found in the urban center. Redevelopment and infill development should occur on underutilized parcels and should include uses that allow them to function as a part of a walkable mixed use neighborhood. The properties to the north are zoned RMF-35 and the property to the west is zoned TSA-UN-T. The maximum building height in the RMF-35 zone is 35 feet and the maximum building height in the TSA-UN-T zone is 50 feet. The proposal has a maximum height of approximately 37 feet measured from grade, which makes it compatible with the development potential of the surrounding properties. The townhomes along the alleyway are approximately 28'6" feet tall, which provides a buffer between the existing single-family homes to the north and the taller by-right height in the TSA zone. A new 5-foot wide sidewalk will be installed around the perimeter of the development.

Approving the four lots without street frontage and allowing modified setbacks creates more efficient use of the land and results in a more enhanced product that meets the purpose of the TSA zone and the development goals within the North Temple Boulevard Master Plan. Allowing an increase in the maximum front yard setback is required to meet the new encroachment standards established in the City and does not impact the overall design or street presence of the development. While the modification of the ground floor glass requirement is not something staff typically recommends approval for, due to the slope of the underground parking garage, the structure must protrude approximately 4 feet above ground. The impact of the above ground portion of the garage will be mitigated by the proposed landscaping. Due to the site's circulation a 10-foot landscape buffer has not been provided. The portion of the lot that requires the buffer is not along a public street and is not expected to have adverse impacts on surrounding properties due to the alleyway being between the subject property and the single-family dwellings to the north. Finally, allowing additional open space will create a more welcoming environment for the residents and reduce the overall impact of the development.

STAFF RECOMMENDATION

Staff is recommending approval of the Planned Development, Design Review, and Preliminary Plat petitions. The proposal meets the purpose of the TSA zoning district, and the requested modifications result in a more enhanced product than would be achievable through strict application of the land use regulations. The proposal also reflects the housing and development goals in both the North Temple Boulevard Plan and Plan Salt Lake. The master plans support infill development that is in scale with the existing and desired development pattern and provides different housing types that support the desire for a walkable, more transit oriented neighborhood.

NEXT STEPS

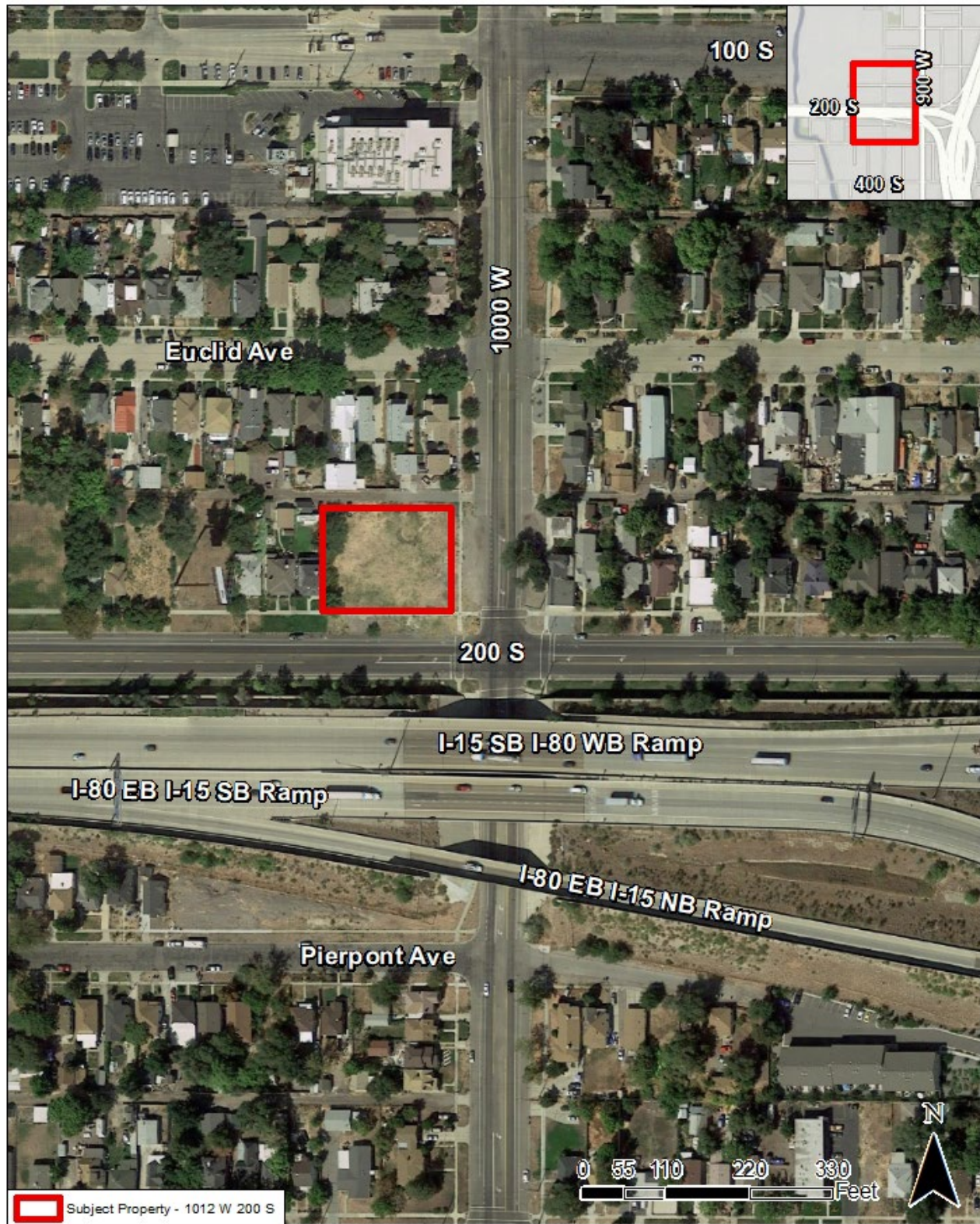
Approval of the Requests

If the petitions are approved by the Planning Commission, the applicant will need to comply with the conditions of approval, including any of the conditions required by City departments and the Planning Commission. A final plat application will need to be submitted and recorded with Salt Lake County. The subdivider will need to execute the Subdivision Improvement Construction Agreement prior to recordation of the final plat. Unless specified in the zoning ordinance as a minor modification, any modification to the development plan must be reviewed and approved by the Planning Commission.

Denial of the Requests

If the petitions are denied, the applicant would not be able to build the two buildings with frontage on the alleyway (Building's 3 and 4) and Building's 1 and 2 would need to comply with the front and side yard setback standards. The open space proposal and building elevations would need to be modified to meet code. If denied, the applicant can submit a new TSA application that obtains at least 125 points and meets the requirements of the TSA-UN-T zoning district, then proceed to building permits with an administrative approval.

ATTACHMENT A: Vicinity Map



Salt Lake City Planning Division 3/2/2022

ATTACHMENT B: Plan Set



Planned Development

SALT LAKE CITY PLANNING

OFFICE USE ONLY

Project #:	Received By:	Date Received:	Zoning:
Project Name:			

PLEASE PROVIDE THE FOLLOWING INFORMATION

Request:	
Address of Subject Property:	
Name of Applicant:	Phone:
Address of Applicant:	
E-mail of Applicant:	Cell/Fax:
Applicant's Interest in Subject Property:	
<input type="checkbox"/> Owner <input type="checkbox"/> Contractor <input type="checkbox"/> Architect <input type="checkbox"/> Other:	
Name of Property Owner (if different from applicant):	
E-mail of Property Owner:	Phone:

Please note that additional information may be required by the project planner to ensure adequate information is provided for staff analysis. All information required for staff analysis will be copied and made public, including professional architectural or engineering drawings, for the purposes of public review by any interested party.

AVAILABLE CONSULTATION

Planners are available for consultation prior to submitting this application. Please email zoning@slcgov.com if you have any questions regarding the requirements of this application.

WHERE TO FILE THE COMPLETE APPLICATION

Apply online through the [Citizen Access Portal](#). There is a [step-by-step guide](#) to learn how to submit online.

REQUIRED FEE

Filing fee of **\$775** plus **\$121** per acre in excess of (1) acre.
Plus additional fee for required public notices will be assessed after application is submitted.

SIGNATURE

If applicable, a notarized statement of consent authorizing applicant to act as an agent will be required.

Signature of Owner or Agent:	Date:
------------------------------	-------

SUBMITTAL REQUIREMENTS

Staff Review

☐☐

1. Project Description

Description of your proposed use and existing use (please attach additional sheet/s)

☐☐

2. Planned Development Information.

Provide the following written and graphic information (please attach additional sheet/s):

- a. Demonstrate how your project meets the purpose and objectives of a planned development as stated in [21A.55.010](#) of the Planned Development ordinance;
- b. Demonstrate how your project meets the Standards for Planned Developments as stated in [21A.55.050](#) of the Planned Development ordinance; and
- c. Describe the plan for long term maintenance of all private infrastructure as stated in [21A.55.110](#) of the Planned Development ordinance.

3. Minimum Plan Requirements

☐☐

A digital (PDF) copy of each plan and elevation drawing

4. Site Plan

☐☐

Site plan (see the *Site Plan Requirements flyer* for further details)

5. Elevation Drawing (if applicable)

☐☐

Detailed elevation, sections and profile drawings with dimensions drawn to scale

☐☐

Type of construction and list the primary exterior construction materials

☐☐

Number, size, and type of dwelling units in each building, and the overall dwelling unit density

INCOMPLETE APPLICATIONS WILL NOT BE ACCEPTED

_____ I acknowledge that Salt Lake City requires the items above to be submitted before my application can be processed. I understand that Planning will not accept my application unless all of the following items are included in the submittal package.

Date: March 11, 2022

To: Salt Lake City Planning Division

Re: Euclid Corners Townhomes Planned Development Proposal

The project requests consideration of exceptions from the following zoning standards:

1. Lots without frontage on a public street

- 4 housing units front the existing alley to the north

2. Reduction in the required 25' setback for R-35

- Building 1 is setback 17' from the north property line to allow for another housing unit.

21A.55.010: Purpose Statement:

The project is a proposed 16 unit 3-story townhome development consisting of a mix of two- and three-bedroom units on a vacant 0.54-acre lot. The project includes private underground parking with 1 stall per 2-bedroom unit and 2 stalls per 3-bedroom unit. There is also a pedestrian plaza level above the parking area consisting of walkway paths, seating areas and landscaping. Each townhome unit also includes a covered front porch and private roof top patio area orientated to the public way or street.

C. Housing: The Euclid Corners Townhomes development will provide an affordable housing option and type for the city in line with the owner, NeighborWork's mission, vision and values.

1. At least 20% of the units will be for those with incomes that are below 80% of the area median income.

2. The proposed development will provide a 2 & 3 bedroom townhome housing type in an area that is primarily single-family homes.

21A.55.050: Standards for Planned Developments:

A. Planned Development Objectives: The planned development encourages the efficient use of land and resources by maximizing the number of housing units on a vacant piece of land and utilizing utility services to a site by serving multiple housing units with single utility stubs to the property. The PD also provides below grade

parking in lieu of on-grade and on-street parking and utilizes shared drives, sidewalks and maintenance / trash areas and common areas for multiple residents.

B. Master Plan Compatibility: This development will provide affordable and alternate housing options and is consistent with area master plan for Poplar Grove.

C. Design & Compatibility:

1. The scale of the proposed project and massing of two of the buildings are greater than the average single-family lot and home and the intensity or lot density (16 units on .54 acres) is greater than the average lot density in the area but compatible with area master plan in that the project will provide an affordable and alternate housing option.
2. The orientation of each of the proposed townhome buildings is compatible with the neighborhood as each unit is oriented to the public street and maintains that physical and visual connection to the neighborhood. The proposed exterior building materials are comprised of EIFS, metal panel siding, brick veneer, concrete and glazing all of which are used in residential structures in this neighborhood. These materials are used in manner that creates contrast, visual interest and quantities that reflect a residential scale.
3. Building setbacks along the perimeter of the development:
 - a. Maintain the visual character of the neighborhood by having a good street presence and setbacks in line with some of the structures and homes in the neighborhood.
 - b. The orientation and location along the street frontage provide sufficient space for private amenities including front porches, private balconies, private below-grade garages and semi-private common area in the interior of the development.
 - c. 3 sides of the proposed development face a public street or private alley. The remaining side is adjacent to the side yard of an existing single-family lot. 2 proposed units are oriented with a similar side yard building face and setback from the adjacent property.
 - d. The front of all proposed townhome units are oriented to a public sidewalk and street or alley. The driveway to the private below-grade parking area is off of the alley.
 - e. A dedicated enclosure is provided for the trash and recycling dumpsters. Additional space for maintenance and

utility service is located in the below-grade parking area and in private garages for each unit.

4. The street and alley facing building facades have been design with ground floor transparency in accordance with the TSA zoning requirements and as indicated on the exterior building elevations. Porches, roof top balconies, a colonnade, mixed building materials and façade lines provide visual interest and facilitate pedestrian interaction.
5. A combination of porch sconce lighting, soffit down lighting, ramp and stair lighting, below grade garage lighting and courtyard lighting been designed into the project for safety, visibility and visual interest.
6. A dedicated enclosure constructed of architectural concrete and painted steel gates is provided for the trash and recycling dumpsters. Additional space for maintenance and utility service is located in the below-grade parking area and in private garages for each unit.
7. Parking areas are buffered from adjacent uses by being located below grade so as to not be seen, buffer noise and provide private and secure parking spaces for the residents.

D. Landscaping:

1. There are no existing mature native trees on the property to be preserved and maintained.
2. There is existing landscaping along the west property line belonging to and abutting the adjacent residential lots that will be preserved but will need to be pruned or trimmed to accommodate the new construction.
3. The proposed landscaping will include new trees along the park strip, lawn, shrubs, plants and flowers against the new buildings and landscaping in the courtyard which will not only buffer the development from adjacent streets but enhance the overall property.
4. A proposed landscaping plan has been developed that thoughtfully locates trees, plants and shrubs throughout the development that addresses the scale and open space of the project. The proposed landscaping provides a greater variety of landscaping and greater number of street trees than the adjacent properties.

E. Mobility:

1. The drive access for the development is located on the alley to the north alley minimizing disruption to 1000 West or 200 South. There are a few on-street parking stalls on 200 South

- with the remaining street frontage dedicated to fire lanes with no parking.
2. Site design and safe circulation.
 - a. Pedestrian walkways, stairs and ramps have been designed and provided to accommodate pedestrian circulation through and around the development and convenient access to and from public sidewalks.
 - b. Secure bicycle parking and storage has been provided in the below grade garage area.
 - c. Interior pedestrian walkways do not accommodate bicycle or automobile traffic. Automobile driveways and parking are below grade.
 3. The site, building and courtyard design promotes convenient access to the public sidewalks. Adjacent uses are public streets and private residences and no access other than public sidewalks is provided from the proposed development.
 4. 'No Parking – Fire Lanes' have been provided along 1000 West and 200 South per fire department and fire code requirements.
 5. A dedicated enclosure for trash and recycling dumpsters has been provided on the north side of the property with service vehicle access from the alley.

F. Existing Site Features: The existing property is an undeveloped site and contains no natural or built features that contribute to the character of the neighborhood. There is existing landscaping along the west property that encroaches onto this property and development and will require pruning and trimming to allow for the proposed construction.

G. Utilities: Existing utilities that will serve this development are located in 200 South, 1000 West and the existing Alley and are sized adequately to serve the development per the project's engineers.



Rebuilding Neighborhoods
house by house... block by block!

622 West 500 North, Salt Lake City, Utah 84116

t: 801.539.1590 f: 801.539.1593

TTY/TDD users should call 7-1-1

www.nwsaltlake.org

To: Salt Lake City Planning
From: Maria Garciaz, CEO
NeighborWorks Salt Lake
Subject: Euclid Corner Project
Date: November 1, 2021

Maria Garciaz, M.Ed
Chief Executive Officer

Board of Directors

RESIDENTS

Alama Uluave
Adebimpe Deji-Olatunde
Bryce Garner
Christy Sapp
Jose Organista
Scot Barraclough
Wayne Murakami

BUSINESS

Autumn Johnson
Morgan Stanley Bank
Charlie Knadler
EnerBank
Curtis Mansfield
Rocky Mountain Power
Jonathan Oetli
Washington Federal Bank
Rob Pedersen
Nelnet Bank
Sean McKenna
Goldman Sachs Bank
Amber Gracani
US Bank

CITY

Melinda Greenwood
Murray City
Ben Kolendar
Salt Lake City

EX-OFFICIO

James Wood
University of Utah

Dear Planning,

NeighborWorks Salt Lake is a non-profit organization established in 1977 in partnership with Salt Lake City to revitalize neighborhoods experiencing blight and decline. Our vision is neighborhood revitalization through partnership and collaboration. Our mission is to revitalize neighborhoods house by house, block by block, neighbor by neighbor. We build on the strengths of the neighborhoods and work in partnership with residents, government, and local business to build and sustain neighborhoods of choice.

We are currently in the pre-development phase of Euclid Corner Project that is located at 172 South-192 South/1000 West & 1012 West -1020 West-200 South in Salt Lake City. This proposed project is a 16-unit townhome development that consists of (8) 3-bedroom and (8) 2-bedroom units. This development will increase net housing for Salt Lake City and develop a once blighted property razed many years ago.

Our intent is to create mixed income housing which aligns with the Transit Station Areas Development Guidelines for new developments. At minimum, 20% of the proposed sixteen dwelling units will be reserved for affordable housing to those with 80% or less of the median household income for Salt Lake City.

As you know, we are in unprecedented times with the high cost of housing that we are currently experiencing in Utah. As a result, NeighborWorks is in process of finalizing our NeighborWorks Community Land Trust (NWCLT) structure. Per our mission to create mixed income housing, we envision Euclid Corner development affordable units will be designated under our NWCLT.

In summary, NeighborWorks goal is to continue to develop and build mixed-income housing in Salt Lake City. Our Euclid Corner project will be developed to conform to the Transit Station Development Guideline objectives. It is our intent, at minimum, 20% of our proposed total dwelling units will be restricted for affordable housing to those with 80% or less of the median household income of the city.



21A.55.110 – Maintenance of Private Infrastructure

- A. See attached Level 1 Reserve Study created by Complex Solutions LTD.
- B. NeighborWorks Salt Lake agrees that this 60-year initial estimate disclosure will be incorporated into the planned Euclid Corner Project, located at 1002 West, 200 South, Salt Lake City, Utah 84116. This will ensure that owners and future owners will receive adequate disclosure of potential infrastructure maintenance and replacement costs.
1. NeighborWorks Salt Lake agrees that the initial Level 1 60-year Reserve Study will be both referenced and recorded on the plat for this planned development. This study was prepared and does reflect within its language for six (6) increments of ten years each.
 2. NeighborWorks Salt Lake agrees that the recorded plat shall contain a statement entitled “notice to purchasers” disclosing the infrastructure is privately owned and that the maintenance, repair, replacement, and operation of the infrastructure is the responsibility of the property owners and will not be assumed by the city.
 3. NeighborWorks Salt Lake agrees that the attached 60-year initial Reserve Study estimate disclosure shall be specifically and separately disclosed to the purchaser of any property in the planned development, upon initial purchase and also upon all future purchases for the duration of the sixty (60) year period.
- C. Yearly Maintenance Statements: NeighborWorks agrees that we will request that the entity responsible for the operation and maintenance of the infrastructure shall, at least once each calendar year, notify all property owners in the planned development of the estimated yearly expenditures for maintenance, repair, operation, or replacement of infrastructure, and at least once each calendar year shall notify all property owners of the actual expenditures incurred, and shall specify the reasons(s) for any variance between the estimated expenditures and the actual expenditures.



Design Review

SALT LAKE CITY PLANNING

OFFICE USE ONLY

Project #:	Received By:	Date Received:	Zoning:
------------	--------------	----------------	---------

Project Name:

PLEASE PROVIDE THE FOLLOWING INFORMATION

Request:			
Address of Subject Property:			
Name of Applicant:		Phone:	
Address of Applicant: 9133 South Monroe Plaza Way, Suite D, Sandy, Utah 84070			
E-mail of Applicant:		Cell/Fax:	
Applicant's Interest in Subject Property:			
Owner	Contractor	Architect	Other:
Name of Property Owner (if different from applicant):			
E-mail of Property Owner:		Phone:	

Please note that additional information may be required by the project planner to ensure adequate information is provided for staff analysis. All information required for staff analysis will be copied and made public, including professional architectural or engineering drawings, for the purposes of public review by any interested party.

AVAILABLE CONSULTATION

Planners are available for consultation prior to submitting this application. Please email zoning@slcgov.com if you have any questions regarding the requirements of this application.

WHERE TO FILE THE COMPLETE APPLICATION

Apply online through the [Citizen Access Portal](#). There is a [step-by-step guide](#) to learn how to submit online

REQUIRED FEE

Filing fee of **\$806** plus **\$121** per acre in excess of (1) acre.

*Additional fee for required public notices will be assessed after the application is submitted.

SIGNATURE

If applicable, a notarized statement of consent authorizing applicant to act as an agent will be required.

Signature of Owner or Agent:	Date:
------------------------------	-------

Date: February 17, 2022

To: Salt Lake City Planning Department

Re: Euclid Corners Townhomes Design Review Application

1. Project Description:

- a. The project is a proposed 16 unit 3-story townhome development consisting of a mix of two- and three-bedroom units on a vacant and undeveloped 0.54-acre lot (**a density of 29.6 units per acre**). The two-bedroom units are 1,460 square feet each and the three-bedroom units are 2,176 square feet each. The project includes private underground parking with 1 stall per 2-bedroom unit and 2 stalls per 3-bedroom unit. There is also a pedestrian plaza level above the parking area consisting of walkway paths, seating areas and landscaping. Each townhome unit also includes a covered front porch and private roof top patio area orientated to the public way or street.
- b. The proposed structure is constructed of wood-framing over a concrete parking garage deck. The design includes a combination of architectural styles with a modern approach to the overall design and form of the buildings while incorporating traditional architectural elements and forms including shed roofs, trellis features, a colonnade, railings and covered patio areas. Modern materials such as **metal wall and roof panels and fiber cement siding** are combined with traditional materials of **brick, pre-cast concrete and stucco**. The townhome units are arranged such that each unit has an orientation, presence and relationship to the city street as well as access and a rear orientation to the shared plaza deck. Each unit provides the occupant with public, private and semi-private opportunities for interaction.
- c. In addition to the use of design and architectural features previously noted, the project includes a shared outdoor plaza area connected to the rear of each unit. This feature was made possible by locating the parking area below grade and under each unit. The outdoor plaza area includes walkways, seating and landscaping areas creating a pleasing environment for the residents and providing opportunities for private and semi-private interaction. The front of each unit fronts a public street or way with a covered porch area connecting the residents to the public or city activity. Each unit also consists of a private exterior roof top patio area that fronts the public street or way with visual access to the city street.

SUBMITTAL REQUIREMENTS

Staff Review

1. Project Description (please attach additional sheet)

- | | | |
|--------------------------|--------------------------|---|
| <input type="checkbox"/> | <input type="checkbox"/> | Written description of your proposal. |
| <input type="checkbox"/> | <input type="checkbox"/> | Type of construction and list the primary exterior construction materials. |
| <input type="checkbox"/> | <input type="checkbox"/> | Number, size, and type of dwelling units in each building, and the overall dwelling unit density. |

2. Minimum Plan Requirements

- | | | |
|--------------------------|--------------------------|--|
| <input type="checkbox"/> | <input type="checkbox"/> | A digital (PDF) copy of each plan and elevation drawing. |
|--------------------------|--------------------------|--|

3. Site Plan

- | | | |
|--------------------------|--------------------------|---|
| <input type="checkbox"/> | <input type="checkbox"/> | Site plan (see Site Plan Requirements flyer for further details). |
|--------------------------|--------------------------|---|

4. Elevation Drawing

- | | | |
|--------------------------|--------------------------|---|
| <input type="checkbox"/> | <input type="checkbox"/> | Detailed elevation, sections and profile drawings with dimensions drawn to scale. |
|--------------------------|--------------------------|---|

5. Additional Requirements

- | | | |
|--------------------------|--------------------------|--|
| <input type="checkbox"/> | <input type="checkbox"/> | All of the application information required for site plan review as identified in Section 21A.58 of this title. |
| <input type="checkbox"/> | <input type="checkbox"/> | Photos showing the facades of adjacent development, trees on the site, general streetscape character, and views to and from the site. |
| <input type="checkbox"/> | <input type="checkbox"/> | Demonstration of compliance with the purpose of the individual zoning district in written narrative and graphic images. |
| <input type="checkbox"/> | <input type="checkbox"/> | Demonstration of compliance with the purpose of the applicable design standards of the individual zoning district in written narrative, graphic images, and relevant calculations. |
| <input type="checkbox"/> | <input type="checkbox"/> | Demonstration of compliance with the applicable design review objectives (Section 21A.59.050) in written narrative, graphics, images, and relevant calculations. |

INCOMPLETE APPLICATIONS WILL NOT BE ACCEPTED

I acknowledge that Salt Lake City requires the items above to be submitted before my application can be processed. I understand that Planning will not accept my application unless all of the following items are included in the submittal package.

2. Minimum Plan Requirements:

- a. See attached plan and elevation drawings.

3. Site Plan:

- a. See attached site plan drawing.

4. Elevation Drawing:

- a. See attached building elevation and section drawings.

5. Additional Requirements:

- a. See attached civil and site plan drawings for site plan review.
- b. See attached sheet A213 for photos of the adjacent development and views to and from the existing site.
- c. See TSA application for compliance with the purpose of the individual zoning district.
- d. See Planned Development application for compliance with the applicable design standards of the individual zoning district (TSA).
- e. Compliance with the applicable design review objectives:

A. Any new development shall comply with the intent of the purpose statement of the zoning district and specific design regulations found within the zoning district in which the project is located as well as the City's adopted "urban design element" and adopted master plan policies and design guidelines governing the specific area of the proposed development.

B. Development shall be primarily oriented to the sidewalk, not an interior courtyard or parking lot.

- 1. Primary entrances shall face the public sidewalk (secondary entrances can face a parking lot).
- 2. Building(s) shall be sited close to the public sidewalk, following and responding to the desired development patterns of the neighborhood.
- 3. Parking shall be located within, behind, or to the side of buildings.

(Complies – All primary entrances are oriented to the public sidewalk, buildings are located as close to the public sidewalk as the zoning ordinance allows and parking is located behind the buildings in a partially underground parking structure, see attached site plan and exterior elevations)

C. Building facades shall include detailing and glass in sufficient quantities to facilitate pedestrian interest and interaction.

1. Locate active ground floor uses at or near the public sidewalk.
2. Maximize transparency of ground floor facades.
3. Use or reinterpret traditional storefront elements like sign bands, clerestory glazing, articulation, and architectural detail at window transitions.
4. Locate outdoor dining patios, courtyards, plazas, habitable landscaped yards, and open spaces so that they have a direct visual connection to the street and outdoor spaces.

(Complies – All primary entrances to townhomes include porches facing and near the public sidewalk and street, entrances are design and articulated appropriate to residential use and ground floor glass complies with ground level glass area requirements, see attached exterior elevations including ground level glass calculations)

D. Large building masses shall be divided into heights and sizes that relate to human scale.

1. Relate building scale and massing to the size and scale of existing and anticipated buildings, such as alignments with established cornice heights, building massing, step-backs and vertical emphasis.
2. Modulate the design of a larger building using a series of vertical or horizontal emphases to equate with the scale (heights and widths) of the buildings in the context and reduce the visual width or height.
3. Include secondary elements such as balconies, porches, vertical bays, belt courses, fenestration and window reveals.
4. Reflect the scale and solid-to-void ratio of windows and doors of the established character of the neighborhood or that which is desired in the master plan.

(Complies – The project is a residential townhome development, which through the use of vertical, horizontal and material articulation, residential sized windows and doors, balconies and porches naturally relate to human scale and existing residential nature of the neighborhood – See attached exterior elevations and building sections)

E. Building facades that exceed a combined contiguous building length of two hundred feet (200') shall include:

1. Changes in vertical plane (breaks in facade);
2. Material changes; and
3. Massing changes.

(Not applicable – No building façade exceeds 200’ but project does include vertical, horizontal and material changes in the facades)

F. If provided, privately-owned public spaces shall include at least three (3) of the six (6) following elements:

1. Sitting space of at least one sitting space for each two hundred fifty (250) square feet shall be included in the plaza. Seating shall be a minimum of sixteen inches (16") in height and thirty inches (30") in width. Ledge benches shall have a minimum depth of thirty inches (30");
2. A mixture of areas that provide seasonal shade;
3. Trees in proportion to the space at a minimum of one tree per eight hundred (800) square feet, at least two inch (2") caliper when planted;
4. Water features or public art;
5. Outdoor dining areas; and
6. Other amenities not listed above that provide a public benefit.

**(Complies – The project contains sitting space in the plaza area that exceeds the required, a mixture of trees that provide seasonal shade and at a proportion greater than 1 tree per 800 square feet (11 trees over 5,880 square feet of plaza area provided)
See attached site plan and landscape plan)**

G. Building height shall be modified to relate to human scale and minimize negative impacts. In downtown and in the CSHBD Sugar House Business District, building height shall contribute to a distinctive City skyline.

1. Human scale:

a. Utilize setbacks to design a building that relate to the height and scale of adjacent and nearby buildings, or where identified, goals for future scale defined in adopted master plans.

b. For buildings more than three (3) stories or buildings with vertical mixed use, compose the design of a building with distinct base, middle and top sections to reduce the sense of apparent height.

2. Negative impacts:

a. Modulate taller buildings vertically and horizontally so that it steps up or down to its neighbors.

b. Minimize shadow impacts of building height on the public realm and semi-public spaces by varying building massing. Demonstrate impact from shadows due to building height for the portions of the building that are subject to the request for additional height.

c. Modify tall buildings to minimize wind impacts on public and private spaces, such as the inclusion of a wind break above the first level of the building.

3. Cornices and rooflines:
 - a. Cohesiveness: Shape and define rooflines to be cohesive with the building's overall form and composition.
 - b. Complement Surrounding Buildings: Include roof forms that complement the rooflines of surrounding buildings.
 - c. Green Roof And Roof Deck: Include a green roof and/or accessible roof deck to support a more visually compelling roof landscape and reduce solar gain, air pollution, and the amount of water entering the stormwater system.

(Complies – The project is a residential townhome development, which through the use of vertical, horizontal and material articulation, residential sized windows and doors, balconies and porches naturally relate to human scale and existing residential nature of the neighborhood. The buildings step back at the 3rd level, any shadows from the buildings would be projected on the alley to the north of the site or to 1000 west street when sun sets in the west, the interior plaza is shielded from the wind by the buildings, the sloped shed roofs relate to the residential roof forms in the neighborhood and all townhomes have a 3rd level roof deck – See attached site and floor plans, exterior elevations and building sections)

H. Parking and on site circulation shall be provided with an emphasis on making safe pedestrian connections to the sidewalk, transit facilities, or midblock walkway.

I. Waste and recycling containers, mechanical equipment, storage areas, and loading docks shall be fully screened from public view and shall incorporate building materials and detailing compatible with the building being served. Service uses shall be set back from the front line of building or located within the structure. (See subsection 21A.37.050K of this title.)

J. Signage shall emphasize the pedestrian/mass transit orientation.

1. Define specific spaces for signage that are integral to building design, such as commercial sign bands framed by a material change, columns for blade signs, or other clearly articulated band on the face of the building.

2. Coordinate signage locations with appropriate lighting, awnings, and other projections.

3. Coordinate sign location with landscaping to avoid conflicts.

(Complies – Parking for the development is located in a partially underground structure, there is no vehicular access to the pedestrian walkways, trash containers are located in a gated enclosure on the north side, outdoor mechanical equipment is located on the 3rd story balconies, there is no signage other

than unit address, – See attached site plan and exterior elevations and building sections)

K. Lighting shall support pedestrian comfort and safety, neighborhood image, and dark sky goals.

1. Provide street lights as indicated in the Salt Lake City Lighting Master Plan.

2. Outdoor lighting should be designed for low-level illumination and to minimize glare and light trespass onto adjacent properties and uplighting directly to the sky.

3. Coordinate lighting with architecture, signage, and pedestrian circulation to accentuate significant building features, improve sign legibility, and support pedestrian comfort and safety.

(Complies –See attached courtyard level site and garage level lighting plans including site photometric plan)

L. Streetscape improvements shall be provided as follows:

1. One street tree chosen from the street tree list consistent with the City's urban forestry guidelines and with the approval of the City's Urban Forester shall be placed for each thirty feet (30') of property frontage on a street. Existing street trees removed as the result of a development project shall be replaced by the developer with trees approved by the City's Urban Forester.

2. Hardscape (paving material) shall be utilized to differentiate privately-owned public spaces from public spaces. Hardscape for public sidewalks shall follow applicable design standards. Permitted materials for privately-owned public spaces shall meet the following standards:

a. Use materials that are durable (withstand wear, pressure, damage), require a minimum of maintenance, and are easily repairable or replaceable should damage or defacement occur.

b. Where practical, as in lower-traffic areas, use materials that allow rainwater to infiltrate into the ground and recharge the water table.

c. Limit contribution to urban heat island effect by limiting use of dark materials and incorporating materials with a high Solar- Reflective Index (SRI).

d. Utilize materials and designs that have an identifiable relationship to the character of the site, the neighborhood, or Salt Lake City.

e. Use materials (like textured ground surfaces) and features (like ramps and seating at key resting points) to support access and comfort for people of all abilities.

f. Asphalt shall be limited to vehicle drive aisles. (Ord. 14-19, 2019)



CARPENTER
STRINGHAM
ARCHITECTS LLC

(Complies – The landscape plan shows the addition of street trees as there are no existing street trees, public streets are paved with asphalt, private drive to underground parking and walkways are concrete paving (no asphalt used other than the public streets), the use of concrete walkways (durable material) and landscaping differentiate the outdoor plaza area and also provide for access through the site with seating areas, the extensive landscaping limits contribution to the heat island effect, the use of brick, siding and EIFS relate to the residential character of the neighborhood– See attached site plan, landscape plan, exterior elevations and building sections)

9133 S MONROE PLAZA WAY
SUITE D
SANDY UT 84070
TEL - 801.890.1092

ARCHITECTURE
PLANNING
INTERIORS

www.carpenterstringham.com

csa@carpenterstringham.com



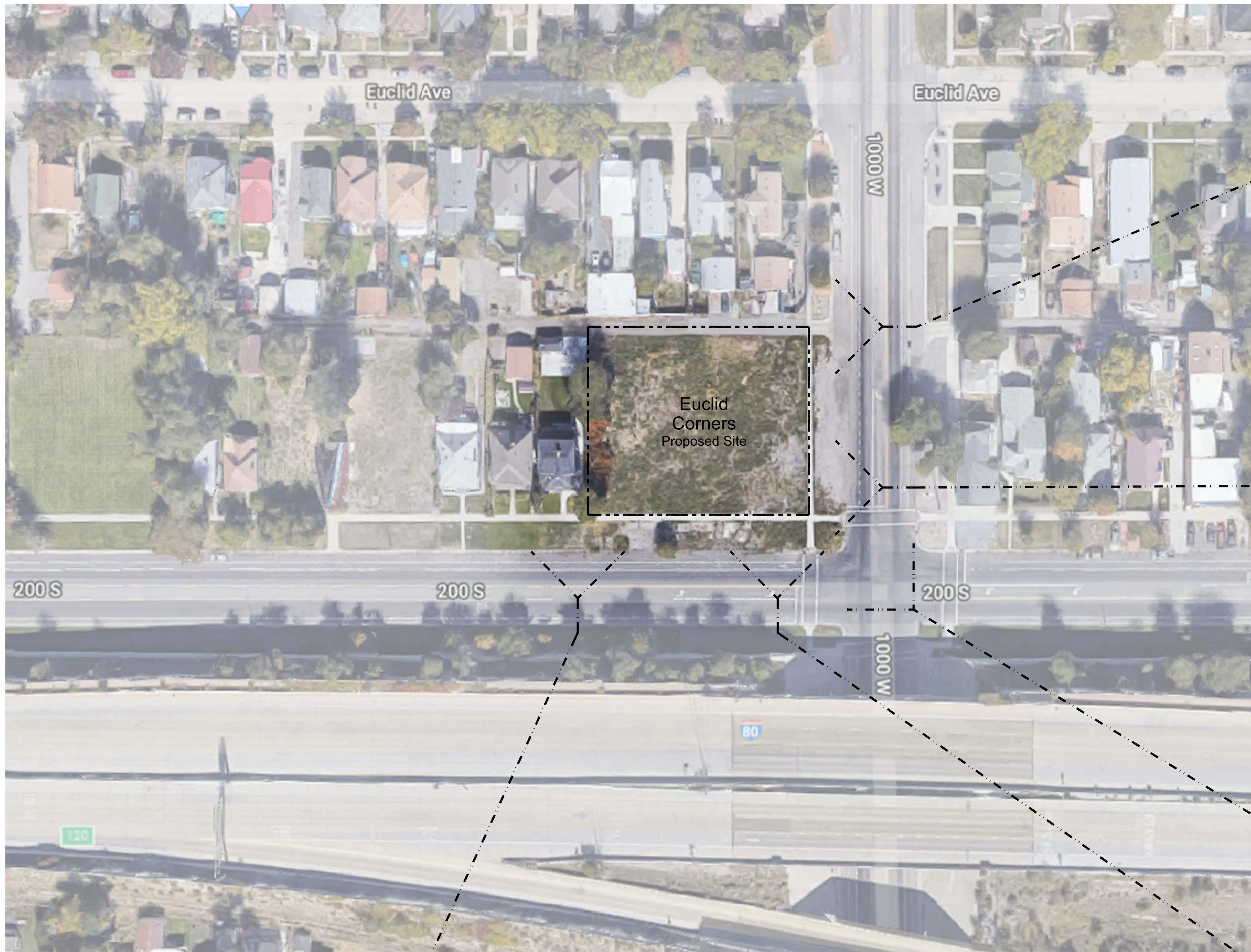
VIEW ACROSS 1000 WEST

SCALE: 4/11" = 1'-0"



VIEW ACROSS 200 SOUTH

SCALE: 4/11" = 1'-0"



AERIAL VIEW OF SITE

SCALE: 4/11" = 1'-0"



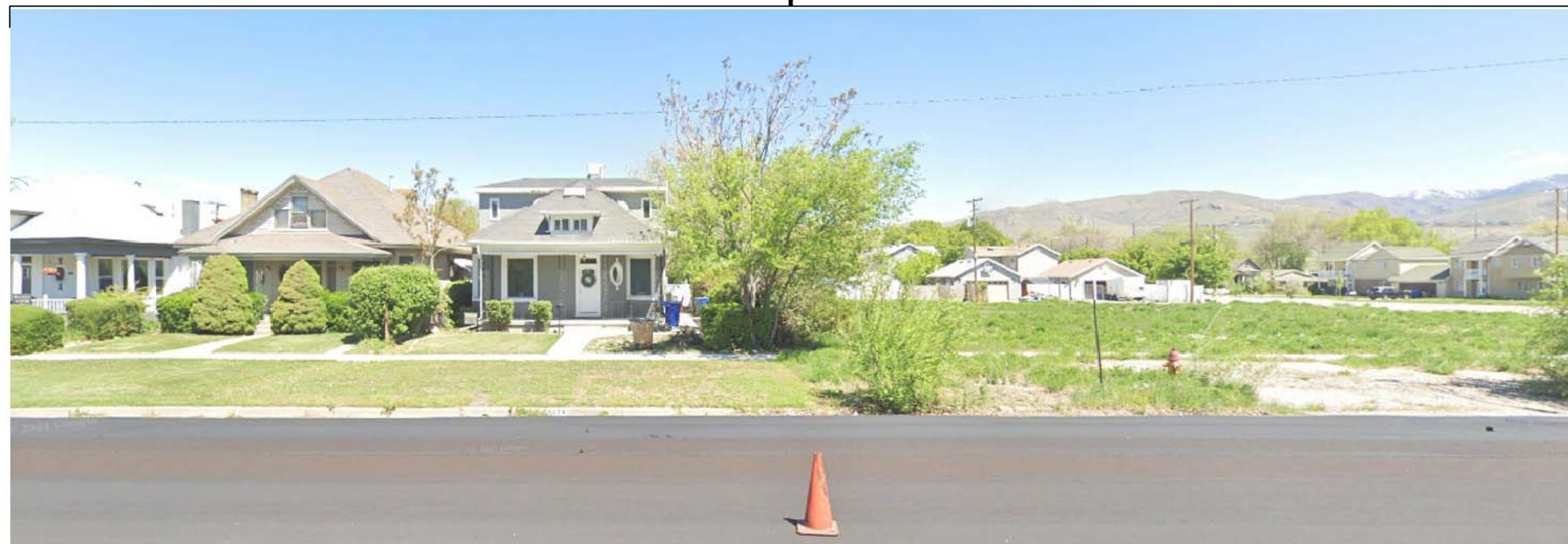
1000 WEST

SCALE: 4/11" = 1'-0"



200 SOUTH 1000 WEST INTERSECTION

SCALE: 4/11" = 1'-0"



200 SOUTH

SCALE: 4/11" = 1'-0"



\\CS\SERVERS\Carpenter Stringham\Projects\16-033-01 Euclid Corners Townhomes - SLOCDS\A001.dwg, April 15, 2022 - 4:30 PM



200 SOUTH

1000 WEST

A

SITE PLAN - GARAGE LEVEL

SCALE: 1/8" = 1'-0"



SITE INFORMATION

JURISDICTION	SALT LAKE CITY, UTAH
ZONING CODE	TSA-UN (URBAN NEIGHBORHOOD TRANSIT STATION)
LOT SIZE	23,311 SQ. FT. (.54 ACRES)
CONSTRUCTION	V-B
EXTERIOR VENEER	BRICK, CONCRETE, METAL, FIBER CEMENT SIDING
BUILDING FOOTPRINT	10,600 SQ. FT.
BUILDING 1	4,200 SQ. FT.
BUILDING 2	3,800 SQ. FT.
BUILDING 3	1,400 SQ. FT.
BUILDING 4	1,400 SQ. FT.
PLAZA FOOTPRINT	5,880 SQ. FT.
LOT COVERAGE	45 %
LOT COVERAGE	25 %
BUILDING UNITS	(4) 2-BEDROOM, (3) 3-BEDROOM
BUILDING 1	(4) 2-BEDROOM, (3) 3-BEDROOM
BUILDING 2	(2) 2-BEDROOM, (3) 3-BEDROOM
BUILDING 3	(1) 2-BEDROOM, (1) 3-BEDROOM
BUILDING 4	(1) 2-BEDROOM, (1) 3-BEDROOM
TOTAL UNITS (16 TOTAL)	(8) 2-BEDROOM, (8) 3-BEDROOM
LOT DENSITY	29.6 UNITS PER ACRE
REQUIRED OFF STREET PARKING	10 STALLS
3-BEDROOM w/ LIVE/WORK SPACE (1 1/4 PER UNIT MIN.)	12 STALLS
2-BEDROOM UNIT (1 1/2 PER UNIT MAX.)	8 STALLS
TOTAL STALL REQUIRED (MIN.)	12 STALLS
TOTAL STALL REQUIRED (MAX.)	18 STALLS
GARAGE PARKING (BELOW GRADE)	24 STALLS
MISCELLANEOUS PARKING REQUIREMENTS	2 SPACES REQUIRED
BIKE PARKING (5% OF MIN. VEHICULAR PARKING)	6 SPACES PROVIDED
ELECTRICAL VEHICLE PARKING	16 STALLS
STREET PARKING	0 STALLS
2-BEDROOM UNITS	2,361 SQ. FT.
1-CAR GARAGE	528 SQ. FT.
LIVING SPACE	1,460 SQ. FT.
EXTERIOR PATIO/TERRACE SPACE	328 SQ. FT.
3-BEDROOM UNITS	3,053 SQ. FT.
2-CAR GARAGE	457 SQ. FT.
LIVING SPACE	2,176 SQ. FT.
EXTERIOR PATIO/TERRACE SPACE	420 SQ. FT.
DEVELOPMENT SCORE:	
INTENSITY/DENSITY: MORE THAN 25 DWELLING UNITS PER ACRE	12
360 DEGREE ARCHITECTURE	20
BUILDING MATERIALS	20
ROOFTOP DESIGN & USE (COMMON SPACE)	6
ROOFTOP DESIGN & USE (SLOPING PLANES / OVERHANGS)	5
EYES ON THE STREET	15
PUBLIC SPACES AND PLAZAS	15
BICYCLE AMENITIES	3
STRUCTURED PARKING	25
ALTERNATIVE VEHICLE PARKING	12
TOTAL DEVELOPMENT SCORE:	133

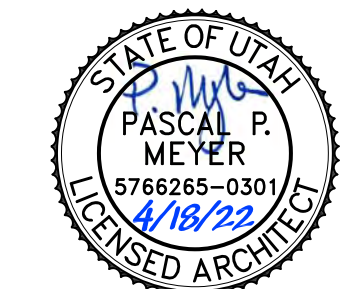
PROJECT
A NEW TOWNHOME DEVELOPMENT FOR:

EUCLID CORNERS

NEIGHBOR WORKS

1012 W. - 1020 W. 200 S. & 172 S. - 192 S. 1000 W.

SALT LAKE CITY, UTAH 84104



SHEET TITLE

Site Plan
Garage Level

REVISIONS

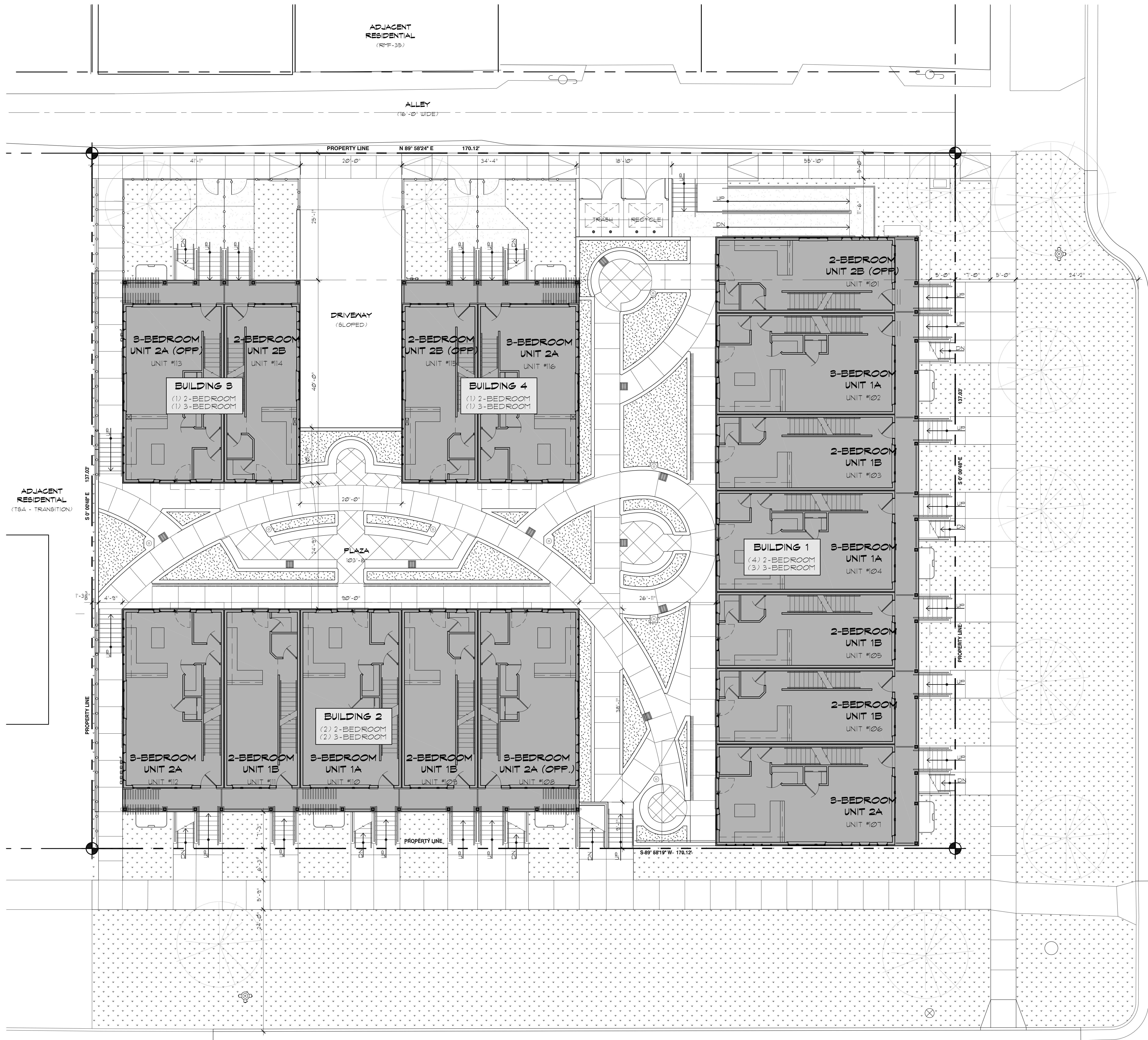
PROJECT: 16-033.01
DATE: April 18, 2022
SCALE: As Shown
DRAWN BY: sj
CHECKED: ppm

SHEET
A001

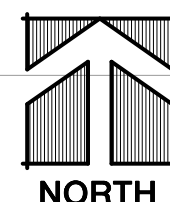
© CARPENTER STRINGHAM ARCHITECTS, LLC 2022

ARCHITECTURE
PLANNING
INTERIORS
**CARPENTER STRINGHAM
ARCHITECTS LLC**
9133 S MONROE PLAZA WAY SUITE D
SANDY UT 84070
TEL: 801-890-1092
www.carpenterstringham.com

\\CS\SERVERS\Carpeters\Stringham\Projects\16-033.01 Euclid Corners Townhomes - SLOCDS\A002.dwg, April 15, 2022 - 4:30 PM



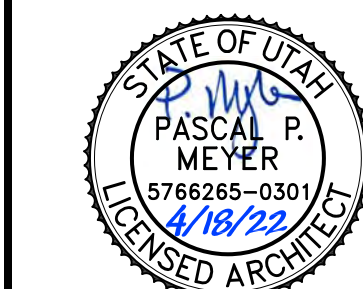
A PROPOSED SITE PLAN - PLAZA LEVEL
SCALE: 1/8" = 1'-0"



SITE INFORMATION

JURISDICTION	SALT LAKE CITY, UTAH
ZONING CODE	TSA-UN (URBAN NEIGHBORHOOD TRANSIT STATION)
LOT SIZE	23,311 SQ. FT. (.54 ACRES)
CONSTRUCTION	V-B
EXTERIOR VENEER	BRICK, CONCRETE, METAL, FIBER CEMENT SIDING
BUILDING FOOTPRINT	10,600 SQ. FT.
BUILDING 1	4,200 SQ. FT.
BUILDING 2	3,800 SQ. FT.
BUILDING 3	1,400 SQ. FT.
BUILDING 4	1,400 SQ. FT.
PLAZA FOOTPRINT	5,880 SQ. FT.
LOT COVERAGE	45 %
LOT COVERAGE	25 %
BUILDING UNITS	(4) 2-BEDROOM, (3) 3-BEDROOM
BUILDING 1	(2) 2-BEDROOM, (3) 3-BEDROOM
BUILDING 2	(1) 2-BEDROOM, (1) 3-BEDROOM
BUILDING 3	(1) 2-BEDROOM, (1) 3-BEDROOM
BUILDING 4	(1) 2-BEDROOM, (1) 3-BEDROOM
TOTAL UNITS (16 TOTAL)	(8) 2-BEDROOM, (8) 3-BEDROOM
LOT DENSITY	29.6 UNITS PER ACRE
REQUIRED OFF STREET PARKING	10 STALLS
3-BEDROOM w/ LIVE/WORK SPACE (1 1/4 PER UNIT MIN.)	12 STALLS
2-BEDROOM UNIT (1 1/2 PER UNIT MAX.)	8 STALLS
TOTAL STALL REQUIRED (MIN.)	12 STALLS
TOTAL STALL REQUIRED (MAX.)	18 STALLS
GARAGE PARKING (BELOW GRADE)	24 STALLS
MISCELLANEOUS PARKING REQUIREMENTS	2 SPACES REQUIRED
BIKE PARKING (5% OF MIN. VEHICULAR PARKING)	6 SPACES PROVIDED
ELECTRICAL VEHICLE PARKING	16 STALLS
STREET PARKING	0 STALLS
2-BEDROOM UNITS	2,361 SQ. FT.
1-CAR GARAGE	528 SQ. FT.
LIVING SPACE	1,460 SQ. FT.
EXTERIOR PATIO/TERRACE SPACE	328 SQ. FT.
3-BEDROOM UNITS	3,053 SQ. FT.
2-CAR GARAGE	457 SQ. FT.
LIVING SPACE	2,176 SQ. FT.
EXTERIOR PATIO/TERRACE SPACE	420 SQ. FT.
DEVELOPMENT SCORE:	
INTENSITY/DENSITY: MORE THAN 25 DWELLING UNITS PER ACRE	12
360 DEGREE ARCHITECTURE:	20
BUILDING MATERIALS:	20
ROOFTOP DESIGN & USE (COMMON SPACE):	6
ROOFTOP DESIGN & USE (SLOPING PLANES / OVERHANGS):	5
EYES ON THE STREET:	15
PUBLIC SPACES AND PLAZAS:	15
BICYCLE AMENITIES:	3
STRUCTURED PARKING:	25
ALTERNATIVE VEHICLE PARKING:	12
TOTAL DEVELOPMENT SCORE:	133

PROJECT
A NEW TOWNHOME DEVELOPMENT FOR:
EUCLID CORNERS
NEIGHBOR WORKS
1012 W. - 1020 W. 200 S. & 172 S. - 192 S. 1000 W.
SALT LAKE CITY, UTAH 84104



SHEET TITLE
**Site Plan
Plaza Level**

REVISIONS

PROJECT: 16-033.01
DATE: April 18, 2022
SCALE: As Shown
DRAWN BY: sj
CHECKED: ppm

SHEET
A002

ARCHITECTURE
PLANNING
INTERIORS
**CARPENTER STRINGHAM
ARCHITECTS LLC**
9133 S MONROE PLAZA WAY SUITE D
SANDY UT 84070
TEL: 801-890-1092
www.carpenterstringham.com



A 1000 WEST ELEVATION (EAST ELEVATION)

SCALE: 4/11" = 1'-0"



B PLAZA ELEVATION OF 1000 WEST BUILDING (WEST ELEVATION)

SCALE: 4/11" = 1'-0"



C ALLEY ELEVATION (NORTH ELEVATION)

SCALE: 4/11" = 1'-0"

EXTERIOR FINISH KEY	
E.I.F.S. NOT USED 1	EXPOSED CONCRETE EXPOSED ARCHITECTURAL FINISH GRADE CONCRETE • COLOR: NATURAL GRAY 8
E.I.F.S. TRIM NOT USED 2	METAL ROOFING STANDING SEAM METAL ROOF • MBCI CRAFTSMAN SERIES - SMALL BATTEN (ROOF) • COLOR: DARK BRONZE 9
METAL PANEL SIDING MBCI METAL WALL PANEL: MASTERLINE 16" OR EQUAL SET HORIZONTALLY • COLOR: SLATE GRAY 3	METAL FACIA / SOFFIT PRE-FINISHED METAL FACIA / SOFFIT (VENTED) • COLOR: DARK BRONZE 10
BRICK VENEER THIN BRICK VENEER (MODULAR SIZE) • INTERSTATE BRICK • COLOR: WALNUT 4	METAL PARAPET CAP 4" PRE-FINISHED METAL CAP FLASHING • MBCI (OR EQUAL) • COLOR: SLATE GRAY 11
FIBER CEMENT SIDING NICHHA - VINTAGE WOOD - SET HORIZONTALLY • COLOR: ASH (EPC764F) 5	METAL FLASHING PRE-FINISHED METAL FLASHING w/ DRIP • MBCI (OR EQUAL) • COLOR: SLATE GRAY 12
FIBER CEMENT SIDING NICHHA - VINTAGE WOOD - SET VERTICALLY • COLOR: ASH (EPC764F) 6	TRELLIS / COLUMNS WOOD w/ PAINTED FINISH • SHERWIN WILLIAMS EPOXY PAINT FINISH • COLOR: SNOWBOUND (SW 7004) 13
SILL / TRIM PRE-CAST CONCRETE WINDOW TRIM / SILL / WATER TABLE • COLOR: NATURAL GRAY 7	GUARDRAIL / HANDRAIL STEEL HAND RAIL GUARD RAIL - PAINTED • COLOR: WHITE 14
NOTE 1: PAINT HOLLOW METAL DOORS & FRAMES TO MATCH ADJACENT METAL WALL FINISH COLORS	

GROUND LEVEL MATERIALS	
NOTE: MEASUREMENTS TAKEN FROM GRADE TO MAIN FLOOR CEILING HEIGHT	
EAST ELEVATION (1000 WEST)	
TOTAL AREA	1,549 SQ. FT.
GLASS & TRANSPARENCY	542 SQ. FT. (35.0%)
NET AREA	1,007 SQ. FT.
DURABLE MATERIAL	1,007 SQ. FT. (100.0%)
BRICK	270 SQ. FT. (26.8%)
FIBER CEMENT SIDING	353 SQ. FT. (35.0%)
ACCENT MATERIALS	0 SQ. FT. (0.0%)
NORTH ELEVATION (ALLEY)	
TOTAL AREA	1,666 SQ. FT.
GLASS & TRANSPARENCY	366 SQ. FT. (22.0%)
NET AREA	1,299 SQ. FT.
DURABLE MATERIAL	1,299 SQ. FT. (100.0%)
BRICK	608 SQ. FT. (46.8%)
FIBER CEMENT SIDING	328 SQ. FT. (25.2%)
ACCENT MATERIALS	0 SQ. FT. (0.0%)

GROUND LEVEL GLASS	
NOTE: MEASUREMENTS TAKEN FROM 3'-0" ABOVE GRADE TO 8'-0" ABOVE GRADE	
EAST ELEVATION (1000 WEST)	
TOTAL AREA (BUILDING #1)	600 SQ. FT.
GLASS & TRANSPARENCY	329 SQ. FT. (54.8%)
OTHER EXTERIOR FINISHES	361 SQ. FT. (60.2%)
NORTH ELEVATIONS (ALLEY)	
TOTAL AREA	706 SQ. FT.
GLASS & TRANSPARENCY	152 SQ. FT. (21.5%)
OTHER EXTERIOR FINISHES	554 SQ. FT. (78.5%)

UPPER LEVEL MATERIALS	
NOTE: MEASUREMENTS TAKEN FROM ABOVE MAIN FLOOR CEILING TO ROOF SOFFIT	
EAST ELEVATION (1000 WEST)	
TOTAL AREA	2,559 SQ. FT.
GLASS & TRANSPARENCY	577 SQ. FT. (22.6%)
NET AREA	1,982 SQ. FT.
DURABLE MATERIAL	1,427 SQ. FT. (72.0%)
BRICK	303 SQ. FT. (15.3%)
FIBER CEMENT SIDING	555 SQ. FT. (28.0%)
ACCENT MATERIALS	0 SQ. FT. (0.0%)
NORTH ELEVATION (ALLEY)	
TOTAL AREA	1,487 SQ. FT.
GLASS & TRANSPARENCY	204 SQ. FT. (13.7%)
NET AREA	1,283 SQ. FT.
DURABLE MATERIAL	1,250 (70.5%)
BRICK	159 SQ. FT. (12.4%)
FIBER CEMENT SIDING	415 SQ. FT. (32.4%)
ACCENT MATERIALS	0 SQ. FT. (0.0%)



A PLAZA ELEVATION OF 200 SOUTH & ALLEY BUILDINGS (EAST ELEVATION)
SCALE: 4/11" = 1'-0"

EXTERIOR FINISH KEY	
E.I.F.S. NOT USED ①	EXPOSED CONCRETE EXPOSED ARCHITECTURAL FINISH GRADE CONCRETE • COLOR: NATURAL GRAY ⑧
E.I.F.S. TRIM NOT USED ②	METAL ROOFING STANDING SEAM METAL ROOF • MBCI CRAFTSMAN SERIES - SMALL BATTEN (ROOF) • COLOR: DARK BRONZE ⑨
METAL PANEL SIDING MBCI METAL WALL PANEL: MASTERLINE 16" OR EQUAL SET HORIZONTALLY • COLOR: SLATE GRAY ③	METAL FACIA / SOFFIT PRE-FINISHED METAL SOFFIT (VENTED) • COLOR: DARK BRONZE ⑩
BRICK VENEER THIN BRICK VENEER (MODULAR SIZE) • INTERSTATE BRICK • COLOR: WALNUT ④	METAL PARAPET CAP 4" PRE-FINISHED METAL CAP FLASHING • MBCI (OR EQUAL) • COLOR: SLATE GRAY ⑪
FIBER CEMENT SIDING NICHHA - VINTAGE WOOD - SET HORIZONTALLY • COLOR: ASH (EPC764F) ⑤	METAL FLASHING PRE-FINISHED METAL FLASHING w/ DRIP • MBCI (OR EQUAL) • COLOR: SLATE GRAY ⑫
FIBER CEMENT SIDING NICHHA - VINTAGE WOOD - SET VERTICALLY • COLOR: ASH (EPC764F) ⑥	TRELLIS / COLUMNS WOOD w/ PAINTED FINISH • SHERWIN WILLIAMS EPOXY PAINT FINISH • COLOR: SNOWBOUND (SW 7004) ⑬
SILL / TRIM PRE-CAST CONCRETE WINDOW TRIM / SILL / WATER TABLE • COLOR: NATURAL GRAY ⑦	GUARDRAIL / HANDRAIL STEEL HAND RAIL GUARD RAIL - PAINTED • COLOR: WHITE ⑭

NOTE 1: PAINT HOLLOW METAL DOORS & FRAMES TO MATCH ADJACENT METAL WALL FINISH COLORS



B SIDE ELEVATION OF 200 SOUTH & ALLEY BUILDING (WEST ELEVATION)
SCALE: 4/11" = 1'-0"

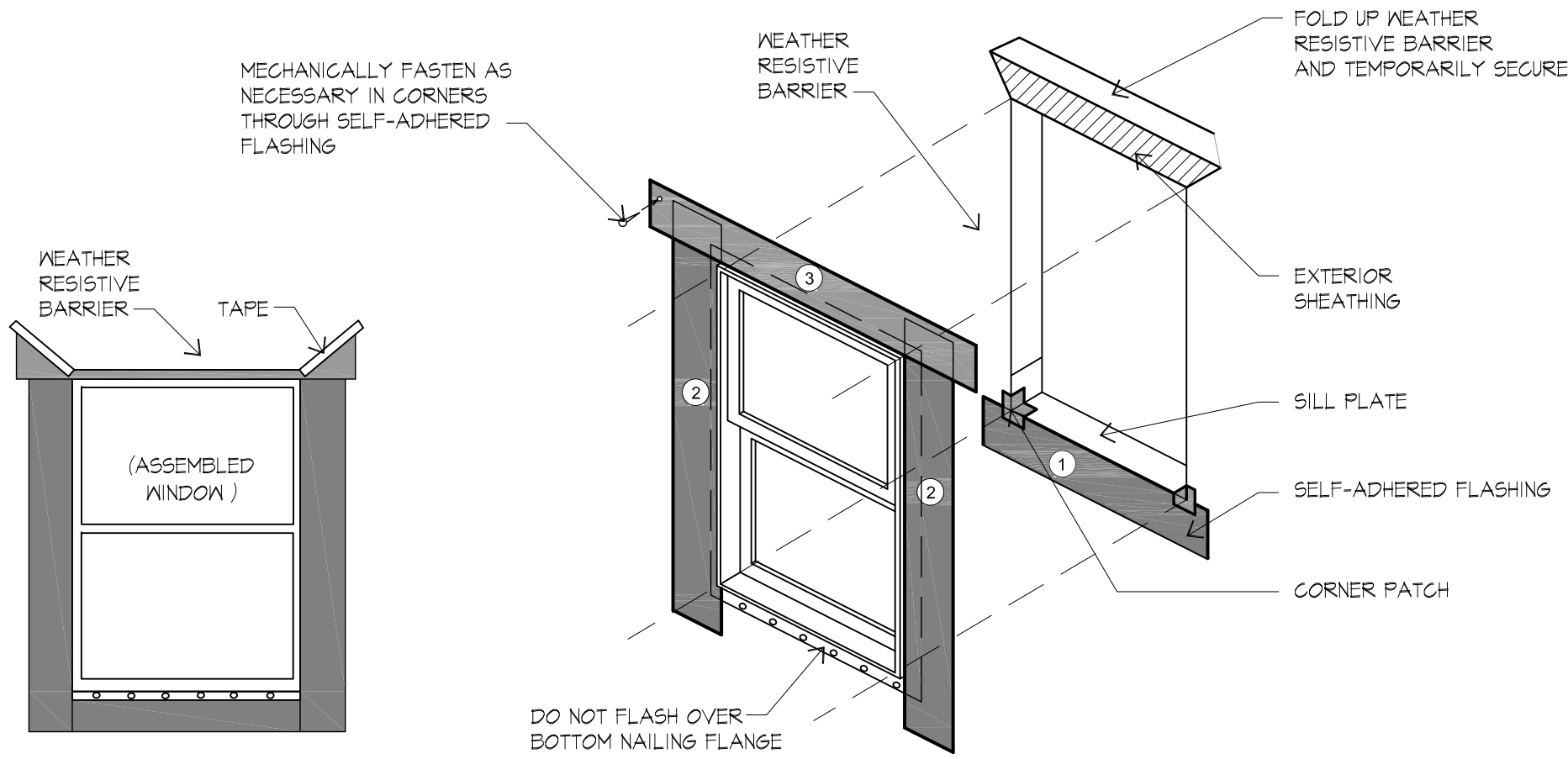


C DRIVEWAY ELEVATION OF ALLEY BUILDING (EAST ELEVATION)
SCALE: 4/11" = 1'-0"



D DRIVEWAY ELEVATION OF ALLEY BUILDING (WEST ELEVATION)
SCALE: 4/11" = 1'-0"

\\CS-SERVERS\Carpenter Stringham\Projects\16-033.01 Euclid Corners Townhomes - SLOCD\A301-3.dwg April 18, 2022 8:23 AM



HEAD FLASHING TIE-IN INSTRUCTIONS:

1. CUT, FOLD UP AND TEMPORARILY SECURE WEATHER RESISTIVE BARRIER ABOVE HEADER TO ALLOW FOR FLASHING INSTALLATION
2. INSTALL GRACE VYCOR HEAD FLASHING UNDER WEATHER RESISTIVE BARRIER ALONG HEADER
3. FOLD WEATHER RESISTIVE BARRIER BACK OVER HEAD FLASHING AND SEAL WITH TAPE AS SHOWN ABOVE

NOTES:

- A. VISIT SELF-ADHERED FLASHING MANUFACTURER FOR THE MOST CURRENT DETAILS, INSTALLATION VIDEO AND PRODUCT DATA SHEETS
- B. REMOVE WEATHER RESISTIVE BARRIER FROM TOP OF WINDOW SILL PLATE
- C. INSTALL SELF-ADHERED FLASHING IN ORDER AS SHOWN BY NUMBERS
- D. INSTALL SELF-ADHERED AND WEATHER RESISTIVE BARRIER TO FORM WATER-SHEEDING LAP

1 SELF-ADHERED FLASHING DETAIL
SCALE: NO SCALE

SHEET NOTES:

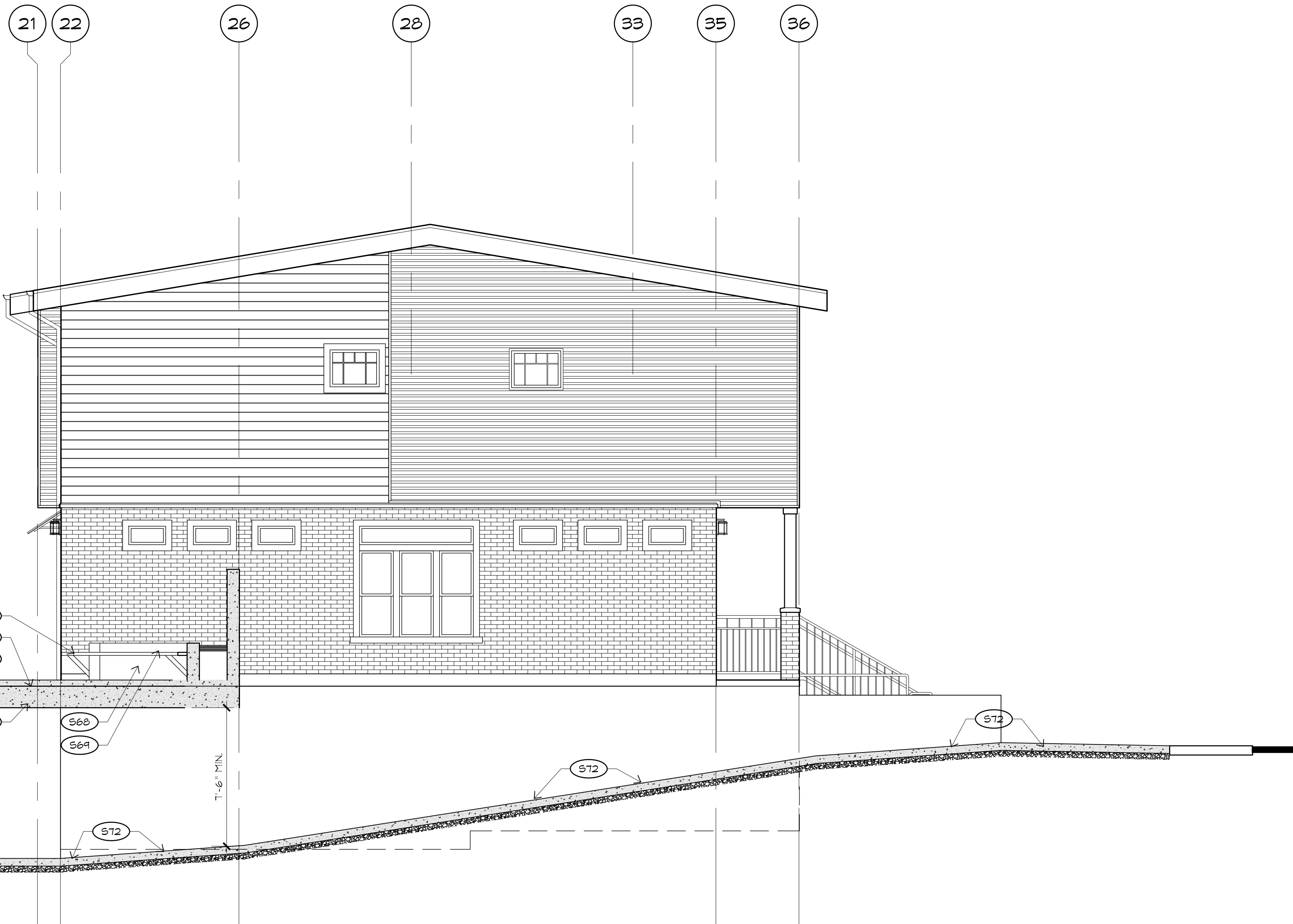
- (S1) BASE / SUBGRADE - SEE CIVIL & STRUCTURAL DRAWINGS
(S2) 10 mil VAPOR BARRIER
(S3) BELOW GRADE CONCRETE FOUNDATION WATER PROOFING SYSTEM
(S4) CONCRETE FOOTING - SEE STRUCTURAL DRAWINGS
(S5) CONCRETE FOUNDATION WALL - SEE STRUCTURAL DRAWINGS
(S6) CONCRETE SLAB ON GRADE - SEE STRUCTURAL DRAWINGS
(S7) FINISH GRADE - SEE CIVIL DRAWINGS
(S8) 14" THICK CONCRETE POST-TENSIONED PODIUM DECK - SEE STRUCTURAL DRAWINGS
(S9) 4" THICK GEO-FOAM - SEE ALSO STRUCTURAL DRAWINGS
(S10) 4" THICK CONCRETE TOPPING SLAB - SEE STRUCTURAL DRAWINGS
(S11) 8" THICK CONCRETE MASONRY UNITS - SEE STRUCTURAL DRAWINGS
(S12) INTERIOR CONCRETE STAIR - SEE 11/A102 & STRUCTURAL DRAWINGS
(S13) 8" THICK CONCRETE CURB - SEE STRUCTURAL DRAWINGS
(S14) 14" THICK CONCRETE CURB - SEE STRUCTURAL DRAWINGS
(S15) TOP OF WALL FIRE STOPPING - SEE DETAIL 2/GN001
(S16) CONCRETE EXPANSION JOINT
(S17) 2x4 WALL FURRING @ 16" O.C.
(S18) 8" THICK CMU DOOR HEADER
(S19) INSULATED OVERHEAD GARAGE DOOR - SEE DOOR SCHEDULE
(S20) EXTERIOR CONCRETE STAIR - SEE DETAILS 1 & 8/A102 & STRUCTURAL DRAWINGS
(S21) 8" THICK CONCRETE RETAINING WALL - SEE STRUCTURAL DRAWINGS
(S22) CONCRETE SIDE WALK - SEE CIVIL DRAWINGS
(S23) METAL RAILING - SEE DETAIL 17/A101
(S24) 2x6 or 2x4 P.T.D.F. SILL PLATE @ ALL AREAS WHERE WOOD MEETS CONCRETE
(S25) 2x6 WOOD STUDS @ 16" O.C.
(S26) 1/2" PLYWOOD WALL SHEATHING - SEE STRUCTURAL DRAWINGS
(S27) 2x4 WOOD STUDS @ 16" O.C.

- (S28) HEADER - SEE STRUCTURAL DRAWINGS
(S29) EXTERIOR WEATHER BARRIER SYSTEM - SEE SPECIFICATIONS & 1/A301
(S30) THIN BRICK VENEER INSTALLED w/ TABS II WALL SYSTEM & TABS II ECONOMY 3mm RAIN SCREEN OVER 1" RIGID EPS INSULATION
(S31) EIFS FINISH OVER EPS SHAPE
(S32) 1-1/2" EIFS SYSTEM
(S33) VINYL WINDOW ASSEMBLY - SEE WINDOW SCHEDULE ON SHEET A131
(S34) DOOR ASSEMBLY - SEE DOOR SCHEDULE ON SHEET A131
(S35) R-19 FOIL FACED BATT INSULATION
(S36) R-13 BATT INSULATION
(S37) R-30 BATT INSULATION
(S38) 5/8" GYPSUM BOARD (PAINTED - SEE FINISH SCHEDULE)
(S39) 6 mil VAPOR BARRIER
(S40) 3/4" T & G PLYWOOD FLOOR SHEATHING
(S41) 11-7/8" TJI JOIST @ 16" O.C. - SEE STRUCTURAL DRAWINGS
(S42) 9-1/2" TJI JOIST @ - SEE STRUCTURAL DRAWINGS
(S43) FLOOR FINISH - SEE FINISH SCHEDULE
(S44) 5/8" GYPSUM BOARD CEILING (PAINTED - SEE FINISH SCHEDULE) ON RESILIENT CHANNELS @ 24" O.C. PERPENDICULAR TO JOISTS
(S45) 5/8" GYPSUM BOARD CEILING (PAINTED - SEE FINISH SCHEDULE)
(S46) 3/4" MARINE GRADE FLY SHEATHING @ ROOF TERRACE
(S47) ROOF TERRACE WATER PROOFING SYSTEM (SLOPE TO DRAINS @ 1/8"12" MIN.) - SEE 1/A305 & SPECIFICATIONS
(S48) ROOF TERRACE TRENCH DRAIN - SEE PLUMBING DRAWINGS AND DETAIL 12/A010
(S49) PRE-FINISHED METAL FLASHING & COUNTER FLASHING
(S50) PRE-FINISHED METAL PARAPET CAP - SEE DETAILS 1 & 8/A101
(S51) 1-1/2" THICK IMPACT RESISTANT EIFS
(S52) 11-7/8" TJI ROOF JOIST @ 192" O.C. - SEE STRUCTURAL DRAWINGS
(S53) RIDGE BEAM - SEE STRUCTURAL DRAWINGS
(S54) R-30 BATT INSULATION w/ 1" MIN. BAFFLES FOR VENTILATION

- (S55) STANDING SEAM METAL ROOFING SYSTEM o/ 1" RIGID INSULATION & CONTINUOUS ICE & WATER SHIELD - INSTALL PER MANUFACTURER RECOMMENDED INSTRUCTIONS
(S56) PRE-FINISHED METAL SOFFIT
(S57) PRE-FINISHED METAL FASCIA (RIBBED) OVER (2) LAYERS 3/4" CDX PLYWOOD
(S58) PRE-FINISHED METAL GUTTER & DOWNSPOUT
(S59) 2x12 STAIR STRINGER - SEE DETAIL 13/A102
(S60) WOOD STAIR TREAD @ BULL-NOSED NOSING - SEE DETAIL 13/A102
(S61) PRE-FINISHED CORRUGATED METAL PANELS o/ 1" RIGID INSULATION & TABS II ECONOMY 3mm RAIN SCREEN
(S62) WOOD WINDOW SILL & APRON (PAINTED) - SEE DETAIL 1 & 2/A131
(S63) V-GROOVE - SEE DETAIL 10/A101
(S64) HANDRAIL & SUPPORTS (PAINTED) SEE 14B/A102
(S65) 3/4" PLYWOOD ROOF SHEATHING
(S66) POST TENSIONING SLAB WATER PROOFING SYSTEM - SEE SPECIFICATIONS
(S67) 6" CMU WALL
(S68) 8" WIDE RAISED LANDSCAPE CURB - SEE DETAILS 9 & 10/A010
(S69) PLANTING MEDIUM - SEE DETAILS 9 & 10/A010 & LANDSCAPE DRAWINGS
(S70) BENCH - SEE DETAIL 9/A010
(S71) 42" HIGH CONCRETE WALL - SEE STRUCTURAL DRAWINGS
(S72) CONCRETE DRIVE w/ SNOW-MELT CABLE @ 10" O.C. - SEE ELECTRICAL DRAWINGS
(S73) 3'-0" D. x 6'-0" W. GALV. METAL WINDOW WELL w/ EGRESS LADDER
(S74) 12" DEEP DRAINAGE LAYER 4" BELOW WINDOW SILL
(S75) 8" THICK CAST-IN-PLACE CONCRETE WALL - SEE STRUCTURAL DRAWINGS
(S76) 1" ALUMINUM FRAME CANOPY w/ PRE-FINISHED STANDING SEAM ROOFING
(S77) 5/8" GLASS-MAT-FIRECODE X SHEATHING OR EQUAL - SEE WALL TYPE 02
(S78) 6x6 WOOD COLUMN w/ PRE-FINISHED METAL COLUMN WRAP
(S79) 6x10 TREX TRELLIS BEAM
(S80) 2x8 TREX TRELLIS BEAM w/ CHAMFERED END @ 8" O.C.

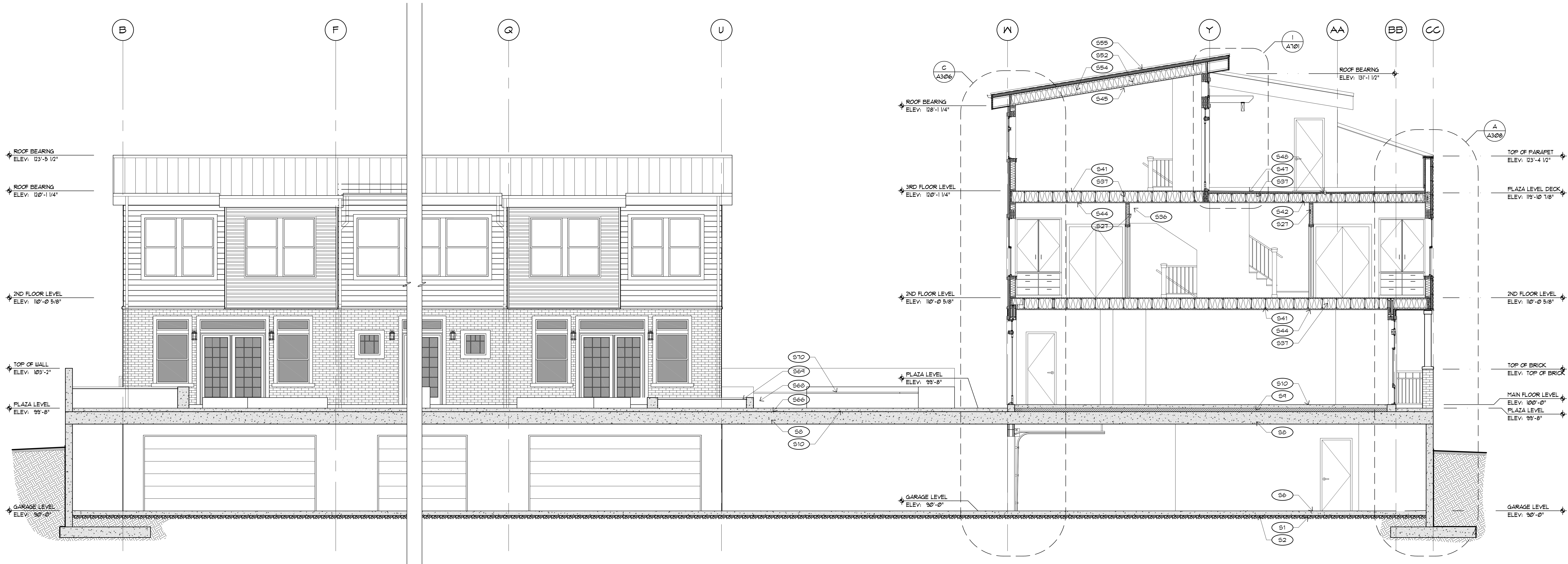
GENERAL NOTES:

- (G1) ANY WOOD IN CONTACT WITH CONCRETE SHALL BE DECAY-RESISTANT.



A BUILDING SECTION
SCALE: 1/4" = 1'-0"

\\CSA-SERVER\ER2\Carpenter Stritham\Projects\16-033.01 Euclid Corners Townhomes - SLC\CDs\A301-3.dwg April 18, 2022 8:32 AM



A BUILDING SECTION
SCALE: 1/4" = 1'-0"

SHEET NOTES:

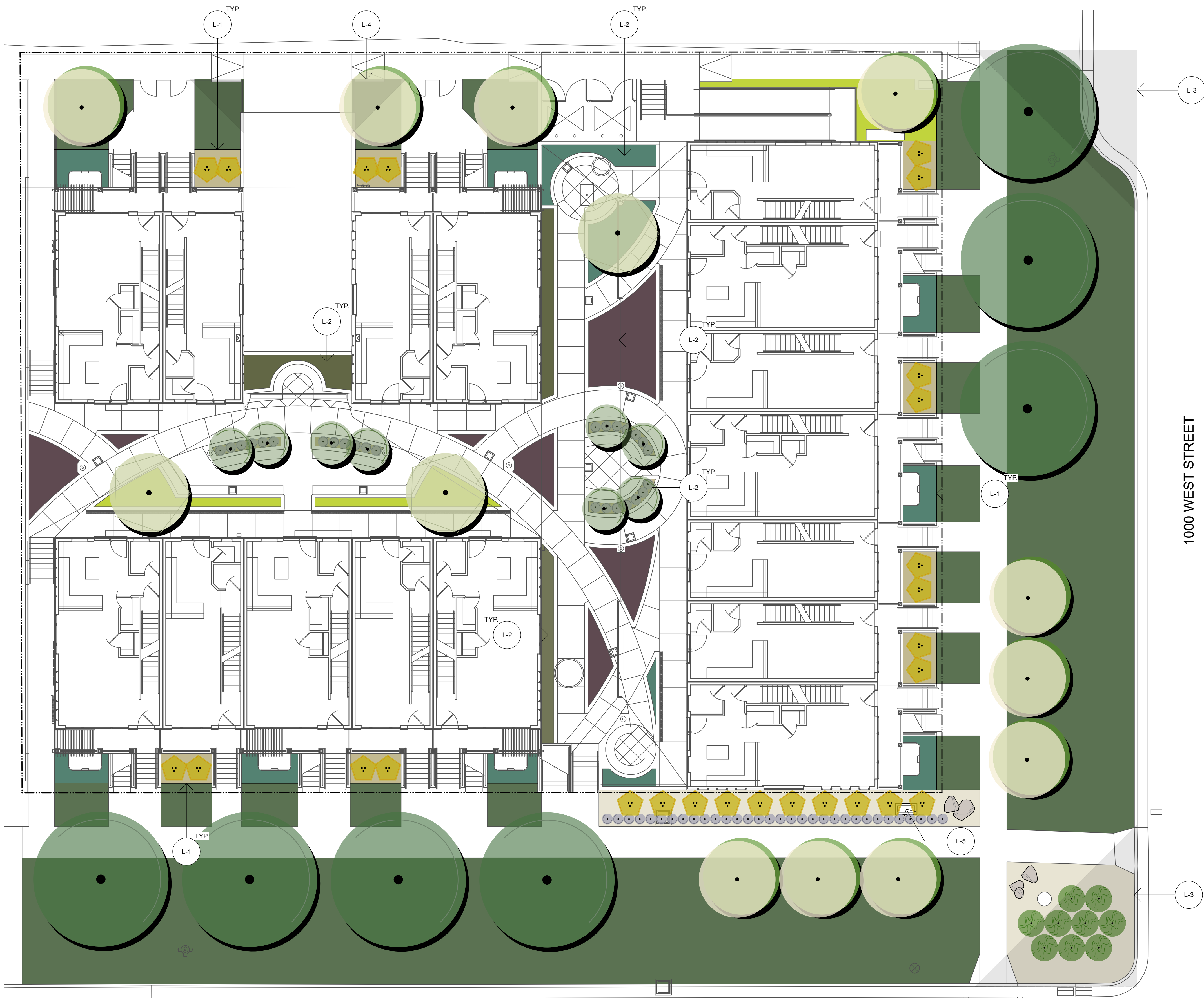
- (S1) BASE / SUBGRADE - SEE CIVIL & STRUCTURAL DRAWINGS
(S2) 10 mil. VAPOR BARRIER
(S3) BELOW GRADE CONCRETE FOUNDATION WATER PROOFING SYSTEM
(S4) CONCRETE FOOTING - SEE STRUCTURAL DRAWINGS
(S5) CONCRETE FOUNDATION WALL - SEE STRUCTURAL DRAWINGS
(S6) CONCRETE SLAB ON GRADE - SEE STRUCTURAL DRAWINGS
(S7) FINISH GRADE - SEE CIVIL DRAWINGS
(S8) 14" THICK CONCRETE POST-TENSIONED PODIUM DECK - SEE STRUCTURAL DRAWINGS
(S9) 4" THICK GEO-FOAM - SEE ALSO STRUCTURAL DRAWINGS
(S10) 4" THICK CONCRETE TOPPING SLAB - SEE STRUCTURAL DRAWINGS
(S11) 8" THICK CONCRETE MASONRY UNITS - SEE STRUCTURAL DRAWINGS
(S12) INTERIOR CONCRETE STAIR - SEE 11/A102 & STRUCTURAL DRAWINGS
(S13) 8" THICK CONCRETE CURB - SEE STRUCTURAL DRAWINGS
(S14) 14" THICK CONCRETE CURB - SEE STRUCTURAL DRAWINGS
(S15) TOP OF WALL FIRE STOPPING - SEE DETAIL 2/GN001
(S16) CONCRETE EXPANSION JOINT
(S17) 2x4 WALL FURRING @ 16" O.C.
(S18) 8" THICK CMU DOOR HEADER
(S19) INSULATED OVERHEAD GARAGE DOOR - SEE DOOR SCHEDULE
(S20) EXTERIOR CONCRETE STAIR - SEE DETAILS 1 & 8/A102 & STRUCTURAL DRAWINGS
(S21) 8" THICK CONCRETE RETAINING WALL - SEE STRUCTURAL DRAWINGS
(S22) CONCRETE SIDE WALK - SEE CIVIL DRAWINGS
(S23) METAL RAILING - SEE DETAIL 17/A101
(S24) 2x6 or 2x4 P.T.D.F. SILL PLATE @ ALL AREAS WHERE WOOD MEETS CONCRETE
(S25) 2x6 WOOD STUDS @ 16" O.C.
(S26) 1/2" PLYWOOD WALL SHEATHING - SEE STRUCTURAL DRAWINGS
(S27) 2x4 WOOD STUDS @ 16" O.C.

- (S28) HEADER - SEE STRUCTURAL DRAWINGS
(S29) EXTERIOR WEATHER BARRIER SYSTEM - SEE SPECIFICATIONS & 1/A301
(S30) TWIN BRICK VENEER INSTALLED w/ TABS 11 WALL SYSTEM & TABS 11 ECONOMY 3mm RAIN SCREEN OVER 1" RIGID EPS INSULATION
(S31) EIFS FINISH OVER EPS SHAPE
(S32) 1-1/2" EIFS SYSTEM
(S33) VINYL WINDOW ASSEMBLY - SEE WINDOW SCHEDULE ON SHEET A131
(S34) DOOR ASSEMBLY - SEE DOOR SCHEDULE ON SHEET A131
(S35) R-19 FOIL FACED BATT INSULATION
(S36) R-13 BATT INSULATION
(S37) R-30 BATT INSULATION
(S38) 5/8" GYPSUM BOARD (PAINTED - SEE FINISH SCHEDULE)
(S39) 6 mil. VAPOR BARRIER
(S40) 3/4" T & G PLYWOOD FLOOR SHEATHING
(S41) 11-7/8" TJI JOIST @ 16" O.C. - SEE STRUCTURAL DRAWINGS
(S42) 9-1/2" TJI JOIST @ - SEE STRUCTURAL DRAWINGS
(S43) FLOOR FINISH - SEE FINISH SCHEDULE
(S44) 5/8" GYPSUM BOARD CEILING (PAINTED - SEE FINISH SCHEDULE) ON RESILIENT CHANNELS @ 24" O.C. PERPENDICULAR TO JOISTS
(S45) 5/8" GYPSUM BOARD CEILING (PAINTED - SEE FINISH SCHEDULE)
(S46) 3/4" MARINE GRADE PLY SHEATHING @ ROOF TERRACE
(S47) ROOF TERRACE WATER PROOFING SYSTEM (SLOPE TO DRAINS @ 1/8"12" MIN. - SEE 1/A305 & SPECIFICATIONS)
(S48) ROOF TERRACE TRENCH DRAIN - SEE PLUMBING DRAWINGS AND DETAIL 12/A010
(S49) PRE-FINISHED METAL FLASHING & COUNTER FLASHING
(S50) PRE-FINISHED METAL PARAPET CAP - SEE DETAILS 1 & 8/A101
(S51) 1-1/2" THICK IMPACT RESISTANT EIFS
(S52) 11-7/8" TJI ROOF JOIST @ 192" O.C. - SEE STRUCTURAL DRAWINGS
(S53) RIDGE BEAM - SEE STRUCTURAL DRAWINGS
(S54) R-30 BATT INSULATION w/ 1" MIN. BAFFLES FOR VENTILATION

- (S55) STANDING BEAM METAL ROOFING SYSTEM o/ 1" RIGID INSULATION & CONTINUOUS ICE & WATER SHIELD - INSTALL PER MANUFACTURER RECOMMENDED INSTRUCTIONS
(S56) PRE-FINISHED METAL SOFFIT
(S57) PRE-FINISHED METAL FASCIA (RIBBED) OVER (2) LAYERS 3/4" CDX PLYWOOD
(S58) PRE-FINISHED METAL GUTTER & DOWNSPOUT
(S59) 2x12 STAIR STRINGER - SEE DETAIL 13/A102
(S60) WOOD STAIR TREAD @ BULL-NOSED NOBING - SEE DETAIL 13/A102
(S61) PRE-FINISHED CORRUGATED METAL PANELS o/ 1" RIGID INSULATION & TABS 11 ECONOMY 3mm RAIN SCREEN
(S62) WOOD WINDOW SILL & APRON (PAINTED) - SEE DETAIL 1 & 2/A131
(S63) V-GROOVE - SEE DETAIL 10/A101
(S64) HANDRAIL & SUPPORTS (PAINTED) SEE 14B/A102
(S65) 3/4" PLYWOOD ROOF SHEATHING
(S66) POST TENSIONS SLAB WATER PROOFING SYSTEM - SEE SPECIFICATIONS
(S67) 6" CMU WALL
(S68) 8" WIDE RAISED LANDSCAPE CURB - SEE DETAILS 9 & 10/A010
(S69) PLANTING MEDIUM - SEE DETAILS 9 & 10/A010 & LANDSCAPE DRAWINGS
(S70) BENCH - SEE DETAIL 9/A010
(S71) 42" HIGH CONCRETE WALL - SEE STRUCTURAL DRAWINGS
(S72) CONCRETE DRIVE w/ SNOW-MELT CABLE @ 10" O.C. - SEE ELECTRICAL DRAWINGS
(S73) 3'-0" D. x 6'-0" W. GALV. METAL WINDOW WELL w/ EGRESS LADDER
(S74) 12" DEEP DRAINAGE LAYER 4" BELOW WINDOW SILL
(S75) 8" THICK CAST-IN-PLACE CONCRETE WALL - SEE STRUCTURAL DRAWINGS
(S76) 1" ALUMINUM FRAME CANOPY w/ PRE-FINISHED STANDING BEAM ROOFING
(S77) 5/8" GLASS-MAT-FIRECODE X SHEATHING OR EQUAL - SEE WALL TYPE 02
(S78) 6x6 WOOD COLUMN w/ PRE-FINISHED METAL COLUMN WRAP
(S79) 6x10 TREX TRELLIS BEAM
(S80) 2x8 TREX TRELLIS BEAM w/ CHAMFERED END @ 8" O.C.

GENERAL NOTES:

- (G1) ANY WOOD IN CONTACT WITH CONCRETE SHALL BE DECAY-RESISTANT.



200 SOUTH STREET

1000 WEST STREET

LANDSCAPE SCHEDULE

SYMBOL	QTY.	COMMON NAME	BOTANICAL NAME	SIZE	HYDROZONE	DETAIL
DECIDUOUS TREES						
	3	CORAL BARK MAPLE	ACER PALMATUM 'SANGO KAKU'	2" CAL.		DL5.01
	8	LOLLIPOP CRABAPPLE	MALUS X 'LOLLIZAM'	2" CAL.		DL5.01
	10	IVORY SILK JAPANESE TREE LILAC	SYRINGA RETICULATA 'IVORY SILK'	2" CAL.	T63	DL5.01
	7	STERLING SILVER LINDEN	TILIA TOMENTOSA 'STERLING'	2" CAL.	T64	DL5.01
SHRUBS						
	9	PANCHITO MANZANITA	ARCTOSTAPHYLOS X COLORADOENSIS	5 GAL.	Se1	BL5.01
	58	HIDCOTE BLUE ENGLISH LAVENDER	LAVANDULA ANGUSTIFOLIA 'HIDCOTE BLUE'	5 GAL.	P2	BL5.01
	26	TIGER EYES SUMAC	RHUS TYPHINA 'BALTIGER'	5 GAL.	Sd2	BL5.01
ORNAMENTAL GRASSES						
	246 S.F.	YAKU JIMA DWARF MAIDEN GRASS	MISCANTHUS SINENSIS 'YAKU JIMA'	5 GAL.	36" O.C.	Tw2 A/L5.01
	742 S.F.	LITTLE BUNNY DWARF FOUNTAIN GRASS	PENNISETUM ALOPECUROIDES 'LITTLE BUNNY'	1 GAL.	18" O.C.	Tw2 A/L5.01
GROUNDCOVERS						
	501 S.F.	TINY MONSTER GERANIUM	GERANIUM 'TINY MONSTER'	18" O.C.		BL5.01 P3
	385 S.F.	ANGELINA STONECROP	SEDUM RUPESTRE 'ANGELINA'	18" O.C.		BL5.01 GV1
SYMBOL						
	QTY.	DESCRIPTION	INSTRUCTIONS	SIZE	SOURCE	DETAIL
	8,908 S.F.	'IMPERIAL BLUE' LAWN SOD	INSTALL OVER MINIMUM 5" TOPSOIL LAYER.		CHANS SHARE FARMS (866) SOD-EASY OR APPROVED EQUAL	
BOULDERS						
	5	'RED VEIN' BOULDERS FROM BROWNS CANYON QUARRY	BURY 1/3 THE DEPTH OF THE BOULDER INTO FINISH GRADE. DO NOT USE BOULDERS THAT ARE LESS THAN 24" DIAMETER. BOULDER SHALL BE WASHED AND FREE OF DIRT AND OTHER FOREIGN DEBRIS.	2'-4"	MOUNTAIN VALLEY STONE (435) 654-0120 OR APPROVED EQUAL.	FL5.01
CRUSHED ROCK						
	977 S.F.	'SOUTH TOWN' CRUSHED ROCK	INSTALLED A MINIMUM 3" DEEP. INSTALL OVER DEWITTS 4.1 WEED BARRIER FABRIC. CRUSHED ROCK SHALL BE FREE OF DIRT & OTHER FOREIGN DEBRIS.	3/4" DIAMETER	UTAH LANDSCAPING ROCK (435) 250-3851 OR APPROVED EQUAL.	HL5.01
MULCH						
	452 S.F.	'SUPREME SHREDDED BARK'	INSTALLED A MINIMUM 4" DEEP. INSTALL OVER DEWITTS 4.1 WEED BARRIER FABRIC. BARK MULCH SHALL BE FREE OF DIRT, ROCK AND OTHER FOREIGN DEBRIS.		MILLER COMPANIES (435) 245-3157 OR APPROVED EQUAL	HL5.01

GENERAL NOTE

- REFER TO COMMON EARTHWORK AND PLANTING NOTES ON SHEET L0.01

REFERENCE NOTES

- L-1. METAL EDGING
DETAIL IL5.01
- L-2. UTELITE INTENSIVE GREEN ROOF LIGHTWEIGHT PLANTING MEDIA (801)243-8548 OR APPROVED EQUAL. INSTALLED ABOVE OTHER GREEN ROOF SYSTEM LAYERS TO 4" BELOW TOP OF WALL AS PER APPLICABLE SOPREMA SPECIFICATIONS - SEE PLANTING NOTE 11 ON SHEET L0.01
- L-3. 30' X 30' CLEAR VIEW SIGHT TRIANGLE
- L-4. 10' X 10' CLEAR VIEW SIGHT TRIANGLE
- L-5. CONCRETE PAD FOR BACKFLOW PREVENTER - SEE SHEET L3.01 FOR IRRIGATION PLAN

0 10 20 30 FT

NORTH

SATL LAKE CITY DATA

ZONED AS	TSA-UN-T	
TOTAL SITE AREA	23,337 S.F. = 0.54 AC.	
ON-SITE LANDSCAPE AREA	2,963 S.F. = 13%	
	REQUIRED	PROVIDED
GENERAL:		
TOTAL TURF AREA ON-SITE	10% MAX.	707 S.F. = 3%
TOTAL USABLE OPEN SPACE AREA ON-SITE	10% MIN.	10,555 S.F. = 45%
DROUGHT TOLERANT PLANT SPECIES	80% MIN.	90%
PARKSTRIP:		
REQUIRED TREES - 1 PER 30'		
200 SOUTH STREET:	206' L.F. / 30 = 6.9	7
1000 WEST STREET:	172' L.F. / 30 = 5.7	6
ALLEYWAY:	189' L.F. / 30 = 6.3	3*

*ONLY FOUR TREES HAVE BEEN PROVIDED ALONG ALLEYWAY DUE TO CONFLICTS WITH UTILITIES, HARDSCAPE, AND LARGE STREET TREE ON 1000 WEST.

Call BEFORE YOU Dig
1-800-862-4111

NOTICE!
THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION, PROTECTION, AND RESTORATION OF ALL BURIED OR ABOVE GROUND UTILITIES SHOWN OR NOT SHOWN ON THE PLANS.



Preliminary Subdivision Plat

☐ New Lots

☐ Amendment

OFFICE USE ONLY

Project #:	Received By:	Date Received:	Zoning:
------------	--------------	----------------	---------

Proposed Subdivision Name:

PLEASE PROVIDE THE FOLLOWING INFORMATION

Property Address(s):

Name of Applicant:	Phone:
--------------------	--------

Address of Applicant:

E-mail of Applicant:	Cell/Fax:
----------------------	-----------

Applicant's Interest in Subject Property:

☐ Owner ☐ Engineer ☐ Architect ☐ Other:

Name of Property Owner (if different from applicant):

E-mail of Property Owner:	Phone:
---------------------------	--------

Please note that additional information may be required by the project planner to ensure adequate information is provided for staff analysis. All information required for staff analysis will be copied and made public, including professional architectural or engineering drawings, for the purposes of public review by any interested party.

WHERE TO FILE THE COMPLETE APPLICATION

Apply online through the [Citizen Access Portal](#). There is a [step-by-step guide](#) to learn how to submit online.

REQUIRED FEE

Filing fee of **\$397** plus **\$121** for each lot proposed on the preliminary plat. Plus, additional fee for required public notices will be assessed after the application is submitted.

SIGNATURE

If applicable, a notarized statement of consent authorizing applicant to act as an agent will be required.

Signature of Owner or Agent:	Date:
------------------------------	-------

SUBMITTAL REQUIREMENTS

Staff Review

Please include with the application (*please attach electronically additional sheet/s if necessary*)

Project Description

☐☐

A written description of what is being proposed.

Legal Description

☐☐

A digital file of the legal description of the current boundaries of the subject property; and, for proposed subdivision of 10 lots or less, the legal descriptions of each of the proposed lots. (See Section [20.16](#) for preliminary plats and Section [20.28](#) for amendments).

Preliminary Plat Drawing

☐☐

A digital (PDF) copy of the preliminary plat drawing

APPEAL PROCESS

- ➔ Any person adversely and materially affected by any final decision made by the planning director or designee may file a petition for review of the decision with the planning commission within ten (10) days after the record of decision is posted to the city's internet site.
- ➔ Any person adversely affected by any final decision made by the planning commission under this chapter may file a petition for review of the decision with the Appeals Hearing Officer within ten (10) days after the decision is rendered.

AVAILABLE CONSULTATION

- ➔ Planners are available for consultation prior to submitting this application. Please email zoning@slcgov.com if you have any questions regarding the requirements of this application.

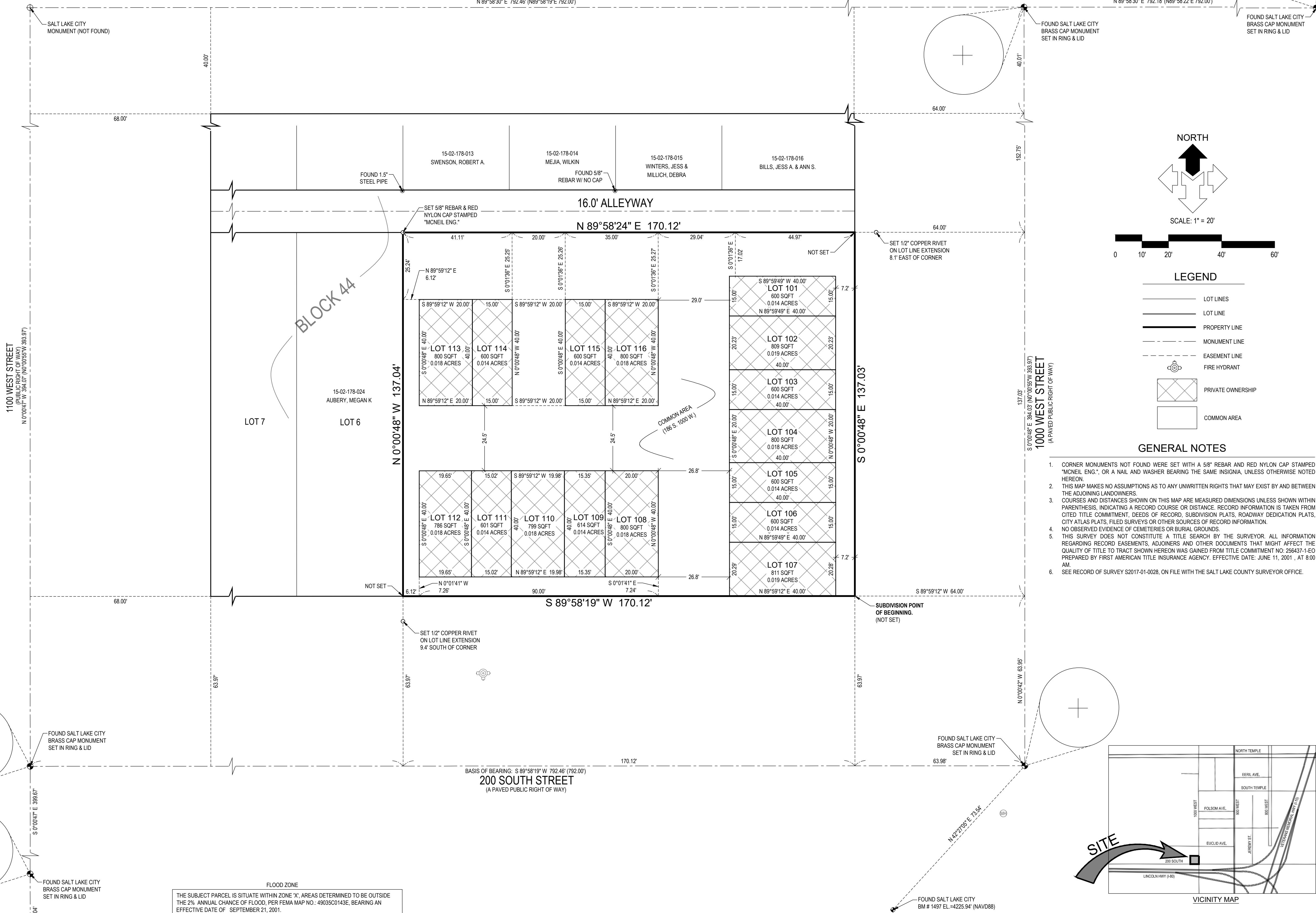
INCOMPLETE APPLICATIONS WILL NOT BE ACCEPTED

_____ I acknowledge that Salt Lake City requires the items above to be submitted before my application can be processed. I understand that Planning will not accept my application unless all of the following items are included in the submittal package.

EUCLID CORNER P.U.D.

A RESIDENTIAL PLANNED UNIT DEVELOPMENT
LOCATED IN THE NORTHWEST CORNER OF SECTION 02, TOWNSHIP 1 SOUTH,
RANGE 1 WEST SALT LAKE BASE AND MERIDIAN.
186 SOUTH 1000 WEST, SALT LAKE CITY, UTAH
* PRELIMINARY PLAT *

EUCLID AVENUE, (140 SOUTH)
PUBLIC RIGHT OF WAY
N 89°58'30" E 732.40' (N89°58'19"E 732.00')



NORTH

SCALE: 1" = 20'

LEGEND

- LOT LINES
- LOT LINE
- PROPERTY LINE
- MONUMENT LINE
- EASEMENT LINE
- FIRE HYDRANT
- PRIVATE OWNERSHIP
- COMMON AREA

GENERAL NOTES

- CORNER MONUMENTS NOT FOUND WERE SET WITH A 5/8" REBAR AND RED NYLON CAP STAMPED "MCNEIL ENG.", OR A NAIL AND WASHER BEARING THE SAME INSIGNIA, UNLESS OTHERWISE NOTED HEREON.
- THIS MAP MAKES NO ASSUMPTIONS AS TO ANY UNWRITTEN RIGHTS THAT MAY EXIST BY AND BETWEEN THE ADJOINING LANDOWNERS.
- COURSES AND DISTANCES SHOWN ON THIS MAP ARE MEASURED DIMENSIONS UNLESS SHOWN WITHIN PARENTHESES, INDICATING A RECORD COURSE OR DISTANCE. RECORD INFORMATION IS TAKEN FROM CITED TITLE COMMITMENT, DEEDS OF RECORD, SUBDIVISION PLATS, ROADWAY DEDICATION PLATS, CITY ATLAS PLATS, FILED SURVEYS OR OTHER SOURCES OF RECORD INFORMATION.
- NO OBSERVED EVIDENCE OF CEMETERIES OR BURIAL GROUNDS.
- THIS SURVEY DOES NOT CONSTITUTE A TITLE SEARCH BY THE SURVEYOR. ALL INFORMATION REGARDING RECORD EASEMENTS, ADJOINERS AND OTHER DOCUMENTS THAT MIGHT AFFECT THE QUALITY OF TITLE TO TRACT SHOWN HEREON WAS GAINED FROM TITLE COMMITMENT NO. 256437-1-EO PREPARED BY FIRST AMERICAN TITLE INSURANCE AGENCY, EFFECTIVE DATE: JUNE 11, 2001, AT 8:00 AM.
- SEE RECORD OF SURVEY S2017-01-0028, ON FILE WITH THE SALT LAKE COUNTY SURVEYOR OFFICE.

SURVEYOR'S CERTIFICATE

I, DENNIS K. WITHERS, DO HEREBY CERTIFY THAT I AM A PROFESSIONAL LAND SURVEYOR AND THAT I HOLD A LICENSE, CERTIFICATE NO. 6135190, IN ACCORDANCE WITH THE PROFESSIONAL ENGINEERS AND LAND SURVEYORS LICENSING ACT FOUND IN TITLE 38, CHAPTER 22 OF THE UTAH CODE. I FURTHER CERTIFY THAT BY AUTHORITY OF THE OWNERS, I HAVE MADE A SURVEY OF THE TRACT OF LAND SHOWN ON THIS PLAT AND DESCRIBED BELOW. I HAVE SUBDIVIDED SAID TRACT OF LAND INTO LOTS, STREETS, AND EASEMENTS, HAVE COMPLETED A SURVEY OF THE PROPERTY DESCRIBED ON THIS PLAT IN ACCORDANCE WITH UTAH CODE SECTION 17-25-17, HAVE VERIFIED ALL MEASUREMENTS, AND HAVE PLACED MONUMENTS AS REPRESENTED ON THE PLAT, TO BE KNOWN AS:

EUCLID CORNER P.U.D.

A RESIDENTIAL PLANNED UNIT DEVELOPMENT

AND THAT THE SAME HAS BEEN CORRECTLY SURVEYED AND STAKED ON THE GROUND AS SHOWN ON THIS PLAT.

BOUNDARY DESCRIPTION

LOT 2 THRU 5, BLOCK 1, KELSEY AND GILLESPIE SUBDIVISION OF BLOCK 44, PLAT 'C' ACCORDING TO THE OFFICIAL PLAT THEREOF, RECORDED IN BOOK "B", AT PAGE 76 OF PLATS ON FILE WITH THE SALT LAKE COUNTY RECORDER'S OFFICE AND BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHEAST CORNER OF LOT 02, KELSEY AND GILLESPIE SUBDIVISION OF BLOCK 44, PLAT 'C' ACCORDING TO THE OFFICIAL PLAT THEREOF, RECORDED IN BOOK "B", AT PAGE 76 OF PLATS ON FILE WITH THE SALT LAKE COUNTY RECORDER'S OFFICE SAID CORNER BEING NORTH 0°00'48" WEST, ALONG THE 1000 WEST STREET MONUMENT LINE, A DISTANCE OF 63.95 FEET AND SOUTH 89°59'12" WEST, PERPENDICULAR TO SAID MONUMENT LINE, A DISTANCE OF 64.00 FEET, FROM THE SALT LAKE CITY MONUMENT MARKING THE INTERSECTION OF 200 SOUTH AND 1000 WEST STREET (BASIS OF BEARING BEING SOUTH 89°58'19" WEST, ALONG THE 200 SOUTH STREET MONUMENT LINE, BETWEEN MONUMENT FOUND AT 1000 WEST AND 1100 WEST) AND RUNNING THENCE SOUTH 89°58'19" WEST, ALONG THE NORTH LINE OF SAID 200 SOUTH STREET, A DISTANCE OF 170.12 FEET, TO THE WEST LINE OF SAID LOT 5; THENCE NORTH 0°00'48" WEST, ALONG SAID WEST LINE, A DISTANCE OF 137.03 FEET, TO THE NORTHWESTERLY CORNER THEREOF AND THE SOUTH LINE OF 16.0' WIDE ALLEYWAY; THENCE NORTH 89°58'24" EAST, ALONG THE SOUTH LINE OF SAID ALLEYWAY AND THE NORTHERLY LINES OF LOTS 2 THRU 5, A DISTANCE OF 170.12 FEET, TO THE WESTERLY LINE OF AFORESAID 1000 WEST STREET; THENCE SOUTH 0°00'48" EAST, ALONG SAID WESTERLY LINE, BEING THE EAST LINE OF SAID LOT 2, A DISTANCE OF 137.03 FEET, TO THE POINT OF BEGINNING.

CONTAINS: 23.31 SQUARE FEET, OR 0.535 ACRES. SALT LAKE COUNTY TAX I.D.: 15-02-178-025

OWNER'S DEDICATION

SALT LAKE NEIGHBORHOOD HOUSING SERVICES, THE OWNER OF THE DESCRIBED TRACT OF LAND TO BE HEREAFTER KNOWN AS EUCLID CORNER PRELIMINARY SUBDIVISION DOES HEREBY DEDICATE FOR THE PERPETUAL USE OF THE PUBLIC ALL STREETS, EASEMENTS AND OTHER PROPERTY AS SHOWN ON THIS PLAT AND HEREBY CONSENTS AND GIVES APPROVAL TO THE RECORDING OF THIS PLAT FOR ALL PURPOSES SHOWN THEREIN.

IN WITNESS WHEREOF, I HAVE HEREUNTO SET MY HAND THIS ____ DAY OF _____, 20__.

SALT LAKE NEIGHBORHOOD HOUSING SERVICES

BY: _____

PRINT NAME: _____

TITLE: _____

CORPORATE ACKNOWLEDGMENT

STATE OF UTAH } S.S.
COUNTY OF SALT LAKE }

ON THE ____ DAY OF _____, 20__, PERSONALLY APPEARED BEFORE ME, WHO BEING BY ME DULY SWORN, DID ACKNOWLEDGE THAT HE/SHE IS THE MANAGER OF SALT LAKE NEIGHBORHOOD HOUSING SERVICES, A UTAH LIMITED LIABILITY COMPANY, AND THAT THE FOREGOING OWNERS DEDICATION REGARDING THE EUCLID CORNER PRELIMINARY SUBDIVISION WAS SIGNED BY HIM/HER ON BEHALF OF SAID SALT LAKE NEIGHBORHOOD HOUSING SERVICES.

COMMISSION NUMBER _____

MY COMMISSION EXPIRES _____

PRINTED NAME _____ A NOTARY PUBLIC COMMISSIONED IN UTAH

LIEN HOLDER'S CONSENT TO RECORD

ON APRIL 25, 2012, WELLS FARGO BANK, NATIONAL ASSOCIATION, A PUBLIC ENTITY, ENTERED INTO A DEED OF TRUST, ASSIGNMENT OF REVENUE, SECURITY AGREEMENT AND FINANCING STATEMENT ("DEED OF TRUST") WITH SALT LAKE NEIGHBORHOOD HOUSING SERVICES, A UTAH LIMITED LIABILITY COMPANY, AND EUCLID CORNER HOUSING L.L.C., A UTAH LIMITED LIABILITY COMPANY TO WELLS FARGO BANK NORTHWEST, NATIONAL ASSOCIATION, TRUSTEE, FOR THE BENEFIT OF WELLS FARGO BANK, NATIONAL ASSOCIATION, BENEFICIARY, DATED APRIL 25, 2012 AND RECORDED MAY 1, 2012 AS ENTRY NO. 11623187, IN BOOK 1013, AT PAGE 2534. MODIFIED BY THAT CERTAIN MODIFICATION AGREEMENT AMENDING DEED OF TRUST RECORDED MARCH 27, 2014 AS ENTRY NO. 11824687 IN BOOK 10219 AT PAGE 8760. SAID DEED OF TRUST WAS PARTIALLY RE-CONVEYED TO OTHER PROPERTY BY INSTRUMENT RECORDED DECEMBER 18, 2014 AS ENTRY NO. 11964650 IN BOOK 10283 AT PAGE 2129.

WELLS FARGO BANK NORTHWEST, NATIONAL ASSOCIATION IS FULLY AWARE THAT EUCLID CORNER GROUP L.L.C. IS IN THE PROCESS OF RECORDING A PLAT CREATING A CONDOMINIUM PROJECT KNOWN AS EUCLID CORNER PRELIMINARY SUBDIVISION AND WELLS FARGO BANK NORTHWEST, NATIONAL ASSOCIATION HEREBY CONSENTS TO THE PLAT FOR ALL PURPOSES SHOWN HEREON.

DATED THIS ____ DAY OF _____, 20__.

By: _____

Printed Name: _____

Its: _____

ACKNOWLEDGEMENT

STATE OF UTAH } S.S.
COUNTY OF SALT LAKE }

ON THE ____ DAY OF _____, 20__, PERSONALLY APPEARED BEFORE ME, _____, WHO BEING BY ME DULY SWORN, DID ACKNOWLEDGE THAT HE/SHE IS THE _____ OF _____, AND THAT THE FOREGOING INSTRUMENT WAS SIGNED BY HIM/HER ON BEHALF OF: WELLS FARGO BANK NORTHWEST, NATIONAL ASSOCIATION.

COMMISSION NUMBER _____

MY COMMISSION EXPIRES _____

(PRINT NAME) _____ A NOTARY PUBLIC COMMISSIONED IN UTAH

EUCLID CORNER P.U.D.

A RESIDENTIAL PLANNED UNIT DEVELOPMENT
LOCATED IN THE NORTHWEST CORNER OF SECTION 02, TOWNSHIP 1 SOUTH, RANGE 1 WEST,
SALT LAKE BASE AND MERIDIAN.
186 SOUTH 1000 WEST, SALT LAKE CITY, UTAH
* PRELIMINARY PLAT *

SALT LAKE COUNTY RECORDER

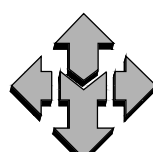
RECORD NO. _____

STATE OF UTAH, COUNTY OF SALT LAKE, RECORDED AND FILED AT THE REQUEST OF _____

DATE: _____ TIME: _____ BOOK: _____ PAGE: _____

FEE \$ _____ SALT LAKE COUNTY RECORDER

PREPARED BY:



**McNEIL
ENGINEERING**
Designing for the Future Since 1983™

8610 So. Sandy Parkway, Suite 200 Sandy, Utah 84070
TEL. (801) 255-7700 FAX (801) 255-8071
E-MAIL info@mcneileng.com WEB SITE AT www.mcneil-group.com

CITY PUBLIC UTILITIES DEPARTMENT

APPROVED THIS ____ DAY OF _____ A.D., 20__.

SALT LAKE CITY PUBLIC UTILITIES DIRECTOR

BUILDING OFFICIAL

APPROVED THIS ____ DAY OF _____ A.D., 20__.

SALT LAKE CITY PUBLIC UTILITIES DIRECTOR

CITY PLANNING DIRECTOR

APPROVED THIS ____ DAY OF _____ A.D., 20__.

BY THE SALT LAKE CITY PLANNING COMMISSION.

PLANNING DIRECTOR

DATE

CITY ATTORNEY

APPROVED THIS ____ DAY OF _____ A.D., 20__.

SALT LAKE CITY ATTORNEY

CITY APPROVAL

PRESENTED TO SALT LAKE CITY THIS ____ DAY OF _____ A.D., 20__, AND IT IS HEREBY APPROVED.

SALT LAKE CITY MAYOR

SALT LAKE CITY RECORDER

COUNTY HEALTH DEPARTMENT

APPROVED THIS ____ DAY OF _____ A.D., 20__.

SALT LAKE COUNTY HEALTH DEPARTMENT

CITY ENGINEERING DIVISION

I HEREBY CERTIFY THAT I HAVE HAD THIS PLAT EXAMINED BY THIS OFFICE AND IT IS CORRECT IN ACCORDANCE WITH THE INFORMATION ON FILE.

CITY ENGINEER

DATE

CITY SURVEYOR

DATE

SHEET

1

OF

1

NUMBER

ACCOUNT

ATTACHMENT C: Property and Vicinity Photos



Subject Property – facing northwest



Subject Property



Existing sidewalk along 200 South – facing west



Existing single-family homes along 200 South – facing north



Properties across the street on 1000 West – facing east



Subject Property looking west – alleyway access is to the right



Existing alleyway (16 feet wide)



Existing residential garages along the alley

ATTACHMENT D: TSA Zoning Standards

TSA Transit Station Area District – 21A.26.078

Purpose Statement: The purpose of the TSA Transit Station Area District is to provide an environment for efficient and attractive transit and pedestrian oriented commercial, residential and mixed use development around transit stations. Redevelopment, infill development and increased development on underutilized parcels should include uses that allow them to function as part of a walkable, Mixed Use District. Existing uses that are complementary to the district, and economically and physically viable, should be integrated into the form and function of a compact, mixed use pedestrian oriented neighborhood. Each transit station is categorized into a station type. These typologies are used to establish appropriate zoning regulations for similar station areas. Each station area will typically have two (2) subsections: the core area and the transition area. Due to the nature of the area around specific stations, the restrictions of Overlay Zoning Districts, and the neighborhood vision, not all station areas are required to have a core area and a transition area.

Transition Area: The purpose of the transition area is to provide areas for a moderate level of land development intensity that incorporates the principles of sustainable transit oriented development. The transition area is intended to provide an important support base to the core area and transit ridership as well as buffer surrounding neighborhoods from the intensity of the core area. These areas reinforce the viability of the core area and provide opportunities for a range of housing types at different densities. Transition areas typically serve the surrounding neighborhood and include a broad range of building forms that house a mix of compatible land uses. Commercial uses may include office, retail, restaurant and other commercial land uses that are necessary to create mixed use neighborhoods.

TSA-UN-T Zoning Standards
Maximum Building Height – No Minimum, Maximum of 50 feet
Discussion: Proposed building height of 37 feet (Buildings 1 and 2) and 28’6” (Buildings 3 and 4).
Condition(s):
Finding: <input checked="" type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input type="checkbox"/> Does not comply <input type="checkbox"/> Not Applicable
Front/Corner Setbacks – No minimum – At least 50% of the street facing building façade shall be within 5 feet of the front or corner property line
Discussion: The applicant is requesting to increase the maximum allowable setback to 7’3” from the front property line. The applicant is asking to modify the side yard setback standard by 2’3” through the Planned Development process.
Condition(s):

Finding: ☐ Complies ☐ Complies with conditions ☒ Does not comply (Modification Requested)
☐ Not Applicable

Interior Side/Rear Yard Setbacks – No minimum

Except a 25 foot setback is required when adjacent to an OS, R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 Zoning District. The minimum shall increase 1 foot for every 1 foot increase in building height above 25 feet and is applied to the portion of the building over 25 feet in height.

Discussion: The side yard setback of the building with frontage on 1000 West is 17 feet. The property abuts a 16 foot alley that separates it from an RMF-35 zone. The applicant is requesting an 8 foot side yard setback reduction, which staff supports. The development does not have a rear yard setback adjacent to the RMF-35 zone. The applicant is asking to modify the side yard setback standard by 8 feet through the Planned Development process.

Condition(s):

Finding: ☐ Complies ☐ Complies with conditions ☒ Does not comply (Modification Requested)
☐ Not Applicable

Lot Size – Minimum: 2,500 square feet

Lots subdivided for single-family attached dwellings are exempt from minimum lot area provided that:

1. Parking for units shall be rear loaded and accessed from a common drive shared by all units in a particular development;
2. Driveway access shall connect to the public street in a maximum of two (2) locations; and
3. No garages shall face the primary street and front yard parking shall be strictly prohibited.

Discussion: Lot size: Approximately 23,311 square feet

1. The parking for the units has one access point off of the shared alley.
2. There are no driveways, but the alley access connects to 1000 West.
3. The garages are located underground. The parking garage level does protrude 4 feet above grade, but it meets the definition of “basement”, which is defined as “A story wherein each exterior wall is fifty percent (50%) or more below grade. For purposes of establishing building height, a basement shall not count toward the maximum number of stories allowed. The exposed portion of the basement wall shall not exceed five feet (5').”

Condition(s):

Finding: ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

Open Space – One square foot for every 10 feet of land area, up to 2,500 SF for transition areas. Open space areas include landscaped yards, patios, public plazas, pocket parks, courtyards, rooftop and terrace gardens and other similar types of open space area amenities. All required open space areas shall be accessible to the users of the building(s).

Discussion: The project has a shared courtyard that is 5,756 square feet and encompasses 25% of the land area.

<p>Condition(s): The applicant is requesting Planned Development approval for additional open space.</p>
<p>Finding: <input type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input checked="" type="checkbox"/> Does not comply (Modification Requested) <input type="checkbox"/> Not Applicable</p>
<p>Circulation & Connectivity – Development within the station area shall be easily accessible from public spaces and provide safe and efficient options for all modes of travel. Circulation networks, whether public or private, require adequate street, pedestrian and bicycle connections to provide access to development. The internal circulation network shall be easily recognizable, formalized and interconnected.</p>
<p>Discussion: The development has pedestrian connections to the interior courtyard. The circulation is interconnected and easily recognizable for residents. There is only one vehicular access point off 1000 West to access the underground garage.</p>
<p>Condition(s):</p>
<p>Finding: <input checked="" type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input type="checkbox"/> Does not comply <input type="checkbox"/> Not Applicable</p>
<p>Off Street Parking & Loading (21A.44.030(H): TSA Transition Zone Minimum Parking: 50% of the required parking in table 21A.44.030, which equates to 1 stall per single-family attached dwelling unit. TSA Transition Zone Maximum Parking: 1 ½ spaces per dwelling unit.</p>
<p>Discussion: The applicant is proposing 2 stalls per three-bedroom unit and 1 stall per two-bedroom unit for a total of 24 stalls, which is the maximum number allowed for 16 units.</p>
<p>Condition(s):</p>
<p>Finding: <input checked="" type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input type="checkbox"/> Does not comply (Modifications Requested) <input type="checkbox"/> Not Applicable</p>
<p>Landscaping & Buffering (21A.48) Lots in the TSA District which abut a lot in an OS, R-1, R-2, SR, RMF-30, RMF-35 or RMF-45 District shall provide a ten foot (10') landscape buffer. "An area of natural or planted vegetation adjoining or surrounding a land use and unoccupied in its entirety by any building, structure, paving or portion of such land use, for the purposes of screening and softening the effects of the land use."</p>
<p>Discussion: The applicant is requesting to reduce the required 10-foot setback through the Planned Development process. Building's 3 and 4 will be setback from the northern property line by approximately 25 feet and setback from the RMF-35 zone by approximately 41 feet.</p>
<p>Condition(s):</p>
<p>Finding: <input checked="" type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input checked="" type="checkbox"/> Does not comply (Modifications Requested) <input type="checkbox"/> Not Applicable</p>

TSA Design Standards

1. Development shall comply with the design standards in chapter 21A.37 of this title when applicable as specified in that chapter.

Discussion: The applicant complies with all the Design Standards in 21A.37 besides the street facing ground floor glass requirement. The applicant is requesting to have less than required 45%, which requires Design Review approval.

Condition(s):

Finding: ☐ Complies ☐ Complies with conditions ☒ Does not comply (Modification Requested)
☐ Not Applicable

2. All developments required to obtain a review score by subsection C of this section shall comply with the following additional design standards. These specific standards may be modified through the design review in chapter 21A.59 of this title if the modifications meet the intent of the specific design standard requested to be modified:

- a. EIFS And Stucco Limitation: Use of Exterior Insulation and Finishing System (EIFS) or traditional stucco is not allowed as a building material on the ground floor of street facing building facades. Use of EIFS and stucco is allowed for up to ten percent (10%) of the upper level street facing facades.
- b. Front And Corner Side Yard Design Requirements:
 - (1) In yards greater than ten feet (10') in depth, one shade tree shall be planted for every thirty feet (30') of street frontage. For the purpose of this section, a shade tree is any tree that has a mature minimum tree canopy of thirty feet (30') and a mature height that is forty feet (40') or greater.
 - (2) At least fifty percent (50%) of the front or corner side yards shall be covered in live plant material. This can include raised planter boxes. This percentage can be reduced to thirty percent (30%) if the yard includes outdoor dining, patios, outdoor public space, or private yards for ground floor residential uses that cover at least fifty percent (50%) of the provided front or corner side yard.
 - (3) At least thirty percent (30%) of the front or corner side yard shall be occupied by outdoor dining areas, patios, outdoor public space, or private yards for ground floor residential uses.
 - (4) Driveways necessary for vehicle access to the site are allowed regardless of compliance with the minimum percentages required by this subsection.
- c. Entry Feature Requirements: All required building entries shall include at least one of the following features:
 - (1) An awning or canopy over the entrance that extends a minimum of five feet (5') from the street facing building facade;
 - (2) A recessed entrance that is recessed at least five feet (5') from the street facing facade;
 - (3) A covered porch that is at least five feet (5') in depth and at least forty (40) square feet in size; or

- (4) A stoop that is at least two feet (2') above sidewalk level and that includes an awning or canopy that extends at least three feet (3') from the street facing building facade.

Discussion:

The proposal meets the additional design standards referred to in this section.

a. There is no EIFS or stucco proposed on the ground floor.

b. There are no front or corner side yards exceeding 10 feet in depth and at least 50% of the yard area is landscaped. There is one vehicular access point proposed, which does not conflict with the percentages of required landscaping.

c. Each building entry consists of a stoop that is approximately 4 feet above the sidewalk. The porch canopy extends the length of the building and is 3'6" in depth.

Condition(s):

Finding: ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

ATTACHMENT E: Planned Development Standards

Planned Development Standards

21A.55.050: Standards for Planned Developments: The planning commission may approve, approve with conditions, or deny a planned development based upon written findings of fact according to each of the following standards. It is the responsibility of the applicant to provide written and graphic evidence demonstrating compliance with the following standards:

A. Planned Development Objectives: The planned development shall meet the purpose statement for a planned development (section 21A.55.010 of this chapter) and will achieve at least one of the objectives stated in said section. To determine if a planned development objective has been achieved, the applicant shall demonstrate that at least one of the strategies associated with the objective are included in the proposed planned development. The applicant shall also demonstrate why modifications to the zoning regulations are necessary to meet the purpose statement for a planned development. The Planning Commission should consider the relationship between the proposed modifications to the zoning regulations and the purpose of a planned development, and determine if the project will result in a more enhanced product than would be achievable through strict application of the land use regulations.

Planned Development Purpose Statement: A planned development is intended to encourage the efficient use of land and resources, promoting greater efficiency in public and utility services and encouraging innovation in the planning and building of all types of development. Further, a planned development implements the purpose statement of the zoning district in which the project is located, utilizing an alternative approach to the design of the property and related physical facilities. A planned development incorporates special development characteristics that help to achieve City goals identified in adopted Master Plans and that provide an overall benefit to the community as determined by the planned development objectives. A planned development will result in a more enhanced product than would be achievable through strict application of land use regulations, while enabling the development to be compatible with adjacent and nearby land developments.

Discussion: The proposed Planned Development achieves the Housing objective by providing different housing choices than what is currently available and by selling at least 20% of the units to those who make 80% or less of the area median income.

Finding: ☒ Meets Purpose Statement ☐ Does Not Meet Purpose Statement

- A. Open Space And Natural Lands:** Preserving, protecting or creating open space and natural lands:
1. Inclusion of community gathering places or public recreational opportunities, such as new trails or trails that connect to existing or planned trail systems, playgrounds or other similar types of facilities.
 2. Preservation of critical lands, watershed areas, riparian corridors and/or the urban forest.

3. Development of connected greenways and/or wildlife corridors.
4. Daylighting of creeks/water bodies.
5. Inclusion of local food production areas, such as community gardens.
6. Clustering of development to preserve open spaces.

Discussion: The applicant is not intending to meet this objective. Only one Planned Development objective must be fulfilled.

Finding: ☐ Objective Satisfied ☒ Objective Not Satisfied

B. Historic Preservation:

1. Preservation, restoration, or adaptive reuse of buildings or structures that contribute to the character of the City either architecturally and/or historically, and that contribute to the general welfare of the residents of the City.
2. Preservation of, or enhancement to, historically significant landscapes that contribute to the character of the City and contribute to the general welfare of the City's residents.

Discussion: The applicant is not intending to meet this objective. Only one Planned Development objective must be fulfilled.

Finding: ☐ Objective Satisfied ☒ Objective Not Satisfied

C. Housing: Providing affordable housing or types of housing that helps achieve the City's housing goals and policies:

1. **At least twenty percent (20%) of the housing must be for those with incomes that are at or below eighty percent (80%) of the area median income.**
2. **The proposal includes housing types that are not commonly found in the existing neighborhood but are of a scale that is typical to the neighborhood.**

Discussion: At least 20% of the housing units will be sold to those with incomes that are at least 80% of the area median income. The proposal consists of single-family attached units, which are uncommon in the area but compatible with the scale of existing development and the surrounding zoning. While the TSA Transition zone allows a building height of up to 50 feet, the units are proposed to be a maximum of 37 feet. The abutting RMF-35 zone allows up to 35 feet by right.

Finding: ☒ Objective Satisfied ☐ Objective Not Satisfied

D. Mobility: Enhances accessibility and mobility:

1. Creating new interior block walkway connections that connect through a block or improve connectivity to transit or the bicycle network.
2. Improvements that encourage transportation options other than just the automobile.

Discussion: The applicant is not intending to meet this objective. Only one Planned Development objective must be fulfilled.

Finding: ☐ Objective Satisfied ☒ Objective Not Satisfied

E. Sustainability: Creation of a project that achieves exceptional performance with regards to resource consumption and impact on natural systems:

1. Energy Use And Generation: Design of the building, its systems, and/or site that allow for a significant reduction in energy usage as compared with other buildings of similar type and/or the generation of energy from an on-site renewable resource.
2. Reuse Of Priority Site: Locate on a brownfield where soil or groundwater contamination has been identified, and where the local, State, or national authority (whichever has jurisdiction) requires its remediation. Perform remediation to the satisfaction of that authority.

Discussion: The applicant is not intending to meet this objective. Only one Planned Development objective must be fulfilled.

Finding: ☐ Objective Satisfied ☒ Objective Not Satisfied

F. Master Plan Implementation: A project that helps implement portions of an adopted Master Plan in instances where the Master Plan provides specific guidance on the character of the immediate vicinity of the proposal:

1. A project that is consistent with the guidance of the Master Plan related to building scale, building orientation, site layout, or other similar character defining features.

Discussion: The project helps implement the housing goals within Plan Salt Lake and the North Temple Boulevard Plan. Master Plan Implementation was discussed in Consideration 1 of the staff report.

Finding: ☒ Objective Satisfied ☐ Objective Not Satisfied

B. Master Plan Compatibility: The proposed planned development is generally consistent with adopted policies set forth in the Citywide, community, and/or small area Master Plan that is applicable to the site where the planned development will be located.

Discussion:

Master Plan Compatibility was discussed in Consideration 1 of the staff report. The proposed development is of a scale appropriate to the TSA Transition zone and none of the requested zoning modifications run contrary to the applicable master plans for the area.

Condition(s):

Finding: ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

C. Design And Compatibility: The proposed planned development is compatible with the area the planned development will be located and is designed to achieve a more enhanced product than would be achievable through strict application of land use regulations. In determining design and compatibility, the Planning Commission should consider:

1. Whether the scale, mass, and intensity of the proposed planned development is compatible with the neighborhood where the planned development will be located and/or the policies stated in an applicable Master Plan related to building and site design;

Discussion:

The North Temple Boulevard Plan states that the desired density of Transitional Areas is 30 dwelling units per acre, which the project achieves. While the scale of the development is larger than the existing development pattern, the surrounding TSA-UN-T and RMF-35 properties could develop similarly. The proposed height of the project is between 28'6" or 37 feet (maximum is 50 feet) and the maximum height in the RMF-35 zone is 35 feet.

The proposal complies with the policies within the master plan that states housing should, "Protect the low density enclave on the 1000 West block of Euclid Avenue with transitional regulations on height and bulk of new development" and is comparable to what could be built with the surrounding zoning.

Condition(s):

Finding: ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

2. Whether the building orientation and building materials in the proposed planned development are compatible with the neighborhood where the planned development will be located and/or the policies stated in an applicable Master Plan related to building and site design;

Discussion:

Each of the 4 buildings is oriented towards a public street or alleyway. The proposed front yard setback along 1000 West and 200 South is 7'3" feet, which is smaller than that of the existing single-family homes near the property.

Building materials include brick, metal and fiber cement siding, and concrete. Durable materials (doesn't include glazing) make up 100% of the ground floor and approximately 70-72% of the upper floors. They are appropriate for the area and meet the applicable zoning and design standards.

Condition(s):

Finding: ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

3. Whether building setbacks along the perimeter of the development:

- a. Maintain the visual character of the neighborhood or the character described in the applicable Master Plan.
- b. Provide sufficient space for private amenities.
- c. Provide sufficient open space buffering between the proposed development and neighboring properties to minimize impacts related to privacy and noise.
- d. Provide adequate sight lines to streets, driveways and sidewalks.
- e. Provide sufficient space for maintenance.

Discussion:

The perimeter setback along 1000 West and 200 South maintains the visual character of the neighborhood. Each unit has a front stoop, which creates a more walkable environment. Private amenities include a rooftop deck overlooking the courtyard in the middle of the development.

Buildings 3 and 4 along the alleyway are setback approximately 43 1/2 feet from the property lines to the north and approximately 6 feet from the single-family home to the west. These buildings provide buffering between the height of the existing single-family homes and the 37 foot height of the proposed buildings in the development. The alley facing units do not include a rooftop deck, which also minimizes their impacts to the privacy of surrounding property owners.

A dedicated enclosure adjacent to the alley is provided for trash and recycling. All other maintenance and utility services are within the underground parking garage or within the private garages of each unit.

Condition(s):

Finding: ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

4. Whether building facades offer ground floor transparency, access, and architectural detailing to facilitate pedestrian interest and interaction;

Discussion:

Porches placed close to the public sidewalk, a colonnade, and building materials provide visual interest and facilitate pedestrian interaction. Approximately 21-41% of the front building facades is glass. The applicant is asking for Design Review approval to reduce the glass requirement from 45% because a part of the "ground floor" measurement includes the above grade portion of the garage.

Condition(s):

Finding: ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

5. Whether lighting is designed for safety and visual interest while minimizing impacts on surrounding property;

Discussion:

The lighting plan includes porch, soffit, stairway, garage, and courtyard lighting features. The applicant has stated that it is designed for safety, visual interest, and pedestrian interaction. The lighting plan will be reviewed in detail during the building permit phase of the development.

Condition(s):

Finding: ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

6. Whether dumpsters, loading docks and/or service areas are appropriately screened;

Discussion:

The dumpster and recycling containers are in a dedicated enclosure constructed of concrete and painted steel gates. The enclosure is off of the alley and not visible from the street. The other services areas are located within the underground parking area.

Condition(s):

Finding: ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

7. Whether parking areas are appropriately buffered from adjacent uses.

Discussion:

The 24 parking stalls are located below grade. Approximately 4 feet of the underground parking structure is above grade, which the applicant has stated is necessary due to the slope of the parking garage access. The above grade portion of the garage is buffered by landscaping. There is no surface parking proposed.

Condition(s):

Finding: ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

D. Landscaping: The proposed planned development preserves, maintains or provides native landscaping where appropriate. In determining the landscaping for the proposed planned development, the Planning Commission should consider:

1. Whether mature native trees located along the periphery of the property and along the street are preserved and maintained;

Discussion:

There are no existing trees on the property.

Condition(s):
Finding: <input checked="" type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input type="checkbox"/> Does not comply <input type="checkbox"/> Not Applicable
2. Whether existing landscaping that provides additional buffering to the abutting properties is maintained and preserved;
Discussion: There is no existing landscaping, but the neighbors landscaping along the western property line will be preserved.
Condition(s):
Finding: <input checked="" type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input type="checkbox"/> Does not comply <input type="checkbox"/> Not Applicable
3. Whether proposed landscaping is designed to lessen potential impacts created by the proposed planned development;
Discussion: The proposed landscaping has been reviewed and approved by Urban Forestry. There will be new trees along the park strips and within the courtyard area. Water wise landscaping will also be added in front of the buildings to disguise the parking garage and provide visual interest.
Condition(s):
Finding: <input checked="" type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input type="checkbox"/> Does not comply <input type="checkbox"/> Not Applicable
4. Whether proposed landscaping is appropriate for the scale of the development.
Discussion: Larger scaled trees (Lindens) will be within the park strips and smaller trees (Japanese Lilacs) will be at the 1000 West/200 South intersection to allow for visibility. The interior courtyard also has 11 proposed trees that include Maple and Crab Apple varieties.
Condition(s):
Finding: <input checked="" type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input type="checkbox"/> Does not comply <input type="checkbox"/> Not Applicable

E. Mobility: The proposed planned development supports Citywide transportation goals and promotes safe and efficient circulation within the site and surrounding neighborhood. In determining mobility, the Planning Commission should consider:

1. Whether drive access to local streets will negatively impact the safety, purpose and character of the street;
Discussion: There is one access point to the development along the existing alleyway to the north. Transportation has reviewed and approved the plans.
Condition(s):
Finding: <input checked="" type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input type="checkbox"/> Does not comply <input type="checkbox"/> Not Applicable
2. Whether the site design considers safe circulation for a range of transportation options including: <ul style="list-style-type: none"> a. Safe and accommodating pedestrian environment and pedestrian oriented design; b. Bicycle facilities and connections where appropriate, and orientation to transit where available; and c. Minimizing conflicts between different transportation modes;
Discussion: The perimeter of the development will have a 5-foot wide sidewalk that accesses each unit and the courtyard. Bicycle facilities are included within the underground parking garage and there is only one vehicular access point. The vehicular access point is existing and runs along the rear of the single-family properties to the north. No additional conflicts are expected.
Condition(s):
Finding: <input checked="" type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input type="checkbox"/> Does not comply <input type="checkbox"/> Not Applicable
3. Whether the site design of the proposed development promotes or enables access to adjacent uses and amenities;
Discussion: The surrounding uses are residential and can be accessed via the public sidewalk.
Condition(s):
Finding: <input checked="" type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input type="checkbox"/> Does not comply <input type="checkbox"/> Not Applicable
4. Whether the proposed design provides adequate emergency vehicle access;
Discussion: Emergency vehicular access has been approved by Fire. The townhomes with frontage on the alleyway were reduced from 37 to 28'6" in height to meet Fire requirements. Building permits will be reviewed for compliance.

Condition(s):
Finding: <input checked="" type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input type="checkbox"/> Does not comply <input type="checkbox"/> Not Applicable
5. Whether loading access and service areas are adequate for the site and minimize impacts to the surrounding area and public rights-of-way.
Discussion: The dedicated trash and recycling enclosure is accessed from the alleyway. Negative impacts are not expected.
Condition(s):
Finding: <input checked="" type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input type="checkbox"/> Does not comply <input type="checkbox"/> Not Applicable

F. Existing Site Features: The proposed planned development preserves natural and built features that significantly contribute to the character of the neighborhood and/or environment.
Discussion: The site is undeveloped and does not have any existing landscaping.
Condition(s):
Finding: <input checked="" type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input type="checkbox"/> Does not comply <input type="checkbox"/> Not Applicable

G. Utilities: Existing and/or planned utilities will adequately serve the development and not have a detrimental effect on the surrounding area.
Discussion: Public Utilities has reviewed and approved the plans. The development will be served by existing utilities. A full review of the utility plans will be conducted when the applicant applies for a building permit.
Condition(s):
Finding: <input checked="" type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input type="checkbox"/> Does not comply <input type="checkbox"/> Not Applicable

ATTACHMENT F: Design Review Standards

21A.59.050: Standards for Design Review: In addition to standards provided in other sections of this title for specific types of approval, the following standards shall be applied to all applications for design review:

A. Any new development shall comply with the intent of the purpose statement of the zoning district and specific design regulations found within the zoning district in which the project is located as well as the City's adopted "urban design element" and adopted master plan policies and design guidelines governing the specific area of the proposed development.

Discussion:

The TSA zoning district is intended to support transit and pedestrian oriented commercial, residential, and mixed use development around transit stations. While the subject property is not directly adjacent to transit, it meets the transition area purpose by providing new building forms at a different density than what is expected in the urban core. Master plan policies have been met as discussed in Consideration 1.

Condition(s):

Finding: ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

B. Development shall be primarily oriented to the sidewalk, not an interior courtyard or parking lot.

1. Primary entrances shall face the public sidewalk (secondary entrances can face a parking lot).
2. Building(s) shall be sited close to the public sidewalk, following and responding to the desired development patterns of the neighborhood.
3. Parking shall be located within, behind, or to the side of buildings.

Discussion: The primary building entrance to each of the townhomes faces the public sidewalk. The secondary entrance is off the interior courtyard and a third entrance is from the underground parking garage. The buildings along 1000 West and 200 South have a small setback, which adheres to the goals of the transition area as outlined in the North Temple Boulevard Master Plan (p.52). There is no above ground parking available.

Condition(s):

Finding: ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

C. Building facades shall include detailing and glass in sufficient quantities to facilitate pedestrian interest and interaction.

1. Locate active ground floor uses at or near the public sidewalk.
2. Maximize transparency of ground floor facades.
3. Use or reinterpret traditional storefront elements like sign bands, clerestory glazing, articulation, and architectural detail at window transitions.
4. Locate outdoor dining patios, courtyards, plazas, habitable landscaped yards, and open spaces so that they have a direct visual connection to the street and outdoor spaces.

Discussion: The applicant has requested a reduction in the ground floor glass requirement due to the location of the parking garage, which extends approximately 4 feet above grade. The concrete façade will be softened by landscaping. The ground floor of the buildings offers high transparency into the residential structures. The courtyard is tucked in the middle of the residential structures but can be accessed from 200 South. The applicant is requesting Design Review approval to decrease the amount of ground floor glass from 45% to between 7-41% depending on the façade.

Condition(s):

Finding: ☐ Complies ☐ Complies with conditions ☒ Does not comply (Modifications Requested)
☐ Not Applicable

D. Large building masses shall be divided into heights and sizes that relate to human scale.

1. Relate building scale and massing to the size and scale of existing and anticipated buildings, such as alignments with established cornice heights, building massing, step-backs and vertical emphasis.
2. Modulate the design of a larger building using a series of vertical or horizontal emphases to equate with the scale (heights and widths) of the buildings in the context and reduce the visual width or height.
3. Include secondary elements such as balconies, porches, vertical bays, belt courses, fenestration and window reveals.
4. Reflect the scale and solid-to-void ratio of windows and doors of the established character of the neighborhood or that which is desired in the master plan.

Discussion: The project encompasses four buildings, which measure a maximum of 37 feet in height. The third floor of Buildings 1 and 2 include a rooftop patio that is stepped back from the front façade and provides a private amenity space for residents. The buildings offer high ground floor and upper floor transparency. The maximum building height in the TSA-UN-T zone is 50 feet, which is out of scale with the neighborhoods existing single-family homes. The zoning adjacent to the subject property allows a height of 35 feet, which is comparable to the projects proposed height. The overall scale of the building is reduced through increased setbacks and the third floor stepback.

Condition(s):
Finding: <input checked="" type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input type="checkbox"/> Does not comply <input type="checkbox"/> Not Applicable
E. Building facades that exceed a combined contiguous building length of two hundred feet (200') shall include:
<ol style="list-style-type: none"> 1. Changes in vertical plane (breaks in facade) 2. Material changes; and 3. Massing changes.
Discussion: There is no building façade over 200 feet in length.
Condition(s):
Finding: <input type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input type="checkbox"/> Does not comply <input checked="" type="checkbox"/> Not Applicable

F. If provided, privately-owned public spaces shall include at least three (3) of the six (6) following elements:
<ol style="list-style-type: none"> 1. Sitting space of at least one sitting space for each two hundred fifty (250) square feet shall be included in the plaza. Seating shall be a minimum of sixteen inches (16") in height and thirty inches (30") in width. Ledge benches shall have a minimum depth of thirty inches (30"); 2. A mixture of areas that provide seasonal shade; 3. Trees in proportion to the space at a minimum of one tree per eight hundred (800) square feet, at least two inch (2") caliper when planted; 4. Water features or public art; 5. Outdoor dining areas; and 6. Other amenities not listed above that provide a public benefit.
Discussion: The project contains sitting space and a variety of trees within the courtyard area that exceed the minimum of 1 tree per 800 square feet. The proposal is for 11 trees within the 5,756 square foot courtyard. The courtyard is not intended to be a public space.
Condition(s):
Finding: <input type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input type="checkbox"/> Does not comply <input checked="" type="checkbox"/> Not Applicable

G. Building height shall be modified to relate to human scale and minimize negative impacts. In downtown and in the CSHBD Sugar House Business District, building height shall contribute to a distinctive City skyline.

1. Human scale:

- a. Utilize stepbacks to design a building that relate to the height and scale of adjacent and nearby buildings, or where identified, goals for future scale defined in adopted master plans.
- b. For buildings more than three (3) stories or buildings with vertical mixed use, compose the design of a building with distinct base, middle and top sections to reduce the sense of apparent height.

2. Negative impacts:

- a. Modulate taller buildings vertically and horizontally so that it steps up or down to its neighbors.
- b. Minimize shadow impacts of building height on the public realm and semi-public spaces by varying building massing. Demonstrate impact from shadows due to building height for the portions of the building that are subject to the request for additional height.
- c. Modify tall buildings to minimize wind impacts on public and private spaces, such as the inclusion of a wind break above the first level of the building.

3. Cornices and rooflines:

- a. Cohesiveness: Shape and define rooflines to be cohesive with the building's overall form and composition.
- b. Complement Surrounding Buildings: Include roof forms that complement the rooflines of surrounding buildings.
- c. Green Roof And Roof Deck: Include a green roof and/or accessible roof deck to support a more visually compelling roof landscape and reduce solar gain, air pollution, and the amount of water entering the stormwater system.

Discussion: Additional building height is not being requested., but the residential development utilizes stepbacks that reduce the overall perceived height of the project. The roofline is sloped, which complements the pitched rooflines of the surrounding single-family homes. The stepbacks minimize negative impacts by modulating the building and minimizing the shadows on the public and private realm.

Condition(s):

Finding: ☐ Complies ☐ Complies with conditions ☐ Does not comply ☒ Not Applicable

H. Parking and on site circulation shall be provided with an emphasis on making safe pedestrian connections to the sidewalk, transit facilities, or midblock walkway.

Discussion: Parking is located in a partially underground structure. Sidewalks encircle the property and provide safe pedestrian connections to each dwelling unit. Pedestrian and vehicular conflicts should be reduced by having only one vehicular access point to the garage.

Condition(s):**Finding:** ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

I. Waste and recycling containers, mechanical equipment, storage areas, and loading docks shall be fully screened from public view and shall incorporate building materials and detailing compatible with the building being served. Service uses shall be set back from the front line of building or located within the structure. (See subsection 21A.37.050K of this title.)

Discussion: Waste and recycling containers are along the alleyway and screened from public view. Outdoor mechanical equipment is located on the third story balconies and not visible from the public right of way.

Condition(s):**Finding:** ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

J. Signage shall emphasize the pedestrian/mass transit orientation.

1. Define specific spaces for signage that are integral to building design, such as commercial sign bands framed by a material change, columns for blade signs, or other clearly articulated band on the face of the building.
2. Coordinate signage locations with appropriate lighting, awnings, and other projections.
3. Coordinate sign location with landscaping to avoid conflicts.

Discussion: Other than unit addresses, there is no signage proposed.

Condition(s):**Finding:** ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

K. Lighting shall support pedestrian comfort and safety, neighborhood image, and dark sky goals.

1. Provide street lights as indicated in the Salt Lake City Lighting Master Plan.
2. Outdoor lighting should be designed for low-level illumination and to minimize glare and light trespass onto adjacent properties and uplighting directly to the sky.
3. Coordinate lighting with architecture, signage, and pedestrian circulation to accentuate significant building features, improve sign legibility, and support pedestrian comfort and safety.

Discussion: The applicant provided a lighting and photometric plan. All lighting must comply with the Lighting Master Plan. The applicant stated in their TSA development score application that lighting will be directed toward the building and will highlight pedestrian circulation.

Condition(s):

Finding: ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

L. Streetscape improvements shall be provided as follows:

1. One street tree chosen from the street tree list consistent with the City's urban forestry guidelines and with the approval of the City's Urban Forester shall be placed for each thirty feet (30') of property frontage on a street. Existing street trees removed as the result of a development project shall be replaced by the developer with trees approved by the City's Urban Forester.
2. Hardscape (paving material) shall be utilized to differentiate privately-owned public spaces from public spaces. Hardscape for public sidewalks shall follow applicable design standards. Permitted materials for privately-owned public spaces shall meet the following standards:
 - a. Use materials that are durable (withstand wear, pressure, damage), require a minimum of maintenance, and are easily repairable or replaceable should damage or defacement occur.
 - b. Where practical, as in lower-traffic areas, use materials that allow rainwater to infiltrate into the ground and recharge the water table.
 - c. Limit contribution to urban heat island effect by limiting use of dark materials and incorporating materials with a high Solar- Reflective Index (SRI).
 - d. Utilize materials and designs that have an identifiable relationship to the character of the site, the neighborhood, or Salt Lake City.
 - e. Use materials (like textured ground surfaces) and features (like ramps and seating at key resting points) to support access and comfort for people of all abilities.
 - f. Asphalt shall be limited to vehicle drive aisles.

Discussion: There are no existing street trees or vegetation on the property. The proposed street trees comply with the City's guidelines and were approved by the Urban Forester. The hardscape material is concrete and includes the courtyard and private drive to the underground parking structure. No asphalt will be used on site. Building materials include brick on the ground and second floor with accents of fiber cement siding on the third floor. The building materials are durable and reflect what has been used throughout the Euclid neighborhood.

Condition(s):

Finding: ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

ATTACHMENT G: Subdivision Standards

Standards of Review – Subdivision
A. The subdivision complies with the general design standards and requirements for subdivisions as established in <u>chapter 20.12</u> of this title;
Discussion: The applicant is requesting to modify subdivision and zoning standards through the Planned Development process. The following subdivision modification is proposed for this development: The applicant is requested Planned Development approval for lots without street frontage and to modify perimeter setback standards. Staff supports the requests.
Condition(s):
Finding: <input type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input checked="" type="checkbox"/> Does not comply (Modifications Requested) <input type="checkbox"/> Not Applicable
B. All buildable lots comply with all applicable zoning standards;
Discussion: The lot complies with the applicable zoning standards, such as lot size and width.
Condition(s):
Finding: <input checked="" type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input type="checkbox"/> Does not comply <input type="checkbox"/> Not Applicable
C. All necessary and required dedications are made;
Discussion: No dedications of property are required for this development.
Condition(s):
Finding: <input checked="" type="checkbox"/> Complies <input type="checkbox"/> Complies with conditions <input type="checkbox"/> Does not comply <input type="checkbox"/> Not Applicable
D. Water supply and sewage disposal shall be satisfactory to the public utilities department director;
Discussion: Public Utilities stated, "The water main in 200 South is a 6" main and will likely need to be upgraded. The property currently does not have water or sewer service connections. Additional requirements will be provided in building permit review if the [proposal] is approved."
Condition(s):

Finding: ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

E. Provisions for the construction of any required public improvements, per section 20.40.010 of this title, are included;

Discussion: Engineering has reviewed and approved the preliminary plat. The applicant must complete a Subdivision Improvement Construction Agreement with the Engineering Division for the public improvements to be installed in 1000 West, 200 South, and the public alley. The subdivider will need to execute the agreement prior to recordation of the final plat. The applicant must also obtain a new address certificate.

Condition(s): A Subdivision Improvement Construction Agreement is required.

Finding: ☐ Complies ☒ Complies with conditions ☐ Does not comply ☐ Not Applicable

F. The subdivision otherwise complies with all applicable laws and regulations;

Discussion: The proposal complies with all other applicable laws and regulations, except where modified through the Planned Development.

This project requires a final plat as the recording instrument, the application for final plat must be submitted within eighteen (18) months of preliminary plat approval, per section 20.20.010.

Condition(s):

Finding: ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

G. If the proposal is an amendment to an existing subdivision and involves vacating a street, right of way, or easement, the amendment does not materially injure the public or any person who owns land within the subdivision or immediately adjacent to it and there is good cause for the amendment.

Discussion: The proposal does not involve vacating a street, right of way, or easement.

Condition(s):

Finding: ☒ Complies ☐ Complies with conditions ☐ Does not comply ☐ Not Applicable

ATTACHMENT H: Public Process & Comments

Public Notice, Meetings, Comments

The following is a list of public meetings that have been held, and other public input opportunities, related to the proposed project since the applications were submitted:

- March 2, 2022 – The Poplar Grove Community Council was sent the 45 day required notice for recognized community organizations. The comment period ended on April 18.
- March 3, 2022 - Property owners and residents within 300 feet of the development were provided early notification of the proposal.
- March 23, 2022 – The applicant presented the project to the Poplar Grove Community Council. Formal written comments were not submitted, but the project received support from those in attendance.
- March - May 2022 – The project was posted to the Online Open House webpage.

Notice of the public hearing for the proposal included:

- May 13, 2022
 - Public hearing notice sign posted on the property
- May 13, 2022
 - Public hearing notice mailed
 - Public notice posted on City and State websites and Planning Division list serve

Public Input:

There was one comment submitted in support of the petitions and one comment in opposition. The resident in opposition is concerned over the increase in traffic along the alleyway. The resident in support of the petition believes the requested zoning modifications are in keeping with the vision of the Euclid neighborhood and should be approved.

From: [Esther Stowell](#)
To: [Roman Amanda](#)
Subject: (EXTERNAL) Euclid Corners
Date: Monday, April 18, 2022 1:59:46 PM

Amanda,

Pascal, along with representatives of NeighborWorks presented at our Community Council on the Euclid Corners development.

Though no concerns were given by the members of the public, in attendance on that day, we did get an email from a couple of concerned neighbors with reasonable requests for consideration. Please see below.

Other members discussed a possible naming opportunity for NeighborWorks to offer to the public.

Let us know if you have any questions.

Best,

Esther Stowell, MPA
Chair, PGCC
District 2

----- Forwarded message -----

From: [REDACTED] >
Date: Thu, Mar 10, 2022, 4:53 PM
Subject: Euclid Corners
To: [REDACTED]

Hello Mr. Lopez,

I am forwarding you the letter I recently sent to the planner in charge of the Euclid Corners project on 10th west and 2nd South. Developers are trying so hard to maximize the square feet of their developments that they are filling all allowable space and using proximity to light rail as an excuse to limit parking. The Euclid Corners proposed development is so high that it requires that fire trucks have access within 15 feet of the building thereby removing most of the curbside parking. Please help me to encourage the developer to either lower their building or install sprinklers so that the traditional street parking remains available to those who live there and won't further impact the surrounding neighbors.

Best Regards,
Dan Bethel
[REDACTED]

Dear Ms. Roman:

I own five properties on Euclid Avenue between 10th and 11th west. While I am happy to hear that someone is going to finally develop the old Malt air Lanes site on the corner of 10th West, and Second South, I am very concerned about street parking in the area and the project's parking access. Apparently, due to the supposed proximity of the light rail whose station is one half mile away and across railroad tracks the zoning is allowing units to be built without sufficient parking. One multi-unit has just been completed on Euclid between 9th and 10th west and there is another under construction. Neither have sufficient parking!

The Euclid Corners project would have sufficient street parking if

it weren't for the curbside fire lanes which remove most of the immediate on street parking. Any overflow parking would need to go elsewhere in the neighborhood. That won't be good for the neighbor houses nor for the owners of the Euclid Corners town houses whose vehicles will be so far away from their own curb frontage that the owners will have a difficult time watching or accessing them.

My other concern is that the plan I have seen is accessing the site parking off of the 16-foot-wide alley. I think the developers should be required to use some of their property to widen the alley from 10th West to their parking entrance so that two cars can pass and so that they have additional turning radius for entering their driveway.

1) Please ask the planning commission to require that the developer either lower the height of the development so that the fire lanes aren't required or that they install sprinklers which would also eliminate the fire lane requirement. This is a very ridiculous situation. Curb side parking is a Salt Lake City residential norm. Removing it because the developers want to maximize the square feet of town houses (not a high rise) is borrowing from the future graceful curbside use of those who live there and congesting the neighbor street frontage.

2) Please ask the planning commission to require that the alley accessing the Euclid Corners project parking be widened by at least nine feet in order to allow two autos to pass each other from the street to the parking entrance.

Respectfully yours,
Dan Bethel



YOUR ONLINE OPEN HOUSE

Open Houses are currently being held electronically without an anchor location.

ATTENDING THE ONLINE OPEN HOUSE

This Open House meeting will not have an anchor location. The Planning Division will be hosting an online version of the open house as an opportunity to review each project and provide comments and questions.

To attend, click on the project link for each item below. Your input is valuable!

Here's the email from the resident. It can be attached to our positive recommendation to the City Planner.

PUBLIC COMMENTS AND QUESTIONS

From: [REDACTED]
To: [Roman, Amanda](#)
Subject: Re: (EXTERNAL) Euclid Corners
Date: Thursday, March 10, 2022 5:04:57 PM

Hi Amanda.

I think that if I were the developer, I would widen the alley onto my own property just to make access easier. Their driveway entrance is showing about 130 feet from 10th west. I think if it were closer to the street, it might work but that far away there are likely going to be cars coming down the alley having to figure out who has the right to go first.

Thanks for responding.
Dan

-----Original Message-----

From: Roman, Amanda <Amanda.Roman@slcgov.com>
To: [REDACTED]
Sent: Thu, Mar 10, 2022 2:15 pm
Subject: RE: (EXTERNAL) Euclid Corners

Hi Dan,

Thank you for your comments. I have sent the plans to our Transportation team for their review. I will ask them specifically about the fire lane proposal and see what potential options we have. We don't typically ask that alleys are widened but I will also bring that up with the team. As for other parking, each townhome will have its own underground parking available, which hopefully alleviates a bit of the overall parking concern. This is a TSA – Transit Station Area zone so the parking requirements are less than what other zones require. Although you are correct that this specific piece of property is quite a ways away from actual transit.

I will also include your comments in my staff report for the Planning Commission to review. Please let me know if you have additional questions.

Amanda Roman
Principal Planner
Planning Division
Pronouns she/her/hers

DEPARTMENT of COMMUNITY and NEIGHBORHOODS
SALT LAKE CITY CORPORATION

TEL 801-535-7660
EMAIL amanda.roman@slcgov.com

WWW.SLC.GOV/CAN
www.ourneighborhoodscan.com

Disclaimer: The Planning Division strives to give the best customer service possible and to respond to questions as accurately as possible based upon the information provided. However, answers given at the counter and/or prior to application are not binding and they are not a substitute for formal Final Action, which may only occur in response to a complete application to the Planning Division. Those relying on verbal input or preliminary written feedback do so at their own risk and do not vest any property with development rights.

From: [REDACTED]
Sent: Monday, March 7, 2022 2:58 PM
To: Roman, Amanda <Amanda.Roman@slcgov.com>
Subject: (EXTERNAL) Euclid Corners

Dear Ms. Roman:

I own five properties on Euclid Avenue between 10th and 11th west. While I am happy to hear that someone is going to finally develop the old Malt air Lanes site on the corner of 10th West, and Second South, I am very concerned about street parking in the area and the project's parking access. Apparently, due to the supposed proximity of the light rail whose station is one half mile away and across railroad tracks the zoning is allowing units to be built without sufficient parking. One multi-unit has just been completed on Euclid between 9th and 10th west and there is another under construction. Neither have sufficient parking!

The Euclid Corners project would have sufficient street parking if it weren't for the curbside fire lanes which remove most of the immediate on street parking. Any overflow parking would need to go elsewhere in the neighborhood. That won't be good for the neighbor houses nor for the owners of the Euclid Corners town houses whose vehicles will be so far away from their own curb frontage that the owners will have a difficult time watching or accessing them.

My other concern is that the plan I have seen is accessing the site parking off of the 16-foot-wide alley. I think the developers should be required to use some of their property to widen the alley from 10th West to their parking entrance so that two cars can pass and so that they have additional turning radius for entering their driveway.

1) Please ask the planning commission to require that the developer either lower the height of the development so that the fire lanes aren't required or that they install sprinklers which would also eliminate the fire lane requirement. This is a very ridiculous situation. Curb side parking is a Salt Lake City residential norm. Removing it because the developers want to maximize the square feet of town houses (not a high rise) is borrowing from the future graceful curbside use of those who live there and congesting the neighbor street frontage.

2) Please ask the planning commission to require that the alley accessing the Euclid Corners project parking be widened by at least nine feet in order to allow two autos to pass each other from the street to the parking entrance.

Respectfully yours,
Dan Bethel
[REDACTED]

From: [Jarod Hall](#)
To: [Roman, Amanda](#)
Subject: (EXTERNAL) Euclid Corners
Date: Monday, March 7, 2022 7:03:14 PM

Amanda

Hi I am writing to voice my support of the Euclid Corners project. It feels to me that the minor modifications they are requesting from the zoning code are minor and in keeping with the master plan for the Euclid Neighborhood. It is providing a type of housing that is not very common in the area or the city as a whole. The project is built to a height that is much lower than the allowed zoning, so the reduced setback doesn't seem problematic.

Thanks

Jarod Hall, AIA

Divelept

e:

c:

w: www.divelept.com

ATTACHMENT I: Department Review Comments

This proposal was reviewed by the following departments. Any requirement identified by a City Department is required to be complied with.

Building: Comments provided by Willian Warlick on 3/16/22

The following comments are intended to help prepare for the building permit submittal so that it will comply with the State Construction Code.

Though described as a townhome development, the residential units are interconnected by a parking garage. So, instead of being regulated by the 2015 International Residential Code, this work would be regulated by the 2018 International Building Code.

As shown on drawing sheet C2.01, some exterior stairways and their required landings (IBC 1011.6) are outside the lot line.

Engineering: Comments provided by Scott Weiler on 4/5/22

No objections to the Planned Development/Design Review applications. Engineering will be submitting separate comments regarding the preliminary plat.

Fire: Comments provided by Douglas Bateman on 3/22/22

*It appears that the 2 duplex units may be greater than 30-feet in height and would need to be provided with aerial apparatus access from a road at least 26-feet in width. The road shall be positioned parallel to one entire side of the building and located not less than 15-feet or greater than 30-feet away. There shall be no overhead obstructions between the road and the building. The measurements are taken from grade to the eave of a pitched roof or top of parapet wall.

*It appears that fire sprinklers are being provided, but unclear to which buildings. The riser is located on the building along 200 south. Please clarify what type of system is being provided and to which buildings.

Sustainability:

No comments provided.

Transportation: Comments provided by Kevin Young on 4/7/22

- The existing curb locations on 200 S and 1000 W, adjacent to this project, are to remain.
- No cut-back parking will be allowed in the existing park strip areas of 200 S and 1000 W.

Police: Comments provided by LaMar Ewell on 3/2/22

Police Department has no issues or concerns with this development proposal.

Public Utilities: Comments provided by Jason Draper on 4/5/222

The following comments do not provide official project review or approval. Planned Development approval does not provide building or utility permit approval. Comments are provided to assist in design and development by providing guidance for project requirements.

- Site stormwater must be collected on site and routed to the public storm drain system. Stormwater cannot discharge across property lines or public sidewalks.
- All utility design and construction must comply with APWA Standards and SLCPU Standard Practices.
- All utilities must meet horizontal and vertical clearance requirements. Water and sewer lines require 10 ft minimum horizontal separation and 18" minimum vertical separation. Sewer must maintain 5 ft minimum horizontal separation and 12" vertical separation from any non-water utilities. Water must maintain 3 ft minimum horizontal separation and 12" vertical separation from any non-sewer utilities.
- Unused utility connections must be capped at the main.
- Utilities cannot cross property lines without appropriate easements and agreements between property owners.
- Site utility and grading plans will be required for building permit review. Other plans such as erosion control plans and plumbing plans may also be required, depending on the scope of work. Submit supporting documents and calculations along with the plans.
- One culinary water meter is permitted per parcel. If the parcel is larger than 0.5 acres, a separate irrigation meter is also permitted. Fire lines will be permitted, as necessary. Each service must have a separate tap to the main.
- Public Utilities does not support the cutback parking or curb setback on 200 S. Water and Sewer Mains are under this area and reservation of a fire lane would limit maintenance access.
- The water main in 200 South is a 6" main and will likely need to be upgraded.
- The property currently does not have water or sewer service connections.
- Additional requirements will be provided in building permit review if the conditional use is accepted.

Urban Forestry: Comments provided by Rick Nelson 3/9/22

Urban Forestry approves of the number, species, and positions of the trees in this proposal.

Zoning: Comments provided by Amanda Roman

- Add the ground level glass percentage (between 3-8 feet) and clarify the percentage of EIFS on the upper floors. EIFS And Stucco Limitation: Use of EIFS or traditional stucco is not allowed as a building material on the ground floor of street facing building facades. Use of EIFS and stucco is allowed for up to ten percent (10%) of the upper level street facing facades.
- Design Review approval is requested for a reduction in the ground floor glass requirement, which is 45% between 3-8 feet above grade for residential uses. The 1000 West elevation has 39.8% ground floor glass, the 200 South elevation has 30% ground floor glass, and the two alley elevations have 21.5% ground floor glass.
- Step in the railing (#14) so it not on top of the perimeter retaining wall. Add height measurements to both the wall and fence. Walls/fences cannot be over 4 feet between the front façade and front property line and no more than 6 feet along the side and rear.
- Provide the width of the perimeter sidewalks on the site plan. It appears that your proposing narrower sidewalks than what is existing. The minimum is 4 feet, but we'd like to see consistency between this development and surrounding properties.
- Provide the width of the covered porch overhang on the building elevations. I don't think it is an issue but the entry feature requirement states covered porches must be 5 feet in depth so I just need to double check.