



Staff Report

PLANNING DIVISION
COMMUNITY & NEIGHBORHOODS

TO: Salt Lake City Planning Commission
FROM: Eric Daems, AICP, Principal Planner
DATE: January 8th, 2020
RE: PLNPCM2017-00753- Off-Street Parking, Mobility, and Loading Ordinance Amendment

ZONING TEXT AMENDMENT

PROPERTY ADDRESS: City-Wide
MASTER PLAN: Plan Salt Lake
ZONING DISTRICTS: All

REQUEST:

A request by Mayor Jackie Biskupski to review and modify Zoning Ordinance Chapter 21A.44 Off-Street Parking, Mobility, and Loading. The overall goal of the project is to make the parking chapter more user friendly while still accomplishing related citywide goals related to economic development, sustainability, and land use. The proposed text amendments to the Off-Street Parking Ordinance include:

1. Updated parking requirements to better reflect current market demand in the City based on community feedback and previous parking studies commissioned by the City and RDA;
2. Simplify confusing parking regulations that are difficult for property owners to understand and use significant staff resource to interpret and administer;
3. Address technical issues that have been identified through the day to day administration of the parking chapter; and
4. Establish a framework that allows for a parking ordinance that can be responsive to the changing dynamics of Salt Lake City's development patterns.

RECOMMENDATION:

Based on the information in this staff report and the factors to consider for zoning text amendments, Planning Staff recommends that the Planning Commission forward a positive recommendation to the City Council to adopt the proposed zoning ordinance text amendments with the following condition:

1. Ordinance language be amended as necessary to ensure consistency with other code sections and references in the zoning ordinance.

ATTACHMENTS:

- A. Petition Initiation
- B. Proposed Parking Ordinance

- C. Off-Street Parking Manual
- D. Parking Context Map
- E. Analysis of Standards
- F. Master Plan Compatibility
- G. Public Process Timeline
- H. Public Comments
- I. City Department Comments

BACKGROUND:

As transportation and land uses change over time, the demand for parking changes. Cities frequently struggle to strike a balance between too much parking and inadequate parking. Parking requirements that are too high can waste land, increase development costs, lead to demolition of structures to meet parking requirements, increase stormwater runoff, compromise water quality, and discourage pedestrian activity. Parking requirements that are too low may lead to increased traffic congestion, difficulty leasing or selling property, and spillover parking onto adjacent residential streets.

In June 2017, the Planning Division hired consulting firm Clarion and Associates to perform a comprehensive review and update of Chapter 21A.44 Off-Street Parking, Mobility, and Loading of the zoning ordinance. The provisions of this chapter determine the parking regulations in all areas of the City, but do not include regulations for on-street parking. The process included internal meetings with City divisions most closely involved with the parking chapter and a thorough public engagement plan that is outlined in Attachment G of this report. Following the completion of the work of the consultant, Planning Staff worked to address commentary received, finish the public engagement efforts, and to produce a fully revised parking ordinance. The proposed revisions are primarily located within Chapter 21A.44, but other sections of the zoning code related to parking are also proposed to be amended.

Project Scope:

This project updates the City’s regulations for off-street parking including:

- Minimum and maximum number of parking stalls required/allowed
- Permitted alternatives to off-street parking requirements
- Parking lot design, access, and dimensional standards

Purpose:

Implement citywide goals related to economic development, sustainability, and land use including:

- Create parking regulations that reflect current market demand in the City
- Reinforce Salt Lake City as a place for people, not cars
- Eliminate barriers to economic growth and affordable/sustainable housing
- Allow for flexibility
- Reduce auto dependency – encourage safe and efficient alternatives
- Protect neighborhoods
- Minimize visual impacts of parking (surface and structured)
- Minimize pedestrian conflicts with vehicles
- Be environmentally friendly (emissions, water quality, heat island)

PROPOSED AMENDMENTS:

General Comments

The following sections introduce the proposed chapter 21A.44 and highlight significant changes. These changes are based on the cumulative feedback of the community and stakeholders, internal staff discussions, feedback from the Planning Commission, objectives identified in Salt Lake City's various master plans, recommendations from project consultant Clarion & Associates, and industry best practices. The proposed ordinance is included in Attachment B. A version of the proposed ordinance which includes comprehensive footnotes documenting each proposed revision is available upon request from the Salt Lake City Planning department.

21A.44.010: Purpose

This section outlines the objectives of the off-street parking chapter. The language has been updated from the previous ordinance to include the stated purpose of:

- A.** Avoiding and mitigating traffic congestion and reducing the financial burden on taxpayer funded roadways;
- B.** Providing necessary access for service and emergency vehicles;
- C.** Providing for safe and convenient interaction between vehicles, bicycles, and pedestrians;
- D.** Providing flexible methods of responding to the transportation and access demands of various land uses in different areas of the city;
- E.** Reducing storm water runoff, reducing heat island effect from large expanses of pavement, improving water quality, and minimizing dust pollution;
- F.** Establishing context-sensitive parking standards to reflect the current and future built environment of neighborhoods; and
- G.** Avoiding and mitigating the adverse visual and environmental impacts of large concentrations of exposed parking.

21A.44.020: Applicability

This section establishes the thresholds and requirements for when developments are required to comply with the parking regulations. All new development is required to comply.

Expansions

The current standards require compliance with the parking regulations for any expansions – large or small. A low threshold tends to discourage small expansion projects as the cost to improve and/or expand the parking facilities may outweigh the benefits of expanding the building or use. The proposed expansion threshold would require expansions (and cumulative expansions over a two-year period) that are larger than 25 percent of usable floor area to come into compliance with the parking regulations. Expansions less than 25 percent would not be required to comply with the proposed provisions. Developments would also be required to comply with the addition of one or more dwelling units, and the addition to or expansion of one or more structures that require conditional use permit approval.

Change of Use

This section proposes significant changes to the applicability thresholds for when a property changes from one type of land use to another. The current zoning code exempts development in the D-1, D-2, and D-3 zoning districts from needing to provide additional parking as a result of a change of use. To allow for broader flexibility and to encourage infill development and redevelopment, this exemption has been expanded to include all developments within the Urban Center Context and Transit Context areas.

Any change of use outside of the Urban Center Context area or Transit Center Context area that would require an increase in the minimum number of off-street parking spaces by 10 or more spaces or by 25 percent or more spaces, would be required to provide additional parking in compliance with the parking regulations.

Older buildings (built prior to 1944) would not require additional parking to be provided for changes in use. This provision is intended to encourage adaptive reuse of older buildings.

Exemptions from Parking Requirements

This section also introduces changes to which developments are exempt from parking requirements all-together. The current zoning code exempts nonresidential uses in buildings smaller than 1,000 square feet within commercial districts and the D-2 and D-3 zoning districts from having to provide parking. This exemption is now expanded to apply city-wide to all uses on lots (other than single-family or two-family dwellings) created prior to April 12, 1995 that are smaller than 5,000 square feet. This adds another level of flexibility and relief for small property and business owners that would otherwise not be able to use or develop the lot due to parking constraints.

Any development that is exempt from providing parking, but that elects to provide parking, will be required to comply with all location and design standards adopted by the City.

21A.44.030: Calculation of Parking

This section explains how parking and loading requirements are calculated in the proposed parking chapter as well as identifies which types of parking spaces do not count toward minimum and maximum parking space requirements. This section has been mostly carried forward from the current code, with grammatical and formatting edits. All parking and loading requirements based on square footage are calculated using “usable floor area” as is current practice in Salt Lake City. Usable floor area includes all areas of a building with the exception of areas devoted to mechanical equipment and unfinished storage.

The section includes a proposed procedure for how parking and loading requirements are determined for a land use that is not listed in the table of *Minimum and Maximum Off-street Parking* requirements. The current zoning ordinance assigns a “catch-all” minimum parking requirement of three (3) spaces per 1,000 square feet for “all other uses.” The proposed section retains that minimum and adds a maximum parking allowed requirement of five (5) spaces per 1,000 square feet. Two additional means have also been introduced by which parking requirements can be assigned to an unlisted use. The Planning Director now has the authority to assign a minimum or maximum number of off-street parking spaces required for an unlisted use based on a listed use with similar operating characteristics, occupancy classification or other factors. The Director can also determine the parking and loading requirements for any use based on a parking study submitted by the applicant that demonstrates the anticipated demand for the proposed development.

21A.44.040: Required Off-street Parking

Context Areas

The current parking ordinance largely treats minimum parking required and maximum parking allowed based solely on the use of the property. Yet, Salt Lake City has a wide variety of development contexts that make any single approach to minimum and maximum parking requirements ineffective. The parking demand for a downtown area served by transit will be lower

than a downtown adjacent neighborhood or suburban shopping center. To ensure that minimum and maximum parking requirements reflect the built context (and future built context) of the area, the proposed parking ordinance includes four distinct “context areas” with minimum and maximum parking standards tailored to each. The *Minimum and Maximum Off-street Parking Table* lists the specific zoning districts included in each context area. The following is a brief narrative introducing each context area:

General Context: This context includes the City’s zoning districts that tend to be more auto dependent and/or suburban in scale and parking needs. This context applies broadly to all zoning districts that are not specifically listed in the other context areas. Areas that fall into this category are the 300 West commercial corridor, the Redwood Road commercial corridor, and other developments that are in zoning districts not identified in a specific context area in the *Minimum and Maximum Off-Street Parking Table*.

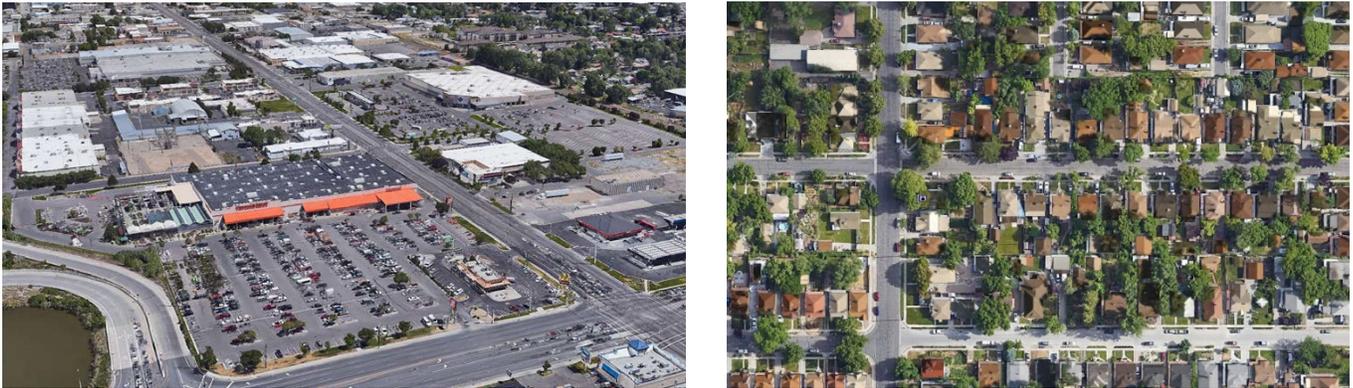


Image 1: Typical Development Patterns within General Context

Neighborhood Center: This context includes areas with small- or moderate-scale shopping, gathering, or activity spaces, often within or adjacent to General Context areas, but that are not necessarily well served by transit. This category includes zoning districts with pedestrian-scale development patterns, building forms, and amenities. Areas that fall into this category are the 9th and 9th commercial node, the 15th and 15th commercial node, and other moderate scale commercial and mixed-use developments that are within the zoning districts identified in the *Minimum and Maximum Off-Street Parking Table*.

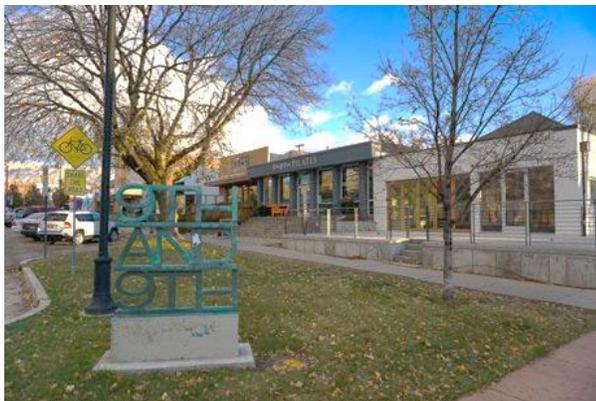


Image 2: Areas such as 9th and 9th are included as part of the Neighborhood Center Context

Urban Center: This context includes zoning districts with dense, pedestrian-oriented development within more intensely developed urban centers. The parking demand in this context is higher than in the Transit Center Context, but lower than areas in the Neighborhood Center Context. Areas that fall into this category are the Sugar House Business District, areas adjacent to Downtown, and other developments that are within the zoning districts identified in the *Minimum and Maximum Off-Street Parking Table*.



Image 3: Areas such as Sugar House are included as part of the Urban Center Context

Transit Context: This context includes those zoning districts that immediately surround mass-transit facilities and/or are in the downtown core. These areas have the lowest parking demand and may be exempt from minimum parking requirements or be required to provide minimal off-street parking. Areas that fall into this category are the Central Business District, Central Ninth, the North Temple/400 South transit corridor, and other developments that are within the zoning districts identified in the *Minimum and Maximum Off-Street Parking Table*.



Image 4: Areas well serviced by mass-transit are included as part of the Transit Context

Required Parking Table

This section replaces and consolidates the current tables in Section 21A.44.030.G(1): Schedule of Minimum Off-street Parking Requirements; Section 21A.44.030.G(2): Table of District Specific Minimum Off-street Parking Requirements; and Section 21A.44.030.H(2): Table of District Specific Maximum Parking Allowance. This table includes all of the use types listed in current Section 21A.33: Land Use Tables, making it clear how much parking is required for each land use allowed in the City. To make the table more user-friendly, similar use types have been grouped into categories and subcategories. All land uses have been included in the table. The inclusion of a land use within the parking table does not authorize the use within a zone or context. Section 21A.33 will still be used to authorize land uses within a zoning district.

Parking Minimums

All minimum parking standards have been reviewed against those used in other large but relatively low-density cities, and numerous changes have been made. In many cases the minimum requirements have been reduced or eliminated altogether (Transit/Urban Center contexts), but in a few cases (notably retail and restaurant uses) the exceptionally low standards in the current ordinance have been increased in order to reduce overflow parking in neighborhoods. The concerns of “spill-over” parking within neighborhoods was a major concern brought to light by the public and within the neighborhood master plans.

Parking Maximums

In effort to limit excess parking on a lot, the current parking chapter limits the amount of parking that can be provide on a property to 125% of the minimum parking amount. The current 125 percent maximum parking standard has been replaced with tailored maximums, by context, and targeted at the limited number of land uses where excessive parking significantly undermines planning goals aimed at walkability and urbanism. Land uses that are not typically associated with over-parking, such as day cares, parks, warehouses, and several industrial uses, do not have maximum parking requirements in the revised chapter. The maximum parking standards column in the table of *Minimum and Maximum Off-Street Parking* clarifies whether the maximum standard applies to only one context area, a combination of context areas, or to all context areas.

Parking provided in structures such as parking garages is proposed to include maximum parking allowed. Well located and planned parking garages can provide shared parking solutions for multiple properties. The placement and design of parking garages is already governed by the parking chapter and design standards of the zoning code. The intent of this provision is to encourage and facilitate parking solutions that serve multiple properties. Discussions with Downtown Alliance also revealed that national employers may insist on certain parking counts being provided for their employees. In this sense, parking garages can be a tool to incentivize employers to relocate downtown.

Electric Vehicle Parking

These standards were carried forward with minor grammatical and formatting edits. No substantive changes are proposed. Although not included as part of the proposed ordinance, Planning Staff worked closely with Salt Lake City’s Sustainability department on new language and standards for Electric Vehicle Readiness. The intent will be to propose requirements that multi-family developments provide a certain percentage of Electric Vehicle Ready parking stalls at the time of development. The language should be anticipated as part of a future ordinance revision and could be included in the design section of the parking ordinance.

Accessible Parking

These standards were carried forward and simplified with minor grammatical and formatting edits. A proposed standard clarifies that parking areas with four (4) or fewer vehicle parking spaces are not required to identify an accessible space with signs or striping, but that a minimum of one (1) space needs to comply with ADA standard dimensions. The table of *Accessible Parking Required* was simplified.

Bicycle Parking

This section has been thoroughly revised as existing standards were not meeting the goals and objectives listed in the various City master plans. The existing bicycle parking standards are based on a percentage of vehicle parking provided. This meant that the less parking a development provided, whether through reductions or otherwise, the less bicycle parking that was required to be provided. That logic does not match the City's goals to be more bikeable and less dependent on automobiles. The proposed standards are based on use and are categorized by context. They have been compared with cities of similar size and dynamic. When a development provides secure/enclosed bicycle parking, the requirement is reduced by half.

21A.44.050: Alternatives to Minimum and Maximum Parking Calculations

To increase flexibility, this section is proposed to include new tools allowing by-right adjustments to parking requirements as well as authorizing the Planning Director to modify parking requirements based on an approved parking study. The adjustments allowed under this section can be used in any combination to reduce the minimum number of required parking spaces identified in the *Minimum and Maximum Off-street Parking* table by up to 40 percent.

Certain alternatives are proposed to be removed from the current ordinance based on community feedback and internal observation. It was found that certain provisions were not reducing overall parking demand and that the burden was shifting from developers to neighboring properties, including along the public right-of-way. For example, the provision to allow on-street parking proved hard to administer and created a sense of ownership or entitlement to parking that should have been public. Removing the standard would allow for future flexibility within the public right-of-way. Those alternatives that are proposed to be eliminated include: credit for on-street parking, pedestrian friendly amenities, off-site parking (as a reduction tool), and use of excess parking in a park and ride lot.

Many of the items currently listed as Transportation Demand Management (TDM) strategies are now included in other sections of the proposed parking chapter and are not listed in this section.

Shared Parking

These standards explain how much parking is required when two (2) or more uses share a parking area. A new system for calculating parking reductions is introduced that establishes reduction factors based on the land uses rather than the hours of operation (which is difficult to enforce and administer). The current maximum distance allowed for shared parking areas of 500 feet has been increased between 600-1,200 feet, based on parking context and to reflect national trends and Salt Lake City's large block sizes. This proposed approach allows mixed-use development the opportunity to reduce the minimum number of required parking spaces to better reflect the parking demands of a mixed-use development. For reference, example calculations have been provided in the text to help users navigate the proposed methods for determining parking requirements. Standards for required documentation for shared parking facilities are introduced and are intended to simplify administration and avoid continued monitoring of cooperation agreements over time.

Proximity to Fixed-Rail Transit

This standard is proposed to allow all multi-family or commercial properties (not just new development) within one-quarter (1/4) mile of a fixed transit station to reduce the number of minimum required parking spaces by 25 percent (down from 50%). The measurement technique is changed from “based on walking distance” to “measured radially in a straight line.” The reduction is available for all contexts except Transit, as the minimums in that context are already based on their proximity to transit.

Affordable and Senior Housing

These standards would allow for a reduction to the number of minimum parking spaces required if the development provides income restricted and/or age restricted units. The current reduction of 50 percent has been decreased to 25 percent to reflect the already reduced parking requirements and tailored minimums in the *Minimum and Maximum Off-street Parking* table. Eligibility requirements and thresholds are also clarified.

New to this chapter is that qualifying affordable or senior housing projects could reduce their parking by an additional 15 percent when they are located within one-quarter mile of a bus stop that is serviced by a high-frequency bus route.

Car Pool and Carshare Parking

These proposed standards would allow developments with 100 or more parking spaces to count every dedicated van pool space towards seven of the minimum parking spaces required, and every car pool space to count towards three of the minimum parking spaces required. Working with the City Sustainability department, the section is proposed to include a provision to allow for parking lots of any size to count four spaces towards each designated carshare vehicle space.

Valet Parking Services

These provisions would allow for parking stalls be replaced on a one-to-one basis for each valet stall provided. The section clarifies qualifying standards but is largely carried over from the current code.

Parking Study Demonstrating Different Parking Needs

This standard is proposed to allow an applicant to submit a parking study to the Planning Director justifying adjustments to the minimum or maximum required parking standards. This provides a “relief valve” for unique projects that justify alternative parking requirements.

21A.44.060: Parking and Loading Location and Design

The current parking and loading location and design standards are found throughout chapter 21A.44 and other parts of the zoning ordinance. This section proposes to consolidate those standards and update them to reflect the proposed context area approach. Notably, the table for parking setbacks has been reorganized into parking contexts and relocated to this section. Specific design standards for the D-1, D-3, D-4, G-MU, TSA, and parking garages have been relocated to this chapter with minor grammatical modifications. The provisions for recreational vehicle parking have been clarified and are located at the end of this section. Some standards were simplified and/or removed because they were no longer necessary as a result of other edits within the parking chapter. Technical standards were largely moved to the proposed *Parking Standards Manual*.

21A.44.070: Off-street Loading Areas

This section includes the proposed standards for how many off-street loading areas are required for developments. These standards were revised to reflect current trends toward more frequent

deliveries by smaller trucks that do not require large spaces to load or unload without blocking traffic or parking areas. The standards were also clarified to include mixed-use buildings.

21A.44.080: Drive-Through Facilities and Vehicle Stacking Areas

This section includes the standards regulating drive-through facilities and vehicle stacking areas. Standards were largely carried over from the existing code, however drive-through stacking spaces were organized by parking context. As part of this revision, redundant provisions for drive-through facilities found in chapter 21A.40.060 have been proposed to be eliminated.

21A.44.090: Modifications to Parking Areas

Administrative Adjustments

This tool is largely carried over from the existing ordinance and would grant the Planning or Transportation Director the authority to make minor modifications to the standards in the parking chapter based on certain criteria. Examples include modifications to dimensions or geometries of parking, loading, or maneuvering areas.

Special Exceptions

The revised ordinance would eliminate the parking “catch-all” exception (#7), currently found in the Special Exceptions chapter 21A.52, as more specific standards have been incorporated into the provisions throughout. The two special exceptions that would continue to be authorized are for front yard parking and surfacing materials for vehicle and equipment storage.

21A.44.100: Use and Maintenance

This section proposes standards for how parking areas can be used as well as the maintenance requirements. These standards were largely carried forward as-is with minimal edits.

21A.44.110: Nonconforming Parking and Loading Facilities

This section includes the proposed standards addressing nonconforming parking and loading facilities. Chapter 21A.38: *Nonconforming Uses and Noncomplying Structures*, lays the foundation for how nonconformities are addressed in the zoning ordinance; however, there is some overlap with the standards provided in chapter 21A.44 specific to parking and loading facilities. The current 21A.38.070.B is proposed to be deleted as this content is now addressed in the proposed parking chapter. The ordinance includes proposed tools that will provide a level of flexibility that should address any concerns related to the reconstruction of parking and loading areas on challenging sites. It also introduces a standard that allows a site made nonconforming as a result of an acquisition of property by eminent domain for a right-of-way to be deemed lawful and conforming. This provides an outlet for a site that is made non-conforming when land area or setbacks are reduced by circumstances outside of their control.

Parking Standards Manual

City staff has elected to create a new *Parking Standards Manual* (Attachment C) in conjunction with this effort to relocate technical/engineering material from chapter 21A.44 into a technical design manual. This approach would simplify the zoning ordinance and remove details from the code that are of little/no interest to the general public. Including design and engineering minutia in the zoning code makes it more difficult for citizens to navigate and find what they are looking for.

This approach would also allow the City to update minor technical/engineering standards without going through the zoning ordinance amendment procedure. It is important for the City to continue updating its technical standards as research and best practices emerge.

Grammar and Minor Corrections

The proposed ordinance includes updated grammar and formatting throughout. Spelling errors, typos, or grammatical errors from the current regulations have been corrected.

Definitions

The revised chapter includes proposed parking-related definitions that were not previously included in chapter 21A.62: *Definitions*.

KEY CONSIDERATIONS:

The following key considerations have been identified for the Planning Commission’s review and potential discussion.

1. Align with the goals of Plan Salt Lake and the various neighborhood master plans
2. Encourage infill development and redevelopment
3. Simplify to be more user-friendly and easier to implement
4. Modernize to reflect best practices and current market trends for parking
5. Reconsider the current “one-size fits all” approach in favor of “context based” parking
6. Required parking minimums for multi-family developments in General Context
7. Parking Reductions for Developments Adjacent to High-Frequency Bus Stops

Consideration 1: Align with the goals of Plan Salt Lake and the various neighborhood master plans

The various City master plans contain the collective goals and objectives of Salt Lake City. Many of these goals and adopted policies relate to how the City grows and how to provide balanced transportation networks that reduce automobile dependency while supporting economic growth and affordability. Goals of the City also focus on neighborhood vitality, providing transportation choices, and enhancing the public realm through design, architecture, and development that is context sensitive. Throughout the revision process, Staff sought to implement as many objectives within the master plans as possible. A comprehensive analysis of those standards and the proposed changes can be found in Attachment F of this report.

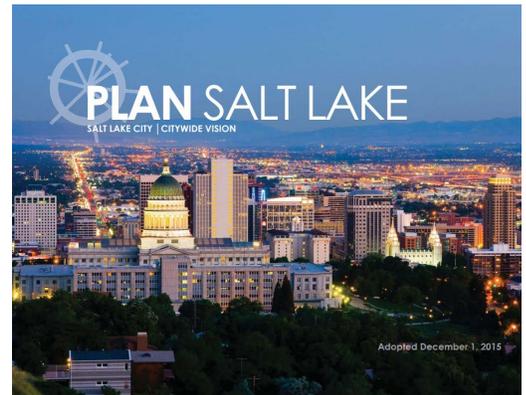


Image 5: Master Plans such as Plan Salt Lake helped frame the proposed ordinance

Consideration 2: Encourage infill development and redevelopment

Infill development and the redevelopment of under-utilized properties are important components for economic growth within an established community like Salt Lake City. The proposed ordinance encourages these types of developments by:

- Establishing context-based parking standards that are responsive to the unique characteristics of Salt Lake City’s neighborhoods and development patterns
- Providing alternative methods to modify minimum and maximum parking
- Allowing parking reductions for affordable/senior housing
- Relaxing parking requirements for change or expansion of use

Consideration 3: Simplify to be more user-friendly and easier to implement

Improved ordinance usability was a focus of the revised parking chapter. The goal was to create a chapter that was clear for the public and developers to use but was also simpler to administer for City Staff. The proposed ordinance has been modified to include:

- Improved ordinance usability through the use of tables and the consolidation of parking standards that were previously scattered throughout the code
- New parking requirement tables with uses organized by category and context
- Simplified processes for adjustments to minor technical matters
- Clarified layout and language throughout

Consideration 4: Modernize to reflect best practices and current market trends for parking

The current parking ordinance for Salt Lake City has not evolved with modern planning practices. Many of the parking count figures are based on outdated models and have not considered the built context or alternative options to traditional parking. The proposed ordinance seeks to implement the latest planning practices and allow for more flexibility based on current market demands for parking. Specifically, the proposed ordinance includes:

- Tailored standards based on four parking contexts
- Market driven minimum and maximums, particularly where mass transit is available
- Adjusted standards for drive-through & loading areas based on best practices
- Revised method of calculating bicycle parking standards to match development activity
- New options for car/van-pool, car share, and shared parking
- Lowered overall required parking in effort to reduce surface area heat gain and water contamination from parking lots and to encourage alternative means of transportation to lower emissions

Consideration 5: Reconsider the current “one-size fits all” approach in favor of “context based” parking

The existing ordinance contains parking minimums and maximums that are largely based only on a proposed land use. In some cases, there were some minor modifications based on the underlying zoning designation, but it was still largely a one-size fits all approach. The idea of standards that were adaptive to their setting or context was a key consideration presented throughout the various master plans of the City and was a common theme throughout the public engagement process. To this end, the revised parking ordinance has been organized to include four parking contexts: Transit Center, Urban Center, Neighborhood Center, and General. These contexts will help the parking standards to be more responsive to the unique circumstances of the various neighborhoods within Salt Lake City. The proposed ordinance also establishes a frame work that would allow for additional parking contexts in the future if needed.

Consideration 6: Required parking minimums for multi-family developments in General Context

Throughout the public engagement process, the minimum parking requirements for multi-family uses was listed at 2 stalls per dwelling unit, regardless of bedroom type. The public was generally supportive of this standard in each of the engagement events held. However, during the work sessions with the Planning Commission, most felt that this number was too high. Staff has now proposed 1 stall per unit for studio and 1-bedroom apartments and 1.25 stalls per unit for units with more than 1 bedroom. Staff felt this would provide some additional parking for residents that have more than 1 vehicle or for guest parking. Furthermore, these would only be the minimums. A development may choose to include surface parking for up to 2 spaces per dwelling unit for studio and 1-bedroom units, or 3 stalls per unit for larger apartments. Typical multi-family developments in the general context are averaging about 1.6 stalls per unit. The proposed standards would still allow for that amount of parking to be provided but would not dictate that it had to be installed if the market demand was for less.

Land Use	Minimum Parking Requirement				Maximum Parking Allowed
	General Context	Neighborhood Center Context	Urban Center Context	Transit Context	
	All zoning districts not listed in another context area	RB, SNB, CB, CN, R-MU-35, R-MU-45, SR-3, FB-UN1, FB-SE	D-2, MU, TSA-T, CSHBD1, CSHBD2	D-1, D-3 D-4, G-MU, TSA-C, UI, FB-UN2, FB-UN3, FB-SC, R-MU	
Vehicle Stacking and Drive-Through Facilities: See 21A.44.040A.6 Outdoor Sales/Display/Leasing/Auction Areas: See 21A.44.040A.7					
Residential Uses					
Household Living					
Artists' loft/studio	1.5 spaces per DU	1 space per DU	0.5 spaces per DU	No Minimum	No Maximum
Manufactured home	2 spaces per DU	1 space per DU	No Minimum	No Minimum	All Contexts: 4 spaces, not including recreational vehicle parking spaces
Mobile home					
Single-family (attached)					
Single-family (detached)					
Single-family cottage development building form	1 space per DU				
Twin home	2 spaces per DU				
Two-family					
Multi-family	Studio and 1 bedrooms: 1 space per DU, 2+ bedrooms: 1.25 space per DU	Studio and 1+ bedrooms: 1 space per DU	Studio: No Minimum 1 bedroom: 0.5 space per DU 2+ bedrooms: 1 space per DU	No Minimum	All Contexts: Studio & 1 Bedroom: 2 spaces per DU 2+ bedrooms: 3 spaces per DU

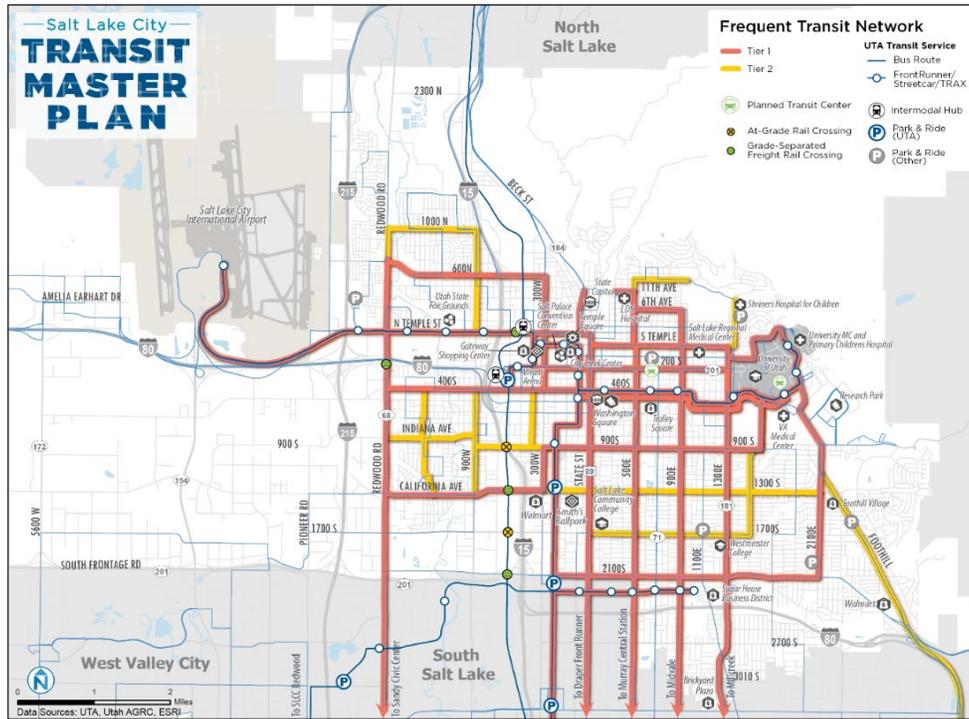
Image 6: Proposed parking for Multi-family in General Context

Consideration 7: Parking Reductions for Developments Adjacent to High-Frequency Bus Stops

This particular consideration has produced a split opinion throughout the revision process. Most residents felt that parking reductions should not be granted alone for proximity to high-frequency bus stops. The reasons cited included that they felt the stops lacked permanency and they feared the service was not reliable enough to be counted on for users other than commuters. This was discussed considerably in the work-sessions with the Planning Commission. Some commissioners agreed with public sentiment, while others felt it would be a lost opportunity considering the amount of investment Salt Lake City has made in these routes, particularly when the other goals of the City, such as improved air quality and providing affordable housing, are considered.

Given these viewpoints, Staff has recommended to tie the reduction specifically to the goal for more affordable housing by allowing for an additional parking reduction of up to 15% for qualifying affordable housing developments located within one-quarter mile of a bus stop that is serviced by high-frequency routes (proposed 21A.44.050.D). Currently, this would include routes 2,9,21, and 200 (see map and chart below). As additional routes improve frequency, including for nights and weekends, they would also qualify for this provision.

PROPOSED FREQUENT TRANSIT NETWORK VISION



Images 7 (map) & 8 (table): Existing and proposed high-frequency bus routes

Bus Service Frequency					
Rte.	Street	Weekday	Sat.	Sun.	After 7PM
2	200 S	15 min	15 min	30 min	30 min
9	900 S	15 min	15 min	30 min	30 min
21	21 st S	15 min	15 min	30 min	30 min
200	State St. North	15 min	15 min	30 min	30 wk/60 Sat
205	500 E	15 min	30 min	60 min	60 min
209	900 E	15 min	30 min	60 min	60 min
217	Redwood Rd.	15 min	30 min	60 min	60 min
220	Highland/1300 E	15 min	30 min	60 min	30 wk./60 Sat

NEXT STEPS:

The City Council has the final authority to make changes to the text of the Zoning Ordinance. The recommendation of the Planning Commission for this petition will be forwarded to the City Council for their review and decision.

ATTACHMENT A: PETITION INITIATION



Petition Initiation Request

**Planning Division
Community & Neighborhoods Department**

To: Mayor Biskupski

From: Nick Norris, Planning Director

Date: September 7, 2017

CC: Patrick Leary, Chief of Staff; Mike Reberg, CAN Director, Mike Akerlow, CAN Deputy Director; file

Re: Initiate Petition to update Zoning Ordinance Chapter 21A.44 Off Street Parking, Mobility and Loading.

In June 2017, the Planning Division finalized an RFP with a consultant (Clarion and Associates) to review and update the off street parking chapter of the zoning ordinance. This update would address the following items:

- Update the parking requirements to better reflect current market demand in the City based on community feedback, and previous parking studies commissioned by the City and RDA;
- Simplify confusing parking regulations that are difficult for property owners to understand and result in an increase amount of staff resource to interpret and administer the regulations; and
- Address technical issues that have been identified through the day to day administration of the parking chapter.

The consultant has started the work on reviewing the parking chapter and is in the process of scheduling stakeholder and community meetings to be held towards the end of September and craft an online public input forum. This project includes staff from multiple City Departments and Divisions, including Community and Neighborhoods (Planning, Housing and Neighborhood Development, Transportation, Building Services) Economic Development (including RDA staff) and Sustainability. It is anticipated that the consultant portion of this project will take approximately 6 months. The Planning Commission process will take approximately 2-3 months. It is unknown how long the City Council process will take.

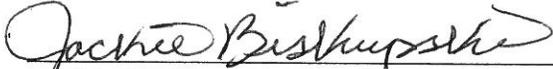
The overall goal of the project is to make the parking chapter more user friendly while still accomplishing related citywide goals related to economic development, sustainability and land use. The Planning Division will follow the regular public review process for this petition including obtaining early public input and conducting hearings with the Planning Commission and City Council. This project will include more public engagement opportunities that include online engagement, and community meetings to help identify current parking issues and solutions, review of draft proposals, and public hearings with the

Planning Commission and City Council. In person and internet based opportunities to provide input will be available to all interested parties.

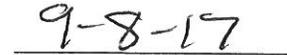
If you have any questions, please contact me at extension 6173 or nick.norris@slcgov.com or JP Goates at extension 7236 or jonathan.goates@slcgov.com.

Thank you.

Concurrence to initiate the zoning text amendment petition as noted above.



Jackie Biskupski, Mayor



Date

ATTACHMENT B: PROPOSED PARKING ORDINANCE

The proposed ordinance revision is largely comprised of a re-written chapter 21A.44- Off-Street Parking, Loading, and Mobility. However, the following related sections of code are also proposed to be altered:

- Parking design elements from the D-1, D-3, D-4, G-MU, and TSA zoning chapters have been relocated to the proposed parking chapter with minor grammatical or other errors corrected
- Special Exception #7 (21A.52) is proposed to be eliminated
- Parking garage design standards (21A.37.050.M) has been relocated to the proposed parking chapter with minor grammatical or other errors corrected
- Some of the drive-through standards found in 21A.40.060 have been relocated to the proposed parking chapter and redundancies have been eliminated



Chapter 21A.44: *Off Street Parking, Mobility, and Loading*

December 2019

(Document format provided for convenience of Planning Commission. Adopted format will be consistent with standard ordinance formatting)

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Chapter 21A.44: Off Street Parking, Mobility, and Loading

21A.44.010 Purpose

This chapter is intended to require that new development and redevelopment projects provide off street parking and loading facilities in proportion to the parking, loading, and transportation demands of the buildings and land uses included in those projects. This chapter is also intended to help protect the public health, safety, and general welfare by:

- A. Avoiding and mitigating traffic congestion and reducing the financial burden on taxpayer funded roadways;
- B. Providing necessary access for service and emergency vehicles;
- C. Providing for safe and convenient interaction between vehicles, bicycles, and pedestrians;
- D. Providing flexible methods of responding to the transportation and access demands of various land uses in different areas of the city;
- E. Reducing storm water runoff, reducing heat island effect from large expanses of pavement, improving water quality, and minimizing dust pollution;
- F. Establishing context-sensitive parking standards to reflect the current and future built environment of neighborhoods; and
- G. Avoiding and mitigating the adverse visual and environmental impacts of large concentrations of exposed parking.

21A.44.020 Applicability

A. Amounts of Parking, Loading, and Drive-Through Facilities Required

The standards of this chapter are intended to establish: minimum and maximum amounts of vehicle parking; minimum required bicycle parking, minimum required loading facilities, and minimum capacity of drive-through facilities and shall apply to projects involving the activities listed below. In some instances, other standards of this chapter provide alternatives for required compliance. Certain exemptions are intended to encourage utilization of existing structures and preserve desirable characteristics of locations built prior to parking requirements.

1. New Development

Unless otherwise exempted by Section 21A.44.020A.4, the standards in this chapter shall apply to all development and land uses upon adoption of this ordinance.

2. Expansion of Use or Structure

The number of off street parking and loading spaces for the expansion of a use or structure shall comply with the requirements of Table 21A.44.040-A: *Minimum and Maximum Off Street Parking* and the standards of this chapter when:

- a. One or more additional dwelling units is created; or
- b. The addition to or expansion of one or more structures or uses that, when considered together with any other expansions during the previous two-year period, would increase the total usable floor area of the structure(s) by more than twenty-five percent (25%); or

- c. The addition to or expansion of one (1) or more structures or uses that requires conditional use permit approval.

3. Change of Use

- a. Except when located within an Urban Center or Transit Context, or as stated in subsection b below, off street parking shall be provided pursuant to this chapter for any change of use that increases the minimum number of required vehicle parking spaces by:
 - (1) More than ten (10) parking spaces; or
 - (2) More than twenty-five percent (25%) of the parking spaces that currently exist on-site or on permitted off-site locations.
- b. For changes in use in buildings built prior to 1944, no additional parking shall be required beyond what is existing.

4. Exemptions from Parking Requirements

The following shall be exempt from providing the minimum parking required by Table 21A.44.040-A: *Minimum and Maximum Off Street Parking*, but shall comply with maximum parking allowed and location and design standards in Section 21A.44.060 if parking is provided:

- a. Lots created prior to April 12, 1995 that are less than five thousand (5,000) square feet in lot area, except those being used for single-family, two-family, and twin home dwelling uses;
- b. Expansions or enlargements that increase the square footage of usable floor area of an existing structure or parking requirements for the use by twenty-five percent (25%) or less, provided that existing off street parking and loading areas are not removed.

B. Location and Design

Section 21A.44.060: *Parking Location and Design*, shall apply to all vehicle parking, bicycle parking, loading, and drive-through facilities, regardless of whether the project is subject to the requirements for additional parking spaces or other facilities pursuant to Section 21A.44.020A above. Parking garages are subject to design standards found in 21A.44.060.A.16 and specific requirements of other zoning districts found in 21A.44.060.B.

21A.44.030 Calculation of Parking

A. Generally

1. All parking and loading requirements that are based on square footage shall be calculated on the basis of usable floor area of the subject use, unless otherwise specified in Table 21A.44.040-A: *Minimum and Maximum Off Street Parking*.
2. Parking spaces shall not be counted more than once for required off-site, shared, and/or alternative parking plans, except where the development complies with off-site, shared, and/or alternative parking standards.
3. Parking spaces designed or designated exclusively for motorcycles, scooters, and other two wheeled vehicles shall not count toward the number of minimum required or maximum allowed off street parking spaces.

4. Parking spaces intended for storage of business vehicles, such as fleet vehicles, delivery vehicles, or vehicles on display associated with sales or rental shall not count toward the number of minimum required or maximum allowed off street parking spaces unless otherwise stated in Table 21A.44.040-A: *Minimum and Maximum Off Street Parking*.
5. Parking spaces designed or designated exclusively for recreational vehicles shall not count toward the number of minimum required or maximum allowed off street parking spaces.
6. When calculations of the number of required off street parking spaces for vehicles or bicycles result in a fractional number, any fraction of 0.5 or larger shall be rounded up to the next higher whole number. Calculations for more than one use in a project shall be calculated for each individual use and may be rounded individually and added, or added then rounded as determined by the applicant.
7. Lots containing more than one (1) use may provide parking and loading based on the shared parking calculations in section 21A.44.050BB: *Shared Parking*.

B. Unlisted Uses

For uses not listed in Table 21A.44.040-A: *Minimum and Maximum Off Street Parking* the Planning Director is authorized to do any of the following:

1. Apply the minimum or maximum off street parking space requirement specified in Table 21A.44.040-A: *Minimum and Maximum Off Street Parking*, for the listed use that is deemed most similar to the proposed use as determined by the Planning Director based on operating characteristics, the most similar related occupancy classification, or other factors related to potential parking demand determined by the Director.
2. Apply a minimum parking requirement of three (3) spaces per one thousand (1,000) square feet of usable floor area for the use and a maximum parking allowance of five (5) spaces per one thousand (1,000) square feet of useable floor area for the use.
3. Establish the minimum off street parking space and loading requirements based on a parking study prepared by the applicant according to Section 21A.44.050F.

21A.44.040 Required Off Street Parking

A. Minimum and Maximum Parking Spaces Required

1. Unless otherwise provided in this Code, each development or land use subject to this chapter pursuant to Section 21A.44.020 shall provide at least the minimum number, and shall not provide more than the maximum number, of off street parking spaces required by Table 21A.44.040-A: *Minimum and Maximum Off Street Parking*.
2. A parking standard shown in Table 21A.44.040-A: *Minimum and Maximum Off Street Parking*, is not an indication of whether the use is allowed or prohibited in the respective zoning district or context area. See Chapter 21A.33: *Land Use Tables* for allowed and prohibited uses.
3. The maximum parking limit does not apply to parking provided in parking garages, stacked or racked parking structures, or to off-site parking that complies with all other requirements of this title.
4. The maximum parking limit does not apply to properties in the M-1, M-2, BP, or Airport zoning districts that are located west of the centerline of Redwood Road.

5. If a conditional use is approved by the Planning Commission in accordance with Chapter 21A.54: *Conditional Uses*, and the conditional use approval states a different parking requirement than that required by this Chapter 21A.44, and is determined necessary to mitigate a detrimental impact, then the parking requirement in the conditional use approval shall apply.
6. All uses with vehicle stacking and/or drive-through facilities shall comply with Section 21A.44.080: *Drive-Through Facilities and Vehicle Stacking Areas*, in addition to the requirements of Table 21A.44.040-A: *Minimum and Maximum Off Street Parking*.
7. All uses with outdoor sales, display, leasing, and/or auction areas shall also provide one-half (1/2) parking space and no more than two (2) parking spaces per one thousand (1,000) sq. ft. of outdoor sales, display, leasing, and/or auction area. This additional parking shall not count toward the maximum allowed per Table 21A.44.040-A: *Minimum and Maximum Off Street Parking*, when a maximum is specified.

Context Approach

Salt Lake City has a wide variety of development contexts that make any single approach to minimum and maximum parking requirements ineffective. The parking demand for a downtown area served by transit will be much lower than a downtown adjacent neighborhood or suburban shopping center. To ensure that minimum and maximum parking requirements reflect the built context (and future built context) of the area, we created four distinct "context areas", and then tailored minimum and maximum parking standards to each. The *Minimum and Maximum Off Street Parking Table* below lists the specific zoning districts included in each context area. The following is a brief narrative introducing each context area:

1. **General Context:** This category includes the City's zoning districts that tend to be more auto-dependent and/or suburban in scale and parking needs. This context applies broadly to all of the zoning districts that are not specifically listed in the other context areas.
2. **Neighborhood Center:** This category includes areas with small- or moderate-scale shopping, gathering, or activity spaces, often within or adjacent to General Context areas, but that are not necessarily well served by transit. This category includes zoning districts with pedestrian-scale development patterns, building forms, and amenities.
3. **Urban Center:** This category includes zoning districts with dense, pedestrian-oriented development within more intensely developed urban centers. The parking demand in this context is higher than in the Neighborhood Center Context, but lower than areas with good transit service.
4. **Transit Context:** This category includes those zoning districts that immediately surround mass-transit facilities and/or are in the downtown core. These areas have the lowest parking demand and may be exempt from minimum parking requirements or be required to provide minimal off street parking.

Table 21A.44.040-A: Minimum and Maximum Off Street Parking

DU = dwelling unit sq. ft. = square feet

Land Use	Minimum Parking Requirement				Maximum Parking Allowed
	General Context	Neighborhood Center Context	Urban Center Context	Transit Context	
	All zoning districts not listed in another context area	RB, SNB, CB, CN, R-MU-35, R-MU-45, SR-3, FB-UN1, FB-SE	D-2, MU, TSA-T, CSHBD1. CSHBD2	D-1, D-3 D-4, G-MU, TSA-C, UI, FB-UN2, FB-UN3, FB-SC, R-MU	

Vehicle Stacking and Drive-Through Facilities: See 21A.44.040A.6

Outdoor Sales/Display/Leasing/Auction Areas: See 21A.44.040A.7

Residential Uses

Household Living

Artists' loft/studio	1.5 spaces per DU	1 space per DU	0.5 spaces per DU	No Minimum	No Maximum
Manufactured home	2 spaces per DU	1 space per DU	No Minimum	No Minimum	All Contexts: 4 spaces, not including recreational vehicle parking spaces
Mobile home					
Single-family (attached)					
Single-family (detached)					
Single-family cottage development building form					
Twin home					
Two-family	2 spaces per DU				
Multi-family	Studio and 1 bedrooms: 1 space per DU, 2+ bedrooms: 1.25 space per DU	Studio and 1+ bedrooms: 1 space per DU	Studio: No Minimum 1 bedroom: 0.5 space per DU 2+ bedrooms: 1 space per DU	No Minimum	All Contexts: Studio & 1 Bedroom: 2 spaces per DU 2+ bedrooms: 3 spaces per DU

Group Living

Assisted living facility	1 space for each 6 infirmary or nursing home beds; plus 1 space for each 4 rooming units; plus 1 space for each 3 DU See Table Note A	1 space for each 6 infirmary or nursing home beds; plus 1 space for each 4 rooming units; plus 1 space for each 3 DU See Table Note A	1 space for each 8 infirmary or nursing home beds; plus 1 space for each 6 rooming units; plus 1 space for each 4 DU See Table Note A	No Minimum	No Maximum
Nursing care facility					
Eleemosynary facility	1 space per 4 persons design capacity; See Table Note A	1 space per 6 persons design capacity; See Table Note A	1 space per 4 persons design capacity; See Table Note A	No Minimum	All Contexts: 1 space per 3 persons design capacity; See Table Note A
Group home					
Residential support					
Dormitory, fraternity, sorority	1 space per 2 persons design capacity	1 space per 3 persons design capacity	1 space per 4 persons design capacity	No Minimum	All Contexts: 1 space per 1 persons design capacity

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Vehicle Stacking and Drive-Through Facilities: See 21A.44.040A.6					
Outdoor Sales/Display/Leasing/Auction Areas: See 21A.44.040A.7					
Rooming (boarding) house	1 space per 2 guest rooms	1 space per 3 guest rooms	1 space per 4 guest rooms	No Minimum	No Maximum
Single room occupancy	0.5 spaces per unit		0.25 spaces per unit	No Minimum	No Maximum
Public, Institutional, and Civic Uses					
Community and Cultural Facilities					
Art gallery	1 space per 1,000 sq. ft.		0.5 spaces per 1,000 sq. ft.	No Minimum	All Contexts: 2 spaces per 1,000 sq. ft.
Studio, Art					
Exhibition hall					
Museum					
Crematorium	2 spaces per 1,000 sq. ft.		1 spaces per 1,000 sq. ft.	No Minimum	No Maximum
Daycare center, adult					
Daycare center, child					
Homeless resource center					
Library					
Community correctional facility,	3 spaces per 1,000 sq. ft.	2.5 spaces per 1,000 sq. ft.			
Community recreation center					
Jail					
Government facility	3 spaces per 1,000 sq. ft. of office area		1 spaces per 1,000 sq. ft. of office area	No Minimum	No Maximum
Social service mission and charity dining hall					
Municipal service use, including city utility use and police and fire station	2 spaces per 1,000 sq. ft. of office area, plus 1 space per service vehicle		1 space per 1,000 sq. ft. of office area, plus 1 space per service vehicle	No Minimum	No Maximum
Club/lodge	1 space per 6 seats in main assembly area	1 space per 8 seats in main assembly area	1 space per 10 seats in main assembly area	No Minimum	All Contexts: 1 space per 4 seats in main assembly area
Meeting hall of membership organization					
Convent/monastery	1 space per 4 persons design capacity	1 space per 6 persons design capacity	1 space per 8 persons design capacity	No Minimum	No Maximum

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Vehicle Stacking and Drive-Through Facilities: See 21A.44.040A.6					
Outdoor Sales/Display/Leasing/Auction Areas: See 21A.44.040A.7					
Funeral home	1 space per 4 seats in main assembly area	1 space per 5 seats in main assembly area	1 space per 6 seats in main assembly area	No Minimum	Urban Center and Transit Center Context: 2 spaces per 4 seats in main assembly areas Neighborhood Center and General Context: No maximum
Place of worship	1 space per 6 seats or 1 space per 300 sq. ft., whichever is less	1 space per 8 seats or 1 space per 400 sq. ft., whichever is less	1 space per 10 seats or 1 space per 500 sq. ft., whichever is less	No Minimum	All Contexts: 1 space per 3.5 seats or 1 space per 200 sq. ft., whichever is greater
Fairground	See Table Note B				No Maximum
Philanthropic use	See Table Note B				All Contexts: 2 spaces per 1,000 sq. ft. of office, plus 1 space per 6 seats in assembly areas
Zoological park	See Table Note B				No Maximum
Ambulance service	No Minimum				
Cemetery					
Plazas					
Park					
Open space					
Educational Facilities					
College and university	2 spaces per 1,000 sq. ft. office, research, and library area, plus 1 space per 6 seats in assembly areas	1 space per 1,000 sq. ft. office, research, and library area, plus 1 space per 10 seats in assembly areas		No Minimum	All Contexts: 4 spaces per 1,000 sq. ft.
K - 12 private	Elementary or Middle: 1 space per 20 students design capacity				
K - 12 public	High Schools: 1 space per 8 students design capacity				
Dance/music studio	3 spaces per 1,000 sq. ft.	2.5 spaces per 1,000 sq. ft.	1 space per 1,000 sq. ft.		
Music conservatory					
Professional and vocational					

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Vehicle Stacking and Drive-Through Facilities: See 21A.44.040A.6					
Outdoor Sales/Display/Leasing/Auction Areas: See 21A.44.040A.7					
Professional and vocational (with outdoor activities)					
Seminary and religious institute					
Healthcare Facilities					
Clinic (medical, dental)	4 spaces per 1,000 sq. ft.		1 space per 1,000 sq. ft.	No Minimum	All Contexts: 6 spaces per 1,000 sq. ft.
Blood donation center	3 spaces per 1,000 sq. ft.		1 space per 1,000 sq. ft.		Transit and Urban Center Context: 3 spaces per 1,000 sq. ft.
Hospital					Neighborhood Center and General Context: 6 spaces per 1,000 sq. ft.
Hospital, including accessory lodging facility	1 space per 3 patient beds design capacity		1 space per 2 patient beds design capacity		All Contexts: 1 space per 2 patient beds design capacity
Commercial Uses					
Agricultural and Animal Uses					
Greenhouse	2 spaces per 1,000 sq. ft.		1 space per 1,000 sq. ft.	No Minimum	Transit and Urban Center Context: 2 spaces per 1,000 sq. ft.
Kennel					
Pound					
Veterinary office					
Cremation service, animal	1 space per 1,000 sq. ft.			No Minimum	Neighborhood Center and General Context: No Maximum
Kennel on lots of 5 acres or larger					
Poultry farm or processing plant					
Raising of furbearing animals					
Slaughterhouse					
Agricultural use	No Minimum				
Community garden					
Farmer's market					
Grain elevator					
Pet cemetery					

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Vehicle Stacking and Drive-Through Facilities: See 21A.44.040A.6					
Outdoor Sales/Display/Leasing/Auction Areas: See 21A.44.040A.7					
Stable	See Table Note B				
Stockyard					
Urban farm					
Botanical garden					
Recreation and Entertainment					
Auditorium	1 space per 4 seats in assembly areas	1 space per 6 seats in assembly areas	1 space per 8 seats in assembly areas	No Minimum	All Contexts: 1 space per 3 seats in assembly areas
Theater, live performance					
Theater, movie					
Amphitheater	See Table Note B				
Athletic Field					
Stadium					
Tennis court (principal use)	2 spaces per court		No Minimum		Transit and Urban Center Context: 2 spaces per court or lane Neighborhood Center and General Context: No Maximum
Bowling	2 spaces per lane				
Convention center	1 space per 1,000 sq. ft.			No Minimum	All Contexts: 3 spaces per 1,000 sq. ft.
Swimming pool, skating rink or natatorium					
Health and fitness facility	2 spaces per 1,000 sq. ft.		1 space per 1,000 sq. ft.		All Contexts: 4 spaces per 1,000 sq. ft.
Performing arts production facility					
Reception center					
Recreation (indoor)	3 spaces per 1,000 sq. ft.	2 spaces per 1,000 sq. ft.			
Recreational vehicle park (minimum 1 acre)	1 space per designated camping or RV spot				No Maximum
Amusement park	See Table Note B				
Recreation (outdoor)	See Table Note B				
Food and Beverage Services					
Brewpub	Indoor tasting/seating area: 2 spaces per 1,000 sq. ft.; Outdoor tasting/seating area:		Indoor tasting/seating	No Minimum	Transit and Urban Center Context: 5 spaces per 1,000

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Vehicle Stacking and Drive-Through Facilities: See 21A.44.040A.6					
Outdoor Sales/Display/Leasing/Auction Areas: See 21A.44.040A.7					
Restaurant	2 spaces per 1,000 sq. ft.		area: 2 spaces per 1,000 sq. ft.; Outdoor tasting/seating area: 1 space per 1,000 sq. ft.		sq. ft indoor tasting/seating area Neighborhood Center and General Context: 7 spaces per 1,000 sq. ft. indoor tasting/seating area
Tavern					All Contexts: Outdoor tasting/seating area: 4 spaces per 1,000 sq. ft.
Social club	1 space per 6 seats in main assembly area, or 1 space per 300 sq. ft., whichever is less	1 space per 8 seats in main assembly area, or 1 space per 400 sq. ft., whichever is less	1 space per 10 seats in main assembly area, or 1 space per 500 sq. ft., whichever is less	No Minimum	All Contexts: 1 space per 4 seats in main assembly area, or 1 space per 200 sq. ft., whichever is greater
Office, Business, and Professional Services					
Check cashing/payday loan business	2 spaces per 1,000 sq. ft.		1 space per 1,000 sq. ft.	No Minimum	General Context: 4 spaces per 1,000
Dental laboratory/ research facility					Neighborhood Center Context: 3 spaces per 1,000
Financial institution					Urban Center and Transit Center Contexts: 2 spaces per 1,000
Research and laboratory facilities					
Office (excluding medical and dental clinic and office)	3 spaces per 1,000 sq. ft.	2 spaces per 1,000 sq. ft.			
Retail Sales & Services					
Photo finishing lab	No Minimum		1 space per 1,000 sq. ft.	No Minimum	Transit and Urban Center Contexts: 2 spaces per 1,000 sq. ft.
Electronic repair shop					
Furniture repair shop					
Upholstery shop					

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	General Context	Neighborhood Center Context	Urban Center Context	Transit Context	
	All zoning districts not listed in another context area	RB, SNB, CB, CN, R-MU-35, R-MU-45, SR-3, FB-UN1, FB-SE	D-2, MU, TSA-T, CSHBD1, CSHBD2	D-1, D-3 D-4, G-MU, TSA-C, UI, FB-UN2, FB-UN3, FB-SC, R-MU	
Vehicle Stacking and Drive-Through Facilities: See 21A.44.040A.6 Outdoor Sales/Display/Leasing/Auction Areas: See 21A.44.040A.7					
Radio, television station	3 spaces per 1,000 sq. ft.	2 spaces per 1,000 sq. ft.	1 space per 1,000 sq. ft.		Neighborhood Center and General Context: 3 spaces per 1,000 sq. ft.
Store, Convenience	3 spaces per 1,000 sq. ft.	2 spaces per 1,000 sq. ft.	1.5 spaces per 1,000 sq. ft.	No Minimum	Transit and Urban Center Contexts: 2 spaces per 1,000 sq. ft. Neighborhood Center: 3 spaces per 1,000 sq. ft. General Context: 5 spaces per 1,000 sq. ft.
Auction, Indoor	2 spaces per 1,000 sq. ft.	1.5 spaces per 1,000 sq. ft.	1 space per 1,000 sq. ft.		Transit Context: 2 spaces per 1,000 sq. ft. Urban Center and Neighborhood Center Context: 3 spaces per 1,000 sq. ft. General Context: 4 spaces per 1,000 sq. ft.
Store, Department					
Fashion oriented development					
Flea market (indoor)					
Flea market (outdoor)					
Store, Mass merchandising					
Store, Pawn shop					
Store, Specialty					
Retail goods establishment					
Retail service establishment					
Store, Superstore and hypermarket					
Store, Warehouse club					

Table 21A.44.040-A: Minimum and Maximum Off Street Parking

DU = dwelling unit sq. ft. = square feet

Land Use	Minimum Parking Requirement				Maximum Parking Allowed
	General Context	Neighborhood Center Context	Urban Center Context	Transit Context	
	All zoning districts not listed in another context area	RB, SNB, CB, CN, R-MU-35, R-MU-45, SR-3, FB-UN1, FB-SE	D-2, MU, TSA-T, CSHBD1. CSHBD2	D-1, D-3 D-4, G-MU, TSA-C, UI, FB-UN2, FB-UN3, FB-SC. R-MU	
Vehicle Stacking and Drive-Through Facilities: See 21A.44.040A.6					
Outdoor Sales/Display/Leasing/Auction Areas: See 21A.44.040A.7					
Retail shopping center over 55,000 sq. ft. usable floor area	Up to 100,000 sq. ft. : 2. spaces per 1,000 sq. ft. . Above 100,000 sq. ft. : sq. ft. 1.5 spaces per 1,000 sq. ft.		Up to 100,000 sq. ft. : 1.5 spaces per 1,000 sq. ft. . Above 100,000 sq. ft. : 1.25 spaces per 1,000 sq. ft.		Transit and Urban Center Contexts: up to 100,000 sq. ft.: 2 spaces per 1,000 sq. ft., above 100,000 sq. ft.: 1.75 spaces per 1,000 sq. ft. Neighborhood Center and General Context: Up to 100,000 sq. ft.: 3 spaces per 1,000 sq. ft., above 100,000 sq. ft.: 2.5 spaces per 1,000 sq. ft.
Plant and garden shop with outdoor retail sales area	2 spaces per 1,000 sq. ft.	1.5 spaces per 1,000 sq. ft.	1 space per 1,000 sq. ft.		Transit and Urban Center Contexts: 1.5 spaces per 1,000 sq. ft. Neighborhood Center: 2 spaces per 1,000 sq. ft. General Context: 3 spaces per 1,000 sq. ft.
Lodging Facilities					
Bed and breakfast	1 space per guest bedroom		0.5 spaces per guest bedroom	No Minimum	All Contexts: 1.25 spaces per guest bedroom
Hotel/motel					All Contexts: 1.5 spaces per guest bedroom
Vehicles and Equipment					
Vehicle Auction	2 spaces per 1,000 sq. ft. of office area plus 1 space per service bay		1 space per 1,000 sq. ft. of office area plus 1 space per service bay	No Minimum	No Maximum
Automobile part sales				No Minimum	All Contexts:

Table 21A.44.040-A: Minimum and Maximum Off Street Parking

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Land Use	Minimum Parking Requirement				Maximum Parking Allowed
	General Context	Neighborhood Center Context	Urban Center Context	Transit Context	
	All zoning districts not listed in another context area	RB, SNB, CB, CN, R-MU-35, R-MU-45, SR-3, FB-UN1, FB-SE	D-2, MU, TSA-T, CSHBD1. CSHBD2	D-1, D-3 D-4, G-MU, TSA-C, UI, FB-UN2, FB-UN3, FB-SC. R-MU	
Vehicle Stacking and Drive-Through Facilities: See 21A.44.040A.6					
Outdoor Sales/Display/Leasing/Auction Areas: See 21A.44.040A.7					
Automobile and truck repair sales/rental and service	2 spaces per 1,000 sq. ft. of indoor sales/leasing/office area plus 1 space per service bay		1 space per 1,000 sq. ft. of indoor sales/leasing/office area plus 1 space per service bay		3 spaces per 1,000 sq. ft. of indoor sales/leasing/ office area, plus 1 space per service bay
Boat/recreational vehicle sales and service (indoor)					
Equipment rental (indoor and/or outdoor)					
Equipment, heavy (rental, sales, service)					
Manufactured/mobile home sales and service					
Recreational vehicle (RV) sales and service					
Truck repair sales and rental (large)					
Car wash	No Minimum				Transit and Urban Center Contexts: 1 space per 1,000 sq. ft.
Car wash as accessory use to gas station or convenience store that sells gas					Neighborhood Center: 2 spaces per 1,000 sq. ft.
					General Context: 5 spaces per 1,000 sq. ft.
Gas station	2 spaces per 1,000 sq. ft.	2 spaces per 1,000 sq. ft.	No Minimum		Neighborhood Center Context: 3 spaces per 1,000 sq. ft.
					Urban Center Context: 1 space per 1,000 sq. ft.
Bus line yard and repair facility	1 space per 1,000 sq. ft. , plus 1 space per commercial fleet vehicle			No Minimum	No Maximum
Impound lot					
Limousine service					
Taxicab facility					

Table 21A.44.040-A: Minimum and Maximum Off Street Parking

DU = dwelling unit sq. ft. = square feet

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	General Context	Neighborhood Center Context	Urban Center Context	Transit Context	
	All zoning districts not listed in another context area	RB, SNB, CB, CN, R-MU-35, R-MU-45, SR-3, FB-UN1, FB-SE	D-2, MU, TSA-T, CSHBD1. CSHBD2	D-1, D-3 D-4, G-MU, TSA-C, UI, FB-UN2, FB-UN3, FB-SC. R-MU	
Vehicle Stacking and Drive-Through Facilities: See 21A.44.040A.6					
Outdoor Sales/Display/Leasing/Auction Areas: See 21A.44.040A.7					
Tire distribution retail/wholesale					
Adult Entertainment Establishments					
Sexually oriented business	3 spaces per 1,000 sq. ft.	1 space per 1,000 sq. ft.	No Minimum	All Contexts: 5 spaces per 1,000 sq. ft.	
Transportation Uses					
Airport	Determined by Airport Authority				No Maximum
Heliport					
Bus line station/terminal	No Minimum				Urban Center and Transit Contexts: 2 spaces per 1,000 sq. ft.
Intermodal transit passenger hub					
Railroad, passenger station					
Transportation terminal, including bus, rail and trucking					
Railroad, repair shop	1 space per 1,000 sq. ft. , plus 1 space per fleet vehicle generally stored on-site	No Minimum		No Maximum	
Truck freight terminal					
Railroad, freight terminal facility	No Minimum				
Industrial Uses					
Manufacturing and Processing					
Artisan food production	1 space per 1,000 sq. ft. of production area, plus 2 spaces per 1,000 sq. ft. of office/retail	0.5 spaces per 1,000 sq. ft. of production area, plus 1.5 spaces per 1,000 sq. ft. of office/retail	No Minimum	Transit and Urban Center Contexts: 1 space per 1,000 sq. ft. of production area, plus 2 spaces per 1,000 sq. ft. of office/retail	

Table 21A.44.040-A: Minimum and Maximum Off Street Parking

DU = dwelling unit sq. ft. = square feet

Land Use	Minimum Parking Requirement				Maximum Parking Allowed
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	All zoning districts not listed in another context area	RB, SNB, CB, CN, R-MU-35, R-MU-45, SR-3, FB-UN1, FB-SE	D-2, MU, TSA-T, CSHBD1. CSHBD2	D-1, D-3 D-4, G-MU, TSA-C, UI, FB-UN2, FB-UN3, FB-SC. R-MU	
Vehicle Stacking and Drive-Through Facilities: See 21A.44.040A.6 Outdoor Sales/Display/Leasing/Auction Areas: See 21A.44.040A.7					
Bakery, commercial					Neighborhood Center and General Context: 2 spaces per 1,000 sq. ft. of production area, plus 3 spaces per 1,000 sq. ft. of office/retail
Automobile salvage and recycling (outdoor)	1 space per 1,000 sq. ft. of office		0.5 space per 1,000 sq. ft. of office	No Minimum	All Contexts: 7 spaces per 1,000 sq. ft. of office/retail
Processing center (outdoor)					
Automobile salvage and recycling (indoor)	1 space per 1,000 sq. ft.			No Minimum	No Maximum
Blacksmith shop					
Bottling plant					
Brewery/Small Brewery					
Chemical manufacturing and/or storage					
Commercial food preparation					
Distillery					
Drop forge industry					
Explosive manufacturing and storage					
Food processing					
Heavy manufacturing					
Incinerator, medical waste/hazardous waste					
Industrial assembly					
Jewelry fabrication					
Laundry, commercial					
Light manufacturing					
Manufacturing and processing, food					
Paint manufacturing				No Minimum	
Printing plant					

Table 21A.44.040-A: Minimum and Maximum Off Street Parking

DU = dwelling unit sq. ft. = square feet

Land Use	Minimum Parking Requirement				Maximum Parking Allowed		
	General Context	Neighborhood Center Context	Urban Center Context	Transit Context			
	All zoning districts not listed in another context area	RB, SNB, CB, CN, R-MU-35, R-MU-45, SR-3, FB-UN1, FB-SE	D-2, MU, TSA-T, CSHBD1. CSHBD2	D-1, D-3 D-4, G-MU, TSA-C, UI, FB-UN2, FB-UN3, FB-SC. R-MU			
Vehicle Stacking and Drive-Through Facilities: See 21A.44.040A.6							
Outdoor Sales/Display/Leasing/Auction Areas: See 21A.44.040A.7							
Processing center (indoor)							
Recycling							
Sign painting/ fabrication							
Studio, motion picture							
Welding shop							
Winery							
Woodworking mill							
Collection station	No Minimum						
Concrete and/or asphalt manufacturing							
Extractive industry							
Manufacturing, concrete or asphalt							
Refinery, petroleum products							
Storage and Warehousing							
Air cargo terminals and package delivery facility	1 space per 1,000 sq. ft. , plus 1 space per fleet vehicle generally stored on-site				No Maximum		
Building materials distribution							
Flammable liquids or gases, heating fuel distribution and storage							
Package delivery facility							
Warehouse							
Warehouse, accessory to retail and wholesale business (maximum 5,000 square foot floor plate)							
Wholesale distribution	2 spaces per 1,000 sq. ft. of office area, plus 1 space per 30 storage units				All Contexts: 1 space for every 15 storage units		
Storage, self						2 spaces per 1,000 sq. ft. of office	
Contractor's yard/office						2 spaces per 1,000 sq. ft. of office area	
Rock, sand and gravel storage and distribution	No Minimum				No Maximum		
Storage (outdoor)							

Table 21A.44.040-A: Minimum and Maximum Off Street Parking

DU = dwelling unit sq. ft. = square feet

Land Use	Minimum Parking Requirement				Maximum Parking Allowed
	General Context	Neighborhood Center Context	Urban Center Context	Transit Context	
	All zoning districts not listed in another context area	RB, SNB, CB, CN, R-MU-35, R-MU-45, SR-3, FB-UN1, FB-SE	D-2, MU, TSA-T, CSHBD1, CSHBD2	D-1, D-3 D-4, G-MU, TSA-C, UI, FB-UN2, FB-UN3, FB-SC, R-MU	
Vehicle Stacking and Drive-Through Facilities: See 21A.44.040A.6					
Outdoor Sales/Display/Leasing/Auction Areas: See 21A.44.040A.7					
Storage and display (outdoor)					
Storage, public (outdoor)					
Public and Semi-Public Utility Uses					
Utility: Building or structure	No Minimum				No Maximum
Antenna, communication tower					
Antenna, communication tower, exceeding the maximum building height in the zone					
Large wind energy system					
Solar array					
Utility: Electric generation facility					
Utility: Sewage treatment plant					
Utility: Solid waste transfer station					
Utility: Transmission wire, line, pipe or pole					
Wireless telecommunications facility					
Accessory Uses					
Accessory Dwelling Unit	See Section 21A.40.200: <i>Accessory Dwelling Units</i>				
Accessory guest and servant's quarter	1 space per DU		No Minimum	All Contexts: 4 spaces per DU	
Living quarter for caretaker or security guard					
Retail, sales and service accessory use when located within a principal building	2 spaces per 1,000	1 space per 1,000	Transit and Urban Center Contexts: 2 spaces per 1,000 sq. ft.		
Retail, sales and service accessory use when located within a principal building and operated primarily for the convenience of employees	No Minimum			Neighborhood Center: 3 spaces per 1,000 sq. ft. General Context: 4 spaces per 1,000 sq. ft.	

Table 21A.44.040-A: Minimum and Maximum Off Street Parking

DU = dwelling unit sq. ft. = square feet

Land Use	Minimum Parking Requirement				Maximum Parking Allowed
	General Context	Neighborhood Center Context	Urban Center Context	Transit Context	
	All zoning districts not listed in another context area	RB, SNB, CB, CN, R-MU-35, R-MU-45, SR-3, FB-UN1, FB-SE	D-2, MU, TSA-T, CSHBD1, CSHBD2	D-1, D-3 D-4, G-MU, TSA-C, UI, FB-UN2, FB-UN3, FB-SC, R-MU	
Vehicle Stacking and Drive-Through Facilities: See 21A.44.040A.6 Outdoor Sales/Display/Leasing/Auction Areas: See 21A.44.040A.7					
Warehouse, accessory	0.5 spaces per 1,000 sq. ft. of warehouse/wholesale		No Minimum		No Maximum
Accessory use, except those that are otherwise specifically regulated elsewhere in this title	No Minimum				
Heliport, accessory					
Reverse vending machine					
Storage, accessory (outdoor)					
Temporary Uses					
Mobile food business (operation in public right-of-way)	No minimum, unless required by temporary use permit or as determined by the Zoning Administrator				No Maximum
Mobile food business (operation on private property)					
Mobile food court					
Vending cart, private property					
Vending cart, public property					
Farm stand, seasonal					
Table Notes: A. Facilities that are (a) occupied by persons who's right to live together is protected by the federal Fair Housing Act, and that (b) occupy a building originally constructed for another residential use shall have the same parking requirements as the residential use for which the building was constructed. B. Parking requirements to be determined by Transportation Director based on considerations of factors such as estimated facility use, vehicle traffic to the facility, transit use to the facility, potential traffic congestion, and likelihood of overflow parking in surrounding neighborhoods.					

B. Electric Vehicle Parking

Each multi-family use shall provide a minimum of one (1) parking space dedicated to electric vehicles for every twenty five (25) parking spaces provided on-site. Electric vehicle parking spaces shall count toward the minimum required number of parking spaces. The electric vehicle parking space shall be:

1. Located in the same lot as the principal use;
2. Located as close to a primary entrance of the principal building as possible;
3. Signed in a clear and conspicuous manner, such as special pavement marking or signage, indicating exclusive availability to electric vehicles; and

4. Outfitted with a standard electric vehicle charging station.

C. Accessible Parking

1. The number and design of accessible (ADA) parking spaces shall be pursuant to the standards provided in the Salt Lake City Off-Street Parking Standards Manual.
2. Parking areas with four (4) or fewer vehicle parking spaces are not required to identify an accessible parking space; however, if parking is provided, a minimum of one (1) parking space shall comply with the ADA standard dimensions.
3. The number of required accessible spaces shall be based on the total number of vehicle spaces provided to serve the principal uses, as shown below in *Table 21A.44.040-B: Accessible Parking Required*

Table 21A.44.040-B: Accessible Parking Required	
Off Street Parking Spaces Provided	Minimum Required Accessible Spaces
1 to 100	1 per 25 parking spaces
101 to 500	1 per 50 parking spaces
501 to 1,000	2 percent of total number of parking spaces
1,001 and more	20, plus 1 for each 100 parking spaces over 1,000

D. Bicycle Parking

1. Applicability

The following regulations apply to all uses except for single-family, two-family, and twin home residential uses and nonresidential uses having less than one thousand square feet (1,000 sq. ft.) of usable floor area.

2. Calculation of Minimum Required Bicycle Parking Spaces³¹

The number of required bicycle spaces shall be based on the use within the defined parking contexts as shown in *Table 21A.44.040-C: Minimum Bicycle Parking Requirements*, unless another City standard requires a different number of bicycle parking spaces for a specific use, in which case the use-specific bicycle parking standard shall apply.

Table 21A.44.040-C: Minimum Bicycle Parking Requirements*				
(Calculation of Bicycle Parking Spaces to be Provided per Residential Unit or Based on Usable Floor Area)				
Use	General Context	Neighborhood Center Context	Urban Center Context	Transit Context
	All zoning districts not listed in another context area	RB, SNB, CB, CN, CSHBD2, R-MU-35, R-MU-45, SR-3, FB-UN1, FB-SE	D-2, D-3, MU, TSA-T, CSHBD1	D-1, D-4, G-MU, TSA-C, UI, FB-UN2, FB-SC, R-MU
Residential Uses	1 per 5 units	1 per 4 units	1 per 3 units	1 per 2 units

Table 21A.44.040-C: Minimum Bicycle Parking Requirements* (Calculation of Bicycle Parking Spaces to be Provided per Residential Unit or Based on Usable Floor Area)				
Use	General Context	Neighborhood Center Context	Urban Center Context	Transit Context
		All zoning districts not listed in another context area	RB, SNB, CB, CN, CSHBD2, R-MU-35, R-MU-45, SR-3, FB-UN1, FB-SE	D-2, D-3, MU, TSA-T, CSHBD1
Public, Institutional, and Civic Uses	1 per 10,000 sq. ft.	1 per 5,000 sq. ft.	1 per 5,000 sq. ft.	1 per 3,000 sq. ft.
Commercial Uses	1 per 20,000 sq. ft.	1 per 5,000 sq. ft.	1 per 4,000 sq. ft.	1 per 2,000 sq. ft.
Industrial Uses	No requirement	No requirement	No requirement	No Requirement
*For all uses: In determining the minimum number of bicycle parking spaces required, fractional spaces are rounded to the nearest whole number, with one-half counted as an additional space				

3. Building Expansions or Changes of Use

Building expansions or changes of use that require additional vehicle parking spaces pursuant to section 21A.44.020 and section 21A.44.040 shall provide additional bicycle parking spaces based on the calculations in *Table 21A.44.040-C: Minimum Bicycle Parking Requirements* for the entire use.

4. Secure/Enclosed Bicycle Parking

Each one (1) bicycle parking space that is within a secure/enclosed bicycle parking facility may be used to satisfy the requirement of two (2) required bicycle parking spaces.

5. Existing Public Bicycle Parking Facilities

Permanent public bicycle racks or bike corrals located within fifty feet (50') of the primary entrance to the principal building may be used to satisfy up to two (2) required bicycle parking spaces.

6. Accessory and Temporary Uses

No bicycle parking spaces are required for accessory or temporary uses.

21A.44.050 Alternatives to Minimum and Maximum Parking Calculations

The amount of off street vehicle parking required pursuant to Table 21A.44.040-A: *Minimum and Maximum Off Street Parking*, may be adjusted by the factors listed in this section. These adjustments may be applied as part of the calculation of parking requirements and do not require discretionary approval by the City.

A. Limitations on Adjustments to Minimum Required Parking

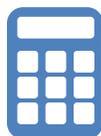
The adjustments listed in sections 21A.44.050B.B through 21A.44.050F.H may be used in any combination, but shall not be combined to reduce the minimum required parking established in Table 21A.44.040-A: *Minimum and Maximum Off Street Parking* by more than forty percent (40%).

B. Shared Parking

1. Shared Parking for Two or More Uses

- a. Where two (2) or more uses listed in Table 21A.44.040-A: *Minimum and Maximum Off Street Parking* share a parking garage or parking lot that is located on one of the properties that is sharing parking, or is located within the maximum permitted distance of all of the properties sharing parking shown in Table 21A.44.060-B: *Maximum Distances for Off-Site Parking*, the total minimum off street parking requirement for those uses may be reduced by the factors shown in Table 21A.44.050-A: *Shared Parking Reduction Factors*.
- b. The minimum number of off street parking spaces shall be the sum of the parking requirements for the uses divided by the factor shown in Table 21A.44.050-A: *Shared Parking Reduction Factors* for that combination of uses.

Example: If a 5,000 square foot art gallery shared a parking lot with a 5,000 square foot retail goods establishment, and a 100 unit multi-family residential use in the Urban Center Context, the minimum off street parking required would be calculated as follows:



- > Use 1: Art Gallery
- > 0.5 per 1,000 sq. ft. x (5,000 sq. ft.) = 3 parking spaces
- > Use 2: Retail Goods Establishment
- > 1 per 1,000 sq. ft. x (5,000 sq. ft.) = 5 parking spaces
- > Use 3: Multi-Family Residential
- > 0 per studio unit x (20 studio units) = 0 parking spaces
- > 0.5 per 1 bedroom unit x (36 1 bedroom units) = 18 parking spaces
- > 1 per 2+ bedroom units x (44 2+ bedroom units) = 44 parking spaces
- > 0+18+44 = 62 parking spaces
- > Sum of two largest minimum parking requirements:
- > 5 (retail goods establishment)+ 62 (multi-family) = 67 parking spaces
- > Reduction Factor (two largest minimums):
- > 67 ÷ 1.2 reduction factor = 55.8 or 56 parking spaces
- > Add Remaining Minimum(s):
- > 56 (retail & multi-family) + 3 (art gallery) = **59 parking spaces required**

Table 21A.44.050-A: Shared Parking Reduction Factors

Property Use	Multi-Family Residential	Public, Institutional, or Civic	Food and Beverage, Recreation and Entertainment, or Lodging	Retail Sales	Other Non-Residential
Multi-Family Residential [1]					
Public, Institutional and Civic	1.1				

Food and Beverage, Recreation and Entertainment, or Lodging	1.1	1.2			
Retail Sales	1.2	1.3	1.3		
Other Non-Residential	1.3	1.5	1.7	1.2	
[1] Applies to multi-family residential, assisted living facility (large), group home (large), and residential support (large) uses					

2. Documentation Required

- a. The owners of record involved in the joint use of shared parking shall submit written documentation of the continued availability of the shared parking arrangement to the Transportation Director for review.
- b. The Director shall approve the shared parking arrangement if the Director determines that the documentation demonstrates the continued availability of the shared parking facility for a reasonable period of time. No zoning or use approval shall be issued until the Director has approved the shared parking documentation.
- c. If the shared parking arrangement is later terminated or modified and the Director determines that the termination or modification has resulted in traffic congestion, overflow parking in residential neighborhoods, or threats to pedestrian, bicycle, or vehicle safety, the property owners involved in the shared parking arrangement may be held in violation of this chapter.

C. Proximity to Fixed-Rail Transit

Required parking for a development located within one-quarter mile (when measured radially in a straight line from the subject property line) of a fixed-rail transit station platform in the General Context, Neighborhood Center Context, and Urban Center Context areas may be reduced by up to twenty-five percent (25%). This shall not apply to single or two-family uses including: single-family (attached or detached), twin homes, or two-family.

D. Affordable and Senior Housing (Multi-family Structures)

The minimum number of required off street parking spaces for multi-family residential developments with at least ten (10) dwelling units may be reduced by twenty-five percent (25%) if the multi-family development has:

1. A minimum of twenty-five percent (25%) of the dwelling units are restricted to residents with no greater than sixty percent (60%) area median income (AMI) for leased units; or
2. A minimum of thirty-five percent (35%) of the dwelling units are restricted to residents with no greater than eighty percent (80%) AMI for sale units; or
3. A minimum of seventy-five percent (75%) of the dwelling units are restricted to persons sixty-five (65) years of age or older.

For a development that meets any of the scenarios above, an additional reduction of up to fifteen percent (15%) may be allowed when the development is located within one-quarter mile (when measured radially in a straight line from the subject property line) of a bus stop that is serviced by the same route at least every fifteen (15) minutes during daytime hours, Monday - Saturday.

E. Car Pool and Carshare Parking

1. For parking lots with one hundred (100) or more parking spaces, each off street parking space designated and signed for the exclusive use of a shared car pool vehicle shall count as three (3) spaces toward the satisfaction of minimum off street vehicle parking requirements.
2. For parking lots with one hundred (100) or more parking spaces, each off street parking space designated and signed for the exclusive use of a shared vanpool vehicle shall count as seven (7) spaces toward the satisfaction of minimum off street vehicle parking requirements.
3. For parking lots of any size, each off street parking space designated and signed for the exclusive use of a carshare vehicle shall count as four (4) spaces toward the satisfaction of minimum off street vehicle parking requirements.

F. Valet Parking Services

Modifications to minimum on site parking spaces may occur on a one-to-one basis if off site valet parking is provided and:

1. The design of the valet parking does not cause customers who do not use the valet services to park off the premises or cause queuing in the right-of-way;
2. The availability of valet parking service is clearly posted outside the establishment and near the main entrance; and
3. The applicant provides adequate written assurances for the continued operation of the valet parking, and a written agreement to notify future owners and tenants of the property of the duty to continue to provide off-site valet parking.

G. Parking Study Demonstrating Different Parking Needs

1. The Transportation Director, in consultation with the Planning Director, may authorize a change in the amount of off street parking spaces. The authorization shall be based on the applicant submitting a parking study that demonstrates a different off street parking demand for the proposed development, use, or combination of uses than calculated from Table 21A.44.040-A: *Minimum and Maximum Off Street Parking*, and subject to the overall limits on parking adjustments in Section 21A.44.050.A above.
2. The Directors shall determine whether the information and assumptions used in the study are reasonable and whether the study accurately reflects anticipated off street parking demand for the proposed development, use, or combination of uses.
3. Considerations for an alternative parking requirement (parking provided below the minimum required or exceeding the maximum allowed) shall be granted only if the following findings are determined:

- a. That the proposed parking plan will satisfy the anticipated parking demand for the use;
- b. That the proposed parking plan will be at least as effective in maintaining traffic circulation patterns, reducing the visibility of parking areas and facilities as would strict compliance with the otherwise applicable off street parking standards;
- c. That the proposed parking plan does not have a materially adverse impact on adjacent or neighboring properties;
- d. That the proposed parking plan includes mitigation strategies for any potential impact on adjacent or neighboring properties; and
- e. That the proposed alternative parking plan is consistent with applicable City plans and policies.

21A.44.060 Parking Location and Design

All required parking areas shall be located and designed in accordance with the standards in this Chapter 21A.44: *Off Street Parking, Mobility, and Loading* and the standards in the Off-Street Parking Standards Manual. Modifications to the standards of this section 21A.44.060 may be granted through the design review process, subject to conformance with the standards and procedures of Chapter 21A.59: Design Review.

A. Generally

1. Parking Located on Same Lot as Use or Building Served

All parking spaces required to serve buildings or uses erected or established after the effective date of this ordinance shall be located on the same lot as the building or use served, unless otherwise allowed pursuant to section 21A.44.060A.4 *Off-Site Parking Permitted*.

2. Biodetention and Landscape Islands in General and Neighborhood Center Contexts

For parking lots with one hundred (100) or more parking spaces in the General Context and Neighborhood Center Context areas, parking lot islands or biodetention areas shall be provided on the interior of the parking lot to help direct traffic flow and to provide landscaped areas within such lots.

3. Parking Location and Setbacks

All parking shall comply with the parking restrictions within yards pursuant to Table 21A.44.060-A: *Parking Location and Setback Requirements*

Table 21A.44.060-A: Parking Location and Setback Requirements

N = parking prohibited between lot line and front line of building

Zoning District	Front Lot Line	Corner Side Lot Line	Interior Side Lot Line	Rear Lot Line
General Context				
Residential (FR Districts, RB, RMF, RO)				
FR	N Parking in driveways that comply with all applicable City standards is exempt from this restriction.		6 ft.	0 ft.
R-1, R-2, SR-1, SR-2			0 ft.	
RMF-30	N		0 ft.; or 10 ft. when abutting any 1-2 family residential district	
RMF-35, RMF-45, RMF-75, RO	N		; 0 ft.; or 10 ft. when abutting any 1-2 family residential district. Limited to 1 side yard except for single-family attached lots.	
Commercial and Manufacturing (CC, CS, CG, M-1, M-2, SNB)				
CC	15 ft.		0 ft.; or 7 ft. when abutting any residential district	
CS			0 ft.; or 15 ft. when abutting any residential district	
CG	10 ft.			
M-1	15 ft.			
M-2	15 ft.		0 ft.; or 50 ft. when abutting any residential district	
Special Purpose Districts				
A	0 ft.		0 ft.	
AG, AG-2, AG-5, AG-20	N			
BP	N		8 ft.; or 30 ft. when abutting any residential district	
EI	10 ft.	30 ft.	30 ft.	20 ft.
FP	20 ft.		6 ft.	0 ft.
I	20 ft.		0 ft.; or 15 ft. when abutting any residential district	
MH	20 ft.		0 ft.	
OS	30 ft.		10 ft.	
PL	30 ft.		0 ft.; or 10 ft. when abutting any residential district	
PL-2	20 ft.			
RP	30 ft.		8 ft.; or 30 ft. when abutting any residential district	
Neighborhood Center Context				
CB, CN, CSHBD2, SNB	N		0 ft.; or 7 ft. when abutting any 1-2 family residential district	
R-MU-35, R-MU-45			Limited to 1 side yard, 0 ft.; or 10 ft. when abutting any 1-2 family residential district	0 ft.; or 10 ft. when abutting any 1-2 family residential district

Table 21A.44.060-A: Parking Location and Setback Requirements				
N = parking prohibited between lot line and front line of building				
Zoning District	Front Lot Line	Corner Side Lot Line	Interior Side Lot Line	Rear Lot Line
RB, SR-3, FB-UN1, FB-SE	N		0 ft.	
Urban Center Context				
D-2	N		0 ft.	
D-3	See section 21A.44.060.B.1			
MU	N		0 ft.; limited to 1 side yard	0 ft.
R-MU	Surface Parking: 30 ft. Parking Garages: 45 ft.		0 ft.; or 10 ft. when abutting any 1-2 family residential district Surface parking at least 30 ft. from front lot line. Parking garages at least 45 ft. from front lot line	0 ft.; or 10 ft. when abutting any 1-2 family residential district
TSA-T	See section 21A.44.060B.22		0 ft.	
CSHBD1	N		0 ft.; or 7 ft. when abutting any residential district	
Transit Context				
D-1	See section 21A.44.060B.11			
D-4	See section Error! Reference source not found. 1		0 ft.	
FB-UN2, FB-SC	N		0 ft.	
TSA-C	See section 21A.44.060B.22		0 ft.	
G-MU	See section Error! Reference source not found. 1		0 ft.	
UI	0 ft; Hospitals: 30 ft.		0 ft.; or 15 ft. when abutting any 1-2 family residential district; Hospitals: 10 ft.	0 ft.; or 15 ft. when abutting any 1-2 family residential district; Hospitals: 10 ft.

4. Off-Site Parking Permitted

When allowed as either a permitted or conditional use per Chapter 21A.33 *Land Use Tables*, off-site parking facilities may be used to satisfy the requirements of this chapter and shall comply with the following standards:

a. Maximum Distance of Off-Site Parking

Off-site parking shall be located according to the distance established in Table 21A.44.060-B: *Maximum Distances for Off-Site Parking* (measured in a straight line from the property boundary of the principal use for which the parking serves to the closest point of the parking area).

Table 21A.44.060-B: Maximum Distances for Off-Site Parking	
Context	Maximum Distance to Off-Site Parking
Neighborhood Center	600 ft.
General	

Legal Nonconforming Use in Residential District	
Urban Center	1,200 ft.
Transit	1,000 ft.

b. Documentation Required

- (1) The owners of record involved in an off-site parking arrangement shall submit written documentation of the continued availability of the off-site parking arrangement to the Planning Director for review.
- (2) The Planning Director shall approve the off-site parking arrangement if the Director determines the location meets the standards of this section. No zoning or use approval shall be issued until the Director has approved the off-site parking arrangement and the documentation has been recorded in the office of the Salt Lake County Recorder.
- (3) If the off-site parking arrangement is later terminated or modified and the Planning Director determines that the termination or modification has resulted in traffic congestion, overflow parking in residential neighborhoods, or threats to pedestrian, bicycle, or vehicle safety, the property owners of the uses for which the off-site parking was provided may be held in violation of this chapter.

5. Circulation Plan Required

Any application for a building permit shall include a site plan, drawn to scale, and fully dimensioned, showing any off street parking or loading facilities to be provided in compliance with this title. A tabulation of the number of off street vehicle and bicycle parking, loading, and stacking spaces required by this chapter shall appear in a conspicuous place on the plan.

6. Driveways and Access

a. Compliance with Other Adopted Regulations

- (1) Parking lots shall be designed in compliance with applicable City codes, ordinances, and standards, including but not limited to title 12 of the City Code: *Vehicles and Traffic* and the Off-Street Parking Standards Manual to the maximum degree practicable, with respect to:
 - (a) Minimum distances between curb cuts;
 - (b) Proximity of curb cuts to intersections;
 - (c) Provisions for shared driveways;
 - (d) Location, quantity and design of landscaped islands; and
 - (e) Design of parking lot interior circulation system.
- (2) Notwithstanding the provisions of 21A.44.060A.6.a(1) above, relocation of a driveway for a single-family, two-family, or twin home residence in any zoning district shall only be required when the residence is replaced, and shall not be required when the residence is expanded or renovated in compliance with the City code.

b. Access Standards

Access to all parking facilities shall comply with the following standards:

- (1) To the maximum extent practicable, all off street parking facilities shall be designed with vehicular access to a street or alley that will least interfere with automobile, bicycle, and pedestrian traffic movement.
- (2) Parking facilities in excess of five (5) spaces that access a public street shall be designed to allow vehicles to enter and exit the lot in a forward direction.
- (3) Parking facilities on lots with less than one hundred feet (100') of street frontage shall have only one (1) curb cut, and lots with one hundred feet (100') of street frontage or more shall be limited to two (2) curb cuts, unless the Transportation Director determines that additional curb cuts are necessary to ensure pedestrian, bicycle, and vehicle safety or to comply with the fire code. Public safety uses shall be exempt from limitations on curb cuts.
- (4) All vehicular access roads/driveways shall be surfaced as required in accordance with section 21A.44.060.A.8 *Surface Materials*.

c. **Driveway Standards**

All driveways shall comply with the following standards:

(1) **Driveway Location in Residential Zoning Districts**

With the exception of legal shared driveways, driveways shall be at least twenty feet (20') from street corner property lines and five feet (5') from any public utility infrastructure such as power poles, fire hydrants, and water meters. Except for entrance and exit driveways leading to approved parking areas, no curb cuts or driveways are permitted.

(2) **Driveway Widths**

All driveways serving residential uses shall be a minimum eight feet wide and shall comply with the standards for maximum driveway widths listed in Table 21A.44.060-C: *Minimum and Maximum Driveway Width*.

Table 21A.44.060-C: Minimum and Maximum Driveway Width		
Zoning District	Minimum Driveway Width (in front and corner side yard)	Maximum Driveway Width* (in front and corner side yard)
SR-1, SR-2 and SR-3	8 ft.	22 ft.
MH	8 ft.	16 ft.
Other Residential Zoning Districts	8 ft.	30 ft.
M-1 and M-2	12 ft. single lane and 24 ft. for two-way	50 ft.
Other Non-Residential Zoning Districts	12 ft. single lane and 24 ft. for two-way	30 ft.
* Maximum width is for all driveways combined when more than one driveway is provided		

(3) **Shared Driveways**

Shared driveways, where two (2) or more properties share one (1) driveway access, may be permitted if the Transportation Director determines that the design and location of the shared driveway access will not create adverse impacts on traffic congestion or public safety.

(4) **Driveway Surface**

All driveways providing access to parking facilities shall be improved and maintained pursuant to the standards in the Off-Street Parking Standards Manual.

7. Minimum Dimensional Standards

All parking spaces shall comply with the dimensional standards in the Off-Street Parking Standards Manual.

8. Surface Materials

All parking spaces shall comply with the standards for surfacing of access, driving, and parking surfacing in the Off-Street Parking Standards Manual.

9. Grading and Stormwater Management

All surface parking areas shall comply with City grading and stormwater management standards and shall be reviewed for best management practices by Salt Lake City Department of Public Utilities. Refer to the Salt Lake City Stormwater Master Plan, Storm Drainage Manual, and Green Infrastructure Toolbox for additional information.

10. Sight Distance Triangles

All driveways and intersections shall comply with the sight distance triangle standards as defined in the Off-Street Parking Standards Manual.

11. Landscaping and Screening

All parking areas and facilities shall comply with the landscaping and screening standards in Chapter 21A.48: *Landscaping and Buffers*.

12. Lighting

Where a parking area or parking lot is illuminated, the light source shall be shielded so that the light source is not directly visible from any abutting property or abutting private or public street.

13. Signs

All signs in parking areas or related to parking facilities shall comply with Chapter 21A.46; *Signs*, and applicable provisions of the Manual on Uniform Traffic Control Devices (MUTCD).

14. Pedestrian Walkways

- a. Surface parking lots with between twenty-five (25) and one hundred (100) parking spaces shall provide a pedestrian walkway or sidewalk through the parking lot to the primary entrance of the principal building. Pedestrian walkways shall be identified by a change in color, material, surface texture, or grade elevation from surrounding driving surfaces.
- b. Parking lots with more than one hundred (100) parking spaces shall provide:
 - (1) One (1) or more grade-separated pedestrian walkway(s), at least five feet (5') in width, and located in an area that is not a driving surface, leading from the furthest row of parking spaces to the primary entrance of the principal building.
 - (2) Vehicles shall not overhang the pedestrian walkway(s).

- (3) Where the walkway(s) crosses a drive aisle, pedestrian walkway(s) shall be identified by a change in color, material, surface texture, or grade elevation from surrounding driving surfaces.
- (4) One (1) pedestrian walkway meeting these standards shall be provided for each one hundred (100) parking spaces provided on site or part thereof, after the first one hundred (100) parking spaces.

15. Parking Garages

The following standards shall apply to all above-ground parking garages except those located in the FB zones subject to 21A.27.030.C.4, whether freestanding or incorporated into a building:

- a. Each façade or a parking garage adjacent to a public street or public space shall have an external skin designed to conceal the view of all parked cars. Examples include heavy gauge metal screen, precast concrete panels, live green or landscaped walls, laminated or safety glass, or decorative photovoltaic panels.
- b. No horizontal length of the parking garage façade shall extend longer than 40 feet without the inclusion of architectural elements such as decorative grillwork, louvers, translucent screens, alternating building materials, and other external features to avoid visual monotony. Facade elements shall align with parking levels.
- c. Internal circulation shall allow parking surfaces to be level (without any slope) along each parking garage facade adjacent to a public street or public space. All ramps between levels shall be located along building facades that are not adjacent to a public street or public space, or shall be located internally so that they are not visible from adjacent public streets or public spaces.
- d. The location of elevators and stairs shall be highlighted through the use of architectural features or changes in façade colors, textures, or materials so that visitors can easily identify these entry points.
- e. Interior parking garage lighting shall not produce glaring sources toward adjacent properties while providing safe and adequate lighting levels. The use of sensor dimmable LEDs and white stained ceilings are recommended to control light levels on-site while improving energy efficiency.
- f. In the Urban Center Context and Transit Context areas, the street-level facades of all parking garages shall be designed to meet applicable building code standards for habitable space to allow at least one (1) permitted or conditional use, other than parking, to be located where the parking garage is located.
- g. Vent and fan locations shall not be located on parking garage facades facing public streets or public spaces, or adjacent to residential uses, to the greatest extent practicable.

16. Tandem Parking

Where more than one (1) parking space is required to be provided for a residential dwelling unit, the parking spaces may be designed as tandem parking spaces, provided that:

- a. No more than two (2) required spaces may be included in the tandem parking layout; and
- b. Each set of two (2) tandem parking spaces shall be designated for a specific residential unit.

17. Cross-Access between Adjacent Uses

The Transportation Director may require that access to one or more lots be through shared access points or cross-access through adjacent parcels when the Transportation Director determines that individual access to abutting parcels or limited distance between access points will create traffic safety hazards due to traffic levels on adjacent streets or nearby intersections. Such a determination shall be consistent with requirements of state law regarding property access from public streets. Required cross-access agreements shall be recorded with the Salt Lake County Recorder’s Office.

B. Zone Specific Location and Design Standards

1. D-1, D-3, D-4, and G-MU Zoning Districts

The following regulations shall apply to surface or above-ground parking facilities. No special design and setback restrictions shall apply to below-ground parking facilities.

a. Block Corner Areas

- (1) Within the D-1 zoning district, above-ground parking facilities located within the block corner areas and on Main Street, shall be located behind principal buildings and;
 - a. All above-ground parking facilities that front a street shall contain uses other than parking along the entire length of the building façade and along all stories or levels of the building.
 - b. Vehicle access to parking shall be located to the side of the building or as far from the street corner as possible unless further restricted by this Title.
- (2) Within the D-3, D-4, or G-MU zoning districts, above-ground parking facilities shall be located behind principal buildings, or at least seventy-five feet (75') from front and corner side lot lines, and shall be landscaped to minimize visual impacts.

b. Mid-Block Areas

- (1) Within the D-1 zoning district, above-ground parking facilities shall be located behind the front line of principal buildings or shall be located at least seventy-five feet (75') from front and corner side lot lines;
 - a. Parking lots proposed as a principal use to facilitate a building demolition are prohibited.
- (2) Within the D-3, D-4, or G-MU zoning districts, parking facilities shall be located behind principal buildings, or at least thirty feet (30') from front and corner side lot lines.
- (3) Parking garages shall meet the following:
 - a. Retail goods/service establishments, offices and/or restaurants shall be provided on the first floor adjacent to the front or corner side lot line. The

facades of such first floors shall be compatible and consistent with the associated retail or office portion of the building and other retail uses in the area.

- b. Levels of parking above the first level facing the front or corner side lot line shall have floors and/or facades that are horizontal, not sloped.

- c. **Landscape Requirements**

Surface parking lots, where allowed shall have a minimum landscaped setback of fifteen feet (15') and shall meet interior parking lot landscaping requirements as outlined in Chapter 21A.48: *Landscaping and Buffers*.

2. TSA Zoning District

New uses and development or redevelopment within the TSA zoning district shall comply with the following standards.

- a. **Surface Parking on Corner Properties**

On corner properties, surface parking lots shall be located behind principal buildings or at least sixty feet (60') from the intersection of the front and corner side lot lines.

- b. **Surface Parking in the Core Area**

Surface parking lots in the core area are required to be located behind or to the side of the principal building.

- (1) **When located to the side of a building, the parking lot shall be:**

- (a) Set back a minimum of thirty feet (30') from a property line adjacent to a public street. The area between the parking lot and the property line adjacent to a public street shall be landscaped or activated with outdoor dining, plazas, or similar features;
 - (b) Screened with a landscaped hedge or wall that is at least thirty-six inches (36") above grade and no taller than forty-two inches (42") above grade. Landscaping berms are not permitted; and
 - (c) No wider than what is required for two (2) rows of parking and one (1) drive aisle as provided in the Off-Street Parking Standards Manual.

- (2) Unless a second driveway is necessary to comply with the fire code, a maximum of one (1) driveway and drive aisle shall be permitted per street frontage. The access point shall be located a minimum of one hundred feet (100') from the intersection of the front and corner side lot lines. If the front or corner side lot line is less than one hundred feet (100') in length, then the edge of the drive approach shall be located within twenty feet (20') of the side or rear property line.

- c. **Surface Parking In the Transition Area**

- (1) Surface parking lots in the transition area are required to be located behind the principal building or to the side of a principal building.

- (2) **When located to the side of a principal building, the parking lot shall be:**

- (a) Set back so that no portion of the parking area (other than the driveway) shall be closer to the street than the front wall setback of the building. In cases where the front wall of the building is located within five feet (5') of a property line adjacent

- to a street, the parking lot shall be set back a minimum of eight feet (8'). The space between the parking lot and the property line adjacent to a street shall be landscaped or activated with outdoor dining, plazas, or similar features; and
- (b) Screened with a landscaped hedge or wall that is at least thirty-six inches (36") above grade and no taller than forty-two inches (42") above grade. Landscaped berms are not permitted.

C. Recreational Vehicle Parking

1. Generally

- a. Recreational vehicle parking spaces shall be in addition to, and not in lieu of, required off street vehicle parking spaces.
- b. Recreational vehicles shall not be used for storage of goods, materials, or equipment other than those that are customarily associated with the recreational vehicle.
- c. All recreational vehicles shall be stored in a safe and secure manner. Any tie downs, tarpaulins, or ropes shall be secured from flapping in windy conditions.
- d. Recreational vehicles shall not be occupied as a dwelling while parked on the property.
- e. Recreational vehicle parking is permitted in any enclosed structure conforming to building code and zoning requirements for the zoning district in which it is located.
- f. Recreational vehicle parking outside of an approved enclosed structure shall be permitted for each residence and shall be limited to one motor home or travel trailer and a total of two (2) recreational vehicles of any type.
- g. Recreational vehicle parking outside of an enclosed structure shall comply with the standards in this section.

2. Front Yard Parking

Recreational vehicle parking is prohibited in any required or provided front yard.

3. Rear Yard Parking

Recreational vehicles may be parked in the rear yard when they are on a hard surfaced pad compliant with surfacing standards in the Off-Street Parking Standards Manual and with access provided by either a hard surfaced driveway, hard surfaced drive strips or an access drive constructed of turf block materials with an irrigation system.

4. Side Yard Parking

Recreational vehicle parking in side yards shall be allowed only when topographical factors, the existence of mature trees, or the existence of properly permitted and constructed structures prohibit access to the rear yard. The existence of a fence or other structure that is not part of a building shall not constitute a lack of rear yard access. Any recreational vehicle parking area in a side yard shall:

- a. Be on a hard surface compliant with the Off-Street Parking Standards Manual;
- b. Be accessed via a driveway compliant with driveway standards of this chapter;
- c. Not obstruct access to other required parking for the use.

21A.44.070 Off Street Loading Areas

A. Number and Size of Loading Areas Required

1. Unless otherwise specified, a required off street loading berth shall be at least ten feet (10') in width by at least thirty-five feet (35') in length for short berths, and twelve feet (12') in width by at least fifty feet (50') in length for long berths, exclusive of aisle and maneuvering space. Maneuvering aprons of appropriate width and orientation shall be provided and shall be subject to approval by the Transportation Director.
2. All loading areas shall have a vertical clearance of at least fourteen feet (14').
3. Off street loading facilities for new developments or for expansion of an existing development shall be provided at the rate specified for a particular use, or if multiple uses, at the rate of the uses combined, in Table 21A.44.070-A: *Off Street Loading Requirements*. Regardless of the combination of uses, all buildings with a gross floor area over 50,000 square feet shall have a minimum of 1 short berth.

Use	Gross Floor Area (Square Feet)	Number and Size of Berths
Hotels, Institutions, and Institutional Living	50,000 - 100,000	1 short
	Each additional 100,000	1 short
Office/Commercial	50,000 - 100,000	1 short
	Each additional 100,000 up to 500,000	1 short
Retail	50,000 - 100,000	1 long
	Each additional 100,000	1 long
Industrial	25,001 - 50,000	1 long
	50,001 - 100,000	2 long
	Each additional 100,000	1 long
Multi- Family Residential ⁸⁶	# of Dwelling Units (Per Building)	Number and Size of Berths
	40-150	1 short
	151-300	2 short
	Greater than 300	1 additional short per 200 units

B. Location and Design of Loading Areas

1. All required loading berths shall be located on the same development site as the use(s) served.
2. No loading berth shall be located within thirty feet (30') of the nearest point of intersection of any two (2) streets.
3. No loading berth shall be located in a required front yard.
4. Each required loading berth shall be located and designed to:

- a. Allow all required vehicle maneuvering and backing movements on-site;
 - b. Minimize conflicts with pedestrian, bicycle, and traffic movement or encroachments into any pedestrian walkway, bicycle lane, public right-of-way, and fire lane; and
 - c. Avoid the need to back into a public street while leaving the site to the maximum extent practicable, as determined by the Planning Director and the Transportation Director.
5. Landscaping and screening of all loading berths shall be provided to comply with the requirements of Chapter 21A.48: *Landscaping and Buffers*.
 6. Where a loading berth is illuminated, the light source shall be shielded so that the light source is not directly visible from any abutting property or abutting private or public street.
 7. All signs in loading areas shall comply with Chapter 21A.46: *Signs*, and applicable provisions of the Manual on Uniform Traffic Control Devices.
 8. All required loading berths shall comply with the surfacing standards of the Off-Street Parking Standards Manual.

21A.44.080 Drive-Through Facilities and Vehicle Stacking Areas

A. Number of Stacking Spaces Required

The following standards apply for all uses with vehicle stacking and/or drive-through facilities.

1. All uses with drive-through facilities shall provide the minimum number of on-site stacking spaces indicated in Table 21A.44.080-A: *Required Vehicle Stacking Spaces*.

Table 21A.44.080-A: Required Vehicle Stacking Spaces				
Use	General Context	Neighborhood Center Context	Urban Center Context	Transit Context
		All zoning districts not listed in another context area	RB, SNB, CB, CN, CSHBD2, R-MU-35, R-MU-45, SR-3, FB-UN1, FB-SE	D-2, D-3, MU, R-MU, TSA-T, CSHBD1
Car Wash, Self-Service	3 spaces per bay or stall		2 spaces per bay or stall	
Car Wash, Automated	4 spaces per lane or stall		3 spaces per lane or stall	
Food and Beverage Service Uses	5 spaces per service lane		4 spaces per service lane	
Other Uses	3 spaces per service lane		3 spaces per service lane	

B. Location and Design of Drive-Through Facilities

1. In zoning districts where uses with drive-through facilities are allowed and where no front or corner side yard setback is required, the drive-through lanes shall not be located between the front or corner side lot line and any walls of the principal building.
2. Drive-through lanes shall be arranged to avoid conflicts with site access points, access to parking or loading spaces, and internal circulation routes, to the maximum extent practicable.

3. In the General Context zoning districts, a by-pass lane, driveway, or other circulation area around a drive-through facility stacking lane shall be provided for all uses other than automated car washes, financial institutions and restaurant/retail uses.
4. All required stacking spaces shall measure nine (9) feet by twenty (20) feet and shall be counted from the point of service, or final service window.
5. Air quality: Drive through facilities shall post idle-free signs pursuant to Chapter 12.58 of the city code.
6. When a drive through use adjoins any residential use or any residential zoning district, a minimum six foot (6') high masonry wall shall be erected and maintained along such property line.
7. Drive through facility will not result in adverse impacts upon the vicinity after giving consideration to the hours of operation, noise and light generation, traffic circulation, and the site plan.

21A.44.090 Modifications to Parking Areas

Applicants requesting development permits or approvals may request adjustments to the standards and requirements in this Chapter 21A.44: *Off Street Parking, Mobility, and Loading*, and the City may approve adjustments to those standards, as described below.

A. Administrative Modifications

The Planning Director or Transportation Director may approve the following types of modifications without requiring approval of a Special Exception, provided that the Director determines that the adjustment will not create adverse impacts on pedestrian, bicycle, or vehicle safety and that the adjustment is required to accommodate an unusual site feature (such as shape, topography, utilities, or access point constraints) and that the need for the adjustment has not been created by the actions of the applicant.

1. Modification to dimensions or geometries of parking, loading, or stacking space, aisles, or maneuvering areas otherwise required by this chapter, other City regulations, or the Off-Street Parking Standards Manual; provided that those modifications are consistent with federal and state laws regarding persons with disabilities, including but not limited to the Americans with Disabilities Act.
2. Modifications to bicycle parking or loading berth location or design standards.

B. Special Exceptions

The following types of exceptions may be approved through the Special Exception process in section 21A.52.040, provided that the application meets the criteria for approval of a Special Exception in section 21A.52.060 in addition to the standards provided in this section.

1. Exceptions Permitted

a. Front Yard Parking Exception

For any zoning district, if front yard parking is prohibited in Table 21A.44.060-A: *Parking Location and Setback Requirements*, it may be allowed if all of the following conditions are met:

- (1) The rear or side yards cannot be reasonably accessed by vehicles, specifically;
 - (a) Clearance for a driveway could not be provided in the side yard on either side of the building that is free from obstructions that cannot reasonably be avoided, such as utilities, window-wells, a specimen tree, a direct elevation change of three feet (3') or greater, or retaining walls three feet (3') high or greater; and
 - (b) There is not a right-of-way or alley adjacent to the property with established rights for access, where:
 - a. The travel distance to the property line is less than one hundred feet (100') from an improved street and the right-of-way or alley has at least a minimum twelve foot (12') clearance that is, or could be paved; or
 - b. The travel distance to the property line is more than one hundred feet (100') from an improved street and the right-of-way or alley has an existing minimum twelve foot (12') wide paved surface.
- (2) It is not feasible to build an attached garage that conforms to yard area and setback requirements;
- (3) Parking is limited to an area that is surfaced in compliance with the Off-Street Parking Standards Manual;
- (4) The parking area is limited to nine feet (9') wide by twenty feet (20') deep;
- (5) Vehicles using the parking area will not project across any sidewalk or into the public right-of-way; and
- (6) Parking is restricted to passenger vehicles only.

b. Vehicle and Equipment Storage Surfacing Exception

Vehicle and equipment storage without hard surfacing may be permitted in the CG, M-1, M-2 and EI zoning districts provided that:

- (1) The lot is used for long-term vehicle storage, not for regular parking and/or maneuvering;
- (2) The vehicles or equipment stored are large and/or are built on tracks that could destroy normal hard surfacing;
- (3) The parking surface is compacted with six inches (6") of road base and other semi-hard material with long lasting dust control chemical applied annually;
- (4) A hard-surfaced cleaning station is installed to prevent tracking of mud and sand onto the public right-of-way; and
- (5) Any vehicles or equipment that contain oil are stored with pans, drains, or other means to ensure that any leaking oil will not enter the soil.

21A.44.100 Use and Maintenance

A. Use of Parking Areas

1. Except as otherwise provided in this section, required off street parking facilities provided for uses listed in Table 21A.44.040-A: *Minimum and Maximum Off Street Parking* shall be solely for the parking of automobiles or authorized temporary uses.

B. Maintenance

1. Space allocated to any off street loading berth or related access or maneuvering area shall not be used to satisfy the parking space requirements for any off street parking.
2. Except in the M-1, M-2, CG, and D districts, no cleaning or maintenance of loading areas using motorized equipment may be performed between ten o'clock (10:00) P.M. and seven o'clock (7:00) A.M. each day, except for snow removal.

21A.44.110 Nonconforming Parking and Loading Facilities

Nonconforming parking and loading facilities shall be subject to the standards established in Chapter 21A.38: *Nonconforming Uses and Noncomplying Structures*, and the criteria established in this section.

A. Continuation of Nonconforming Parking and Loading Facilities

Any parking spaces, loading facilities, or access to public rights-of-way that were lawfully existing or created prior to the effective date of this Ordinance, but that have since become nonconforming with the provisions of this chapter through the actions of the City or any governmental entity, shall be allowed to continue, but any expansion of the use or structure, or change of use, after the adoption date of this Ordinance shall comply with the provisions of this Chapter 21A.44: *Off Street Parking, Mobility, and Loading*.

B. Nonconformity Due to Governmental Acquisition

Where a lot, tract, or parcel is occupied by a lawful structure or use, and where the acquisition of right-of-way by eminent domain, dedication, or purchase by a City, county, state, or federal agency creates noncompliance of the parking, loading, or drive-through facilities with any requirement of this chapter, the parking, loading, or drive-through facility shall be deemed lawful and conforming. This designation shall apply only to noncompliance resulting directly from the acquisition of right-of-way.

C. Damage or Destruction

Reconstruction, reestablishment, or repair of any nonconforming parking, loading, or drive-through area involuntarily damaged or destroyed by fire, collapse, explosion or other natural cause is not required to comply with the standards of this chapter. The parking and loading facilities may be restored or continued as they existed prior to the damage or destruction, or in a manner that reduces any nonconformity that existed prior to the damage or destruction.

D. Legalization of Garages Converted to Residential Use

Garages attached to single-family and two-family residential structures converted to residential uses before April 12, 1995, and any associated front yard parking, may be legalized by complying with the following requirements:

1. The property owner shall obtain a building permit for all building modifications associated with converting the garage to residential use and the City shall inspect the conversion for substantial compliance with adopted life safety regulations.

-
2. The driveway leading to the converted garage shall not be removed without replacing the same number of parking spaces (up to the minimum required by this chapter) in a location authorized by this chapter.
 3. Parking on the driveway in the front yard is restricted to passenger vehicles only.

Definitions

AUTOMOBILE

A self-propelled vehicle with wheels that can legally operate within a public right-of-way. The term includes but is not limited to passenger cars, light trucks, and recreational vehicles.

BIODETENTION

A low impact development term also sometimes called a rain garden, biofilter or porous landscape detention that achieves on-site retention of stormwater through the use of vegetated depressions engineered to collect, store, and facilitate runoff infiltration.

CAR POOL

A group of two or more commuters, including the driver, who share the ride to and from work or other destination on a regularly scheduled basis.

CARSHARE¹²⁹

A membership-based model of car use where people rent or borrow cars for short periods of time, often by the hour. Vehicles may be made available through private individuals, a property owner/manager, or commercial companies, but are managed through a facilitator.

CHANGE OF USE

The replacement of an existing use by a new use, or a change in the nature of an existing. A change of ownership, tenancy, name or management, or a change in product or service within the same use classification where the previous nature of the use, line of business, or other function is substantially unchanged is not a change of use. The conversion of existing residential units to condominiums is not a change of use.

COMMERCIAL VEHICLE

A vehicle associated with a business that exceeds one (1) ton capacity. This includes but is not limited to buses, dump trucks, stake body trucks, step vans, tow trucks and tractor trailers. Taxis and limousines shall also be considered commercial vehicles.

DESIGN CAPACITY

The maximum occupancy of a building or structure based on the Fire and/or Building Code, whichever allows occupancy by a larger group of people.

DEVELOPMENT

- A. The carrying out of any building activity, the making of any material change in the use or appearance of any structure or land, or the dividing of land into parcels by any person. The following activities or uses shall be taken for the purposes of these regulations to involve "development":
1. The construction of any principal building or structure;
 2. Increase in the intensity of use of land, such as an increase in the number of dwelling units or an increase in nonresidential use intensity that requires additional parking;
 3. Alteration of a shore or bank of a pond, river, stream, lake or other waterway;
 4. Commencement of drilling (except to obtain soil samples), the driving of piles, or excavation on a parcel of land;
 5. Demolition of a structure;
 6. Clearing of land as an adjunct of construction, including clearing or removal of vegetation and including any significant disturbance of vegetation or soil manipulation; and

7. Deposit of refuse, solid or liquid waste, or fill on a parcel of land.

B. The following operations or uses shall not be taken for the purpose of these regulations to involve "development":

1. Work by a highway or road agency or railroad company for the maintenance of a road or railroad track, if the work is carried out on land within the boundaries of the right of way;
2. Utility installations as stated in subsection 21A.02.050.B of this title;
3. Landscaping for residential uses; and
4. Work involving the maintenance of existing landscaped areas and existing rights of way such as setbacks and other planting areas.

FLOOR AREA, GROSS¹³⁸

- A. For determining size of establishment, the sum of the gross horizontal area of all floors of the building measured from the exterior face of the exterior walls or from the centerline of walls separating two (2) buildings. The floor area of a building shall include basement floor area, penthouses, attic space having headroom of seven feet (7') or more, interior balconies and mezzanines, enclosed porches, and floor area devoted to accessory uses. Space devoted to open air off street parking or loading shall not be included in floor area.
- B. The floor area of structures devoted to bulk storage of materials including, but not limited to, grain elevators and petroleum storage tanks, shall be determined on the basis of height in feet (i.e., 10 feet in height shall equal 1 floor).

FLOOR AREA, USABLE¹³⁸

For determining off street parking and loading requirements, the sum of the gross horizontal areas of all floors of the building, as measured from the outside of the exterior walls, devoted to the principal use, including accessory storage areas located within selling or working space such as counters, racks, or closets, and any floor area devoted to retailing activities, to the production or processing of goods or to business or professional offices. Floor area for the purposes of measurement for off street parking spaces shall not include:

- A. Floor area devoted primarily to mechanical equipment or unfinished storage areas;
- B. Floor area devoted to off street parking or loading facilities, including aisles, ramps, and maneuvering space.

GARAGE

An accessory building or portion of a building designed or used for the storage of vehicles used by the occupants of the principle building.

GARAGE, ATTACHED

A garage that has a roof or wall of which fifty percent (50%) or more is attached to and in common with a principal building. An attached garage shall be considered part of the principal building and shall be subject to all yard requirements of the principal building.

HARD SURFACED¹³⁸

A concrete, asphalt, brick, stone, turf block, or other surface approved by the City Engineer that is suitable for vehicle traffic.

OFF STREET PARKING¹³⁸

A site or portion of a site devoted to the parking of automobiles in an area that is not a public or private street or other public right-of-way, including parking spaces, aisles, driveways, and associated landscaped areas.

OFF-SITE¹³⁸

A lot that is separate from the lot on which the principal use is located.

OUTDOOR DINING¹³⁸

A dining area with seats and/or table(s) located outdoors of a restaurant, brewpub, social club, tavern, market, deli, or other retail sales establishment that sells food and/or drinks, and which is either:

- A. Located entirely outside the walls of the building of the subject business, or
- B. Enclosed on two (2) sides or less by the walls of the building with or without a solid roof cover, or
- C. Enclosed on three (3) sides by the walls of the building without a solid roof cover.

PARKING GARAGE

A structure or part of a structure used primarily for the housing, parking, or storage of automobiles.

PARKING LOT

An area on the surface of the land used for the parking of more than four (4) automobiles. Areas designated for the display of new and used vehicles for sale are not included in this definition.

PARKING, OFF-SITE

An off-street parking area intended to serve one or more uses and that is located on a different parcel or lot than the use(s) it is intended to serve.

PARK AND RIDE LOT¹³⁸

An area or structure intended to accommodate parked vehicles for the general public, where commuters park their vehicles and continue travel to another destination via public transit, carpool, vanpool, or bicycle. Parking lot may be shared with other uses or stand alone.

PARKING, SHARED

Joint use of a parking lot or area for more than one principal use.

PARKING SPACE¹³⁸

Space within a parking area of certain dimensions as defined in Chapter 21A.44 of this title, exclusive of access drives, aisles, ramps, columns, for the storage of one vehicle.

PARKING STUDY

A study prepared by a licensed professional traffic engineer specifically addressing the parking demand generated by a use and which provides information necessary to determine whether proposed parking will have a material negative impact to adjacent or neighboring properties.

PARKING, TANDEM¹³⁸

The in-line parking of one vehicle behind another in such a way that one parking space can only be accessed through another parking space.

PLANNING DIRECTOR¹⁴⁴

The director of the Salt Lake City Planning Division, or his/her designee.

PRIMARY ENTRANCE

The entrance to a building, parcel, or development most used by the public for day-to-day ingress and egress.

STREET¹⁴⁴

A vehicular way which may also serve for all or part of its width as a way for pedestrian traffic, whether called street, highway, thoroughfare, parkway, throughway, road, avenue, boulevard, lane, place, mall or otherwise designated.

VANPOOL

A group of seven (7) to fifteen (15) commuters, including the driver, who share the ride to and from work or other destination on a regularly scheduled basis.

VEHICLE¹³⁸

A device by which any person or property may be transported upon a public highway except devices used exclusively upon stationary rails or tracks or exclusively moved by human power.

VEHICLE, ELECTRIC

A device which is considered a vehicle that uses electricity as its primary source of power, such as a plug-in electric vehicle or a plug-in hybrid electric vehicle. An electric vehicle does not include devices that are moved by human power.

VEHICLE, RECREATIONAL¹³⁸

Any motorized vehicle and/or associated non-motorized equipment used for camping, traveling, boating, or other leisure activities including, but not limited to campers, boats, travel trailers, motor homes, snow mobiles, wave runners, and other vehicles designed for traveling on water (motorized and non-motorized). Trailers used for transporting this type of vehicle are also included within this definition.

ATTACHMENT C: OFF-STREET PARKING MANUAL

Salt Lake City // 2019

OFF-STREET PARKING

STANDARDS MANUAL



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The standards of this manual can change, visit our website to ensure latest version of the manual.

Version 1.0 // NOV 2019

[slc.gov/page - TBD](#)

1.1 GENERAL OFF STREET PARKING DIMENSIONS

- 1.1.1 All off-street parking designs shall conform to the accompanying standards and be approved by the Transportation Director or his/her designee.¹
- 1.1.2 The dimensions for parking spaces and associated aisles are established by the Transportation Division and are set forth in [Table 1](#) of this manual.²
- 1.1.3 Stalls shall be striped to 80% of the vehicle projection to encourage pulling further into the stall.³
- 1.1.4 Substandard stalls shall not be allowed in new uses or developments even when they are not needed to meet parking requirements.
- Designated compact car stalls shall not be allowed. The dimensions given in the policy are for a 'one size fits all' design.⁴
- 1.1.5 Requests for parking angles other than those shown on [Table 1](#) of this manual may be approved by the city Transportation Director or his/her designee.⁵
- 1.1.6 If a public alley is used as a parking aisle additional space shall be required on the lot to provide the full width of aisle as required on [Table 1](#) of this manual.⁶
- 1.1.7 Tandem parking is allowed for single-family dwellings, two-family dwellings or twin homes.⁷
- 1.1.8 The dimensions of parking spaces in a valet attended parking lot can be modified with approval of the Transportation Director or his/her designee.⁸
- 1.1.9 Parking spaces in an automated parking garage are exempt from the off-street parking dimensions found in this [Table 1](#) provided the design of the automated parking garage has been approved by the Transportation Director or his/her designee.⁹

1 From SLC Engineering Standards – Section F1.c2.

2 From current 21A.44.020.E(1).

3 From SLC Engineering Standards – Section F1.c2.

4 From SLC Engineering Standards – Section F1.c2.

5 From current 21A.44.020.E(2)(B). Revised to reflect staff's redline edits.

6 From current 21A.44.020.E(2)(C). Revised to reflect staff's redline edits.

7 New provision to reflect staff's redline edits

8 From current 21A.44.020.E(2)(D).

9 From current 21A.44.020.E(2)(E).

Figure 1 // Diagrammatic Legend for Table 1

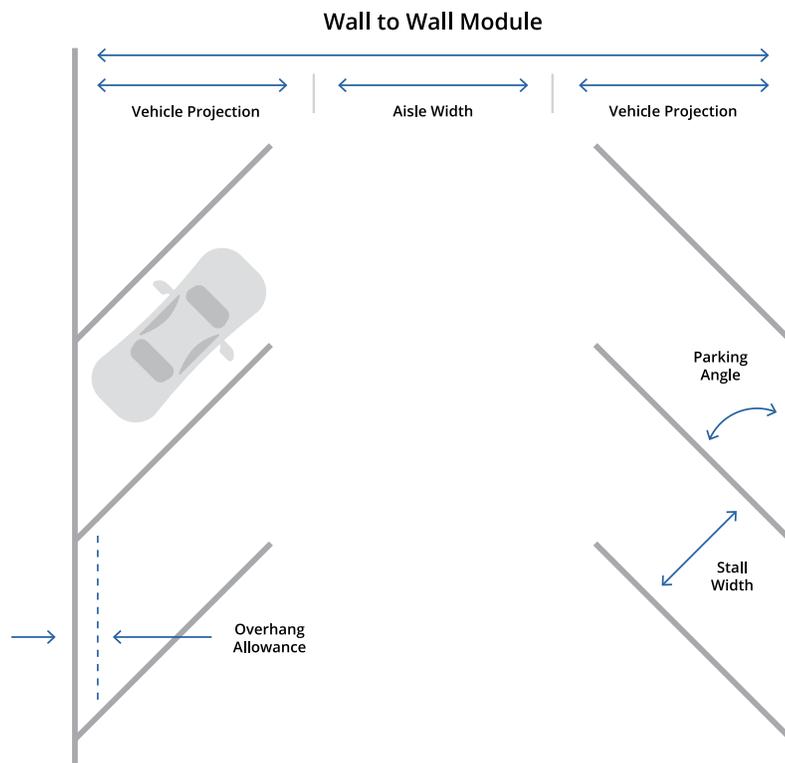


Table 1 // Off Street parking Dimensions ¹⁰

Parking Angle	Stall Width	Vehicle Projection	Aisle Width	Wall-to-Wall Module Width	Interlock Reduction	Overhang Allowance
0	22'0"	8'3"	12'8"	29'2"	0'0"	2'0"
45	8'3"	16'10"	14'11"	48'7"	2'3"	2'0"
50	8'3"	17'5"	15'6"	50'4"	2'0"	2'0"
55	8'3"	17'11"	16'2"	52'0"	1'10"	2'1"
60	8'3"	18'3"	16'10"	53'4"	1'7"	2'2"
65	8'3"	18'6"	17'9"	54'9"	1'4"	2'3"
70	8'3"	18'7"	18'7"	55'9"	1'1"	2'4"
75	8'3"	18'6"	20'1"	57'1"	0'10"	2'5"
90	8'3"	17'6"	24'10"	59'10"	0'0"	2'6"

¹⁰ Multiple adjustments have been suggested by staff to simplify the table so that it is more intuitive for the audience. Future adjustments to the table will be considered and made by the Transportation Department as part of future updates to the Off-Street Parking Standards Manual.

Table 1 // Continued

Parking Angle	Stall Width	Vehicle Projection	Aisle Width	Wall-to-Wall Module Width	Interlock Reduction	Overhang Allowance
0	22'0"	8'6"	11'11"	28'11"	0'0"	2'0"
45	8'6"	16'10"	14'2"	47'10"	2'3"	2'0"
50	8'6"	17'5"	14'9"	49'7"	2'0"	2'0"
55	8'6"	17'11"	15'5"	51'3"	1'10"	2'1"
60	8'6"	18'3"	16'1"	52'7"	1'7"	2'2"
65	8'6"	18'6"	17'0"	54'0"	1'4"	2'3"
70	8'6"	18'7"	17'10"	55'0"	1'1"	2'4"
75	8'6"	18'6"	19'4"	56'4"	0'10"	2'5"
90	8'6"	17'6"	24'1"	59'1"	0'0"	2'6"
0	22'0"	8'9"	10'8"	28'2"	0'0"	2'0"
45	8'9"	16'10"	13'5"	47'1"	2'3"	2'0"
50	8'9"	17'5"	14'0"	48'10"	2'0"	2'0"
55	8'9"	17'11"	14'8"	50'6"	1'10"	2'1"
60	8'9"	18'3"	15'4"	51'10"	1'7"	2'2"
65	8'9"	18'6"	16'3"	53'3"	1'4"	2'3"
70	8'9"	18'7"	17'1"	54'3"	1'1"	2'4"
75	8'9"	18'6"	18'7"	55'7"	0'10"	2'5"
90	8'9"	17'6"	23'4"	58'4"	0'0"	2'6"
0	22'0"	9'0"	9'5"	27'5"	0'0"	2'0"
45	9'0"	16'10"	12'6"	46'4"	2'3"	2'0"
50	9'0"	17'5"	13'3"	48'1"	2'0"	2'0"
55	9'0"	17'11"	13'11"	49'9"	1'10"	2'1"
60	9'0"	18'3"	14'7"	51'1"	1'7"	2'2"
65	9'0"	18'6"	15'6"	52'6"	1'4"	2'3"
70	9'0"	18'7"	16'4"	53'6"	1'1"	2'4"
75	9'0"	18'6"	17'10"	54'10"	0'10"	2'5"
90	9'0"	17'6"	22'7"	57'7"	0'0"	2'6"

1.2 DRIVE AISLES¹¹

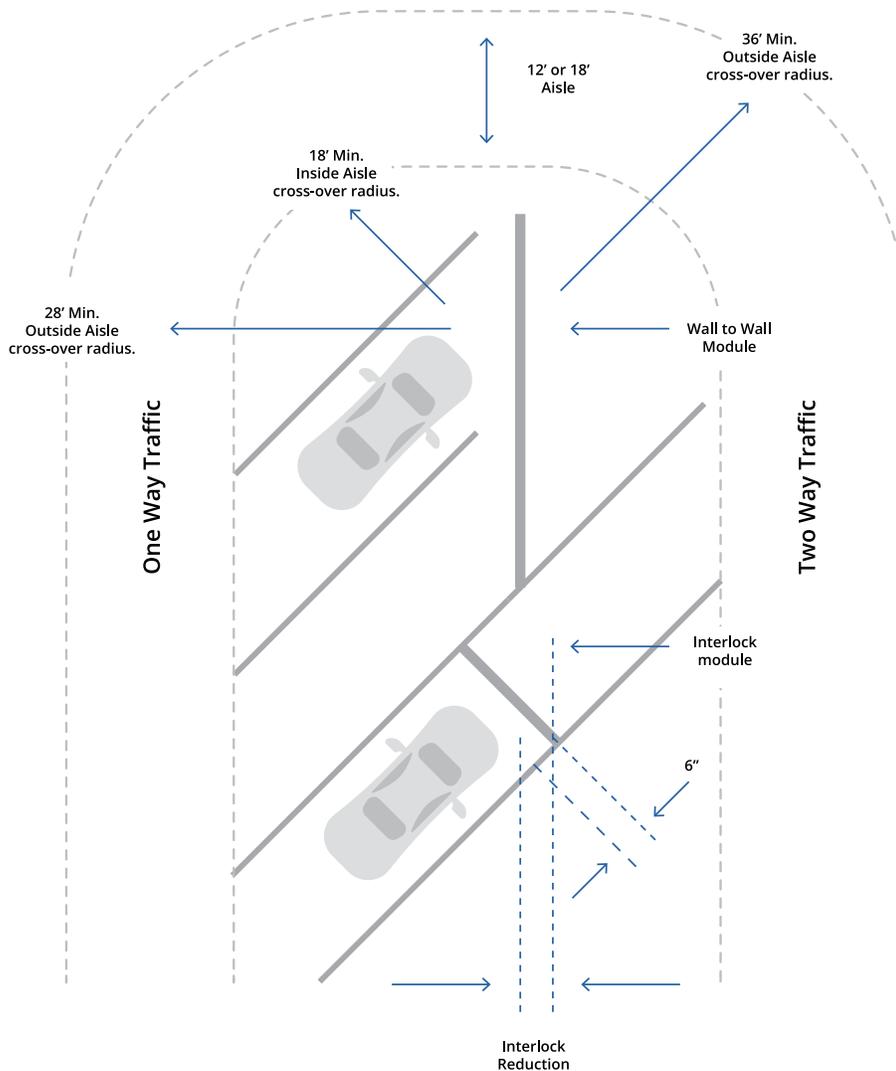
1.2.1 Maneuverability around the end of the aisles (aisle cross-overs) is dependent on the minimum acceptable turning radius of the vehicle.

For one-way traffic, the minimum inside radius is 18 feet and the minimum outside radius is 28 feet. For two-way traffic, the minimum inside radius is 18 feet and the minimum outside radius is 36 feet.

If perimeter parking is provided, then the cross-over aisle dimension shall be the greater of that required for access to the stall or that required for turning.

1.2.2 The width of the drive aisle shall be increased by one foot when no curb stops are provided.

Figure 2 // Aisle Dimensional Standards

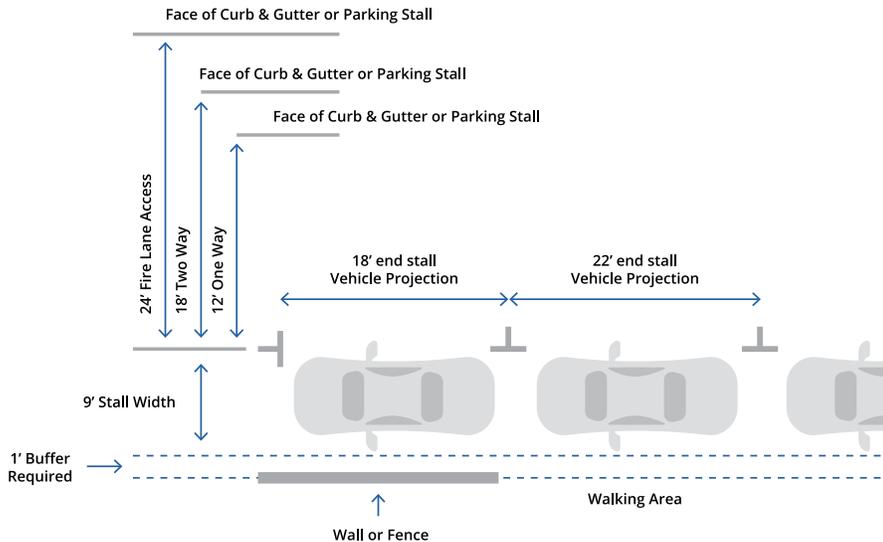


¹¹ From SLC Engineering Standards – Section F1.c2.

1.3 PARALLEL PARKING¹²

1.3.1 Parallel parking spaces shall comply with the dimensional standards provided in *Figure 3: Parallel Parking Dimensional Standards*.

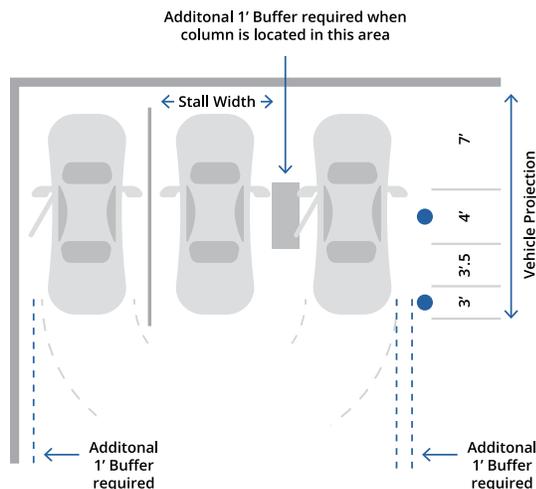
Figure 3 // Parallel Parking Dimensional Standards



1.4 PARKING STALLS ADJACENT TO COLUMNS OR SIDEWALKS¹³

1.4.1 The stall width for parking spaces located adjacent to walls or columns, where door opening is impacted, shall be one foot (1') wider to accommodate door opening clearance and vehicle maneuverability.

Figure 4 // Dimensional Standards for Parking Adjacent to Columns or Side Walls



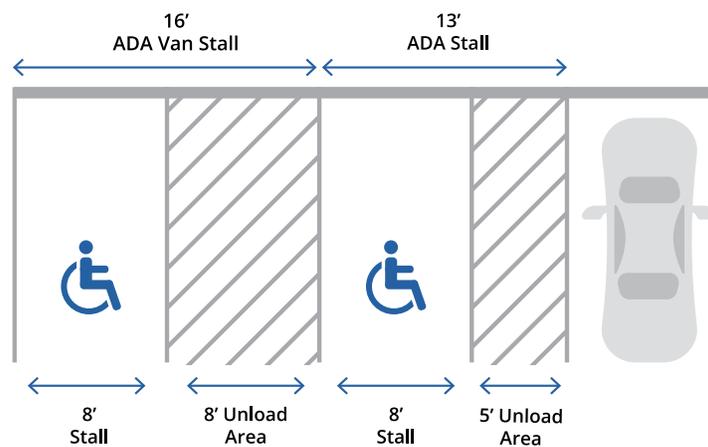
¹² From SLC Engineering Standards – Section F1.c.2 and current 21A.44.020.E(2)(A). Modified to reflect staff's redline edits from previous staff comments.

¹³ From SLC Engineering Standards – Section F1.c.2.

1.5 ADA PARKING ¹⁴

- 1.5.1 The first ADA (handicap) stall shall be van accessible, sixteen feet (16') wide (*eight foot (8') stall and eight foot (8') unload area*).
- 1.5.2 The standard ADA stalls after that shall be a minimum of thirteen feet (13') wide (*eight foot (8') stall and five foot (5') unload area*).
- 1.5.3 The number and design of accessible parking spaces shall be pursuant to the International Building Code (IBC) as adopted in the Salt Lake City Code and the Americans with Disabilities Act (ADA), as amended.

Figure 6 // ADA Parking



¹⁴ From SLC Engineering Standards – Section F1.c2.

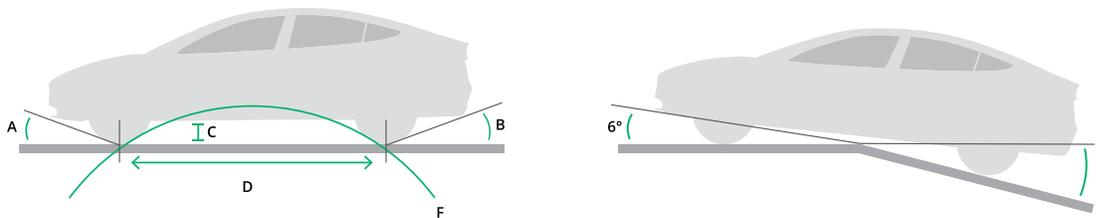
2.1 MAXIMUM DRIVEWAY SLOPES AND CRITICAL ANGLES ¹⁵

- 2.1.1 Driveways leaving a public right-of-way should not exceed a maximum slope of eight percent (8%) or (4.57°) from gutter to property line.
- 2.1.2 The slope should be transitioned beyond the property line no more than a maximum of sixteen percent (16%) or (9.09°) average grade to the parking pad.
- 2.1.3 Driveways cross-slopes of four percent (4%) to six percent (6%) or (2.3° to 3.4°) maximum.

Table 2 // Drive Slopes and Critical Angles

Driveway Slope and Angle Requirements	Stall Width
A) Maximum approach angle	20.2° = 36.8%
B) Maximum departure angle	9.2° = 16.2%
C) Minimum running ground clearance	4.3"
D) Design vehicle wheelbase	10.8' (Salt Lake City Design = 11')
E) Maximum ramp breakover angle	8.2° (Salt Lake City Design = 10.5% (6°))
F) Crest of curve arc	Design vehicle wheelbase ÷ Maximum ramp breakover angle (Salt Lake City Design = 1.05)

Figure 6 // Diagrammatic Legend for *Table 2*

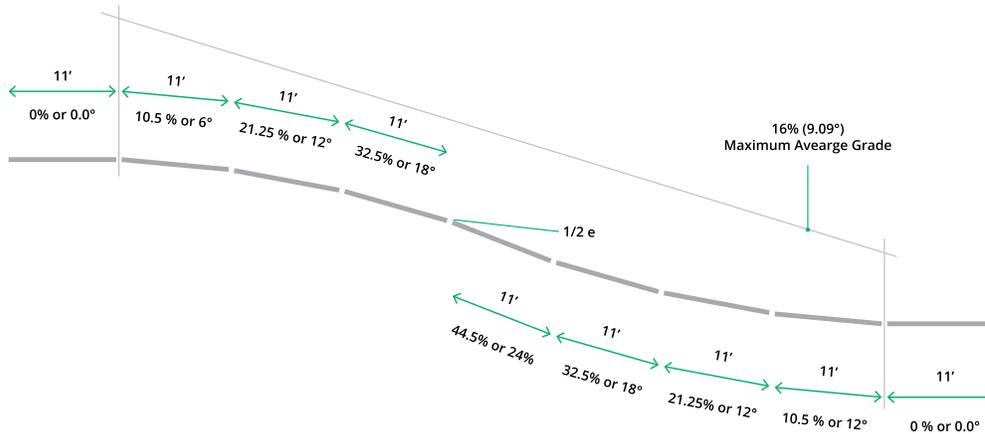


- A) Maximum approach angle
- B) Maximum departure angle
- C) Minimum running ground clearance

- D) Design vehicle wheel base
- E) Maximum ramp breakover angle
- F) Crest of curve arc

¹⁵ From SLC Engineering Standards - Section E2.b1.

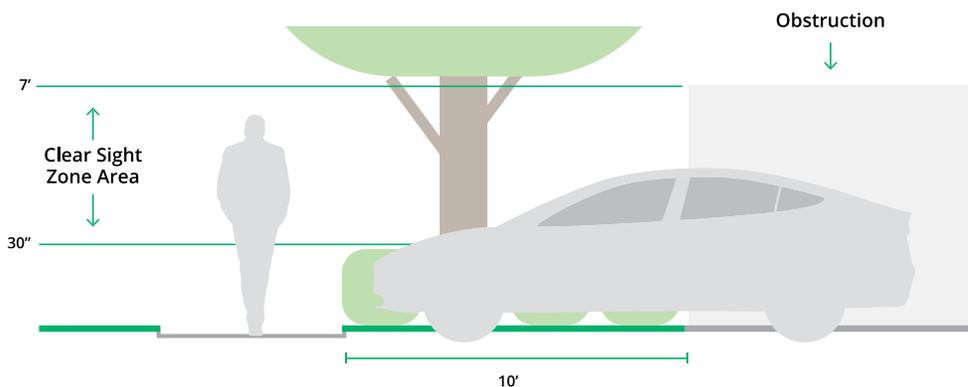
Figure 7 // Driveway Slope



2.2 VISUALLY CLEAR SIGHT ZONE AREAS AT RESIDENTIAL DRIVEWAYS ¹⁶

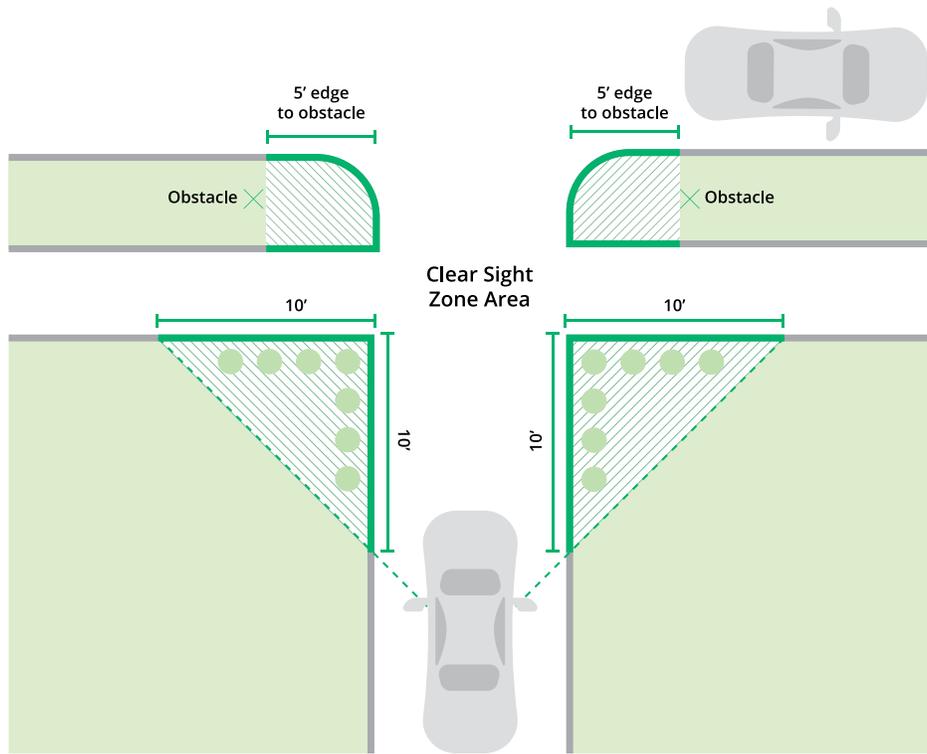
- 2.2.1 Bushes, trees, and other types of vegetation as well as walls and fences can visually block pedestrians, bicyclists, and cars from being seen by drivers entering the street from driveways. To provide the needed visibility for safety, vegetation should be kept trimmed within the clear sight zone areas on both sides of driveways as illustrated in [Figures 8 and 9](#).
- 2.2.2 A clear sight zone area is achieved when vision is not blocked between thirty inches (30") and seven feet (7') above ground within a ten foot (10') by ten foot (10') triangle on both sides of the driveway and between the sidewalk and the street.
- 2.2.3 A new proposed driveway needs to provide a five foot (5') clearance in the park strip between the edge of driveway and edge of obstacle such as trees, poles and fire hydrants as illustrated in [Figure 9](#).
- 2.2.4 When permitted by the zoning ordinance, a proposed retaining wall or fence located in the clear sight zone area as illustrated in [Figures 8 and 9](#).

Figure 8 // Clear Sight Zone Perspective from Sidewalk



¹⁶ From SLC Engineering Standards - Section E2.c2.

Figure 9 // Clear Sight Zone Dimensions



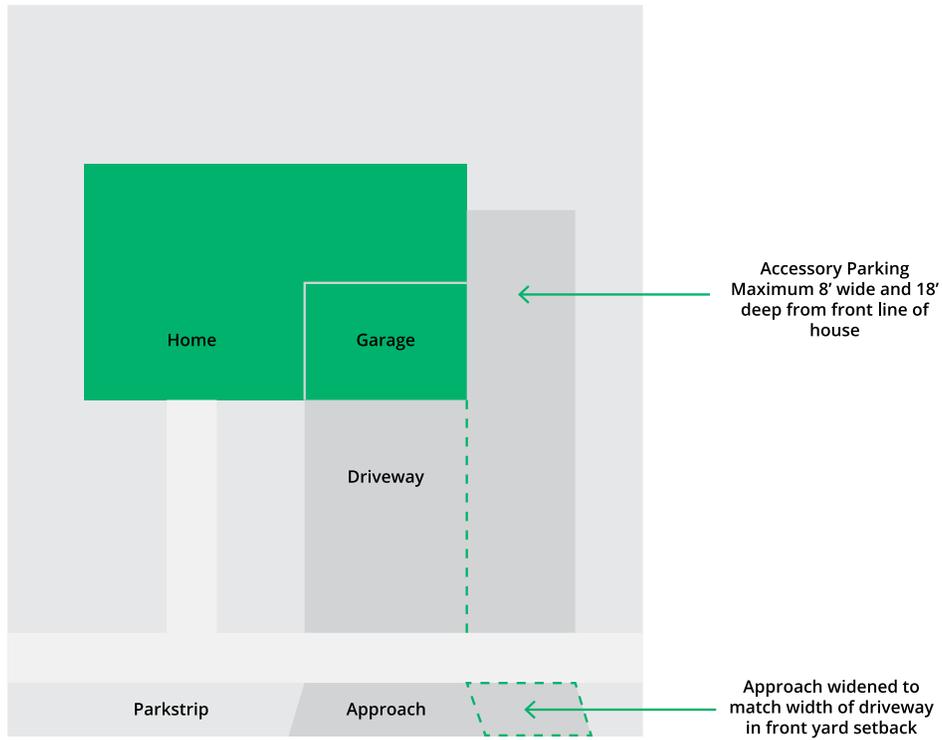
2.3 ADDITIONAL PARKING ACCESS STANDARDS ¹⁷

2.3.1 Access to additional parking shall be provided by either;

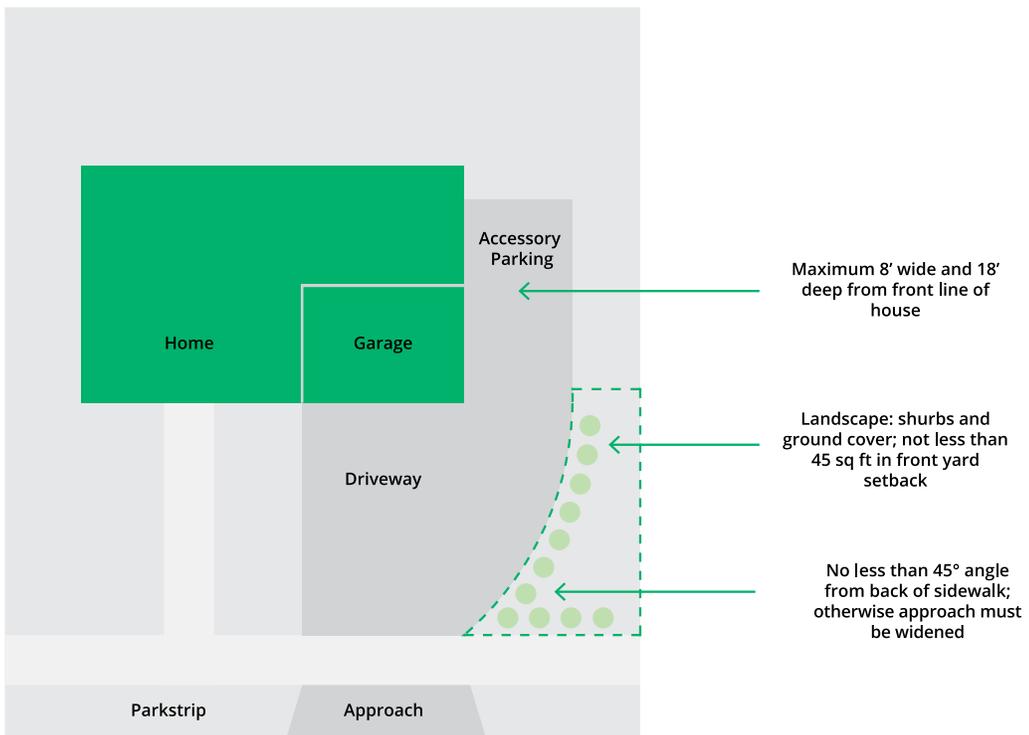
- A. Widening the approach from the street to match the width of the new driveway provided all provisions for driveways from [21A.44.060.B.3.c](#) can be met;
- B. A driveway taper from the sidewalk at no less than a forty five degree (45°) angle with the remnant area in the front yard area landscaped with a minimum of shrubs and ground cover, provided that this option is not allowed if the remnant landscaped area is less than forty five (45) square feet or if curb, gutter and sidewalk are not present.

¹⁷ Newly proposed section to clarify options for drive approaches to widened driveways.

Option A //



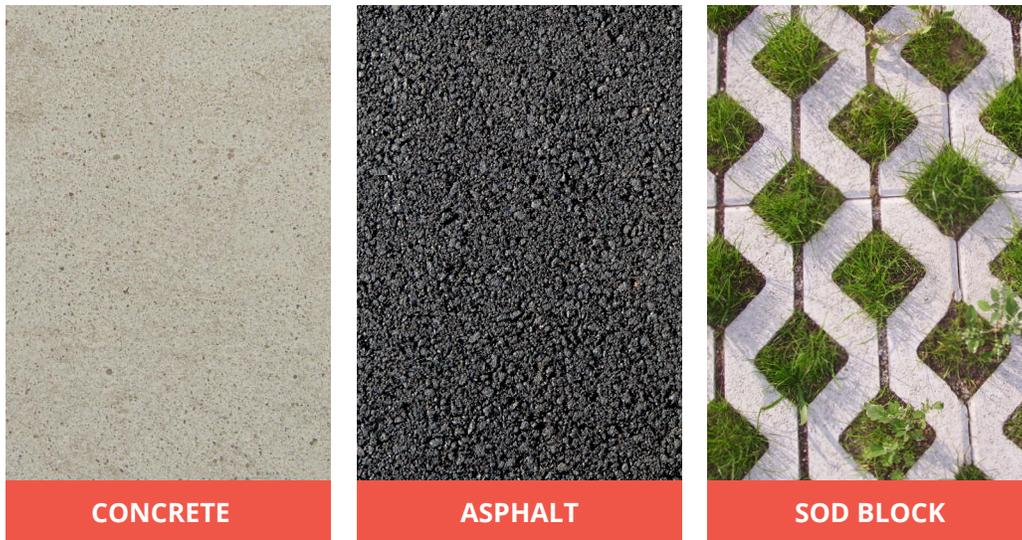
Option B //



3.1 MATERIALS ¹⁸

- 3.1.1 All driveways, parking areas or lots, and loading berths shall be improved and maintained as hard surface according to the following standards:
- A. Materials used for driveway and parking areas for manufacturing, commercial or multi-family residential uses are limited to the following:
 - 1) Concrete: Minimum four inch (4") depth that covers the entire area used for access, maneuvering, and parking;
 - 2) Asphalt: Minimum two and one-half inch (2 ½") depth that covers the entire area used for access, maneuvering, and parking; or
 - 3) Masonry or stone pavers rated for vehicle weight that cover the entire area used for access, maneuvering, and parking.
 - B. Materials used for driveway and parking areas for any single-family, two-family, or twin home uses are limited to the following:
 - 1) Concrete: Minimum four inch (4") depth that covers the entire area used for access, maneuvering, and parking;
 - 2) Asphalt: Minimum two and one-half inch (2 ½") depth that covers the entire area used for access, maneuvering, and parking; or
 - 3) Masonry or stone pavers rated for vehicle weight that cover the entire area used for access, maneuvering, and parking;
 - 4) Sod block

Driveway Surface Materials //



CONCRETE

ASPHALT

SOD BLOCK

¹⁸ Newly proposed standards related to acceptable hard surface materials, which are currently defined to "concrete, asphalt, brick, stone, turf block, or other surface approved by the City Engineer that is suitable for vehicle traffic"



MASONRY



STONE PAVERS

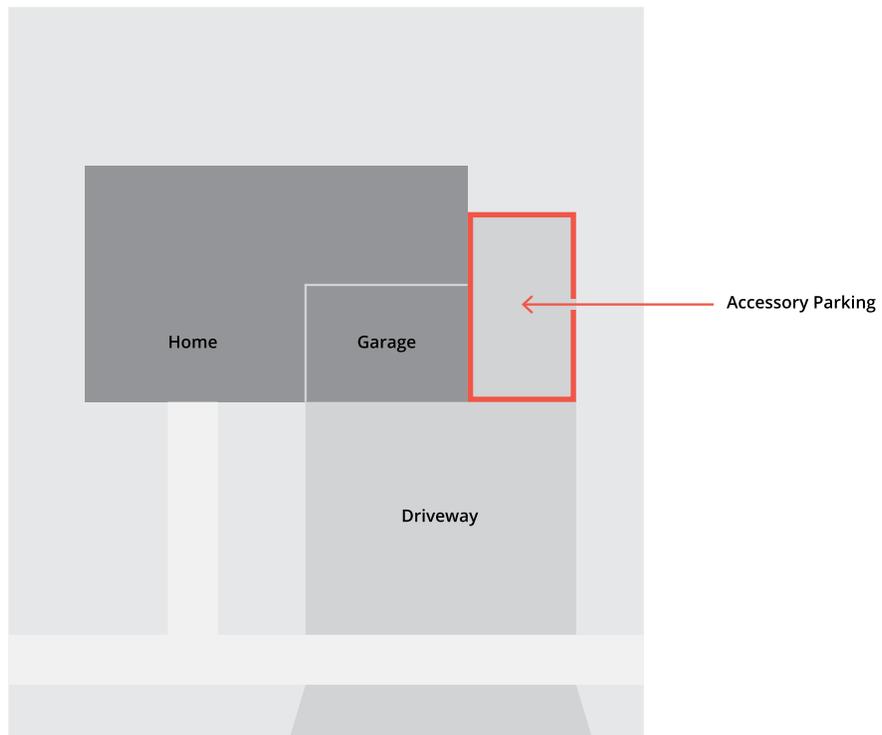
3.1.2 Driveway and parking spaces for single-family, two-family, or twin home uses can be formed as a continuous pad or as drive strips designed to accommodate the wheels of the vehicle.

- A. Each drive strip should be between sixteen inches (16") and twenty four inches (24") wide.
- B. The interior space between the drive strips may be filled with ground cover, landscaping or gravel, but not dirt or road base.

3.1.3 Surface Materials for Recreational Vehicle Parking

- A. The portion of the driveway providing access to the accessory parking area and the accessory parking area itself shall be constructed with:
 - 1) An approved hard surface material or drive strips as described in subsection 3.1.1 of this manual; or
 - 2) A gravel or crushed concrete surface, provided:
 - I) The gravel or crushed concrete is at least four inches (4") deep, compacted, and is sized between half inch (½") to one and one half inch (1 ½").
 - II) A poured concrete or masonry border with a minimum four inch (4") width and four inch (4") depth is constructed on all exterior sides of the surface with masonry being limited to either brick, stone or precast concrete.
 - III) Road-base or other materials shall not be substituted for gravel or crushed concrete.
 - IV) The surface material is properly maintained, kept free of weeds or other vegetation and is kept fully contained within the allowed area.

Recreational Vehicle Parking //



Alternative Surface Materials //



Border Materials //



4.1 LOCATION AND DISTRIBUTION¹⁹

- 4.1.1 Except for multi-family uses that have provided a portion of their required bicycle parking spaces in an enclosed facility within a secure location, bicycle parking spaces, shall be:
- A. Located on the same lot as the principal use;
 - B. Located within a principal building or located outside a principal building in a location(s) that is no more than fifty feet (50') from the primary entrance of each principal building, and that does not interfere with pedestrian access to any primary entrance of a building;
 - C. Distributed to serve all buildings if the development has multiple buildings on one or more lots;²⁰
 - D. Racks should:
 - 1) Be visible from within the building.
 - 2) Be placed in parking garages only if the garage serves employees (not visitors) and is staffed.²¹
 - 3) Be connected to the right-of-way, sidewalk or bicycle lane by a path that is clearly distinguished from the parking lot and drive lanes by color, materials, surface texture, or grade separation.²²

¹⁹ From current 21A.44.050.B(4). Requirements simplified to avoid subjective standards.

Did not carry forward design standards from 21A.44.050.B(5).

²⁰ Revised to require distribution to different buildings, but not different entrances of a principal building.

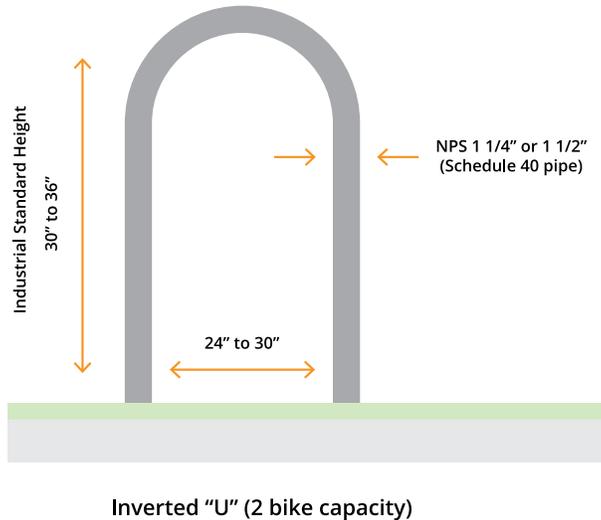
²¹ From SLC Engineering Standards – Section F1.f2

²² From current 21A.44.050.B(4). Requirement simplified to avoid subjective standards.

4.2 PREFERRED BICYCLE RACK

- 4.2.1 Concrete Pier Foundations: On private property, rack feet may be submerged in concrete without hardware
- 4.2.2 Material: Galvanized, paint over galvanization, powder-coated, or stainless steel.

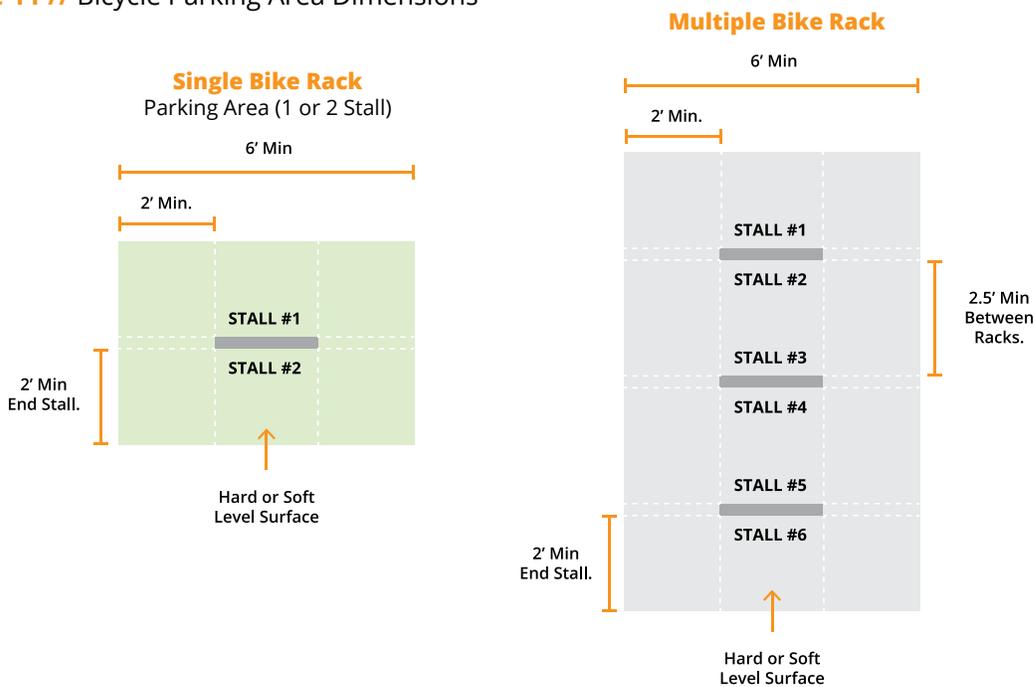
Figure 10 // Standard Dimensions for an Inverted “U” Rack



4.3 BICYCLE RACK PARKING AREA DIMENSIONS

- 4.3.1 Bicycle parking stall dimensions shall be provided as illustrated in *Figure 11: Bicycle Parking Area Dimensions.*

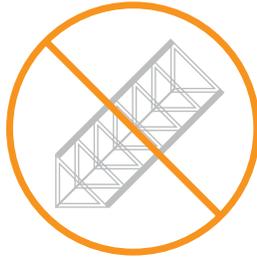
Figure 11 // Bicycle Parking Area Dimensions



4.4 UNPERMITTED RACKS

4.4.1 The following racks do not meet the bicycle parking standard and will not be permitted to meet the requirements of Salt Lake City's bicycle parking ordinance (*Salt Lake City Code, Section 21A.44.080*).

Figure 12 // Unpermitted Bicycle Racks



TOAST



WAVE



COMB

4.5 BICYCLE PARKING AREA OUTLINE

4.5.1 It is recommended that paint or pavers be used to outline the footprint and discourage intrusion of merchandise, motor vehicles, etc., into the bicycle parking area.



4.6 COVERED BICYCLE RACKS

- 4.6.1 It is recommended that bicycle racks be installed under an overhang or roof (pictured), with a seven foot (7') minimum overhead clearance. The roof should cover the entire bicycle area footprint.



4.7 CUSTOM RACK DESIGNS

- 4.7.1 Bicycle racks shall be approved by the Transportation Division on a case-by-case basis and shall:
- A. Support the bicycle frame at two contact points;
 - B. Meet specifications for materials and diameter provided in [Section 4.1: Preferred Bicycle Rack](#); and
 - C. Enable the frame and one wheel to be secured with a U-lock.

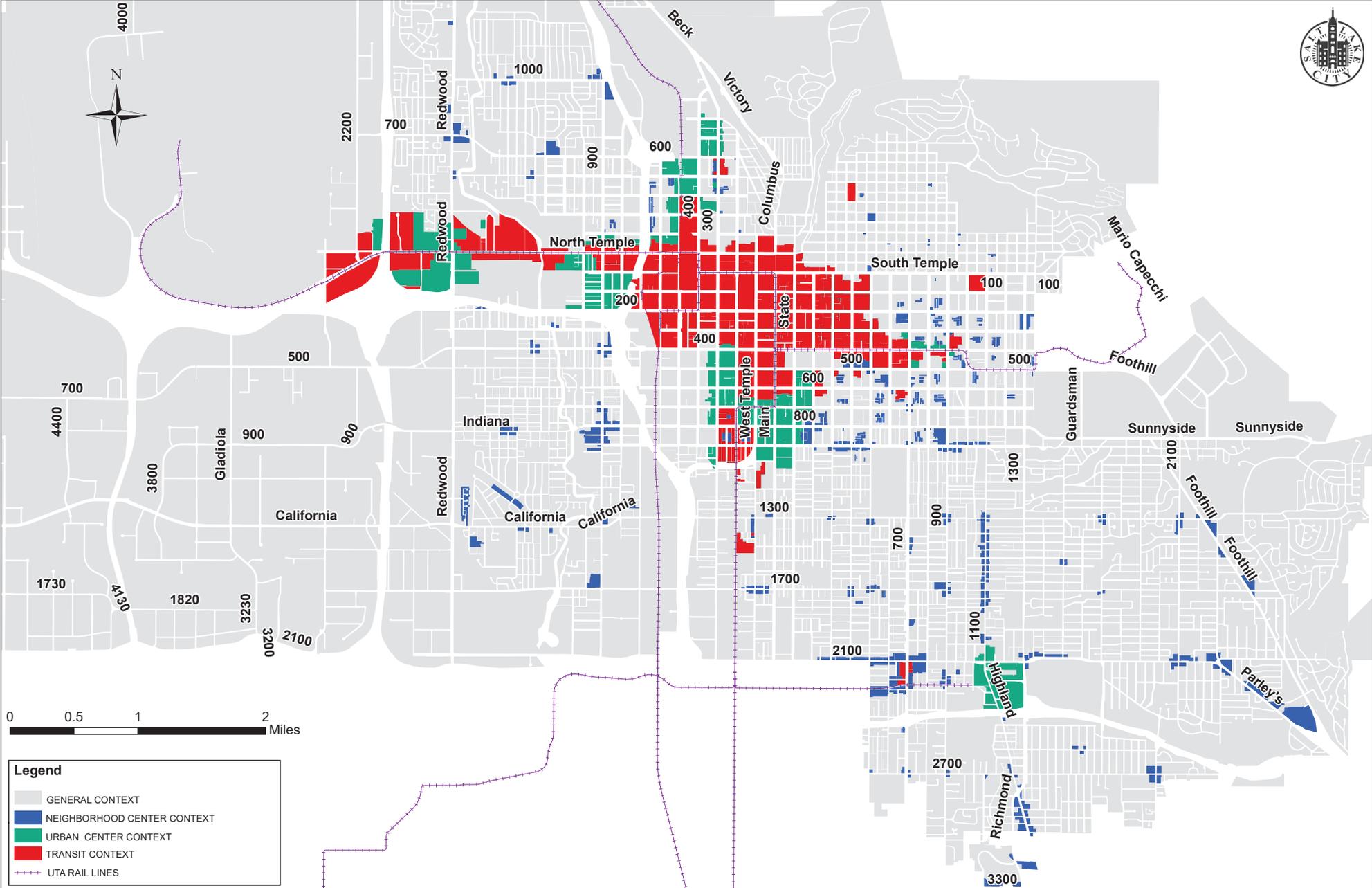
4.8 RACKS ON PUBLIC PROPERTY

- 4.8.1 Bicycle racks located on public property (sidewalk, park strip, etc.) shall be approved by the SLC Transportation Division's Design Section and City Property Management ([see Section F1.g1, "Rack Install on City Property"](#)).²³

²³ STAFF: This information was not provided in the materials we received for the manual. If it is available, we can include those standards here and remove the reference.

ATTACHMENT D: PARKING CONTEXT MAP

Map of Proposed Parking Context Areas (based on current zoning districts)



ATTACHMENT E: ANALYSIS OF STANDARDS

As per section 21A.50.050, a decision to amend the text of this title or the zoning map by general amendment is a matter committed to the legislative discretion of the city council and is not controlled by any one standard.

Factor	Finding	Rationale
1. Whether a proposed text amendment is consistent with the purposes, goals, objectives, and policies of the city as stated through its various adopted planning documents;	Complies	<p>As outlined above in the ‘Key Considerations’ section, the proposed text amendments support multiple principles and initiatives of Plan Salt Lake (2015).</p> <p>In addition, the proposed amendments are consistent with many of the goals and objectives of Salt Lake City’s Neighborhood Master Plans. A comprehensive list of those goals can be found in Attachment F of this report.</p> <p>Staff finds that the proposed text amendments are consistent with City purposes, goals, and policies.</p>
2. Whether a proposed text amendment furthers the specific purpose statements of the zoning ordinance;	Complies	<p>The proposed text amendments advance the following purposes and intents of the Zoning Ordinance:</p> <p><i>...to promote the health, safety, morals, convenience, order, prosperity and welfare of the present and future inhabitants of Salt Lake City, to implement the adopted plans of the city...</i></p> <p>Specifically, the purposes are to:</p> <p><i>A. Lessen congestion in the streets or roads:</i></p> <p>By introducing four different parking contexts, the proposed ordinance encourages more multi-modal transportation solutions. Traditional development patterns are encouraged through a reduction of required surface parking, making areas more walkable and conducive to mass transit.</p> <p>In the Neighborhood Center context, parking minimums were increased for restaurant and retail uses. The intention is to reduce parking from spilling into neighboring residential streets.</p> <p><i>D. Classify land uses and distribute land development and utilization;</i></p> <p>The introduction of parking context areas would create more specific classifications for parking as a land use and allows for more efficient utilization of land for development. The general reductions of minimum parking requirements and revised parking alternatives are proposed to allow for</p>

		<p>more remnant parcels or underutilized land to be redeveloped.</p> <p><i>E. Protect the tax base;</i></p> <p>The proposed standards encourage economic vitality by reducing constraints for the reuse of existing buildings and by lowering costs associated with installing parking that would otherwise not be in line with market demand.</p> <p><i>F. Secure economy in governmental expenditures;</i></p> <p>Low, wide, suburban style development yields a very inefficient use of a city's tax dollars by requiring a large amount of service (road maintenance, snow removal, utility lines) for a very small percentage of users. Surface parking exasperates that inefficiency by spreading taxable entities over a larger area. By reducing excessive surface parking, the ordinance encourages tighter more traditional style development especially in the Transit, Urban Center, and Neighborhood Center context areas. That development pattern is intended to encourage a more efficient use of governmental expenditures.</p> <p>Additionally, the proposed ordinance has been streamlined in a way that is easier and less time consuming for City Staff to administer and interpret, which is anticipated to result in added governmental economy.</p> <p><i>G. Foster the city's industrial, business and residential development.</i></p> <p>The City's industrial areas are proposed within the General parking context. Many of the minimum and maximum parking requirements for common industrial and manufacturing uses are proposed to be removed. The intent is to allow the businesses to install parking according to their needs, rather than by an imposed number. Our studies show that most industrial businesses were required to provide more parking than they needed. Businesses can be more profitable by not spending money on parking that would not be utilized.</p> <p>Within the Transit and Urban Center contexts, many of the minimums and maximums parking requirements have been reduced. This would allow for more residential infill development and encourages use of alternative transportations solutions such as car share or mass transit.</p>
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		<p>Affordable housing has also been proposed with lower parking requirements in effort to encourage additional development.</p> <p><i>H. Protect the environment.</i></p> <p>The proposed amendments would foster increased mobility choices and allow for a reduced dependency on the automobile. They would reward development that chooses to locate in areas that are better serviced by mass transit. The standards are intended to encourage efficient development that does not devote large expanses to surface parking. These changes would help reduce impacts to air quality and the environment.</p>
<p>3. Whether a proposed text amendment is consistent with the purposes and provisions of any applicable overlay zoning districts which may impose additional standards;</p>	<p>Complies</p>	<p>The proposed parking standards are consistent with the purposes of the zoning overlays in that they are context based and therefore more tailored to the underlying zoning and development patterns of a given area. The lower proposed parking minimums and more flexible shared parking standards would help protect properties within the historic overlays that may be threatened by the need to provide large amounts of parking.</p>
<p>4. The extent to which a proposed text amendment implements best current, professional practices of urban planning and design.</p>	<p>Complies</p>	<p>Many of the elements of the proposed ordinance are derived from the research and principles presented in <i>Parking Reform Made Easy</i> by Chuck Marohn and <i>The High Cost of Free Parking</i> by Donald Shoup. These principles are supported by data and observation collected by Salt Lake City and Clarion Associates.</p> <p>Additionally, the American Planning Association (APA), Congress for New Urbanism (CNU), Urban Land Institute (ULI), and planning advocacy groups such as Strong Towns have all published numerous articles in support of reducing or eliminating parking minimums, capping maximums in certain situations, and general movement towards market-based parking solutions.</p> <p>The proposed text amendments implement best current planning practices for off-street parking and establish a framework for continued changes as the City continues to grow.</p> <p>In particular, the context-based parking approach, modified minimum and maximums, and revised parking alternatives would allow for more vibrant and walkable urban spaces.</p>

ATTACHMENT F: MASTER PLAN COMPATABILITY

Master Plan	Principle or Initiative	Additional Discussion
<p style="text-align: center;">PLAN SALT LAKE</p>	<ul style="list-style-type: none"> • Guiding Principle #2: Growing responsibly while providing people with choices about where they live, how they live, and how they get around. • Growth Initiative #3: promote infill and redevelopment of underutilized land. • Transportation and Mobility initiative #4: reduce automobile dependency and single occupancy vehicle trips. • Transportation and Mobility initiative #7: Encourage transit-oriented development • Air Quality initiative #3: increase mode share for public transit, cycling, walking and carpooling. • Air Quality initiative #4 Minimize impact of car emissions. • Beautiful City initiative #5: support and encourage architecture, development, and infrastructure that is people focused and responds to its surrounding context and enhances the public realm. • Preservation initiative #1: preserve and enhance neighborhood and district character • Equity initiative #3: support policies that providing housing choices, including affordability, accessibility and aging in place. • Economy initiative #2; support the economic growth of Downtown • Economy initiative #3: support the growth of small businesses, entrepreneurship, and neighborhood business nodes. 	<ul style="list-style-type: none"> • The objectives of Plan Salt Lake were directly targeted with the proposed parking standards. • The parking contexts, revised parking alternatives (including for affordable and senior housing), and the generally lower minimums and maximums would support the initiatives outlined in the plan.
<p style="text-align: center;">TRANSPORTATION</p>	<ul style="list-style-type: none"> • Focus on Public Transit: Examples of TDM programs include limiting development of new parking spaces in congested areas. • On-street parking could be eliminated to provide bicycle lanes. • SLC will lower the max allowable parking requirements in the downtown area, in conjunction with implementation of trip reduction strategies. • Residential neighborhoods will be protected from the negative impact of overflow parking from adjacent uses. 	<ul style="list-style-type: none"> • On-street parking would no longer count toward required parking under the proposed provisions. • The revised parking standards would help ensure appropriate parking is provided for each of the identified context areas of the City.
<p style="text-align: center;">DOWNTOWN</p>	<ul style="list-style-type: none"> • Challenge #1: unrealized development potential. Surface parking is a dominant land use, comprising 27% of all developable land downtown. 	<ul style="list-style-type: none"> • Allowed surface parking would be greatly reduced in the

	<ul style="list-style-type: none"> • Vibrant and Active, Goal 3, initiative 3: On pedestrian oriented streets, active ground floor uses should be prioritized over parking uses. Structured parking should be designed to accommodate, where feasible, street level businesses and other active uses. • Is Connected Goal 4, initiative #1; examine parking policy to ensure adequate parking is provided. • Initiative #2: update zoning regulations to locate surface parking lots in appropriate locations. • Granary District Initiatives, is vibrant and active: rethink and reclaim public rights of way and find creative solutions to enabling people to use more of the right of way, including median parking. • Is prosperous: allow on-street parking to count towards parking requirements • South State District Initiatives, is prosperous: allow on-street parking to count towards parking requirements. 	<p>downtown, however, structured parking would not include maximums.</p> <ul style="list-style-type: none"> • Although on-street parking would not count towards parking requirements, most of the downtown is part of the Transit or Urban Center parking context and would require very low or no minimum parking requirements.
<p style="text-align: center;">AVENUES</p>	<ul style="list-style-type: none"> • Guidelines for either redevelopment or a new use of existing structures: sufficient parking to meet realistic needs must be provided on site without encroaching into required yard 	<ul style="list-style-type: none"> • Parking for reuse of existing structures is proposed with updated regulations and is more market-based which would help ensure that businesses have the flexibility to provide parking according to their needs.
<p style="text-align: center;">CAPITOL HILL</p>	<ul style="list-style-type: none"> • Ensure adequate community parking while mitigating adverse effects of parking that comes from outside the community. • Develop a parking plan for Marmalade, Kimball, and West Capitol Hill which analyses various solutions including the following: Shared parking arrangements • Cut back on parking 	<ul style="list-style-type: none"> • The proposed parking contexts ensure more appropriate standards that provide for the needs of businesses and help reduce parking from spilling into the neighborhoods.
<p style="text-align: center;">CENTRAL COMMUNITY</p>	<ul style="list-style-type: none"> • Encourage commercial centers to minimize parking and traffic congestion impacts upon surrounding residential neighborhoods. • Support shared parking facilities • Encourage parking solutions to support commercial, neighborhood and transit-oriented development. • Investigate the use of shared parking between day and evening land uses to encourage off-street parking. • Develop transportation and parking policies that favor the use of mass transit and non-motorized transportation methods in order to help reduce cumulative air emissions. • Commercial land uses: periodically evaluate municipal regulations to ensure zoning, business licensing and parking regulations do not hamper the success of small locally owned businesses. 	<ul style="list-style-type: none"> • The proposed increased parking maximums for retail and restaurants will provide a path for developments to provide more parking on-site. • The proposed provisions for shared parking and the introduction of context-based parking help favor the use of mass transit and non-motorized transportation methods.

	<ul style="list-style-type: none"> Institutional land uses: provide tools like residential parking or shared parking lots to help mitigate the effect of traffic and parking congestion caused by existing institutional land uses. 	
EAST BENCH	<ul style="list-style-type: none"> Mitigate parking impacts on properties adjacent to neighborhood business districts. 	<ul style="list-style-type: none"> The proposed increases to the parking maximums for retail and restaurants would provide a path for developments to provide more parking on-site.
NORTHWEST	<ul style="list-style-type: none"> Reduction in parking requirements should not be granted 	<ul style="list-style-type: none"> The Northwest Master Plan is nearly 30 years old. The proposed chapter has fewer options for parking reductions but includes uses that are proposed to have no parking minimums and others that would qualify for reductions. This would be justified through the implementation of the goals and objectives listed in Plan Salt Lake, which is a much newer planning document that addresses current challenges facing Salt Lake City.
NORTHWEST QUADRANT	<ul style="list-style-type: none"> Restrict runoff from parking lots flowing directly into natural areas, wetlands, and green corridors Minimize the size of parking lots 	<ul style="list-style-type: none"> The proposed parking requirements rely on more market-based parking counts that are intended to result in a reduction of excess parking for some uses.
SUGAR HOUSE	<ul style="list-style-type: none"> High Intensity Mixed Use policies: Improve all modes of mobility including street and trail networks, transit, pedestrian and bicycle movement opportunities, and off-street cooperative parking facilities. Multifamily housing needs to provide open space amenities, adequate off-street parking, etc. Incorporate adequate off-street parking into development with identified access, proper buffering and landscaping and encourage coordinated and structured parking Provide for coordinated and structured parking, with underground parking wherever possible. Flexibility in parking requirements may be an option in the future as light rail develops in the area. Shared parking and parking structures should be encouraged. 	<ul style="list-style-type: none"> The introduction of the proposed parking contexts would require parking that is more sensitive to the unique situations within Sugar House. The revised shared parking standards and elimination on maximum parking provided within parking structures would help make it more economically feasible for businesses

	<ul style="list-style-type: none"> • Transportation demand management techniques should be used within the business district. • Encourage coordinated parking in the business district and around commercial nodes. • Encourage structured and underground parking wherever feasible to minimize the impacts upon surrounding land uses and reduce the land area used. • Require adequate parking for each development and flexibility on parking standards when served by other mobility options. • A well-run and utilized transit system has many benefits, including a reduction in parking requirements • Minimize cutbacks for on-street parking areas in order to maintain wider sidewalks and landscape areas. • Locate parking lots behind buildings in every possible circumstance in order to encourage use of transit, facilitate pedestrian circulation and improve aesthetics • Encourage coordinated or structured parking facilities with subsurface parking lots • Encourage the implementation of shared parking agreements • Evaluate the feasibility of reducing the parking requirements for new structures in the business district when coordinated with shared parking arrangements and alternatively mobility options 	<p>to build shared parking garages to serve an area as a regional parking amenity.</p> <ul style="list-style-type: none"> • The proposed elimination of some TDM allowances will help ensure that multi-family projects provide enough parking to meet their demand. • The proposed parking alternatives are intended to encourage more practical alternatives than are currently allowed.
<p>WESTSIDE</p>	<ul style="list-style-type: none"> • Neighborhood nodes contain little parking as they are not normally major attractions for residents outside of the neighborhood. • Office and businesses should be integrated with one another to take advantage of shared parking to make more efficient use of developable land. • Glendale Plaza should have flexible parking. • Parking should be required for all uses but located behind or to the side of buildings and shared parking should be encouraged to maximize developable space. • Redwood Rd section: residential development should be encouraged via parking reductions and other incentives. 	<ul style="list-style-type: none"> • These objectives are largely made possible through the more flexible provisions of the proposed ordinance and the proposed revisions to the shared parking section.

ATTACHMENT G: PUBLIC PROCESS TIMELINE

May 2017	Planning staff developed and released a <i>Request for Proposal</i> to re-write Chapter 21A.44, Off-street Parking, Mobility and Loading Chapter of the zoning ordinance Clarion Associates submitted the lone response to the request
June 2017	Selection committee awarded contract to Clarion Associates. Committee represented the following divisions/departments: Planning, Transportation, Redevelopment Authority, and Housing and Neighborhood Development
July 2017	Contract finalized, and project kickoff meeting held with Clarion Associates to discuss issues and goals
September 2017	Mayor initiates the petition PLNPCM2017-00753 regarding Chapter 21A.44 amendment City's Public Engagement Team conducts meeting with Clarion Associates and identified business and developer stakeholders to gather initial comments Collected comments from Bicycle Advisory Board at monthly meeting Internal meetings with the following divisions: Planning, Building Services, and Transportation divisions
October 2017	Planning Commission briefing Business Advisory Board briefing
November 2017	Public survey conducted online with results given to Clarion Associates for consideration
December 2017	Public open house held at Liberty Senior Center
February 2018	Draft chapter received from Clarion Associates
March – April 2018	Draft chapter circulated to city departments for review and comment Department comments sent to Clarion for incorporation into a public draft
May 2018	Clarion provided first public draft and met with external steering group
June – Dec 2018	Project on hold due to changes in Planning staff, new project manager

Feb – June 2019	Planning staff re-started work on the project and began public outreach with The Downtown Alliance and community council presentations (Ballpark, Sugar House, ELPCO)
May 2019	Draft chapter published on city's website and emailed to more than 2,000 public contacts for review
June – July 2019	<p>Planning staff conducted six public open houses to acquire public input</p> <ul style="list-style-type: none"> - Main library (2) - Glendale library - Partners in the Park evening event - Sugar House fire station (2)
September 2019	Planning staff held two work sessions with the Planning Commission (September 11 and September 25)

ATTACHMENT H: PUBLIC COMMENTS

Public Survey:

As part of this update process, an online community survey was made available in November of 2017. Respondents were given the opportunity to offer their opinions and thoughts about off-street parking in Salt Lake City. Six questions were asked with the intended purpose of using the results to help inform future changes to the City's Off-Street Parking Chapter. Over 700 people participated in the survey.

Common themes of the survey:

- Minimize spill-over parking into residential neighborhoods.
- All projects (regardless of size) should provide adequate parking to meet the anticipated demand.
- The market should have a larger role in controlling the amount of parking provided.
- Elevate the level of quality and design required for large parking lots.
- Empower City Staff to approve minor adjustments to parking requirements if specific and consistent criteria are met.

Community Councils:

All recognized community-based organizations were notified of the proposed text amendments at the beginning of the project and once the draft ordinance was available. Most opted to direct their residents to our project website or to attend one of the open houses. Staff did attend community council meetings with Sugar House, ELPCO, and Ballpark Community Councils. Primarily they wanted an overview of the proposed changes and then to focus on the specifics of the parking contexts within their council areas, including parking counts for common land uses. Additionally, most were interested in the changes to alternative parking options. Most were pleased to see that the Transportation Demand Management options being eliminated as they felt that the provision was not resulting in decreased parking demand. They were also interested to see what provisions were put in place to encourage shared parking arrangements. The delicate balance of providing enough parking within neighborhood centers, without destroying character or spilling into the neighborhoods was also discussed.

Focus Groups: Planning Staff met the following groups:

- Bicycle Advisory Board
- Business Advisory Board
- Downtown Alliance
- Developer Stakeholders

The groups provided feedback at the onset of the project and in response to the draft ordinance. Some of the themes that emerged from these meetings includes that they want an ordinance that:

- Is clear and predictable
- Allows for more market driven parking counts
- Does not limit parking within parking garages
- Prioritizes alternative transportation methods where feasible

Open Houses:

One open house was held as a kick-off for the project. Once the draft ordinance was made available, Staff held an additional six open house events in different parts of the City and at different times of day. Staff presented a summary of the proposed ordinance, the parking context map, and information sheets on each of the parking contexts. Although attendance was generally low, nearly all that came expressed support of the proposed revisions. The feedback was nearly unanimously positive.

Commission Briefings:

The following points and recommendations were made during the most recent briefings with the Planning Commission where they were asked for their direct feedback on the proposed changes:

PC Briefing – September 2019

- Project scope, purpose, and general updates
- Public process updates and key take-aways
- Proposed alternatives to parking calculations
- Parking minimums for Cottage Developments
- Parking minimums for Multi-Family Developments in the General Context
- Parking reductions for development located near high-frequency bus stops

Planning Commission Notice of the public hearing for the proposal included:

- Agenda posted on the Planning Division and Utah Public Meeting Notice websites on Friday, December 20, 2019
- Newspaper notice ran on Saturday, December 28, 2019.

ATTACHMENT I: CITY DEPARTMENT COMMENTS

Throughout the revision process, Planning Staff has worked closely with applicable City divisions/departments, including:

- Housing and Neighborhood Development
- Fire
- Building Department
- Engineering
- Public Utilities
- Attorney's Office
- Sustainability
- Police Department
- Transportation
- Zoning

Each department has provided feedback and suggested revisions which have been incorporated into the various drafts. The comments below represent any final comments for the proposed off-street parking ordinance:

Housing and Neighborhood Development: The section on affordable housing on page 23 (item D) covers all the groups of residents that HAND requested be included for consideration of parking requirement reductions. I'll look forward to seeing what the Planning Commission and Council move ahead with.

Public Utilities: Stormwater quality treatment is required for all off-street parking. Wherever possible low impact green infrastructure should be used. Interior landscape islands and park strips should be used for stormwater filtering and treatment. Best management practices for stormwater must be reviewed and approved by Salt Lake City Department of Public Utilities. Refer to the SLC Stormwater Master Plan, Storm Drainage Manual, and Green Infrastructure Toolbox for additional information.

Transportation:

1. In 21A.44.020A.3, Change of Use, a. – The first paragraph refers to subsections b and c, but there is no subsection c. Also, the existing ordinance says that only the incremental increase in parking is to be added. Does this ordinance state the project must provide the entire minimum parking requirement?
2. Table 21A.44.040. I am not sure what a single-family cottage development is. Also, it seems to me that there should at least two spaces required for single family developments in the Neighborhood Center Context.
3. There are some boxes in the Table 21A.44.040-A that are blank.
4. In some parts of the Table 21A.44.040-A, the maximum allowance does not cover all of the contexts.
5. In Table 21A.44.040-A, the minimum parking requirement for restaurants is listed as 2 per 1,000 sf. This number could be boosted up to maybe 3 per 1,000 sf.

6. Retail Sales and Service. There is no minimum parking requirement for Photo finishing lab, Electronics repair shop, Furniture repair shop and Upholstery shop. This causes employees and customers to park on street. There should be a minimum parking requirement.
7. Bicycle parking. This section has been substantially changed. I will need to run this by our bicycle coordinator, Becka, for her input. It would be safe to leave this part of the existing ordinance as is.
8. Valet Parking Services. "Modifications to on-site parking spaces may occur on a one-to-one basis..." I'm not sure what this means.
9. Table 21A.44.060-A. In some of the boxes in the table, there is the letter "N". I'm assuming that this means "None". It is unclear.
10. Sight Distance Triangle. We may need to adjust other ordinances (21A.40.120.E and 21A.62.050, Illustration I) so as not to be redundant or conflicting.
11. There is no mention in the ordinance of parking ramps (slopes and maximum breakover points). There should be a referral to the Off-streets Parking Standards Manual.

Staff Discussion on Comments from Transportation:

Minor corrections were made to the draft ordinance based on comments 1, 3, 4, & 10. Modifications to the required parking counts as mentioned in comments 2, 5, & 6 were not made as these counts have already gone through the public process and have been presented to the Planning Commission in work sessions without requests to be modified. Staff provided the reviewer with explanations requested in comments 1, 2, 8, & 10. The bicycle standards (comment 7) have not been modified since the draft was last sent for department review in May. Planning staff is inclined to present them to the Planning Commission as presented in the proposed ordinance attached to this report.